

ATTACHMENT A
Application Package

PLANNING DIVISION



File No. (internal use only): _____

2600 Hollywood Boulevard Room 315
Hollywood, FL 33022

GENERAL APPLICATION



Tel: (954) 921-3471

Fax: (954) 921-3347

This application must be completed in full and submitted with all documents to be placed on a Board or Committee's agenda.

The applicant is responsible for obtaining the appropriate checklist for each type of application.

Applicant(s) or their authorized legal agent must be present at all Board or Committee meetings.

At least one set of the submitted plans for each application must be signed and sealed (i.e. Architect or Engineer).

Documents and forms can be accessed on the City's website at

<http://www.hollywoodfl.org/DocumentCenter/Home/View/21>



APPLICATION TYPE (CHECK ONE):

Technical Advisory Committee Historic Preservation Board
 City Commission Planning and Development Board

Date of Application: _____

Location Address: 2741 & 2742 Polk St Hollywood FL 33020

Lot(s): Lot 11 & West 1/2 of Lot 12 Block(s): Block 31 & Block 32 Subdivision: Hollywood Little Ranches

Folio Number(s): 514216023350, 514216023630

Zoning Classification: Trans Core TC-1 /MC-1 North Lot Land Use Classification: Reg Act Center - RAC

Existing Property Use: Single Family Sq Ft/Number of Units: 2

Is the request the result of a violation notice? () Yes (✓) No If yes, attach a copy of violation.

Has this property been presented to the City before? If yes, check all that apply and provide File Number(s) and Resolution(s): File No. 20-DP-41

Economic Roundtable Technical Advisory Committee Historic Preservation Board
 City Commission Planning and Development

Explanation of Request: Proposed construction of 45 apartment unit project all 1 bedroom with parking

Number of units/rooms: ● 44 Sq Ft: 650 sqft per Unit

Value of Improvement: \$4 Million Estimated Date of Completion: 15 Months after Permit

Will Project be Phased? () Yes (✓) No If Phased, Estimated Completion of Each Phase

Name of Current Property Owner: S&B ENT LLC

Address of Property Owner: 17555 COLLINS AVE APT 1606 SUNNY ISLES BEACH, FL 33160-2166

Telephone: 917-912-0851 Fax: Email Address: benz1018@gmail.com

Name of **Consultant**/Representative/Tenant (circle one): Tom Benedict - Architect

Address: 1525 NW 3rd St. Suite 1 Deerfield Beach FL 33442 Telephone: 954-570-9500

Fax: 954-570-9550 Email Address: tom@tbbg.net

Date of Purchase: 09/20/2019 Is there an option to purchase the Property? Yes () No (✓)

If Yes, Attach Copy of the Contract.

List Anyone Else Who Should Receive Notice of the Hearing: _____

Address: _____
Email Address: _____



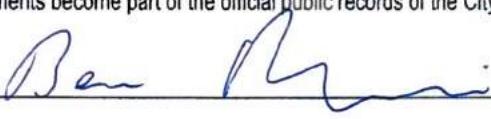
2600 Hollywood Boulevard Room 315
Hollywood, FL 33022

GENERAL APPLICATION

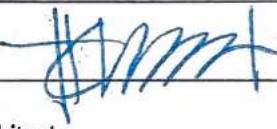
CERTIFICATION OF COMPLIANCE WITH APPLICABLE REGULATIONS

The applicant/owner(s) signature certifies that he/she has been made aware of the criteria, regulations and guidelines applicable to the request. This information can be obtained in Room 315 of City Hall or on our website at www.hollywoodfl.org. The owner(s) further certifies that when required by applicable law, including but not limited to the City's Zoning and Land Development Regulations, they will post the site with a sign provided by the Office of Planning and Development Services. The owner(s) will photograph the sign the day of posting and submit photographs to the Office of Planning and Development Services as required by applicable law. Failure to post the sign will result in violation of State and Municipal Notification Requirements and Laws.

(I)(We) certify that (I) (we) understand and will comply with the provisions and regulations of the City's Zoning and Land Development Regulations, Design Guidelines, Design Guidelines for Historic Properties and City's Comprehensive Plan as they apply to this project. (I)(We) further certify that the above statements and drawings made on any paper or plans submitted herewith are true to the best of (my)(our) knowledge. (I)(We) understand that the application and attachments become part of the official public records of the City and are not returnable.

Signature of Current Owner: Ben Rasabi  Date: 1/15/21

PRINT NAME: Ben Rasabi S&B ENT LLC Date: _____

Signature of Consultant/Representative: Thomas Benedict  Date: 1-7-2021

PRINT NAME: Thomas Benedict - Architect Date: 1-7-2021

Signature of Tenant: _____ Date: _____

PRINT NAME: _____ Date: _____

Current Owner Power of Attorney

I am the current owner of the described real property and that I am aware of the nature and effect the request for the development plans _____ to my property, which is hereby made by me or I am hereby authorizing _____ to be my legal representative before the tech. advisory _____ (Board and/or Committee) relative to all matters concerning this application.

Sworn to and subscribed before me
this 15 day of January

Notary Public

State of Florida

My Commission Expires: 3/11/23 (Check One) Personally known to me; OR Produced Identification DRIVERS LICENSE



Ellianni Marin
Commission # GG310447
Expires: March 11, 2023
Bonded Thru Aaron Notary

Signature of Current Owner

Print Name

July 26, 2021

Subject: CRITERIA STATEMENT: DESIGN CRITERIA

POLK STREET APARTMENTS I & II

Request: Site Plan Approval by the Planning and Development Board (PDB)

TAC # 20-DP-40 & 20-DP-41

This is a request for approval of our proposed project located on two sites: 2718/20 & 2742 Polk Street with additional parking located at 2723 & 2741 Polk Street, respectively. Each construction site is proposing 44 one bedroom apartment units in identical buildings that are the mirror image of one another. The total unit count will be 88 units. Unit size is 650 SF.

The following is a summary of the elements proposed to conform to the Design Criteria requirements of the City of Hollywood.

1) Architecture

This project proposes 3 stories of 1 bedroom, 1 1/2 bath rental apartments over street level parking with ground floor arrival, Lobby, two elevators, stair and support spaces including rest rooms. The roof level offers a gym, rest rooms, covered Lanai and swimming pool with 2 spas.

After preliminary review and several meetings regarding the design of this project, the architecture has been revised to more closely meet our understanding of City criteria for the vision of future development. Architectural features introduced (different from our first submission) include metal awnings, additional balconies with horizontal aluminum railing and most significantly pitched metal roofs. These elements assist in the visual expression of the residential character of the higher density housing anticipated in this neighborhood.

The base of the proposed structure is to be clad in porcelain with recessed glass entry door surrounded by sidelights and automobile garage entry both facing Polk Street. The 3 levels above this base are presented in smooth stucco with substantial areas of glass and railed balconies again facing Polk Street. The upper gym, recreation deck, pool and spa level is recessed further from Polk Street allowing the front building profile to be predominately 3 levels above parking capped by a pitched roof, as seen from the north-side sidewalk area adjacent to Polk Street. The recreation level is intended to be seen from the street although set back to lower the edge profile.

Material choices and construction methods will include Green Building Practices. Further evidence of this will be identified by our Green building consultant as we move forward with the construction document preparation.

2) Compatibility

The proposed project is located on Polk Street east of 28th Avenue. The proposed architecture is intended to be as consistent with the intent of the RAC zoning for this neighborhood as possible. Considerable discussion regarding building mass and the impact of the applied floor area ratio has resulted in our proposed design. The existing neighborhood is identified by older one story single family homes some of which are being used for business/ commercial activities and is an area of transition to new development.

This project is among the first to be proposed within the changed zoning within this area and will be an incentive for future new development some of which may be by this owner/developer.

3) Scale/Massing

To assist in diminishing the impact of this project, the setback proposed from Polk Street has been increased to 20' at the ground floor. 15' is required. The second through 4th floor of the building is setback 15' with balconies to articulate the facade. The front entry door arrival area is setback 24' and remains in that plane for the height of the front elevation. The interior stair enclosure could have been pushed within the front setback but was kept at the setback to offer greater landscaped space. The upper level gym / recreation space has been pushed back 24' to 34', further animating the facade.

4) Landscaping

Landscaping for this project has been carefully coordinated with the City's landscape department to ensure the variety of native and preferred plant types are integrated into the design. All efforts to maintain existing trees were made. The existing trees and their mitigation and the new trees and planting have been thoroughly documented, discussed and approved by the City landscape department.



THE MERCHANT STRATEGY

May 7, 2021

City of Hollywood
Zoning and Land Development
PO Box 229045
Hollywood FL 33022-9045

**Re: S & B ENT, LLC
Polk Street Apartments Project - File Nos.: 20-DP-40 and 20-DP-41**

Dear Sir/Madam:

The Merchant Strategy was retained by S & B Ent., LLC, to handle the public outreach/participation for this project. Pursuant to the City of Hollywood's Zoning and Land Development Regulations, Ordinance 5.1, we held a public participation meeting on Tuesday, April 6, 2021, at 6:00 pm, via Zoom.

In preparation for this meeting, we mailed invitations to all property owners, businesses, and certified/registered civic and neighborhood associations within 500 feet of the proposed project. We also placed signage directly at the project locations to further advise residents and/or stakeholders of the meeting as well as emailing the certified/registered civic and neighborhood associations so they could provide their residents a link for the Zoom meeting.

We are enclosing the completed Public Participation Meeting Certification Package which includes the following:

- The attendance list and Meeting Agenda – since this was a virtual meeting, we were not able to physically have people sign in. Therefore, we tracked and noted the names of the attendees. There is a recording of the meeting which can be provided via email if requested
- PowerPoint presentation
- Meeting notes
- Meeting invitation and signage

Thank you for your consideration.

Sincerely,

Cheryl Scott
Operations Manager

Polk Street Apartments Public Meeting
April 6, 2021 at 6:00 pm

Attendees:

Karen Newman Albertson
Patricia Antrican
Theresa
10503COH
Karen Caputo
Casey Carlson
Mayor Josh Levy
10496COH
Patty
Robin S.
Brian Tetzlaff
Vice Mayor Linda Sherwood]
Carlos Naranjo
Commissioner Linda Hill Anderson
Caryl Shuham
Commissioner Traci Callari

Questions/Comments From Meeting:

From Robin S.: What street does the under building parking open onto? Does this face onto the alley or Polk street?

From Robin S.: Is there a wall around the pool area? How many feet is the setback from the pool to the edge of the building?

From Karen Newman Anderson: How many people will you rent to in these one bedroom apartments?

From Robin S: What street modifications or traffic signal changes will be made to accommodate all the extra traffic?

From Theresa: how tall are these buildings?
will the parking be landscaped?
which direction do the balconies face?

From Theresa: what is the potential price range for these units?

From Theresa: how tall is the screening wall?

From Carlos Naranja: Is the price range of units within the average price range of existing homes in the area?

From Brian Tetzlaff: Timelines for the project? as well as hours of operation for construction

From Karen: My concern is the safety crossing the street to the parking area. Since the new crosswalks on Hollywood Boulevard are new and are already NOT WORKING in front of the City Hall.

From Karen Caputo: The TAC has returned the application for a dozen second requests and about 50 areas of concern. When will your amended application be ready to be viewed by the civic association?

From Casey Carlson: I am at 2620 Taylor st. What your looking at on my screen is my sky line from my back yard. What does that mean to the single family homes and their privacy?

From Robin S: There are two additional 1/2 acre lots owned by S&B Ent on that block of Polk that is not included here. Are there plans for two more building of this type to be added as well?

From Casey Carlson: This company also bought two more properties on Polk.. Is there another building going to go up as part of this project?

From Carlos Naranjo: We need to have these units fit the existing price range of existing comparable residencies. I'm at Polk and 27th

From Robin S: Will all these people have to exit onto 28th ave or the circle? These are already really busy traffic areas.

From: Brian Tetzlaff : Agreed Robin^ wish there were additional plans to direct traffic. Even modifications to traffic lights during high traffic times. could someone from DOT or city do a traffic study to help alleviate congestion? green light length. etc..

From Theresa: can you share some other projects you have done with us?

Also from Theresa: names - cities? Adding on to her last question

From Robin S: I'm worried about adding 200 people to our little neighborhood. The circle is a nightmare and Polk is narrow.

Patty: When the train comes in the station all the traffic comes to a standstill

Your Input is Wanted on the Proposed Polk Street Apartments Project



A rendering of one of the proposed identical mirror imaged buildings

YOUR INPUT AND OPINION IS IMPORTANT

S & B Ent, LLC, is committed to proactive resident and stakeholder outreach throughout the project. Please feel free to contact our team at info@themerchantstrategy.com with any questions or concerns.

We are here to help!

YOU ARE INVITED

S&B ENT, LLC invites you to a public meeting to discuss the proposed Polk Street Apartments project located at 2718, 2720 and 2742 Polk Street with additional parking at 2741 and 2723 Polk Street. Both sites are proposing forty five 1 bedroom apartment units in identical buildings - the mirror image of each other. The total unit count will be ninety units.

What: Proposed Polk Street Apartments Project

When: Tuesday, April 6 at 6:00 pm

Where: Via Zoom

Zoom Meeting Link:

<https://us02web.zoom.us/j/87562518728?pwd=dHgzdkZkREY2eHlaNTV4a0FJeHdiQT09>

Meeting ID: 875 6251 8728

Passcode: 720414

One Tap Mobile

+13126266799,,87562518728#,,,*720414#US (Chicago)
+16465588656,,87562518728#,,,*720414#US (New York)

Your Input is Wanted on the Proposed Polk Street Apartments Project



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We are here to help!

YOU ARE INVITED

S&B ENT, LLC invites you to a public meeting to discuss the proposed Polk Street Apartments project located at 2018, 2020 and 2742 Polk Street with additional parking at 2023 and 2043 Polk Street. Both sites are proposing forty five 1 bedroom apartment units in identical buildings - the mirror image of each other. The total unit count will be ninety units.

What: Proposed Polk Street Apartments Project

When: Tuesday, April 6 2021 at 6:00 pm

Where: Via Zoom

Zoom Meeting Link:

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Meeting ID: 875 6251 8728

Passcode: 720414

One Tap Mobile

+13126266799,,87562518728#,,,*720414#US (Chicago)
+16465588656,,87562518728#,,,*720414#US (New York)



Virtual Meeting Agenda

Polk Street Apartments Project

April 6, 2021

6:00 pm

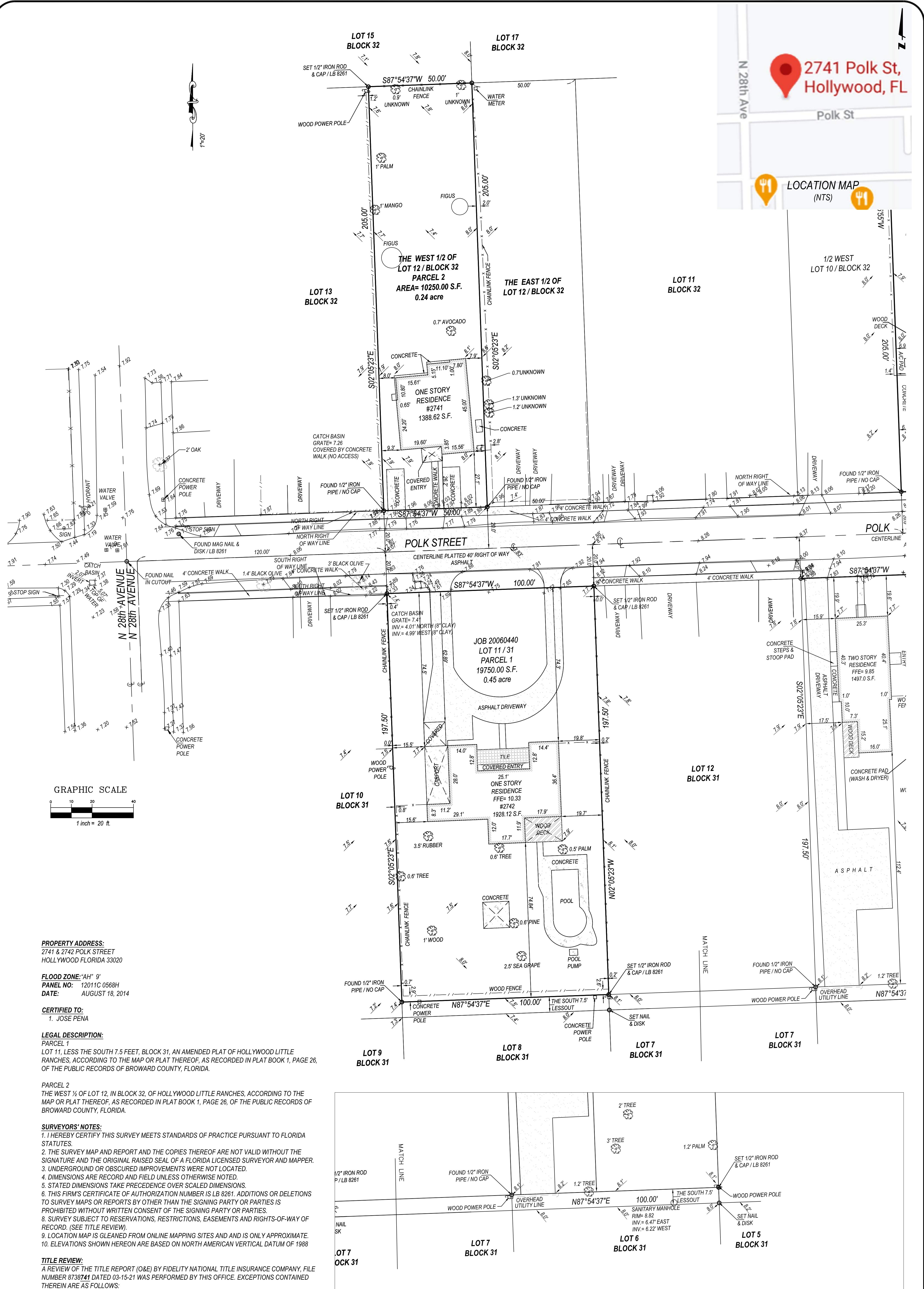
<https://us02web.zoom.us/j/87562518728?pwd=dHgzdkZkREY2eHlaNTV4a0FJeHdiQT09>

- I. Presenter Introductions – Sharon J. Merchant
 - a. Thomas Benedict, The Benedict Bullock Group, PA – Architect
 - b. Brian Bullock, The Benedict Bullock Group, PA – Architect
 - c. Sharon Merchant, The Merchant Strategy – Public Involvement

This meeting will be recorded and will be available for anyone unable to join us this evening. If you have a question after this meeting or would like access to the recorded presentation, please email Info@themerchantstrategy.com.

- II. Opening remarks – Thomas Benedict
- III. Proposed project overview, presentation and community benefits such as upgrading the neighborhood and additional tax money to the city and Presentations – Thomas Benedict will be speaking and Brian Bullock will be handling the presentation.
- IV. Q&A – Thomas Benedict
- V. Close – Sharon Merchant

This meeting will be recorded and will be available for anyone unable to join us this evening. If you have a question after this meeting or would like access to the recorded presentation, please email Info@themerchantstrategy.com.



PRINCIPAL MERIDIAN SURVEYING, Inc.
LICENSED BUSINESS No. 8261
4546 CAMBRIDGE STREET
WEST PALM BEACH, FL 33415
OFFICE 561-478-7764

ALTA / NSPS LAND TITLE SURVEY

DOUG WALKER
LICENSURE NUMBER: 7211
SIGNED: *Doug Walker*
PROFESSIONAL LAND SURVEYOR AND MAPPER
FLORIDA CERTIFICATE No. 7211

DATE: 03-18-21

JPM	FIELD	06-17-21
JH	JH	03-18-21
JH	CD	12-24-20
CD	DRAFT	07-13-20
DATE		06-17-21



2742 POLK STREET

The
Benedict
Bullock
Group, PA
www.tbbg.net
Lic. No. AA 26003089

POLK STREET APARTMENTS II

2742-2741- POLK STREET HOLLYWOOD, FLORIDA

ARCHITECT'S SEAL

BRIAN BULLOCK, ARCHITECT

AR 55/54

AR55754

DATE 02-03-2021

DRAWN BY RST

PROJECT NO. BS2030

SITE PLAN

SP-1

DEV.BRD PERMIT 08-13-2021

ALL IDEAS, DESIGNS, ARRANGEMENTS & PLANS INDICATED OR REPRESENTED BY THIS DRAWING ARE OWNED BY & THE PROPERTY OF THE ARCHITECT. THEY WERE CREATED, EVOLVED & DEVELOPED FOR USE ON, IN CONNECTION WITH THE SPECIFIED PROJECT. NONE OF SUCH IDEAS, DESIGNS, ARRANGEMENTS, OR PLANS SHALL BE USED BY, OR DISCLOSED TO ANY PERSON, FIRM, OR CORPORATION, FOR ANY PURPOSE WHATSOEVER WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT. WRITTEN DIMENSIONS ON THESE DRAWINGS SHALL HAVE PRECEDENCE OVER SCALE DIMENSIONS. CONTRACTORS SHALL VERIFY & BE RESPONSIBLE FOR ALL DIMENSIONS & CONDITIONS ON THE JOB. THE ARCHITECT MUST BE NOTIFIED OF ANY VARIATIONS FROM THE DIMENSIONS AND CONDITIONS SHOWN BY THESE DRAWINGS. SHOP DETAILS OF ADEQUATE SCALE MUST BE SUBMITTED TO THE ARCHITECT FOR APPROVAL BEFORE PROCEEDING WITH FABRICATION OR FABRICATION ON ITEMS NOTED.

REV.	BY DATE
REVISIONS	
The Benedict Bullock Group, PA	
ARCHITECTURE ■ PLANNING	
DESIGN AND DEVELOPMENT	
DEFINITION BEACH, FLORIDA 33042	
TOLL FREE 800.545.5089	
www.thebenedictgroup.com Lic. No. AA 5605089	

2741 POLK ST.

POLK STREET

16 Parking Spaces

SITE PLAN

SCALE 1/16'-1"-0"

NOTES

1. ALL CHANGES TO THE DESIGN WILL REQUIRE PLANNING REVIEW AND MAYBE SUBJECT TO BOARD APPROVAL.
2. PAVEMENT MARKING SHALL COMPLY WITH THE MANUAL UNIFORM TRAFFIC CONTROL DEVICES.
3. ALL SIGNAGE SHALL BE IN COMPLIANCE WITH THE ZONING AND LANDSCAPE DEVELOPMENT REGULATION.
4. THIRD PARTY FLORIDA GREEN BUILDING COALITION CERTIFICATION TO BE PROVIDED.

F.A.R. / ENCLOSED AREA CALCULATION

FIRST FLOOR	2,858 SQ. FT.
2ND THRU 4TH FLOOR	37,491 SQ. FT.
ROOF POOL DECK	3,169 SQ. FT.
TOTAL ENCLOSED AREA	43,518 SQ.FT.

PARKING CALCULATION:

44 UNITS x 1/U	= 44 SPACES
GUEST 1 SPACE/10U	= 4.4 SPACES
= 48.4 SPACES	
= 49 SPACES REQUIRED	

2741 POLK STREET. LANDSCAPE CALCULATION:

VUA = SITE - BUFFER AREA x 15%	
VUA = 10,250 - 2,450 x 15%	= 1,170 SQ. FT.
PERVIOUS AREA	
LANDSCAPE REQUIRED (40%)	= 4100 SQ.FT.
LANDSCAPE AREA	= 1,665 SQ.FT.
BUFFER AREA	= 2,534 SQ.FT.
LANDSCAPE PROVIDED (40.2%)	= 4119 SQ.FT.

LEGAL DESCRIPTION

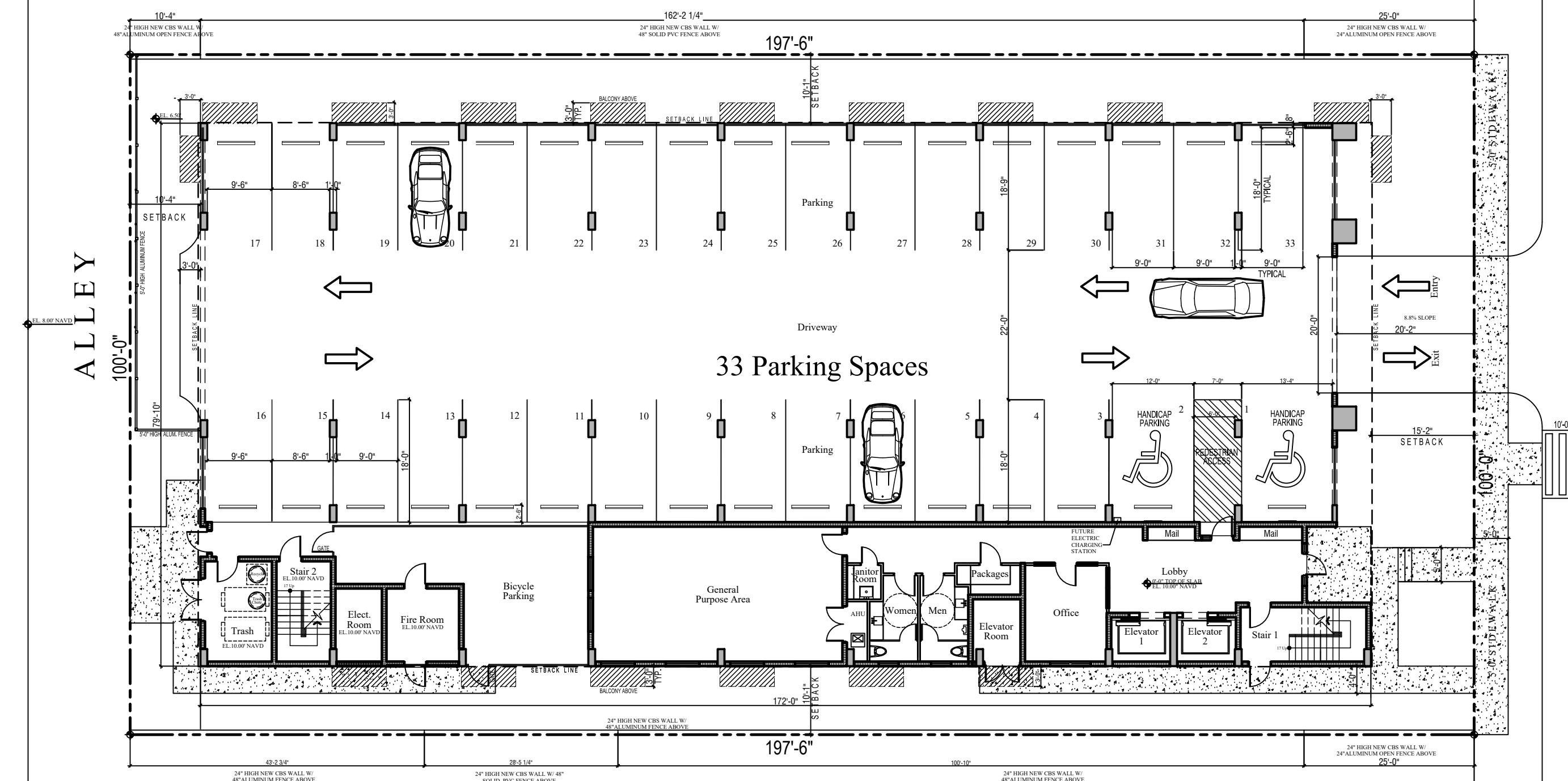
2741 POLK ST.	THE WEST OF 1/2 LOT 12, IN BLOCK 32 OF HOLLYWOOD LITTLE RANCHES ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 1, PAGE(S) 26 OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.
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2742 POLK ST.

LOT 11 LESS THE SOUTH 7.5 FEET THERE OF, BLOCK 31, AN AMENDED PLAT OF HOLLYWOOD LITTLE RANCHES ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 1, PAGE(S) 26 OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.

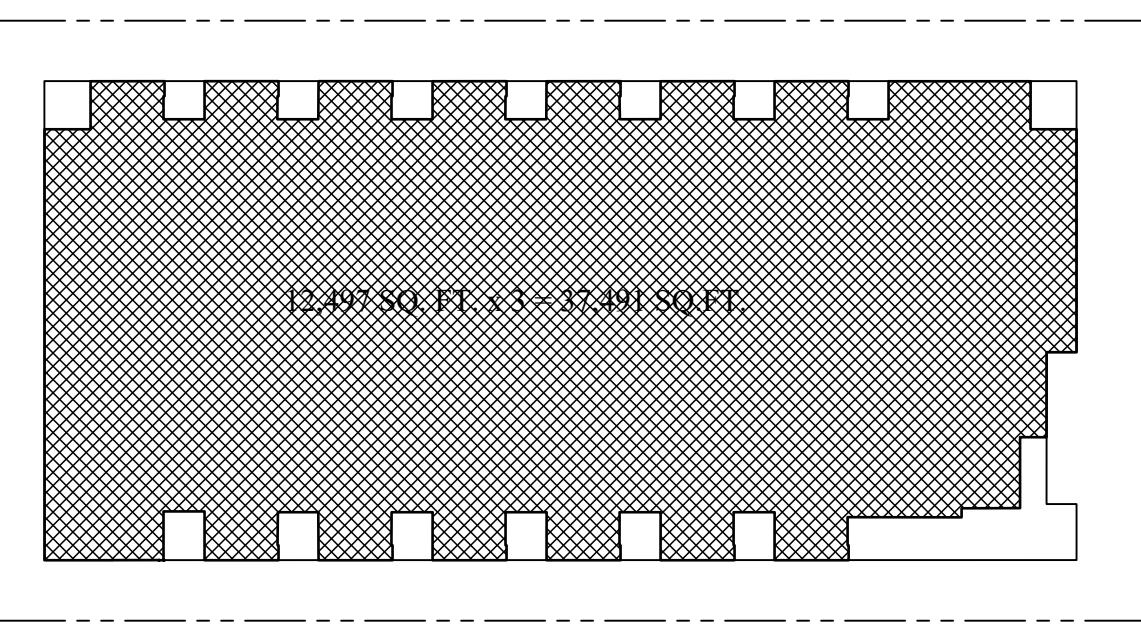
ZONING INFORMATION			
ZONE	REMARKS	REQUIRED/ALLOWED	PROVIDED
ZONE	TC-1 (SOUTH OF POLK STREET)		
ZONE	MC-1 (NORTH OF POLK STREET)		
LAND USE	REGIONAL ACTIVITY CENTER RAC		
LOT SIZE	BLDG. LOT - 197.5'x100', PARKING LOT 205'x 50'		
LOT AREA	BLDG. LOT - 197.5' x 100' = 19,750 SQ.FT. = 0.453 ACRES		
LOT AREA	PARKING LOT - 205' x 50' = 10,250 SQ.FT. = 0.235 ACRES		
TOTAL AREA OF LOTS	30,000 SQ.FT. = 0.689 ACRES		
F.A.R.	30,000 x 1.5 = 45,000 SQ.FT.	45,000 SQ.FT.	43,518 SQ.FT.
FRONT SETBACK		15 Ft. MIN.	15'-2" & 20'-0"
SIDE SETBACK		10 Ft. MIN.	10'-1"
REAR SETBACK		10 Ft. MIN.	10'-4"
PERVIOUS AREA	2742/2741 POLK ST.		4,855 SQ.FT. = 0.11 ACRES
IMPERVIOUS AREA	2742/2741 POLK ST.		14,895 SQ.FT. = 0.34 ACRES
PARKING	47 REGULAR PARKING SPACES + 2 HANDICAP PARKING	49 SPACES	49 SPACES
PARKING SPACE SIZES		GARAGE PARKING (9'-0" Clear x 18'-0") OPEN PARKING (8'-6" x 18'-0")	33 SPACES 16 SPACES
LIGHTING LEVEL	MAX. 0.5 FOOT-CANDLE AT ALL PROPERTY LINES		
NO. OF UNITS			44
UNIT SIZE	AIRCONDITIONED SPACE	650 SQ. FT. MIN. AVERAGE	650 SQ. FT. Interior Unit 652 SQ. FT. End Unit
BALCONY SIZE	END UNITS = 105 SQ. FT. INTERIOR UNITS = 81 SQ. FT.		
UNIT TYPE	1 BEDROOM, 1 1/2 BATH		44
AREA TABULATION			
FIRST FLOOR	GARAGE NON-A/C = 11,269 SQ.FT. A/C AREA = 2,462 SQ.FT.		13,731 SQ. FT.
TYPICAL FLOOR (2ND THRU 4TH)	A/C AREA = 12,356 SQ.FT. NON A/C AREA = 1,722 SQ.FT.		14,078 SQ. FT.
RECREATION LEVEL	A/C AREA = 2,990 SQ.FT. ROOF AREA = 13,677 SQ.FT.	30% OF ROOF = 4,013 SQ.FT.	3,964 SQ. FT. (29.0%)
TOTAL A/C AREA			42,555 SQ.FT.
TOTAL BUILDING LENGTH			172'-0"
STRUCTURE HEIGHT		50 Ft.	40'-0"
MAX. HEIGHT OF VERTICAL ENCROACHMENT			17'-0" OR TOTAL OF 57'-0"

2742 POLK ST.



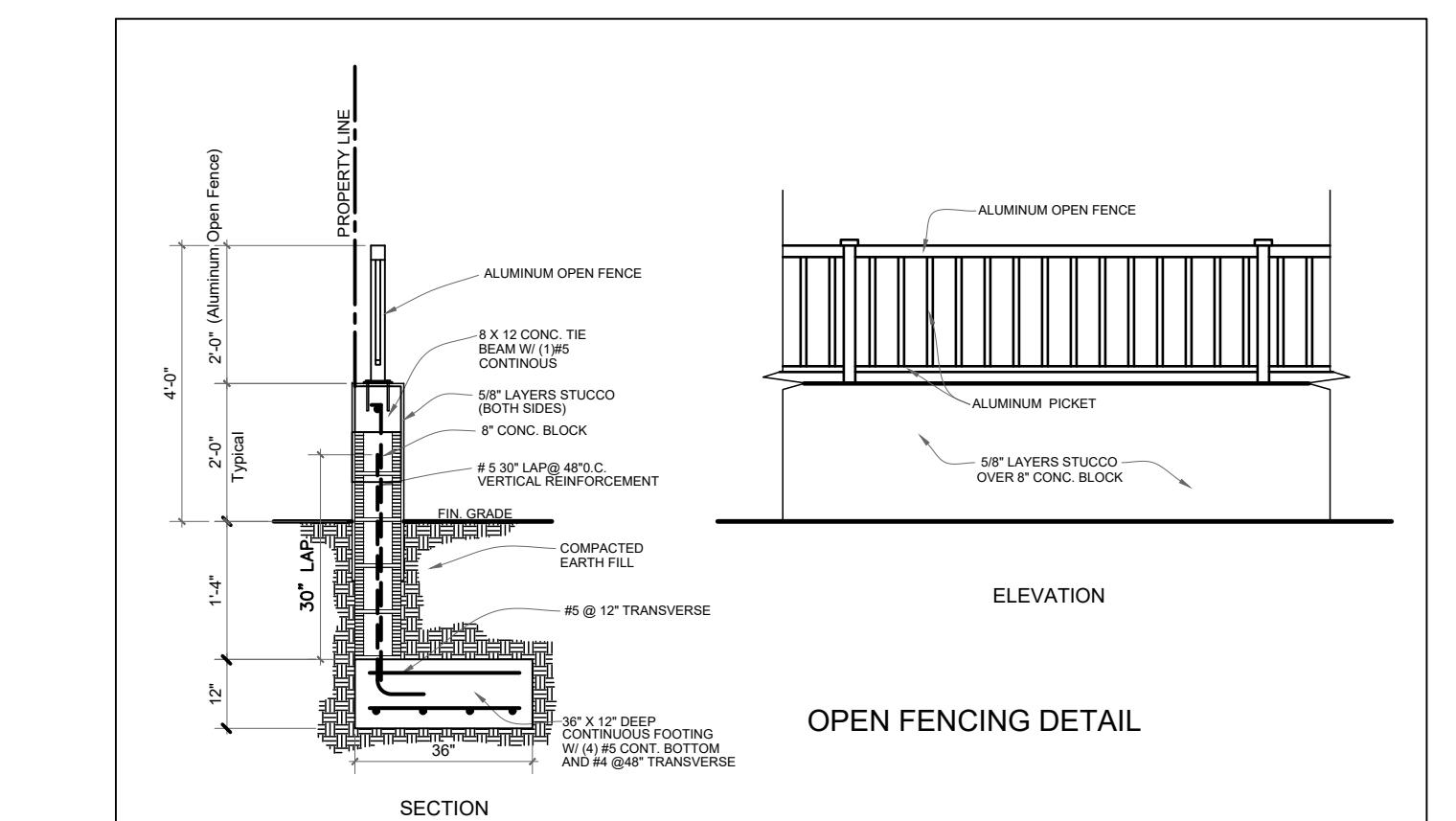
FIRST FLOOR ENCLOSED AREA DIAGRAM

SCALE 1/32'-1"-0"



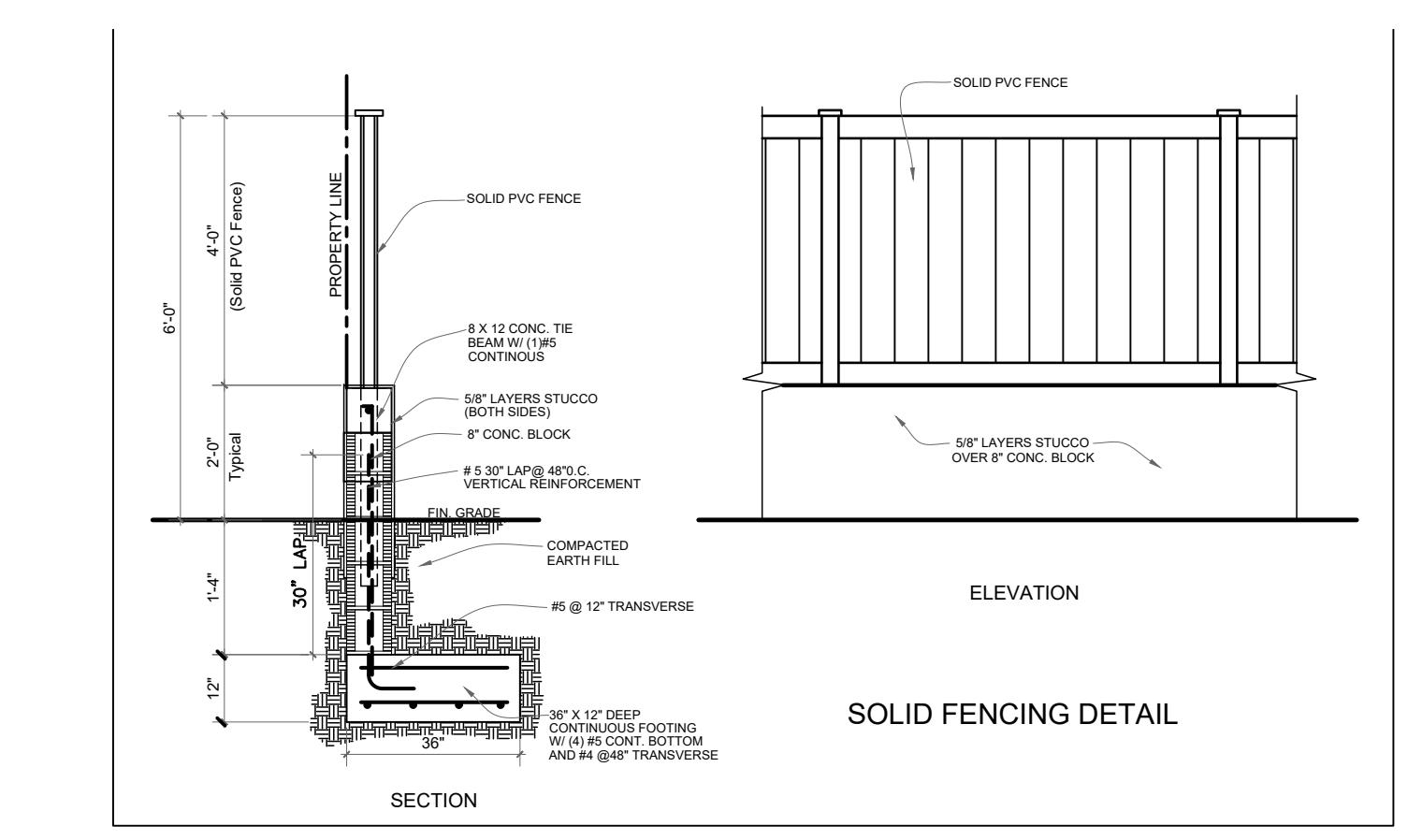
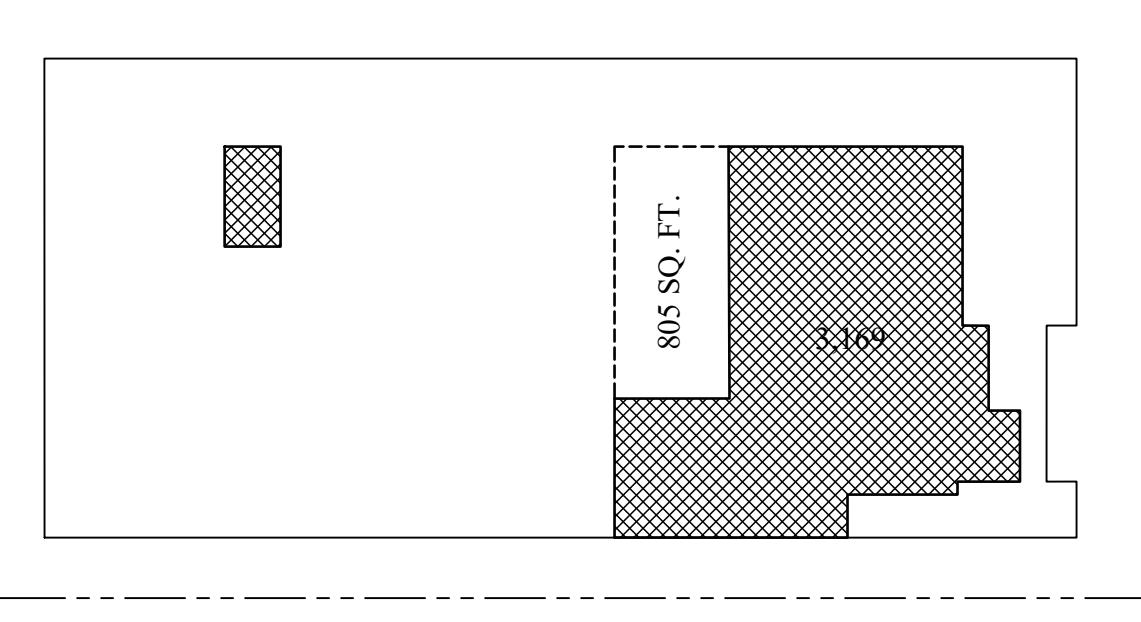
PERVIOUS DIAGRAM

SCALE 1/32'-1"-0"



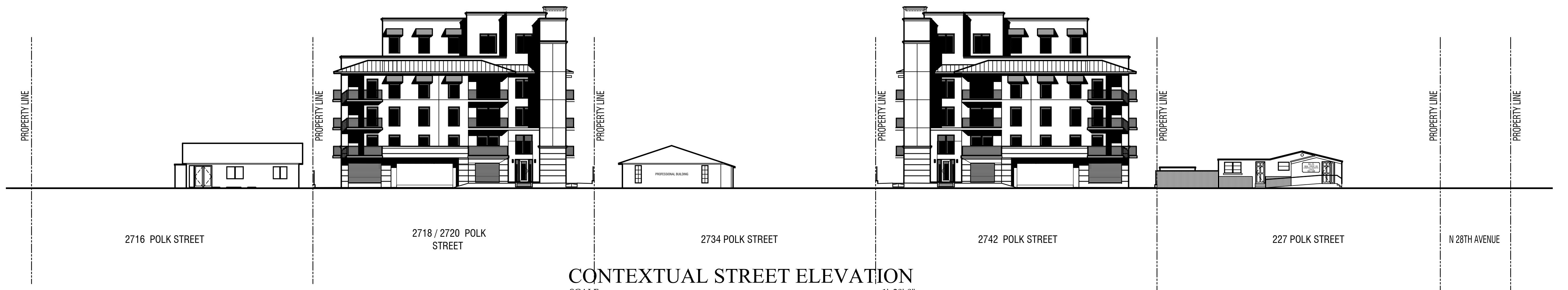
2ND THRU 4TH FLOOR ENCLOSED AREA DIAGRAM

SCALE 1/32'-1"-0"

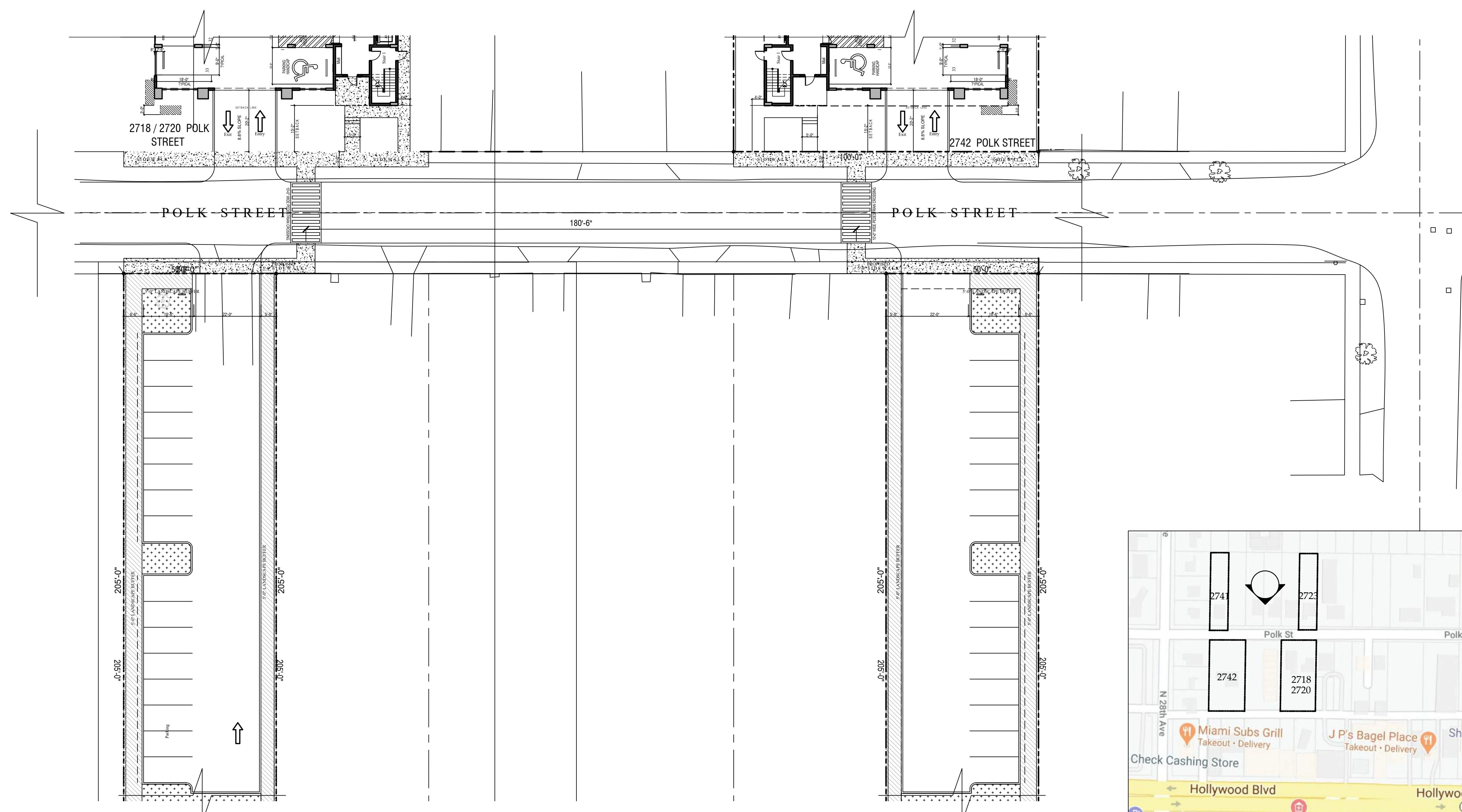


ROOF POOL DECK ENCLOSED AREA DIAGRAM

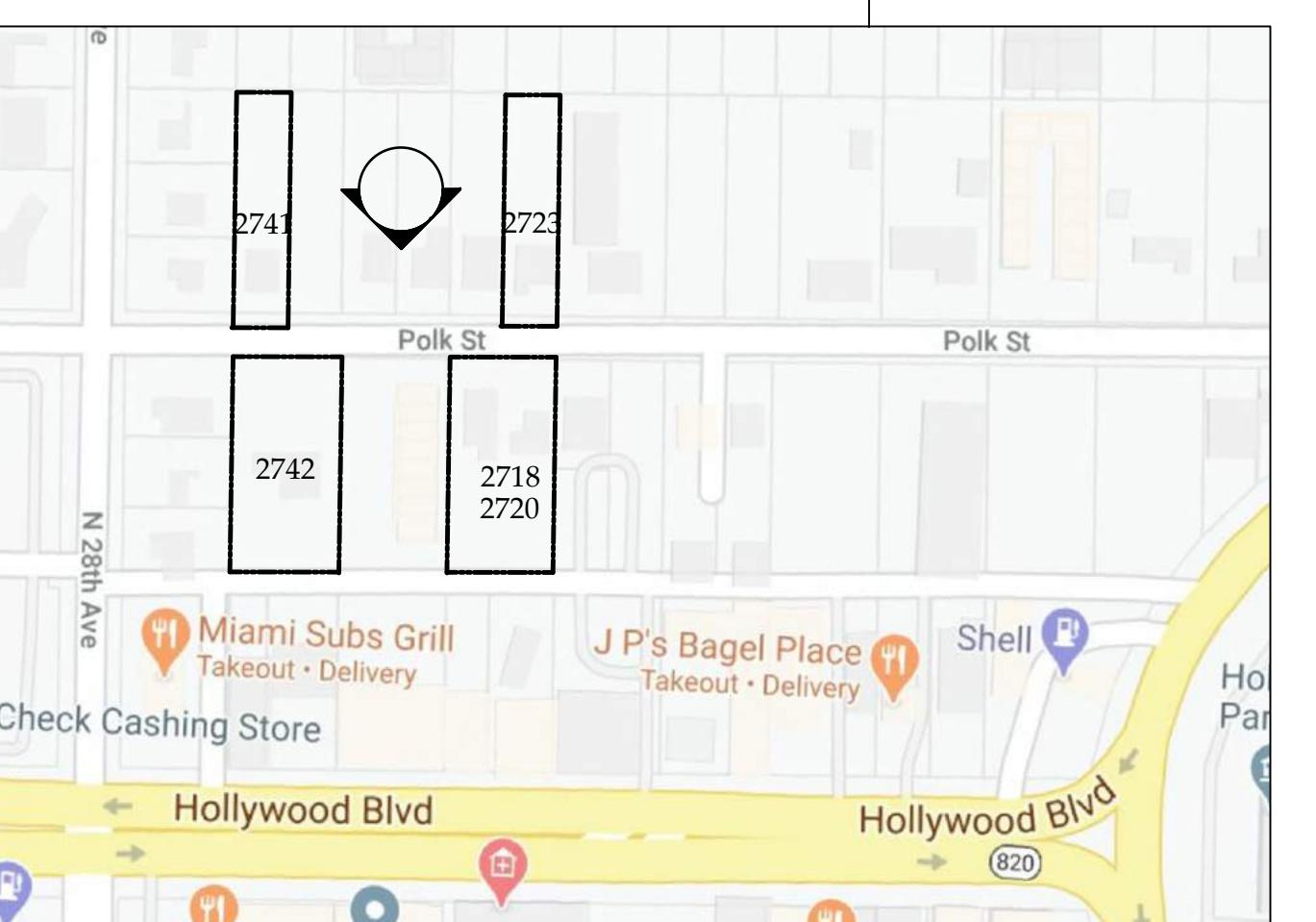
SCALE 1/32'-1"-0"



CONTEXTUAL STREET ELEVATION
SCALE
1'-0" to 20'-0"
POLK STREET LOOKING SOUTH



PROPOSED CROSSWALK PLAN
SCALE
1'-0" to 20'-0"



LOCATION MAP
N.T.S.
NORTH

ALL IDEAS, DESIGNS, ARRANGEMENTS & PLANS INDICATED OR REPRESENTED BY THIS DRAWING ARE OWNED BY, & THE PROPERTY OF THE ARCHITECT. THEY WERE CREATED, EVOLVED & DEVELOPED FOR USE ON, IN CONNECTION WITH THE SPECIFIED PROJECT. NONE OF SUCH IDEAS, DESIGNS, ARRANGEMENTS, OR PLANS SHALL BE USED BY, OR DISCLOSED TO ANY PERSON, FIRM, OR CORPORATION, FOR ANY PURPOSE WHATSOEVER WITHOUT THE WRITTEN PERMISSION OF BRIAN IRA BULLOCK, ARCHITECT. WRITTEN DIMENSIONS ON THESE DRAWINGS SHALL HAVE PRECEDENCE OVER SCALE DIMENSIONS. CONTRACTORS SHALL VERIFY & BE RESPONSIBLE FOR ALL DIMENSIONS & CONDITIONS SHOWN. THESE DRAWINGS SHIP DETAILS OF ADEQUATE SCALE MUST BE SUBMITTED TO THE ARCHITECT FOR APPROVAL BEFORE PROCEEDING WITH FABRICATION OR ITEMS NOTED.

REVISIONS	BY DATE
REV.	
The Benedict Bullock Group, PA	
ARCHITECTURE ■ PLANNING ■ DESIGN ■ DEVELOPMENT ■ CONSTRUCTION	
1300 N BROADWAY ■ SUITE 1001 ■ DAVENPORT ■ FLORIDA 32423	
PHONE: (321) 232-5086 ■ FAX: (321) 232-5086	
www.thebenedictgroup.com ■ Lic. No. AA 2605089	

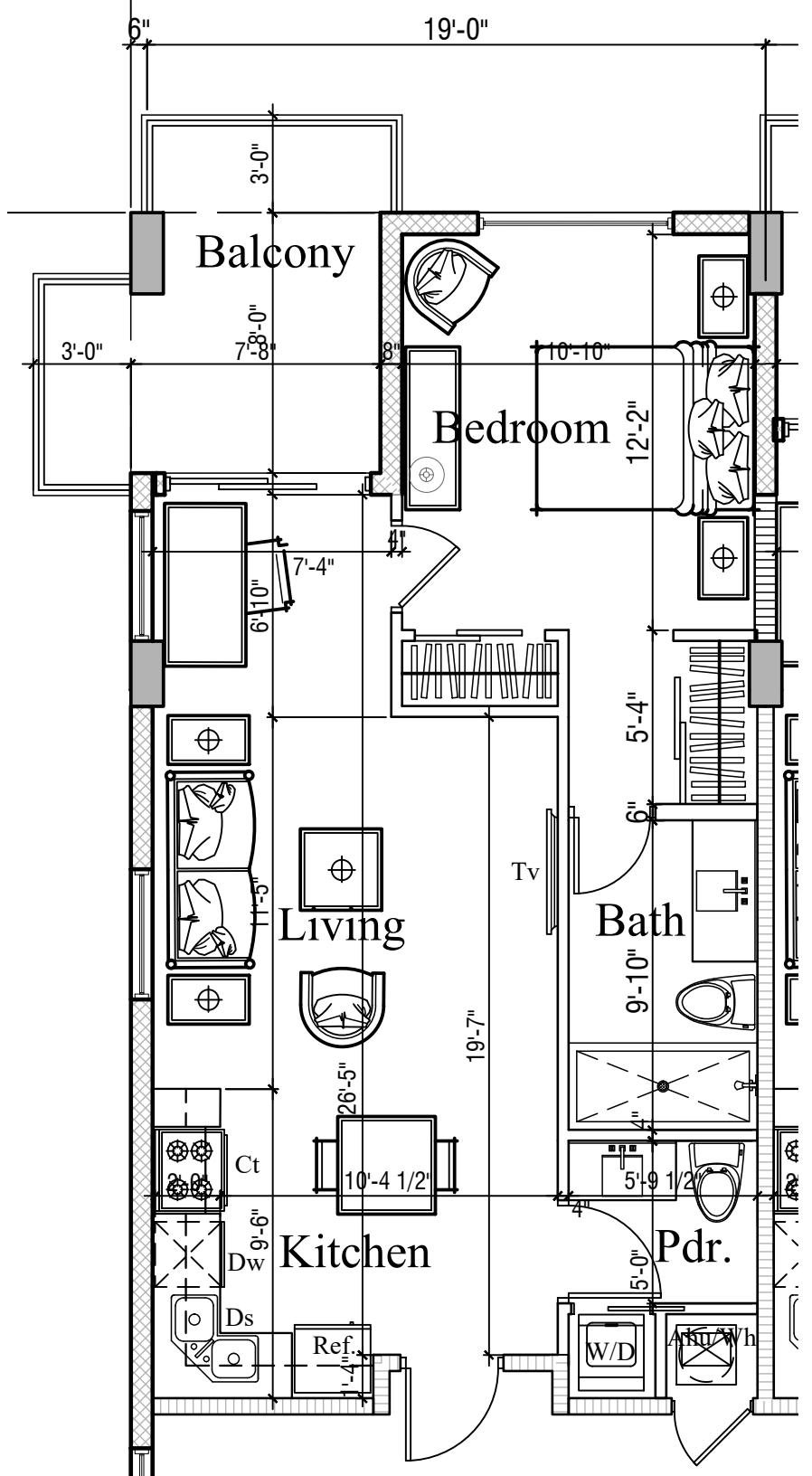
POLK STREET APARTMENTS II
2742-2741- POLK STREET HOLLYWOOD, FLORIDA

ARCHITECT'S SEAL
BRIAN IRA BULLOCK
AR95754
BRIAN BULLOCK, ARCHITECT
AR 93754

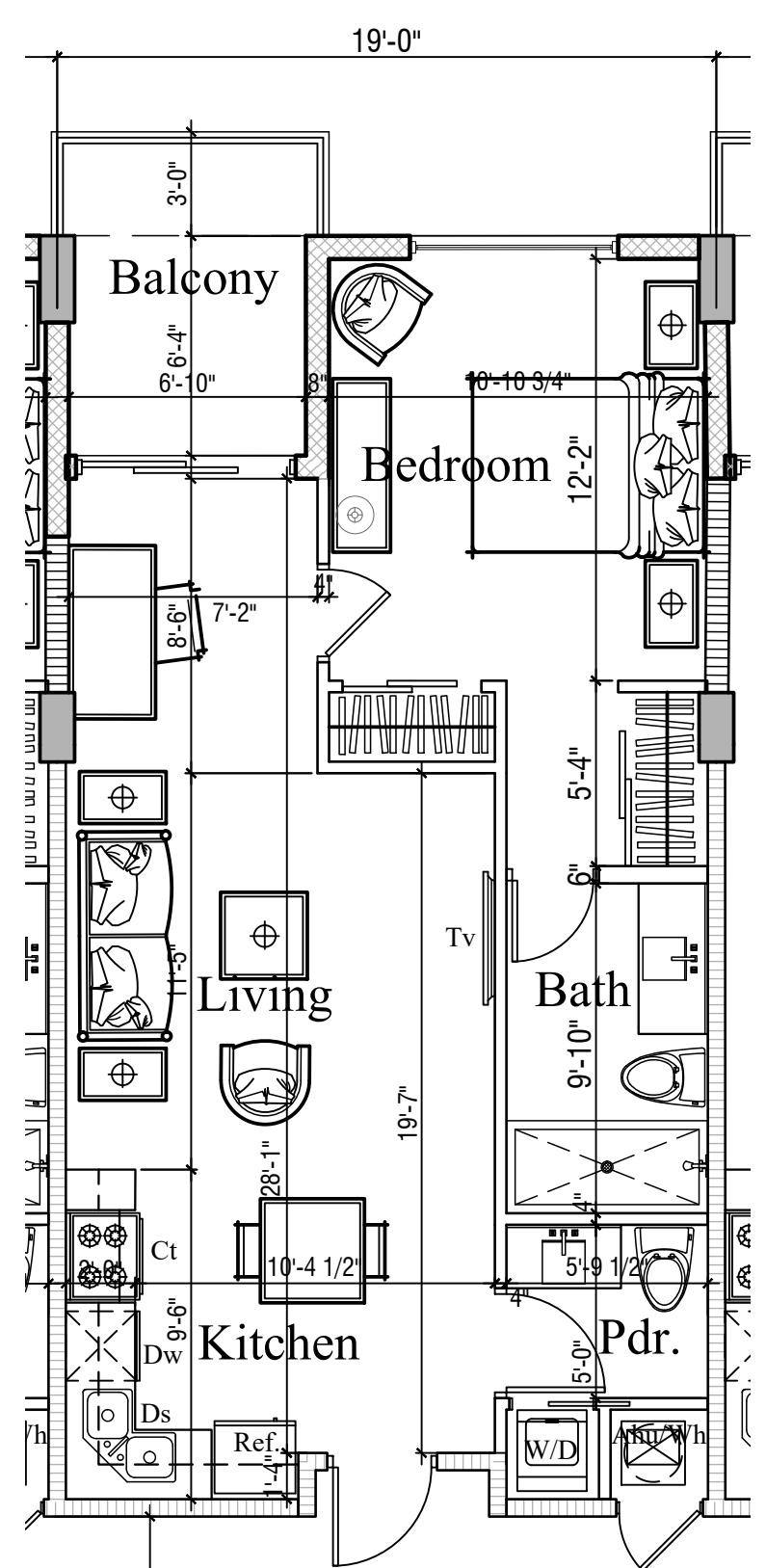
SCALE	1/16" = 1'-0"
DATE	02-03-2021
DRAWN BY	RST
PROJECT NO.	BS2030

CONTEXTUAL
STREET
ELEVATION
CS-1

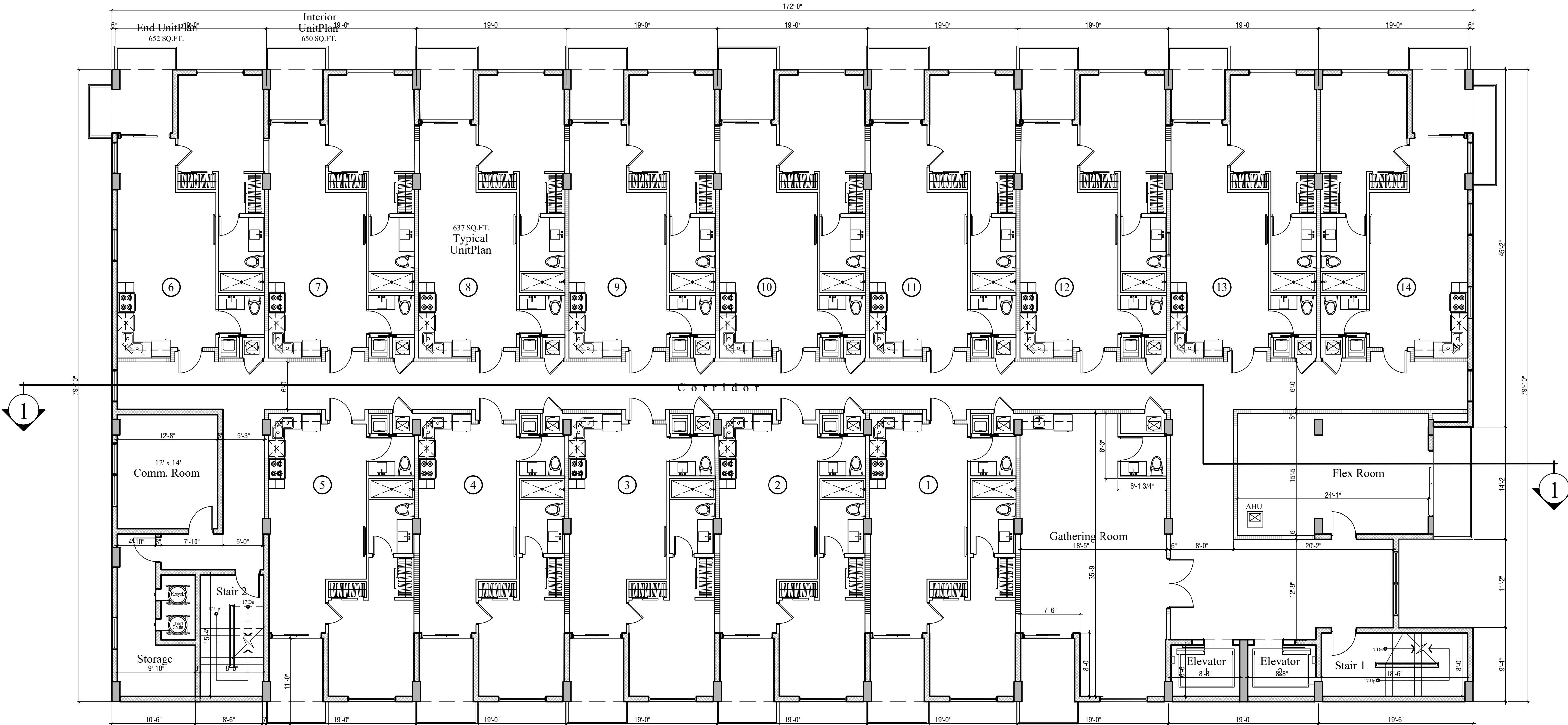
DEV.BRD PERMIT 08-13-2021



1BR, 1 1/2 BATH
END UNIT PLAN
SCALE: 3/16"=1'-0"
652 SQ.FT.



1BR, 1 1/2 BATH
INTERIOR UNIT PLAN
SCALE: 3/16"=1'-0"
650 SQ.FT.

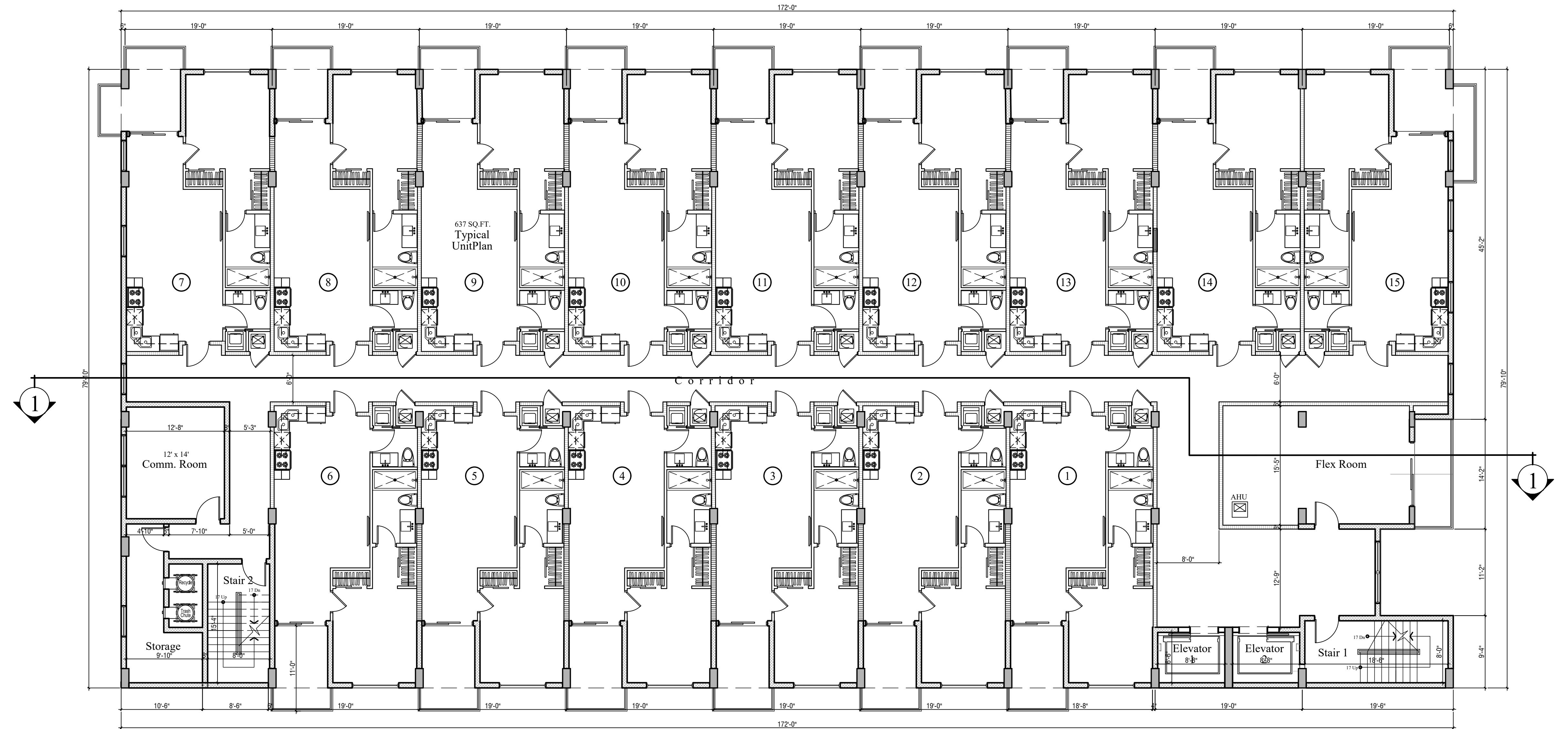


SECOND FLOOR BUILDING PLAN

SCALE: 1/8"=1'-0"

ALL IDEAS, DESIGNS, ARRANGEMENTS, & LANS INDICATED OR REPRESENTED BY THIS DRAWING ARE OWNED BY & THE PROPERTY OF THE ARCHITECT & WERE CREATED, DEVELOPED, & PROVIDED FOR USE ON & IN CONNECTION WITH THE SPECIFIED PROJECT. NONE OF SUCH IDEAS, DESIGNS, ARRANGEMENTS, OR PLANS SHALL BE USED BY OR DISCLOSED TO ANY PERSON, FIRM, OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT. WRITTEN DIMENSIONS ON THESE DRAWINGS SHALL PRECEDENCE OVER SCALE DIMENSIONS. CONTRACTORS SHALL VERIFY & BE RESPONSIBLE FOR ALL DIMENSIONS & CONDITIONS ON THE JOB. THE ARCHITECT MUST BE NOTIFIED OF ANY VARIATIONS FROM THE DRAWINGS.	
BY DATE	
REVISIONS	
REV.	
The Benedict Bullock Group, PA	
ARCHITECTURE PLANNING	
1325 NW 5TH STREET SUITE NO.11 DEERFIELD BEACH, FLORIDA 33442 (305) 727-2000 FAX (305) 727-2009 www.thebenedictgroup.com Lic. No. AA 2600089	
W N SW SE NE S E	
POLK STREET APARTMENTS II 2742-2741- POLK STREET HOLLYWOOD, FLORIDA	
ARCHITECT'S SEAL	
<p>BRIAN BULLOCK, ARCHITECT AR 95754</p>	
SCALE	1/8"=1'-0"
DATE	02-03-2021
DRAWN BY	RST
PROJECT NO.	BS2030
SECOND FLOOR BUILDING PLAN & TYP. UNIT PLAN	
DEV.BRD PERMIT 08-13-2021	

A-2



THIRD THRU FOURTH FLOOR BUILDING PLAN

SCALE:

1/8"=1'-0"

ALL IDEAS, DESIGNS, ARRANGEMENTS, & PLANS INDICATED OR REPRESENTED BY THIS DRAWING ARE OWNED BY & THE PROPERTY OF THE ARCHITECT & WERE CREATED, REVOLVED, & DEVELOPED FOR USE ON & IN CONNECTION WITH THE SPECIFIED PROJECT. NONE OF SUCH IDEAS, DESIGNS, ARRANGEMENTS, OR PLANS SHALL BE USED BY OR DISCLOSED TO ANY PERSON, FIRM, OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT THE WRITTEN PERMISSION OF BRIAN IRA BULLOCK, ARCHITECT. WRITTEN DIMENSIONS ON THESE DRAWINGS SHALL PRECEDENCE OVER SCALE. CONTRACTORS SHALL VERIFY & BE RESPONSIBLE FOR ALL DIMENSIONS & CONDITIONS ON THE JOB. THE ARCHITECT MUST BE NOTIFIED OF ANY VARIATIONS FROM THE DIMENSIONS AND CONDITIONS SHOWN BY THESE DRAWINGS. SHOP DETAILS OF FABRICATION ON ITEMS SO NOTED.

REVISIONS	BY DATE
REV.	DATE
The Benedict Bullock Group, PA	
ARCHITECTURE • PLANNING	
1325 NW 50TH STREET, SUITE NO. 11 DEERFIELD BEACH, FLORIDA 33442	
TEL: 305-727-2000 FAX: 305-727-2000	
www.thebenedictgroup.com Lic. No. AA 260089	
N NW SW S SE E NE	

POLK STREET APARTMENTS II
2742-2741- POLK STREET HOLLYWOOD, FLORIDA

STATE OF FLORIDA
BRIAN IRA BULLOCK
AR985754
BRIAN BULLOCK, ARCHITECT
AR 93754
SCALE 1/8"=1'-0"
DATE 02-03-2021
DRAWN BY RST
PROJECT NO. BS2030

THIRD THRU FOURTH FLOOR BUILDING PLAN

DEV.BRD PERMIT 08-13-2021

A-3

THE SPECIFIED PROJECT. NONE OF SUCH IDEAS, DESIGNS, ARRANGEMENTS, OR PLANS SHALL BE USED BY, OR DISCLOSED TO ANY PERSON, FIRM, CONTRACTORS SHALL VERIFY & BE RESPONSIBLE FOR ALL DIMENSIONS & CONDITIONS ON THE JOB. THE ARCHITECT MUST BE NOTIFIED OF ANY NOTED.

The Benedict Bullock Group, PA

ARCHITECTURE ■ PLANNING
NW 3RD STREET SUITE NO.1
RFIELD BEACH, FLORIDA 33442
570-9500 FAX (954) 570-9550
www.thebenedictgroup.com Lic. No. AA 26003089

**POLK STREET APARTMENTS II
2742-2741- POLK STREET HOLLYWOOD, FLORIDA**

ARCHITECT	
=1'-0"	
3-2021	
ST	
2030	
4	
-	
-	
-	

TOTAL ALLOWED COVERED AREA = 30%

TOTAL PROPOSED COVERED AREA $\frac{13,677}{4,094 (100)} = 30\%$

197'-6"

49 Total Units
- 4 Lobby Etc.
A/C - 45 Units

100'-0"

1

1

Flat Roof

Mech. / Flat Roof

22 A/C Condensers

Mech. / Flat Roof

16 A/C Condensers

General Purpose
Lobby Office
Flex
Lobby / Gym

Pool Deck

Spa

Covered Deck

Gym

Elevator Lobby

Stair 2

Stair 1

Mech. / Flat Roof

8 A/C Condensers

5'-0" deep Pool

16'-10" Women
16'-10" Men

AHU

197'-6"

10'-1"

12'-8"

7'-7"

10'-0"

13'-8"

4'-6"

11'-0"

10'-6"

6'-0"

13'-0"

6'-0"

15'-6"

4'-2"

10'-7"

37'-6"

10'-7"

4'-6"

7'-9"

6'-5"

6'-3"

5'-0"

11'-2"

9'-4"

16'-8"

5'-0"

4'-18"

25'-2"

19'-6"

10'-6"

13'-4"

9'-4"

66'-2"

49'-4"

12'-4"

19'-0"

7'-2"

21'-4"

17'-2"

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10'-6"

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37'-6"

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12'-8"

7'-7"

10'-0"

13'-8"

4'-6"

11'-0"

10'-6"

6'-0"

13'-0"

6'-0"

15'-6"

4'-2"

10'-7"

37'-6"

10'-7"

4'-6"

7'-9"

6'-5"

6'-3"

5'-0"

<b

ROOF / POOL DECK PLAN

SCALE: 1/8"-1'-0"

The seal is circular with a double outer border. The outer border contains the words 'STATE OF FLORIDA' in a stylized font. The inner circle contains the name 'BRIAN IRA BULLOCK' in a serif font, with a small star below the name. At the bottom of the inner circle, the number 'AR95754' is printed. A signature of 'Brian Ira Bullock' is written across the bottom of the seal.

AN BULLOCK, ARCHITECT
AR 95754

DATE	1/8"=1'-0"
DATE	02-03-2021

AWN BY RST

PROJECT NO. | BS2030

ROOF PLAN

Journal of Health Politics, Policy and Law, Vol. 32, No. 4, December 2007
DOI 10.1215/03616878-32-4 © 2007 by the Southern Political Science Association

◀ 1 ▶

A-4

10.1007/s00332-007-0332-0

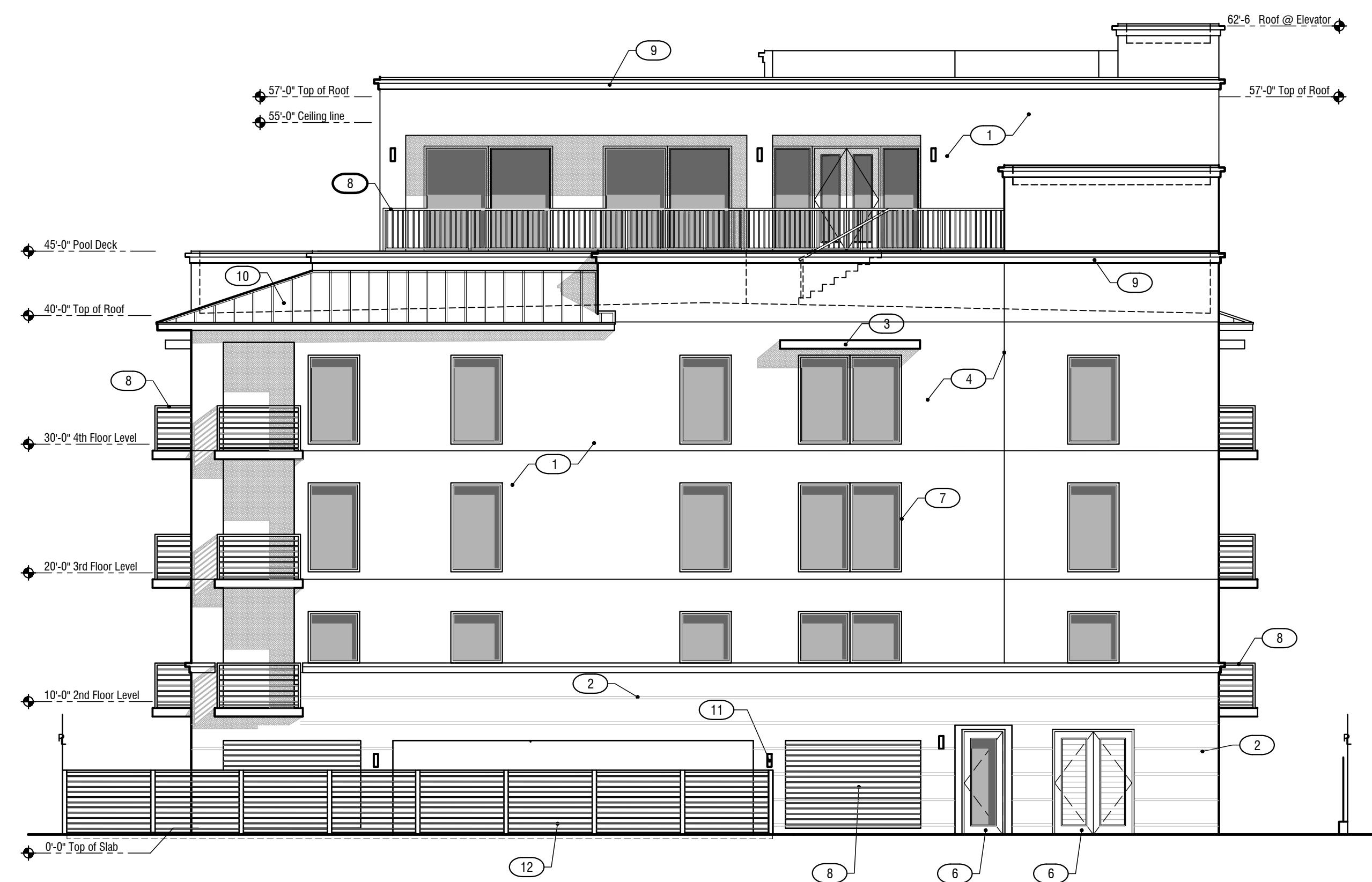
10.1007/s00332-007-0332-0

A-4



NORTH ELEVATION

SCALE: 1/8"-1'-0"



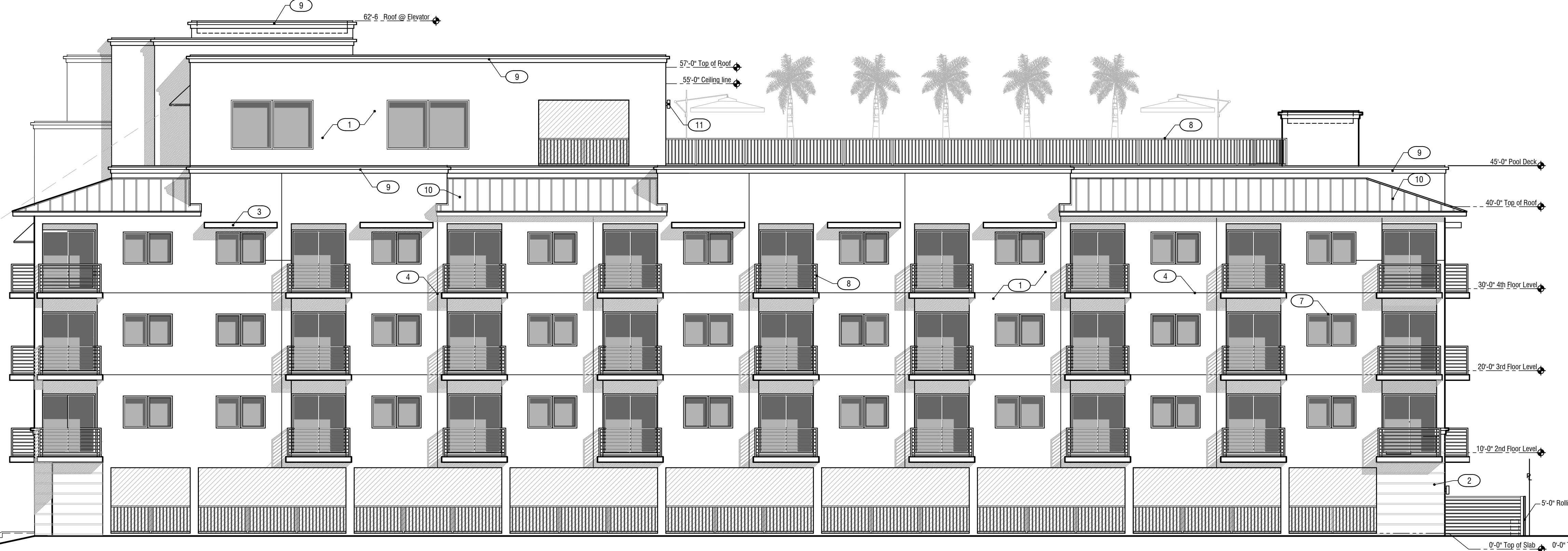
SOUTH ELEVATION

SCALE: 1/8"-1'-0"



ELEVATION NOTES

1. LIGHT TEXTURE STUCCO
2. HORIZONTAL PORCELAIN TILES
3. CONCRETE EYEBROW
4. VINYL REGLET
5. ALUMINUM FRENCH DOOR WITH SIDELITES
6. STEEL DOOR
7. BRONZE ALUUMINUM FRAME WITH GREY TINT
8. ALUMINUM RAILING
9. SMOOTH STUCCO CROWN
10. METAL ROOF
11. EXTERIOR LIGHTING
12. 5'-0" HIGH ALUMINUM FENCE
13. LOUVERED ALUMINUM AWNING



WEST ELEVATION

SCALE: 1/8" - 1'-0"

SCALE: 1/8"-1'-0"

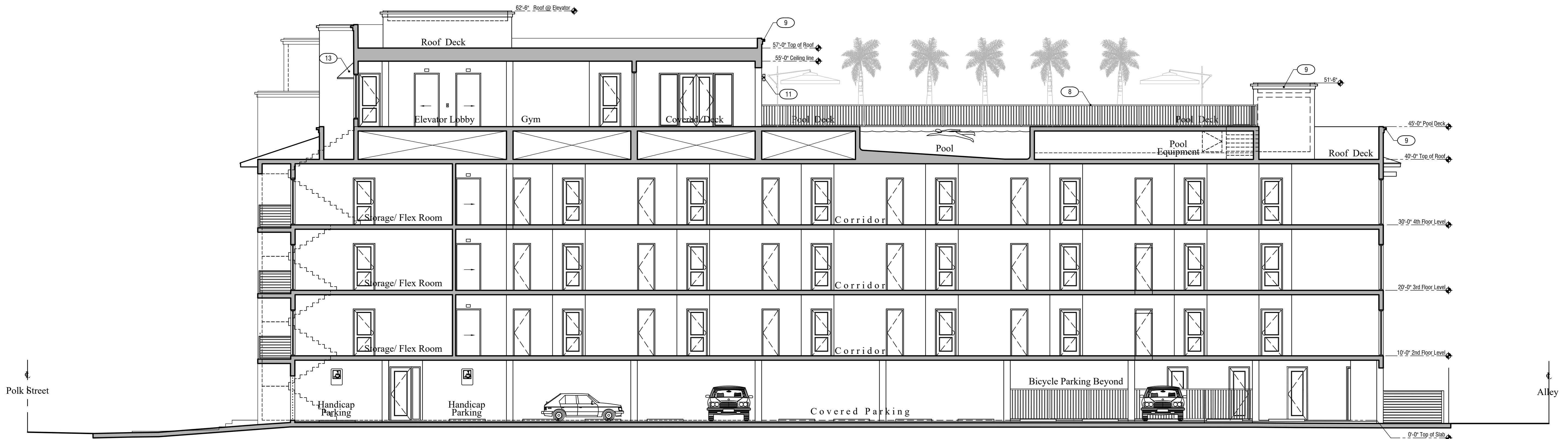
TH THE SPECIFIED PROJECT. NONE OF SUCH IDEAS, DESIGNS, ARRANGEMENTS, OR PLANS SHALL BE USED BY, OR DISCLOSED TO ANY PERSON, FIRM, CO. CONTRACTORS SHALL VERIFY & BE RESPONSIBLE FOR ALL DIMENSIONS & CONDITIONS ON THE JOB. THE ARCHITECT MUST BE NOTIFIED OF ANY S SO NOTED.

POLK STREET APARTMENTS III 2742-2741- POLK STREET HOLLYWOOD, FLORIDA

IAN BULLOCK, ARCHITECT AR 95754	
NAME	
DATE	1/8/2021
AWN BY	RST
JECT NO.	BS2030
ELEVATIONS	

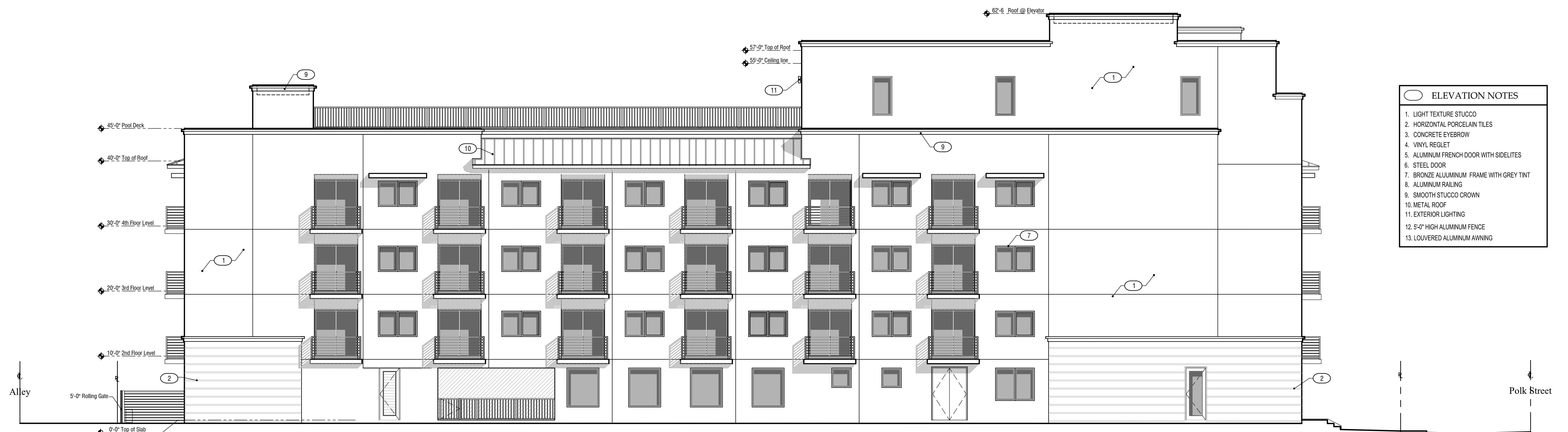
A E

A-5



EAST ELEVATION

SCALE: 1/8"-1'-0"



SECTION - 1

SCALE: 1/8"-1'-0"

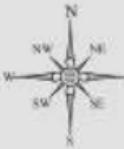
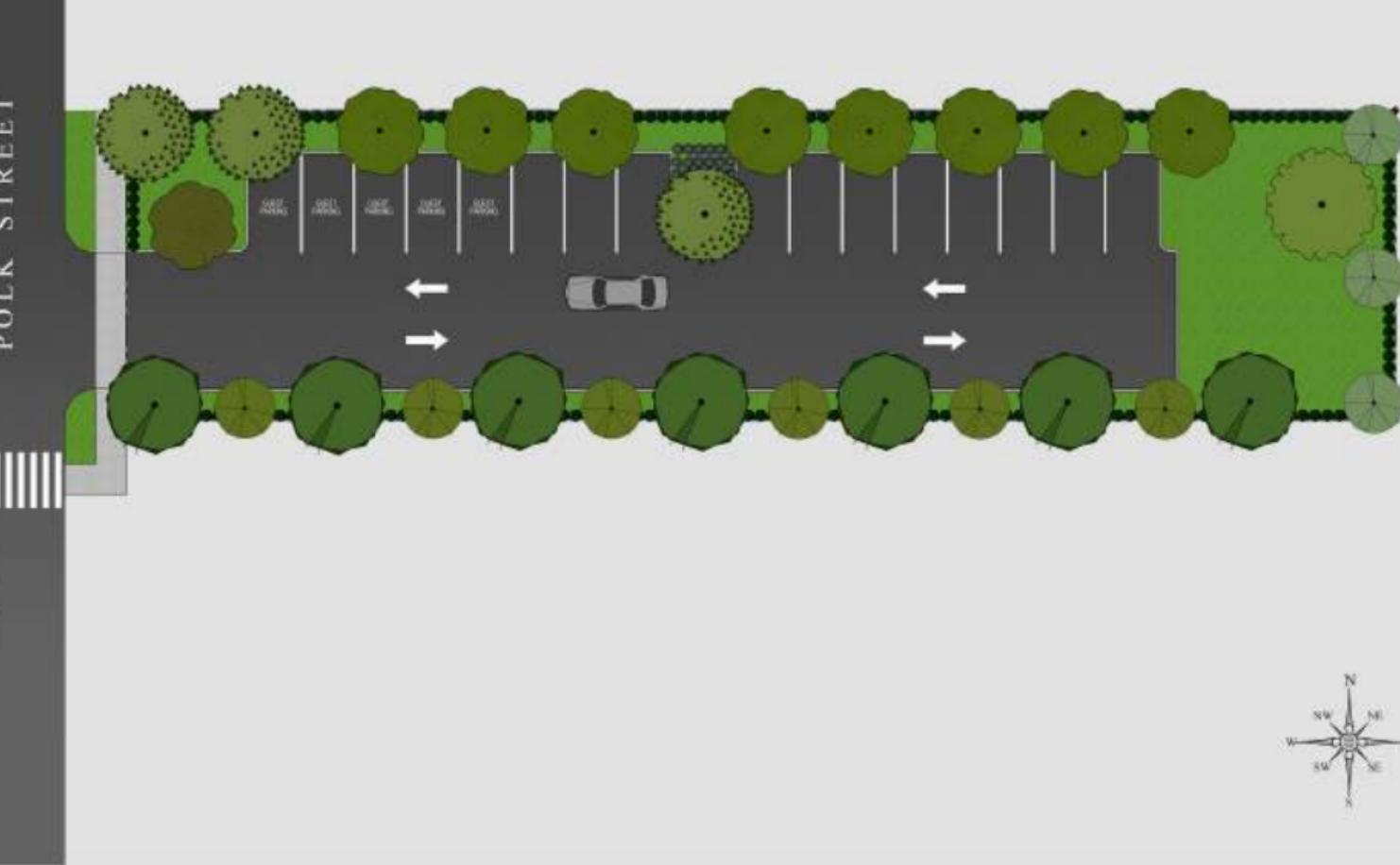
**POLK STREET APARTMENTS II
2742-2741- POLK STREET HOLLYWOOD, FLORIDA**

AR 95754	1/8"=1'-0"
	02-03-2021
N BY	RST
CT NO.	BS2030

LEVATION & SECTION

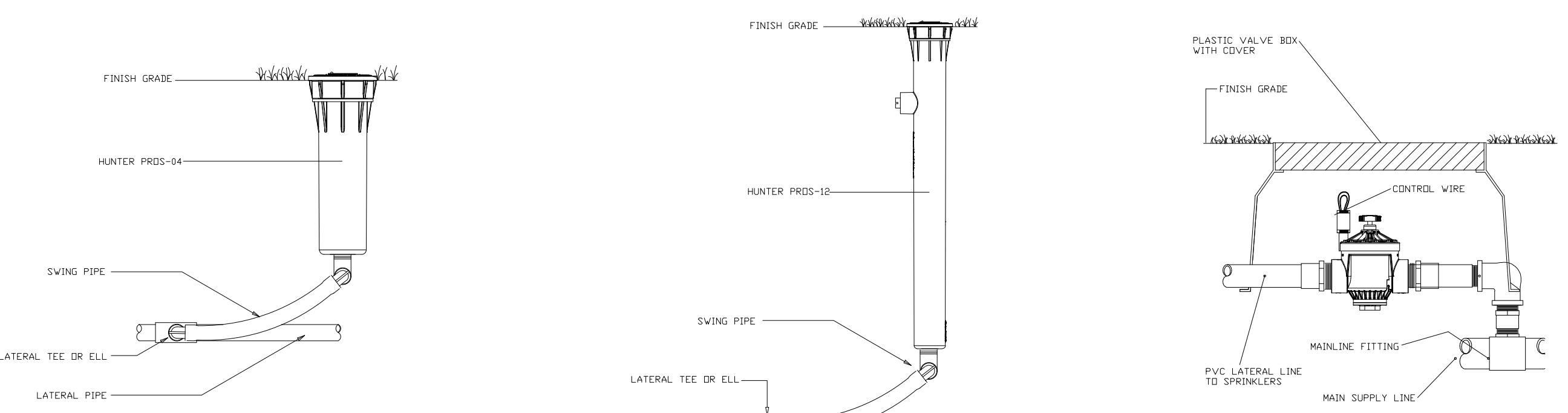
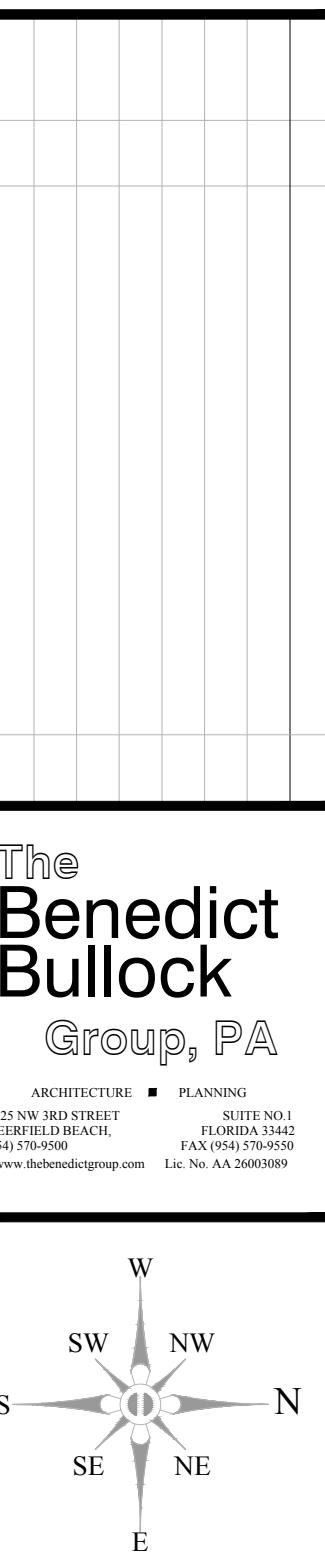
A-6

1



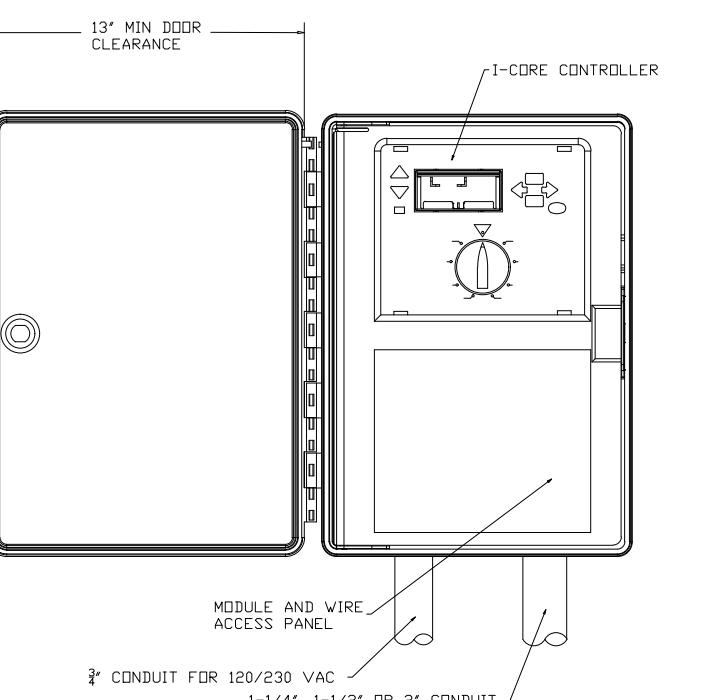
POLK STREET APARTMENTS II

2742-2741 POLK STREET HOLLYWOOD, FLORIDA



POP-UP SPRAY SPRINKLER

POP-UP SPRAY SPRINKLER

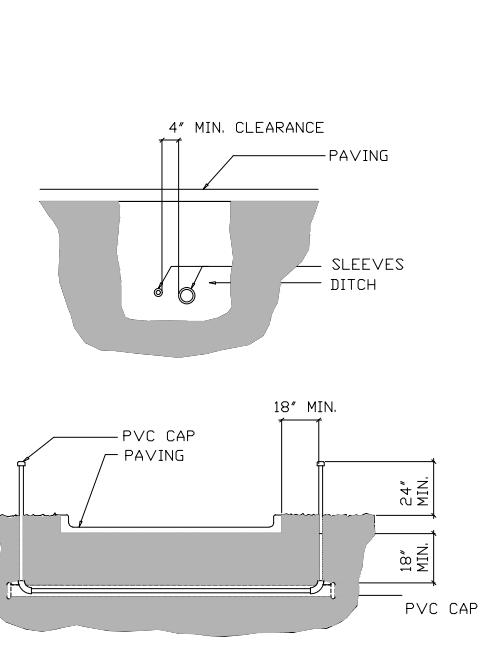


REMOTE CONTROL VALVE

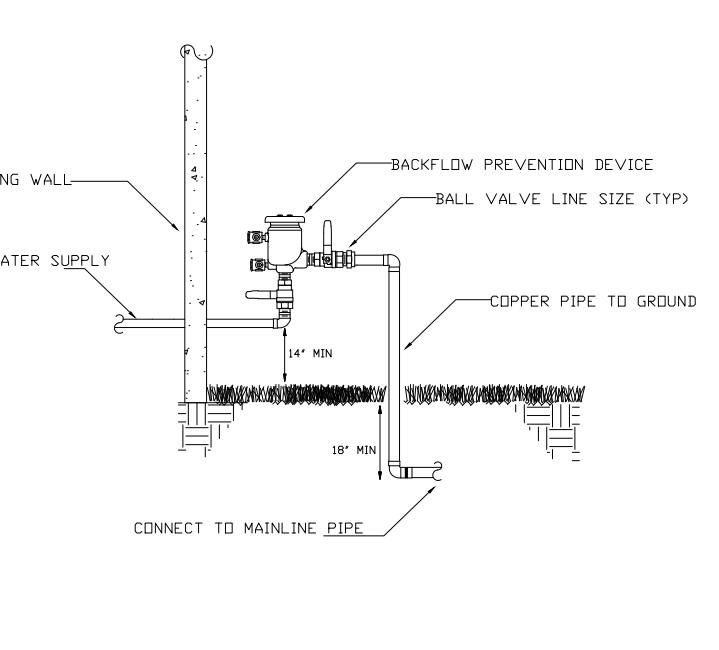
HUNTER I-CORE CONTROLLER

IRRIGATION SPECIFICATIONS

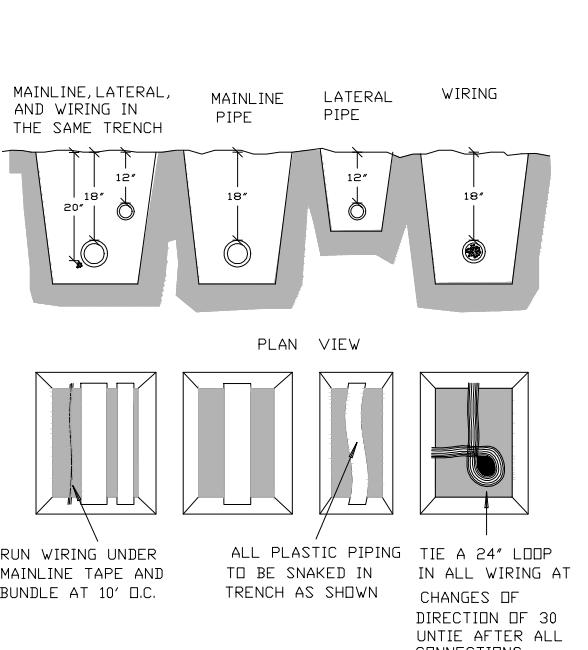
- IRRIGATION POINT OF CONNECTION SHALL BE CAPABLE OF DELIVERING A VARIABLE FLOW RATE OF 35 GPM AT A CONSTANT PRESSURE OF 60 PSI DOWNSTREAM OF BACKFLOW PREVENTION DEVICE. POINT OF CONNECTION SHALL BE ABLE TO MAINTAIN THE MAXIMUM FLOW RATE AND PRESSURE FOR THE DURATION OF AN IRRIGATION CYCLE. CONTRACTOR SHALL VERIFY THESE PARAMETERS PRIOR TO CONSTRUCTION, AND NOTIFY OWNER'S REPRESENTATIVE AND IRRIGATION CONSULTANT IF THEY CANNOT BE MET.
- IF THE POINT OF CONNECTION EXCEEDS THE ABOVE PRESSURE REQUIREMENTS, A PRESSURE REGULATOR SHALL BE INSTALLED AT THE OWNER'S EXPENSE. PRESSURE REGULATOR SHALL BE SET AT THE PRESSURE RECOMMENDED ABOVE.
- AN PVB TYPE BACKFLOW PREVENTION DEVICE SHALL BE INSTALLED IN A MANNER SATISFYING LOCAL CODES AND MANUFACTURER'S RECOMMENDATIONS.
- POWER FOR THE IRRIGATION CONTROLLER, PUMP AND OTHER ELECTRICAL COMPONENTS SHALL BE PROVIDED BY OTHER. CONTRACTOR SHALL VERIFY POWER AVAILABLE MEETS THE REQUIREMENTS OF THE COMPONENT'S MANUFACTURER. IF POWER AVAILABLE IS INADEQUATE, CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE PRIOR TO CONSTRUCTION.
- IRRIGATION SYSTEM IS DISPLAYED SCHEMATIC IN NATURE. MINOR FIELD ADJUSTMENTS MAY BE NECESSARY TO ACCOMMODATE FOR LANDSCAPING CHANGES, PLANTING BEDS OR OTHER OBSTRUCTIONS. THESE ADJUSTMENTS MAY BE MADE ONLY AFTER NOTIFYING THE OWNER'S REPRESENTATIVE.
- SOME IRRIGATION COMPONENTS AND PIPING ARE SHOWN IN HARDCAPE AREAS AND OUTSIDE OF PROPERTY LINES TO IMPROVE ON THE READABILITY OF THE IRRIGATION PLAN. ALL COMPONENTS AND PIPING SHALL BE INSTALLED INSIDE OF THE PROPERTY LINES AND OUTSIDE OF HARDCAPE AREAS.
- MAINLINE, LATERTALS AND CONTROL WIRES SHALL BE INSTALLED INSIDE THE SAME TRENCH WHENEVER POSSIBLE..
- SYSTEM TO BE INSTALLED PER MANUFACTURER'S SPECIFICATIONS.
- CONTROLER SHALL BE GROUNDED PER MANUFACTURER'S SPECIFICATIONS.
- ALL WIRE SHALL BE #18 DIRECT BURIAL U.F. MULTI-CABLE.
- ALL FIELD WIRE ABOVE GRADE OR WITHIN STRUCTURE TO BE INSTALLED IN CONDUIT PER LOCAL CODE.
- ALL UNDERGROUND SPLICES TO UTILIZE 3M DBY, OR KING WATER PROOF SPLICE KITS, DEPENDING ON NUMBER AND SIZE OF WIRES. ALL SPLICES SHALL BE MADE INSIDE A VALVE BOX.
- DEPTH OF IRRIGATION PIPING; 18" ON MAINLINE; 12" ON LATERTALS.
- ALL SLEEVING UNDER PAVED AREAS SHALL BE INSTALLED AT A DEPTH OF 24".



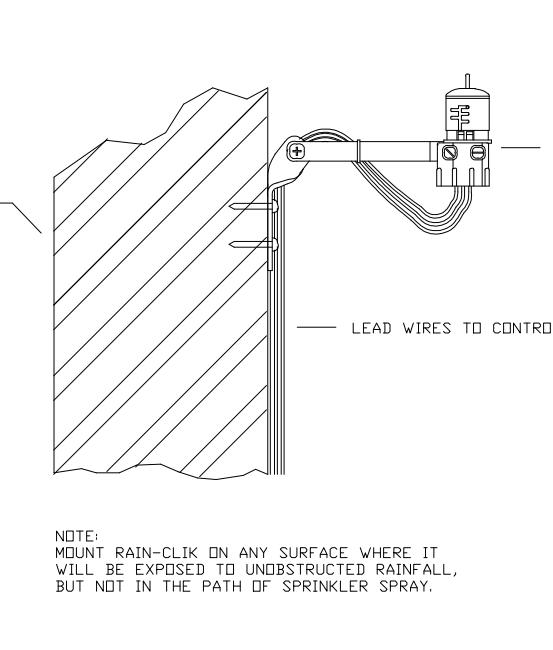
IRRIGATION SLEEVING



PRESSURE VACUUM BREAKER



PIPE AND WIRE TRENCH



HUNTER RAIN-CLICK RAIN SENSOR

CRITICAL ANALYSIS

P.O.C. NUMBER: 01

Water Source Information:

FLOW AVAILABLE

Water Meter Size: 1-1/2" 75.00 gpm

PRESSURE AVAILABLE

Static Pressure at POC: 60.00 PSI

Elevation Change: 5.00 ft

Service Line Size: 3"

Length of Service Line: 20 ft

Pressure Available: 57.00 psi

DESIGN ANALYSIS

Maximum Station Flow: 27.52 gpm

Flow Available at POC: 75.00 gpm

Residual Flow Available: 47.48 gpm

Pressure Req. at Critical Station: 38.30 psi

Loss for Fittings: 0.05 psi

Loss for Main Line: 0.47 psi

Loss for POC to Valve Elevation: 0.00 psi

Loss for Backflow: 2.58 psi

Loss for Water Meter: 1.55 psi

Critical Station Pressure at POC: 42.95 psi

Pressure Available: 57.00 psi

Residual Pressure Available: 14.05 psi

CRITICAL ANALYSIS

P.O.C. NUMBER: 02

Water Source Information:

FLOW AVAILABLE

Water Meter Size: 1-1/2" 75.00 gpm

Flow Available: 75.00 gpm

PRESSURE AVAILABLE

Static Pressure at POC: 60.00 PSI

Elevation Change: 5.00 ft

Service Line Size: 3"

Length of Service Line: 20 ft

Pressure Available: 57.00 psi

DESIGN ANALYSIS

Maximum Station Flow: 30.74 gpm

Flow Available at POC: 75.00 gpm

Residual Flow Available: 44.26 gpm

Pressure Req. at Critical Station: 37.63 psi

Loss for Fittings: 0.03 psi

Loss for Main Line: 0.29 psi

Loss for POC to Valve Elevation: 0.00 psi

Loss for Backflow: 2.61 psi

Loss for Water Meter: 1.92 psi

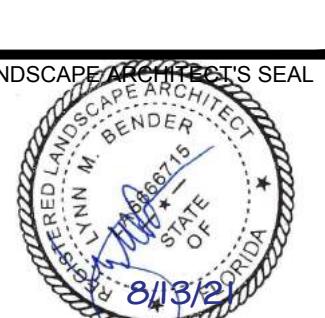
Critical Station Pressure at POC: 42.48 psi

Pressure Available: 57.00 psi

Residual Pressure Available: 14.52 psi

VALVE SCHEDULE

NUMBER	MODEL	SIZE	TYPE	GPM	HEADS	PIPE	DESIGN PSI	FRICITION LOSS	VALVE LOSS	PSI	PSI @ POC	PRECIP
A1	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	27.52	28	343.6	30	5.3	3	38.31	42.95	1.66 in/h
A2	Hunter PGV-101G	1"	Turf Spray	16.50	27	244.3	30	2.36	2.11	34.47	37.75	1.71 in/h
A3	Hunter PGV-101G	1"	Shrub Spray	10.56	13	121.2	30	0.1	1.87	31.97	34.69	1.7 in/h
A4	Hunter PGV-101G	1"	Turf Spray	21.41	39	300.9	30	2.61	3.69	36.3	40.37	1.81 in/h
A5	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	27.30	23	279.3	30	4.25	3	37.24	42.36	1.72 in/h
B1	Hunter PGV-101G	1"	Shrub Spray	22.75	19	260.6	30	2.97	4.07	37.04	40.71	1.7 in/h
B2	Hunter PGV-101G	1"	Turf Spray	21.68	21	253.3	30	3.12	3.77	36.89	40.47	1.64 in/h
B3	Hunter PGV-101G	1"	Shrub Spray	24.02	28	316.7	30	3.61	4.43	38.03	41.95	1.57 in/h
B4	Hunter PGV-151 Globe	1-1/2"	Turf Spray	30.74	25	307.0	30	4.62	3	37.62	42.48	1.81 in/h
C1	Hunter PGV-101G	1"	Shrub Spray	10.44	14	117.2	30	0.08	1.87	31.96	34.67	1.78 in/h
C2	Hunter PGV-101G	1"	Turf Spray	22.53	40	299.6	30	2.42	4.01	36.43	40.69	1.8 in/h
C3	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	25.90	23	283.0	30	4.04	3	37.03	41.9	1.65 in/h
C4	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	27.52	27	344.9	30	5.28	3	38.28	42.92	1.65 in/h
C5	Hunter PGV-101G	1"	Turf Spray	17.38	28	239.1	30	2.62	2.41	35.03	38.38	1.76 in/h
D1	Hunter PGV-101G	1"	Shrub Spray	22.75	19	260.2	30	2.95	4.07	37.02	40.69	1.72 in/h
D2	Hunter PGV-101G	1"	Turf Spray	19.91	20	247.2	30	2.7	3.27	35.97	39.34	1.59 in/h
D3	Hunter PGV-151 Globe	1-1/2"	Turf Spray	29.44	24	313.3	30	4.93	3	37.93	42.57	1.69 in/h
D4	Hunter PGV-101G	1"	Shrub Spray	24.02	28	309.7	30	3.53	4.43	37.95	41.93	1.57 in/h



LANDSCAPE ARCHITECT
LYNN M BENDER
8/13/15
DATE DRAWN BY LB
PROJECT NO. 20-536

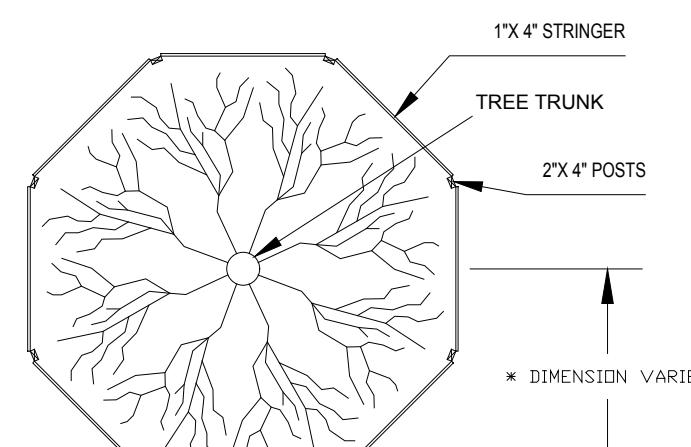
IRRIGATION DETAILS
IR-2
2 OF 2



It's fast. It's free. It's the law.

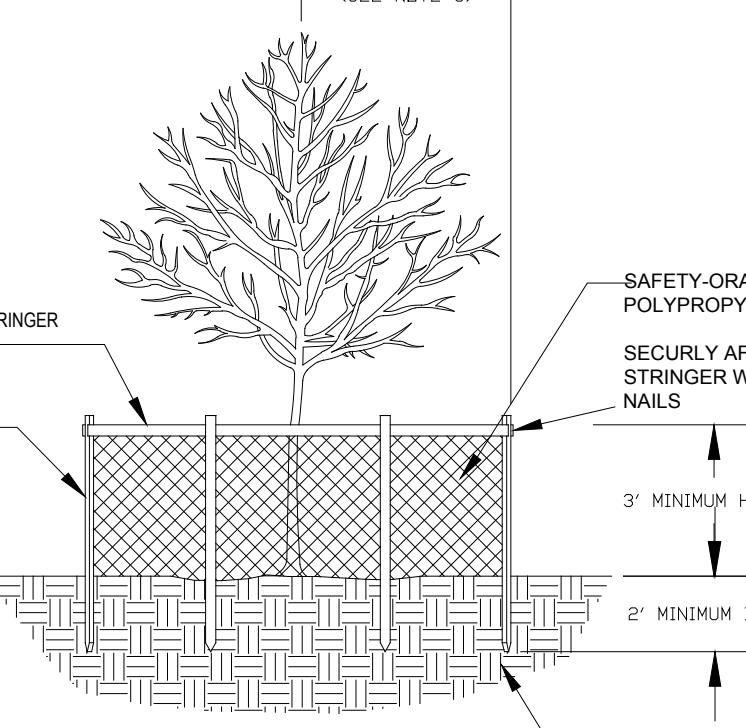
www.callsunshine.com

TREE PROTECTION ZONE



NOTE: FOR GROUPS OF TREES, PLACE BARRICADES BETWEEN TREES AND CONSTRUCTION ACTIVITY.

* DIMENSION VARIES



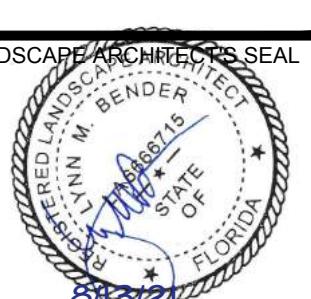
NOTES FOR TREE PROTECTION BARRICADES:

1. BARRICADE CIRCLE TO BE CENTERED ON TREE TRUNK(S). FOR GROUPS OF TREES, PLACE BARRICADES BETWEEN TREES AND CONSTRUCTION ACTIVITY.

2. NO GRADE CHANGES, TRAFFIC, STORAGE, TRENCHING OR ROOT CUTTING ALLOWED IN "TREE PROTECTION ZONE". CONTRACTOR TO REPLACE OR MITIGATE DAMAGED TREES OR VEGETATION AT CONTRACTOR'S EXPENSE PER LOCAL GOVERNMENT CODE REQUIREMENTS.

3. TREE PROTECTION BARRICADES SHALL BE LOCATED TO PROTECT EXISTING, RELOCATED AND NEW TREES IN CONSTRUCTION AREAS. THE PROTECTIVE BARRIER SHALL BE PLACED AROUND THE TREE AT A DISTANCE 6 FEET FROM THE TREE TRUNK OR AT THE CANOPY Drip LINE, WHICHEVER IS GREATER OR AS SHOWN ON LANDSCAPE PLAN.

POLK STREET APARTMENTS II



LYNN M BENDER
Landscape Architect
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RE: Arborist Report

Prepared for:
City of Hollywood Environmental Department
Prepared by:
Juan Masson
305-525-2830
usjmasson@aol.com
International Society of Arboriculture Professional Member #198726
ISA Certified Arborist FL-6184A
President, US INTERMED CORP.

Scope of Work

To inspect all trees and evaluate their conditions. To determine any trees that require protecting species, high value, endangered and in excellent condition which would make good candidate to relocate on site. Trees are given a rating of Poor, Fair, and Good. Neighbors' trees were inspected to determine impact to adjacent property

City of Hollywood Department of Environmental Services

from construction. We have provided a legend for easy reference. The following evaluation were done at the site and reflects today's condition as to size, shape, and health.

Work Performed

Trees on sight were inspected on April 08, 2021. After inspecting all trees and palms on site and property line we found that they fell mostly into the poor category with few in the Fair category. Neighbors' trees were inspected to address the impact that construction will have on trees that have grown on property line. Trees in the Fair category have a required TPZ radius if tree is to remain in place. Most trees have not been maintained for many years and evidence of damage from previous storms is obvious. Trees have been damaged from power lines, chain link fences or just planted in the wrong place. We recommend that all trees to remain be put on a maintenance and nutrition program to promote healthy growth.

Summary

I performed a tree resource evaluation on a section of the property located at **2718-2723 and 2742-2741 Polk Street Hollywood, Florida**. The approximate locations of these trees can be found on the schematic in Appendix B.

City of Hollywood Department of Environmental Services

The evaluation in Appendix A includes tree measurements, condition rating and recommended radius of the tree protection zone (TPZ) for each tree.

I rated the trees in accordance with ANSI A300 9part 5- 2005, Annex A, Management Report Information. The trees are rated Good, Fair or Poor, see Appendix D.

I also followed the Levels and Scope of Tree Risk Assessment from the ANSI A300 Part 9: Tree, Shrub, and Other Woody Plant Management- Standard Practices. Level 1 limited visual, Level 2 basic and Level 3 Advanced Tree Risk Assessments. The scope of this report /evaluation was limited to a Level 1 Assessment for all the trees onsite.

To perform all measurements, I used a forestry diameter measuring tape, a measuring pole, a measuring wheel. I rounded-off to the nearest inch when measuring trunk diameter, heights, and canopy diameters are approximate.

Any trees that are to remain should have their canopies cleared of dead wood and hazardous branches by a certified arborist. Protective barriers must be placed and maintained around remaining trees according to the City of Hollywood tree protection code. This code can be found in Appendix E. Appendix F contains the ANSI A300 definitions of Tree Protection Zone (TPZ) and Critical Root Zone (CRZ)

City of Hollywood Department of Environmental Services

Appendix - D
ANSI A300 (Part 5- 2005, Annex A)

Management report information
Examples of suitability ratings

Good: These are trees with good health and/or structural stability that have the potential for longevity at the site.

Fair: Trees in this category have fair health and/or structural defects that may be abated with treatment. Trees in this category require more management and monitoring and may have shorter lifespan than those in the "good" category.

Poor: Trees in this category are in poor health or have significant defects in structure that cannot be abated with treatment. These trees can be expected to decline regardless of management. The species or individual tree may possess either characteristics that are undesirable in landscape settings or be unsuited for use areas.

City of Hollywood Department of Environmental Services

REVISIONS	REV/
BY	DATE

The Benedict Bullock Group, PA

ARCHITECTURE ■ PLANNING
DESIGN-BUILD-OPERATE ■ DEVELOPMENT
www.thebenedictgroup.com Lic. No. 200000000000

POLK STREET APARTMENTS II 2742-2741 POLK STREET HOLLYWOOD, FLORIDA

Appendix - E

City of Hollywood tree protection.

Tree protection during construction. Trees shall be protected during construction activity as determined by the building department through the use of protective barriers in accordance with the landscape manual. Trees that are to remain shall be clearly identified with tags. A protected area with a radius of ten feet shall be maintained around trees to remain in accordance with the landscape manual, unless a certified arborist otherwise determines in writing that a smaller or larger protected area is acceptable for each tree, or alternative tree protection method is approved.

During demolition and/or development or construction, including installation of irrigation system or any other underground installations, protective barriers shall be placed around each tree and shall remain in order to prevent the destruction of damaging of roots, stems, or crowns of such trees. The barriers shall remain in place and intact until approved landscape operations begin however, barriers may be removed temporarily to accommodate construction needs, provided that the manner and purpose for such temporary removal will not harm the trees. The trees shall be properly irrigated throughout the building process. Trees damaged during construction shall be subject to mitigation.

City of Hollywood Department of Environmental Services

Appendix - F – Critical Root Zone and Tree Protection Zone

ANSI A300 (Part 5) – 2010 Management of Trees and Shrubs during Site Planning, Site Development and Construction

Critical Root Zone (CRZ): The minimum volume of roots necessary to have for tree health and stability.

Tree Protection Zone (TPZ): The area surrounding a tree defined by a specified distance, in which excavation and other construction - related activities should be avoided. The TPZ is variable depending on species, factors, age and health of the plant, soil conditions, and proposed construction. The zone may be accomplished by physical barriers or soil protection layers or treatment.

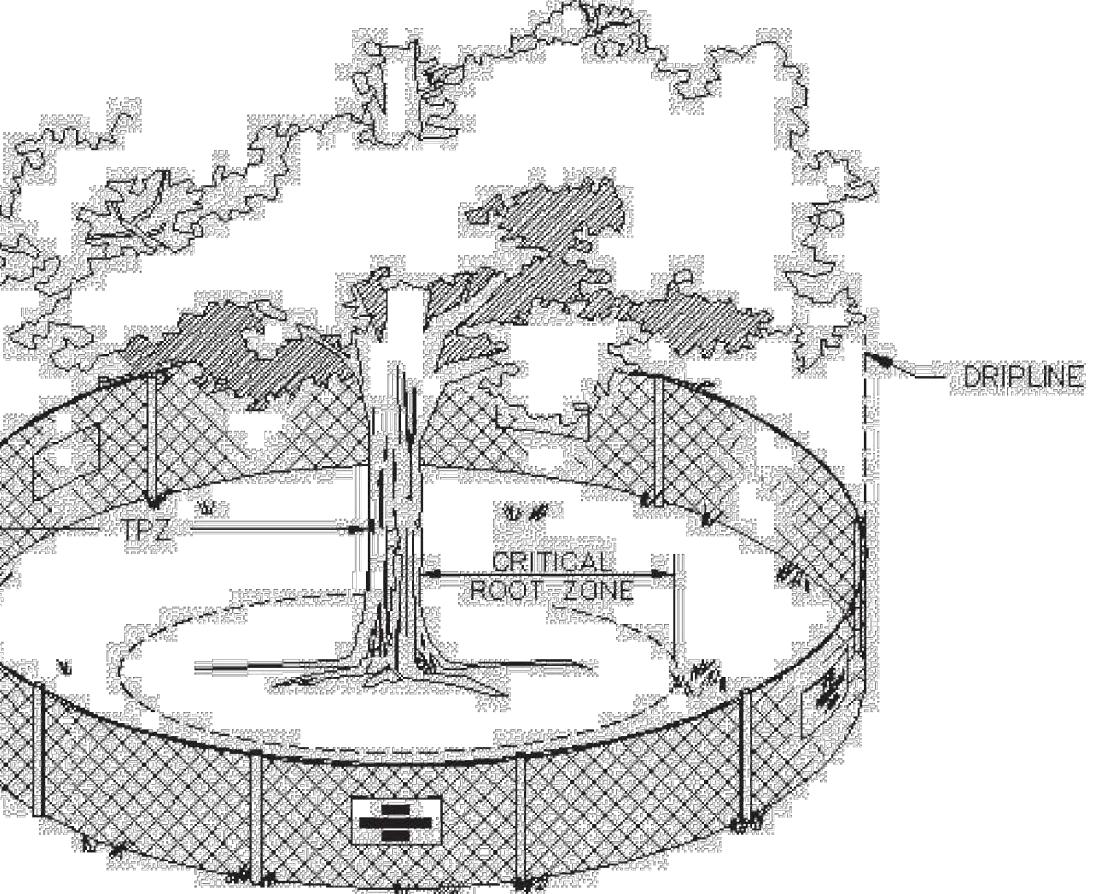
ANSI A300 (Part 5) – 2010 54.7

A tree protection zone (TPZ) shall be delineated around all trees to be protected during a project.

The area and dimensions of the TPZ should be calculated based on species tolerance, age, and health, root structure, rooting depth, and soil conditions.

City of Hollywood Department of Environmental Services

Appendix - G – Schematic for tree protection during construction



The dimensions for the tree protection zones for all trees to remain onsite are shown in Appendix A. This area shall be encircled with a 4-foot-high sturdy fence supported by steel rods or pipes to support the fence every 6

City of Hollywood Department of Environmental Services

feet. There shall be signage on the fence in English or Spanish not allowing storage of any materials, change of grade or movement of equipment. This fence shall be inspected regularly by the contractor to ensure compliance.

Appendix - H – Assumptions and Limiting Conditions

US INTERMED CORP DBA U.S.I. Landscaping

Qualifications, Assumptions, and Limiting Conditions

Any legal description provided to the consultant is assumed to be correct. Any title or ownerships of properties are assumed to be good and marketable. All property is appraised or evaluated as though free and clear, under responsible ownership and competent management.

All property is presumed to be in conformance with applicable codes, ordinances, statutes, or other regulations.

Care has been taken to obtain information from reliable sources. However, the consultant cannot be responsible for the accuracy of information provided by others.

City of Hollywood Department of Environmental Services

Certification of Performance

I, Juan Masson, certify:

That I have personally inspected the trees and/or the property referred to in this report and have stated my findings accurately. The extent of the evaluation is stated in the attached report.

That my analysis, opinions, and conclusions stated herein are my own.

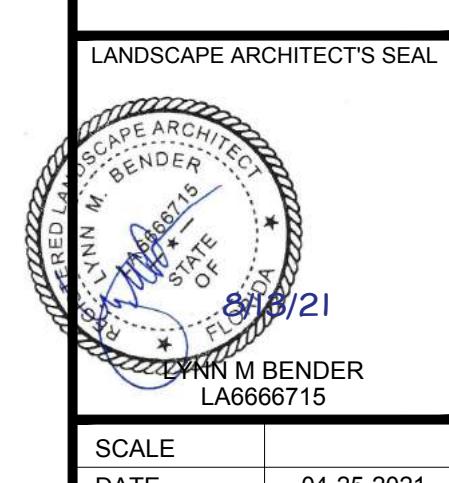
That my analysis, opinions, and conclusions were developed, and this report has been prepared per commonly accepted arboricultural practices.

That no one provided significant professional assistance to the arborist, except as indicated within the report.

That my compensation is not contingent upon the reporting of a predetermined conclusion that favors the cause of the client or any other party. I further certify that I am a member of the International Society of Arborists and acknowledge, accept, and adhere to the ISA Standards of Professional Practice. I am an International Society of Arboriculture Certified Arborist FL-6184A and have been involved in the practice of arboriculture and the study of trees for over twenty years.

Juan Masson ISA

City of Hollywood Department of Environmental Services



ARBORIST REPORT
ATTACHED FOR
INFORMATIONAL
PURPOSES ONLY

LANDSCAPE ARCHITECT CONSULTANT
LBLA, Inc.
Lynn Bender Landscape Architecture
5610 Adair Way
Lake Worth, FL 33467

OF 2

EROSION AND SEDIMENT CONTROL NOTES

1. THE CONTRACTOR IS RESPONSIBLE FOR FOLLOWING THE BEST EROSION AND SEDIMENT CONTROL PRACTICES AS OUTLINED IN THE PLANS, SPECIFICATIONS AND APPLICABLE WATER MANAGEMENT DISTRICT PERMIT(S) FOR THIS PROJECT.

2. FOR ADDITIONAL INFORMATION ON SEDIMENT AND EROSION CONTROL REFER TO "THE STATE OF FLORIDA EROSION AND SEDIMENT CONTROL DESIGNER AND REVIEWERS MANUAL" FROM THE STATE OF FLORIDA DEPARTMENT OF ENVIRONMENTAL REGULATION (FDER).

3. THIS PLAN INDICATES THE MINIMUM EROSION AND SEDIMENT CONTROL MEASURES REQUIRED FOR THIS PROJECT THE CONTRACTOR IS RESPONSIBLE FOR MEETING ALL APPLICABLE RULES, REGULATIONS AND WATER QUALITY GUIDELINES AND MAY NEED TO INSTALL ADDITIONAL CONTROLS.

4. ALL EXCAVATIONS AND EARTHWORK SHALL BE DONE IN A MANNER TO MINIMIZE WATER TURBIDITY AND POLLUTION. DISCHARGE SHALL BE CONTROLLED AND REROUTED THROUGH FILTERS, SILTATION DIAPERS AND SUMPS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PREVENTION, CORRECTION, CONTROL AND ABATEMENT OF EROSION AND WATER POLLUTION IN ACCORDANCE WITH CHAPTER 62-302, FLORIDA ADMINISTRATIVE CODE.

5. THE CONTRACTOR SHALL PAY FOR ANY WATER QUALITY CONTROL VIOLATIONS FROM ANY AGENCY THAT RESULTS IN FINES BEING ASSESSED TO THE OWNER BECAUSE OF THE CONTRACTOR'S FAILURE TO ELIMINATE TURBID RUNOFF FROM LEAVING THE SITE AND RAISING BACKGROUND LEVELS ABOVE EXISTING BACKGROUND LEVEL.

6. THE SITE CONTRACTOR IS RESPONSIBLE FOR REMOVING THE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER COMPLETION OF CONSTRUCTION AND ONLY WHEN AREAS HAVE BEEN STABILIZED.

7. ADDITIONAL PROTECTION - ON-SITE PROTECTION MUST BE PROVIDED THAT WILL NOT PERMIT SILT TO LEAVE THE PROJECT CONFINES DO TO UNFORSEEN CONDITIONS OR ACCIDENTS.

8. SILT FENCES SHALL BE USED ALONG THE PROPERTY LINES TO MINIMIZE OFFSITE SILTATION MITIGATION.

9. SILT FENCES SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.

10. SHOULD THE FABRIC ON A SILT FENCE DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE AND THE BARRIER IS STILL NECESSARY, THE FABRIC SHALL BE REPLACED PROMPTLY.

11. FILTER FABRIC SHALL BE INSTALL UNDER INLET GRATES AND EXTEND A MINIMUM OF 1 FOOT BEYOND EACH SIDE OF THE INLET STRUCTURE. IF MORE THAN ONE STRIP OF FABRIC IS NECESSARY, THE STRIPS SHALL BE OVERLAPPED 1 FOOT.

12. SEDIMENT DEPOSITS SHOULD BE REMOVED AFTER EACH RAINFALL AND AS NEEDED.

13. ANY DISCHARGE FROM DEWATERING ACTIVITY SHALL BE FILTERED AND CONVEYED TO THE OUTFALL IN A MANNER WHICH PREVENTS EROSION AND TRANSPORTATION OF SUSPENDED SOLIDS TO THE RECEIVING OUTFALL.

14. DEWATERING PUMPS SHALL NOT EXCEED THE CAPACITY OF THAT WHICH REQUIRES A CONSUMPTIVE USE PERMIT FROM THE APPLICABLE WATER MANAGEMENT DISTRICT.

15. ALL DISTURBED AREAS SHALL BE GRASSED, FERTILIZED, MULCHED AND MAINTAINED UNTIL A PERMANENT VEGETATIVE COVER IS ESTABLISHED.

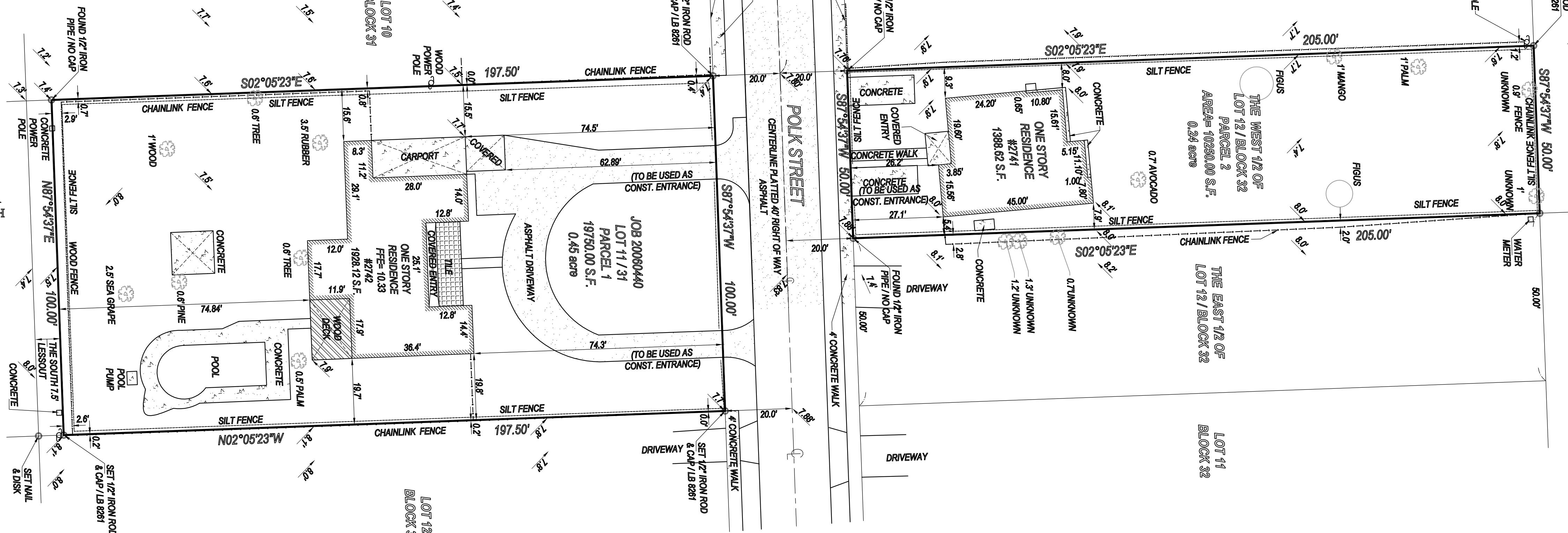
16. SOD SHALL BE PLACED IN AREAS WHICH MAY REQUIRE IMMEDIATE EROSION PROTECTION TO ENSURE WATER QUALITY STANDARDS ARE MAINTAINED.

17. ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER BARRIER ARE NO LONGER REQUIRED SHALL BE DRESSED TO CONFORM TO THE EXISTING GRADE, PREPARED AND SEEDED.

18. CONTRACTOR SHALL INSURE THAT ALL DRAINAGE STRUCTURES, PIPES, ETC. ARE CLEANED OUT AND WORKING PROPERLY AT TIME OF ACCEPTANCE.

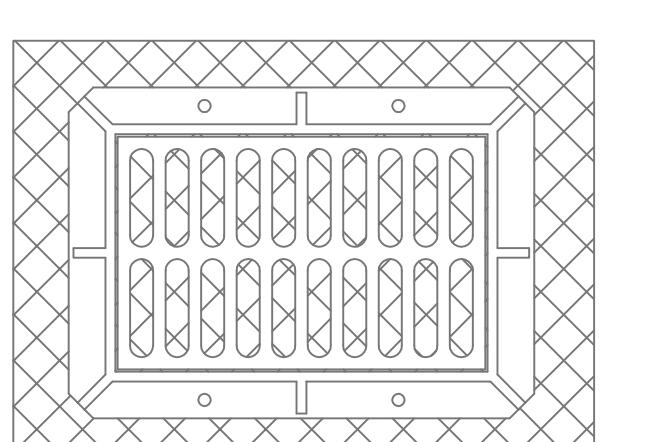
19. FLOATING TURBIDITY BARRIERS WILL BE PLACED OFF SET FROM THE SEAWALL ADJACENT TO THE PROPERTY. IF SEAGRASSES ARE PRESENT BARRIERS WILL NOT BE PLACED OVER THEM. THE FLOATING TURBIDITY BARRIERS SHALL ALSO BE INSTALLED IN A MANNER TO PREVENT MANATEE ENTANGLEMENT.

20. ALL DEATERING, EROSION, AND SEDIMENT CONTROL SHALL REMAIN IN PLACE UNTIL AFTER COMPLETION OF CONSTRUCTION AND SHALL BE REMOVED WHEN AREAS HAVE BEEN STABILIZED.



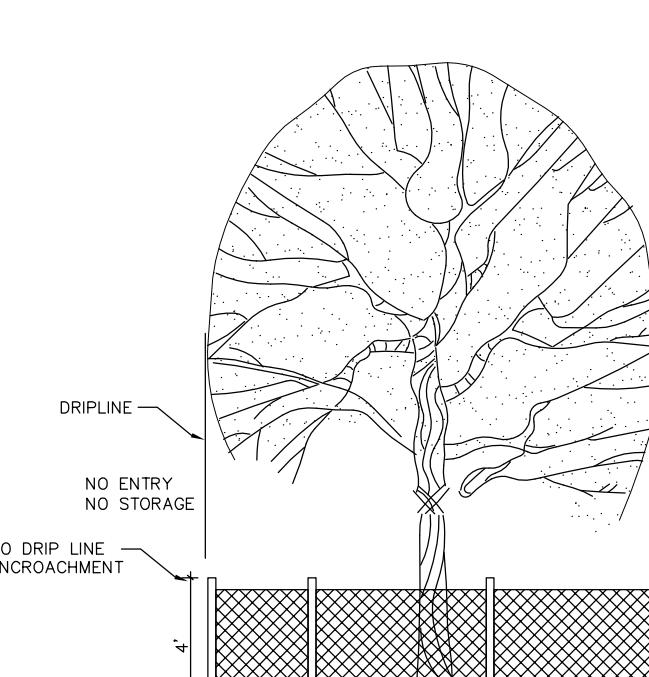
EROSION AND SEDIMENT CONTROL PLAN

SCALE: 1"=20'

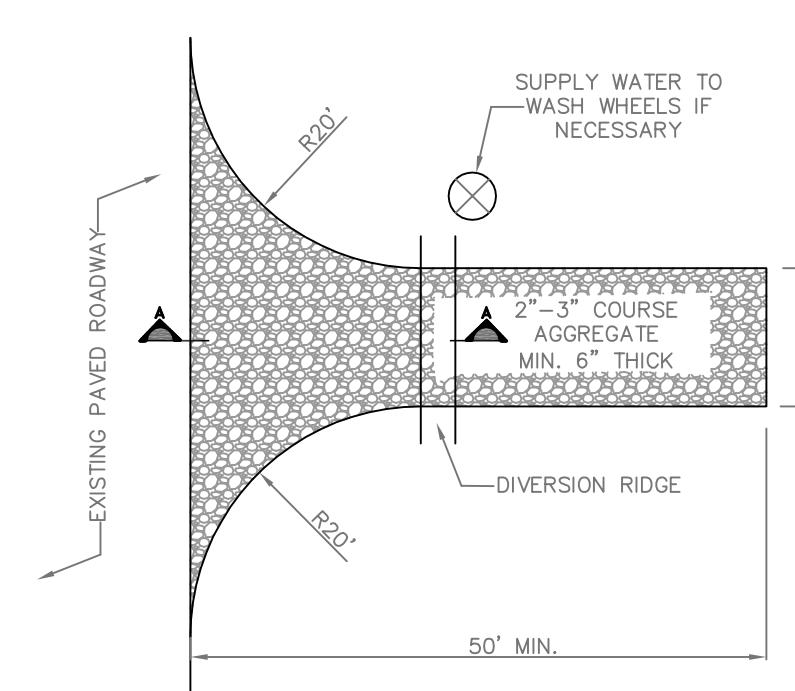


NOTE: INSTALL FILTER FABRIC UNDER ALL GRATES TO PREVENT SILT AND CONSTRUCTION DEBRIS FROM ENTERING THE SYSTEM.

INLET PROTECTION



TREE PRESERVATION
BARRICADE FENCING DETAIL

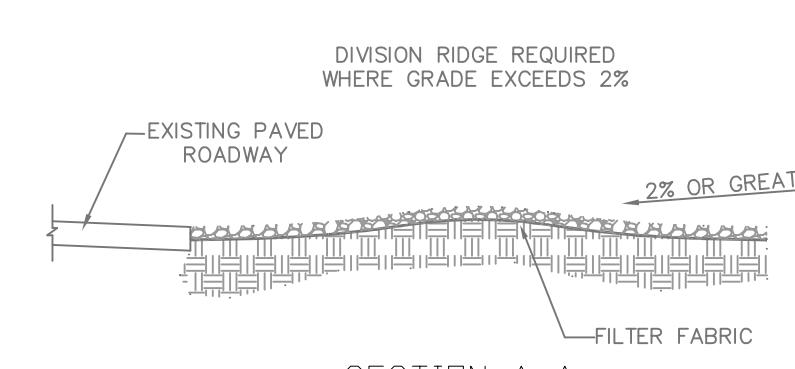


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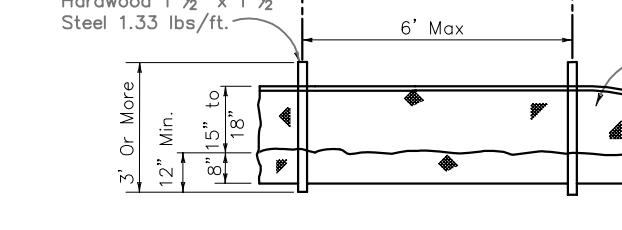
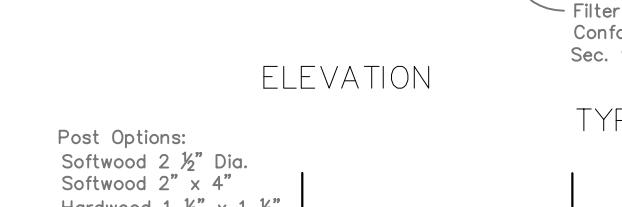
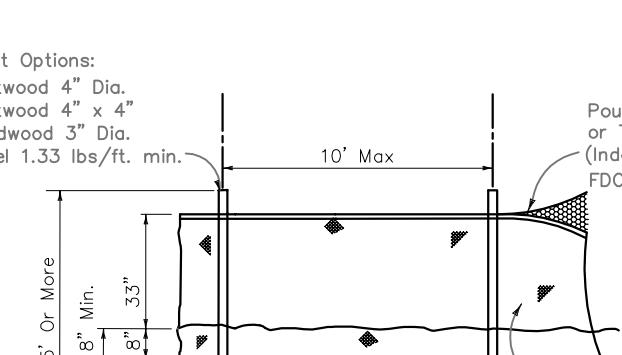
1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.

2. WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.

3. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN.

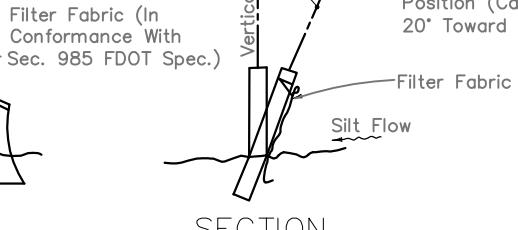
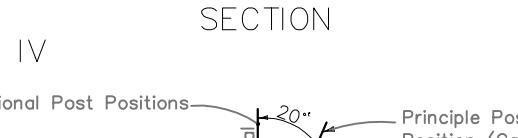
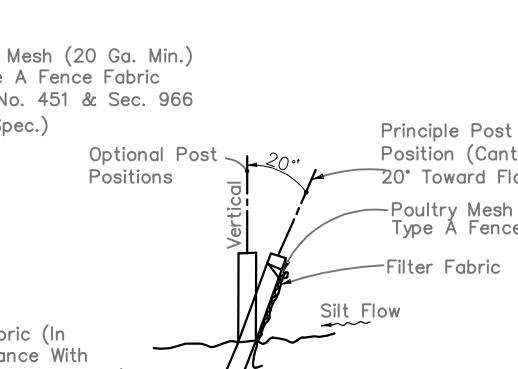


TEMPORARY GRAVEL CONSTRUCTION ENTRANCE



ELEVATION

SECTION



ELEVATION

SECTION

ELEVATION

SECTION

NOTE: FOR SLIT FENCE APPLICATIONS AND NOTES, SEE SHEET 3 OF 3 FOOT INDEX No. 102.

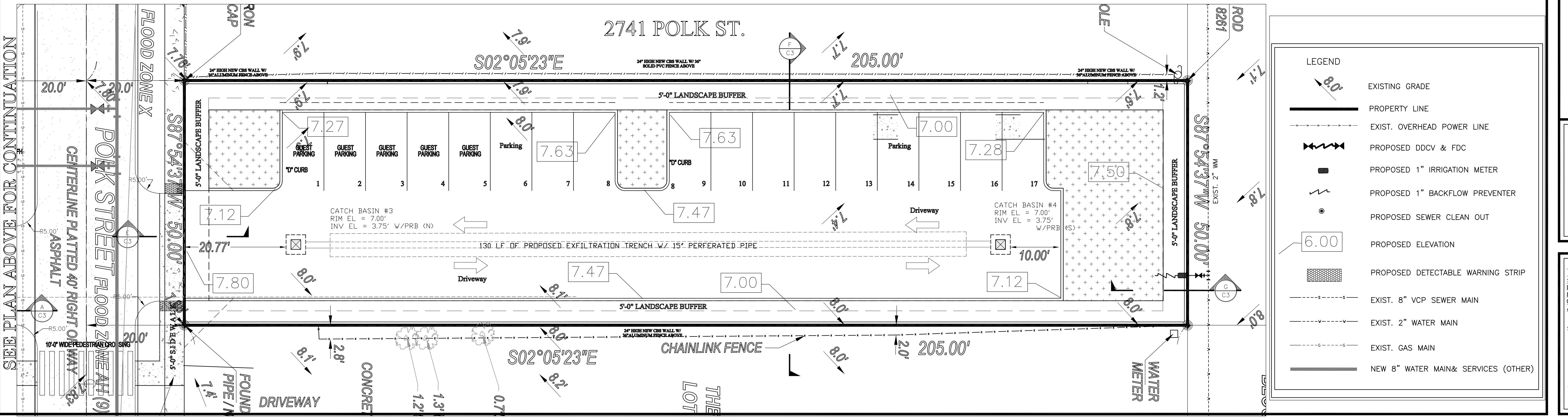
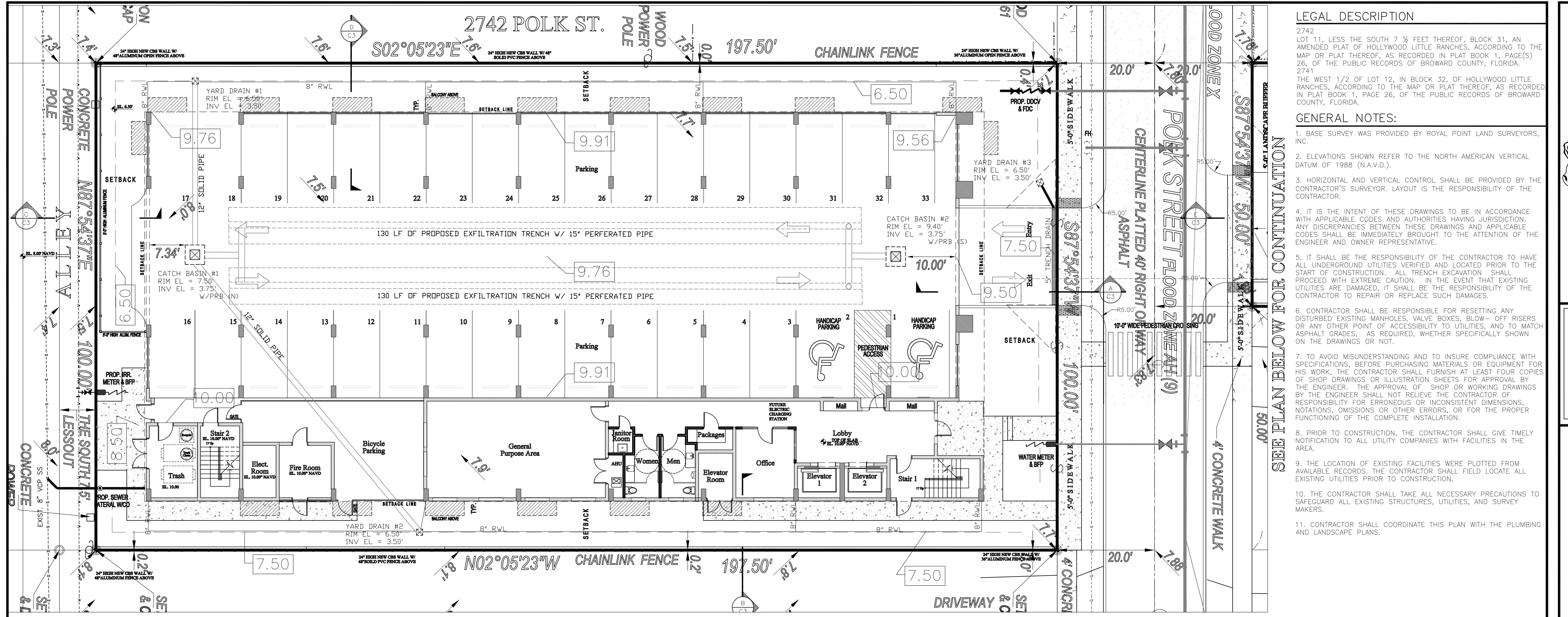
SILT FENCE DETAIL

POLK STREET APARTMENTS // 2742 & 2741 POLK STREET HOLLYWOOD, FL

REVISIONS	NO.	DATE	DESCRIPTION

GEA PROJECT NO.: 21046a
DATE: 06-07-2021
SCALE: AS SHOWN
DESIGNED BY: R.B.J.
DRAFTER: R.B.J.
CHECKED BY: R.B.J.
APPROVED BY: R.B.J.
SHEET TITLE
EROSION & SEDIMENT CONTROL PLAN
C1 OF 7





SPECIFIC NOTES:

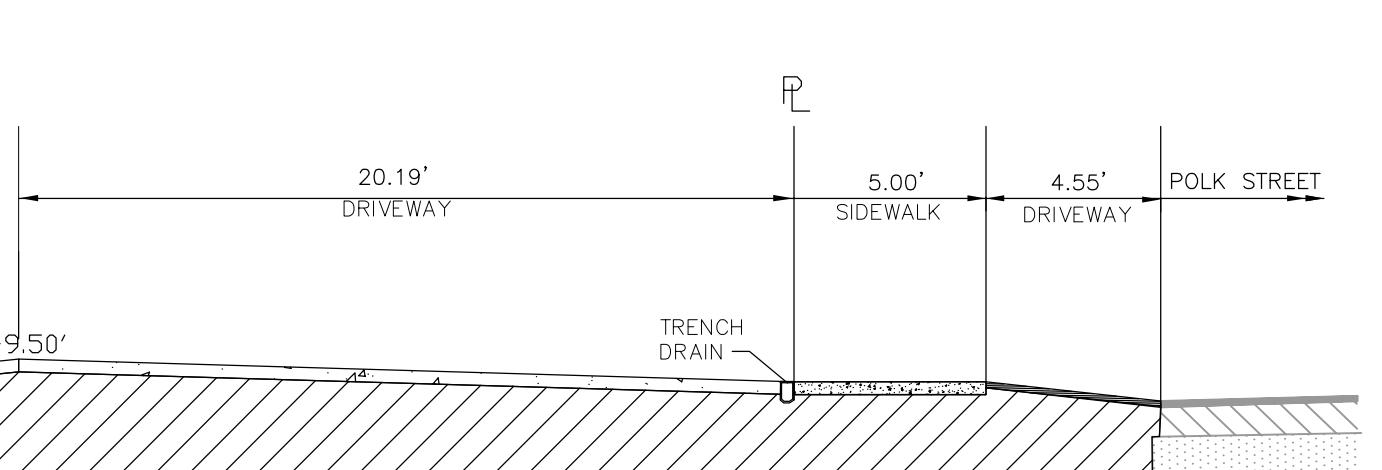
1. PROVIDE FILTER FABRIC OR OTHER METHOD OF SEDIMENT PROTECTION FOR ANY EXISTING CATCH BASIN/INLET WITHIN 100 FEET OF THE PROPERTY. ANY SEDIMENT THAT IS TRACKED ONTO ROADS MUST BE SWEEPED UP IMMEDIATELY. SEDIMENT SHALL NOT BE REMOVED BY WASHING/FLUSHING WITH WATER AT THE RIGHT OF WAY. PRIOR TO THE START OF CONSTRUCTION, AN ENGINEERING ROUGH INSPECTION MUST BE SCHEDULED TO VERIFY EROSION AND SEDIMENTATION CONTROL IS SETUP PROPERLY. TO REQUEST AN INSPECTION, CALL (954) 828-5191.
2. AT ALL TIMES DURING CONSTRUCTION, ALL STORMWATER MUST REMAIN ONSITE. NO DISCHARGE INTO THE PUBLIC RIGHT OF WAY IS ALLOWED.
3. SIDEWALKS, PAVEMENT, SWALES AND DRAINAGE SYSTEMS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THESE PLANS AND DETAILS.
4. THESE PLANS SHALL BE COORDINATED WITH THE LANDSCAPE PLANS. NO TREE SHALL BE INSTALL SUCH THAT THE BOTTOM SWALE ELEVATIONS AND VOLUMES ARE MINIMIZED.
5. ALL POTABLE WATER SERVICE CONNECTIONS REQUIRE BACKFLOW PREVENTERS.
6. CONTRACTOR SHALL VERIFY THE LOCATION OF THE EXISTING SEWER CLEAN-OUT. ALL NEW CLEAN-OUTS SHALL BE CONSTRUCTED PER CITY STANDARDS, DETAILS AND CRITERIA. THE USE OF EXISTING SEWER LATERALS ARE BEING PROPOSED FOR USE, THEREFORE THE INTEGRITY OF THE LATERAL IS TO BE VERIFIED THAT THERE ARE NO CRACKS OR LEAKS FROM THE MAIN TO CLEAN-OUT AT THE PROPERTY LINE. IF A LEAK OR CRACK IS IDENTIFIED, PROVIDE A NEW CLEAN-OUT AND LATERAL AT THE PROPERTY LINE AND INSERT A CURED-IN-PLACE PIPE INSIDE THE EXISTING SANITARY LATERAL PER ASTM F2561-11.
7. THE FINISH SURFACE OF BASE COURSE AND THAT OF THE WEARING SURFACE SHALL NOT VARY MORE THAN .04 FOOT FROM THE APPROVED GRADING PLAN (TEMPLATE) AND ALL AREA SHALL BE GRADED TO DRAIN. ANY IRRREGULARITIES EXCEEDING THIS LIMIT SHALL BE CORRECTED.
8. THE ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL AN AS-BUILT DRAWING OF THE LIMEROCK BASE HAS BEEN SUBMITTED AND APPROVED BY THE EOR AND THE CITY ENGINEER OR DESIGNEE.
9. ALL APPLICABLE FEDERAL, STATE, COUNTY, DRAINAGE DISTRICT AND CITY PERMITS FOR CONSTRUCTION OF PAVING, GRADING, DRAINAGE, WATER, AND SANITARY SEWER SHALL BE OBTAINED PRIOR TO CONSTRUCTION.
10. ALL VEGETATION, MUCK AND ANY DELETERIOUS MATERIAL WITHIN THE ROW LIMITS OF ALL STREET AND ALLEYS AND REQUIRED OFF-STREET PARKING AREAS MUST BE REMOVED AND REPLACE WITH CLEAN FILL MATERIAL, FREE OF STUMPS, LARGE ROOTS OR OTHER MATTER NOT SUITABLE FOR INCLUSION IN ROADWAY FILL.
11. PAVEMENT MARKINGS SHALL BE FDOT REFLECTIVE PAINT EXCEPT AT ENTRANCE WHERE IT SHALL BE THERMOPLASTIC.

DEMAND TABLE

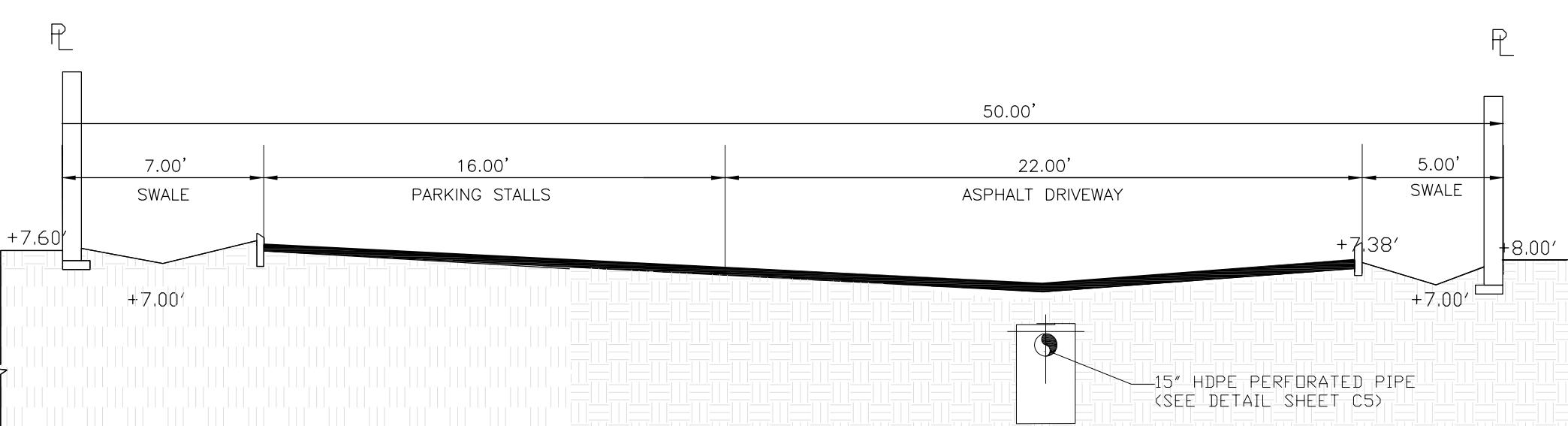
Type Demand	Type Unit	Demand /Unit	Unit	No. of Units	Demand
Prior Water Demand	Single Family Resident	300 gpd	ea.	2	600 gpd
Prior Wastewater Demand	Single Family Resident	300 gpd	ea.	2	600 gpd
Proposed Water Demand	Apartments	250 gpd	ea.	45	11,250 gpd
Proposed Wastewater Demand	Apartments	250 gpd	ea.	45	11,250 gpd

SITE CALCULATIONS:

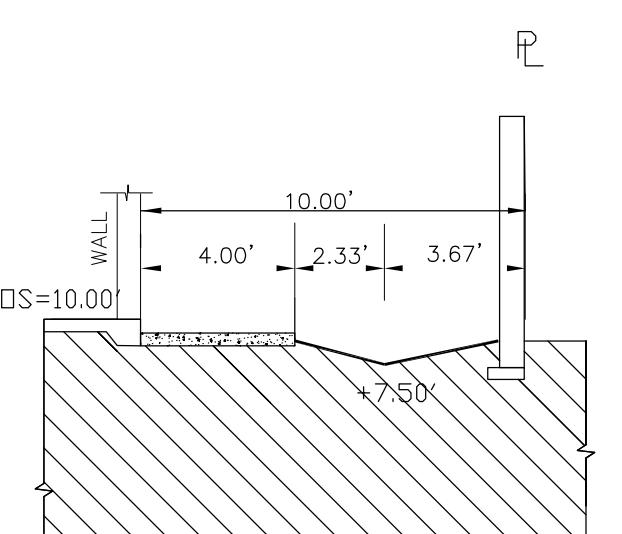
		EXISTING	PROPOSED
BUILDING FOOTPRINT		1,927.91 SQ FT	2,878.64 SQ FT
DRIVEWAY/WALKWAYS/PADS/POOL		4,038.41 SQ FT	10,245.01 SQ FT
LANDSCAPING		13,789.68 SQ FT	6,632.35 SQ FT
TOTAL AREA		19,756.00 SQ FT	19,756.00 SQ FT
TOTAL PERVIOUS AREA		13,789.68 SQ FT	6,632.35 SQ FT
TOTAL IMPERVIOUS AREA		5,966.32 SQ FT	13,123.65 SQ FT
2742		EXISTING	PROPOSED
BUILDING FOOTPRINT		1,387.82 SQ FT	0.00 SQ FT
DRIVEWAY/WALKWAYS/PADS		533.01 SQ FT	6,361.30 SQ FT
LANDSCAPING		8,335.17 SQ FT	3,894.70 SQ FT
TOTAL AREA		10,256.00 SQ FT	10,256.00 SQ FT
TOTAL PERVIOUS AREA		8,335.17 SQ FT	3,894.70 SQ FT
TOTAL IMPERVIOUS AREA		1,920.83 SQ FT	6,361.30 SQ FT



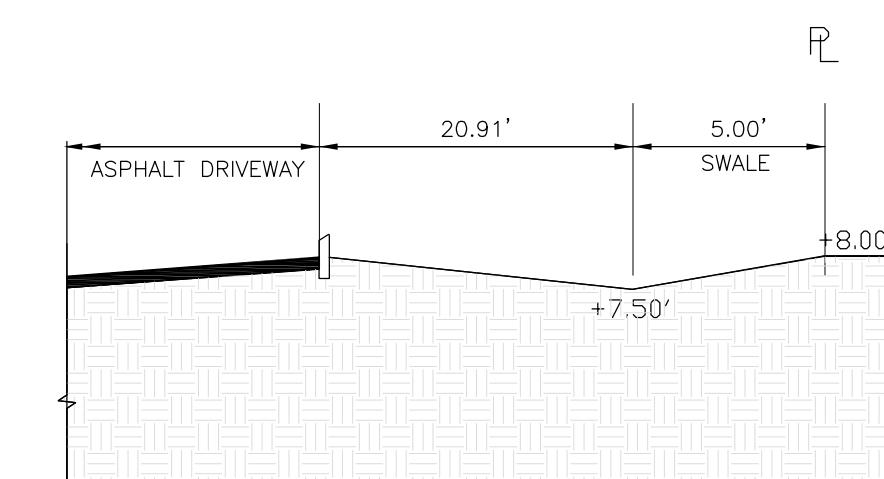
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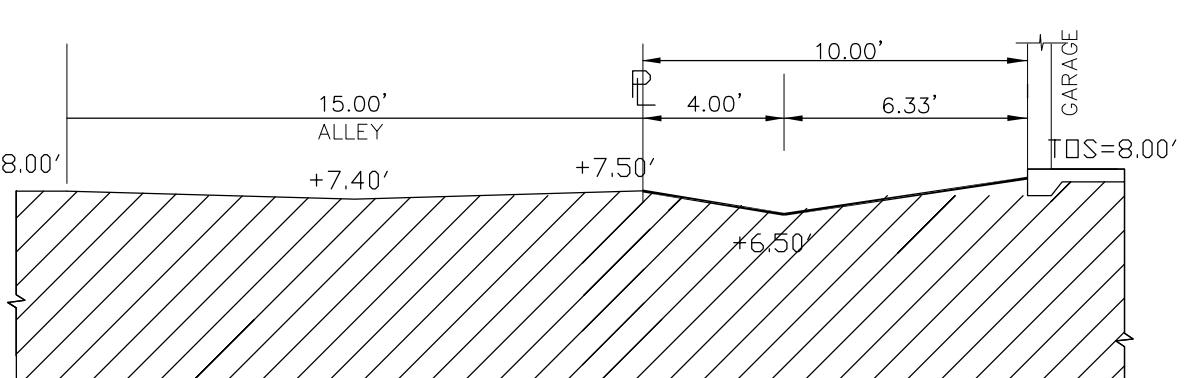
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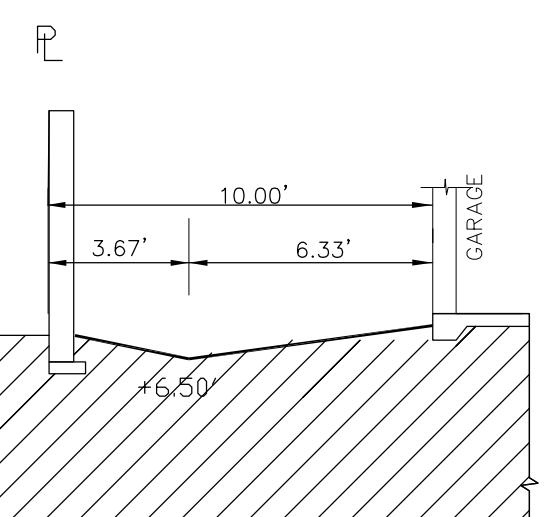
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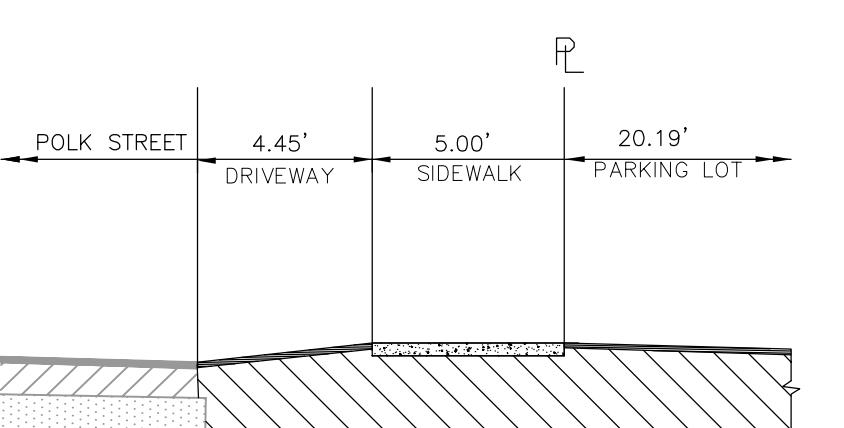
SECTION G-G
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SECTION C-C
NTS



SECTION D-D
NTS



SECTION E-E
NTS

POLK STREET APARTMENTS //
2742 & 2741 POLK STREET
HOLLYWOOD, FL

REVISIONS	DATE	DESCRIPTION

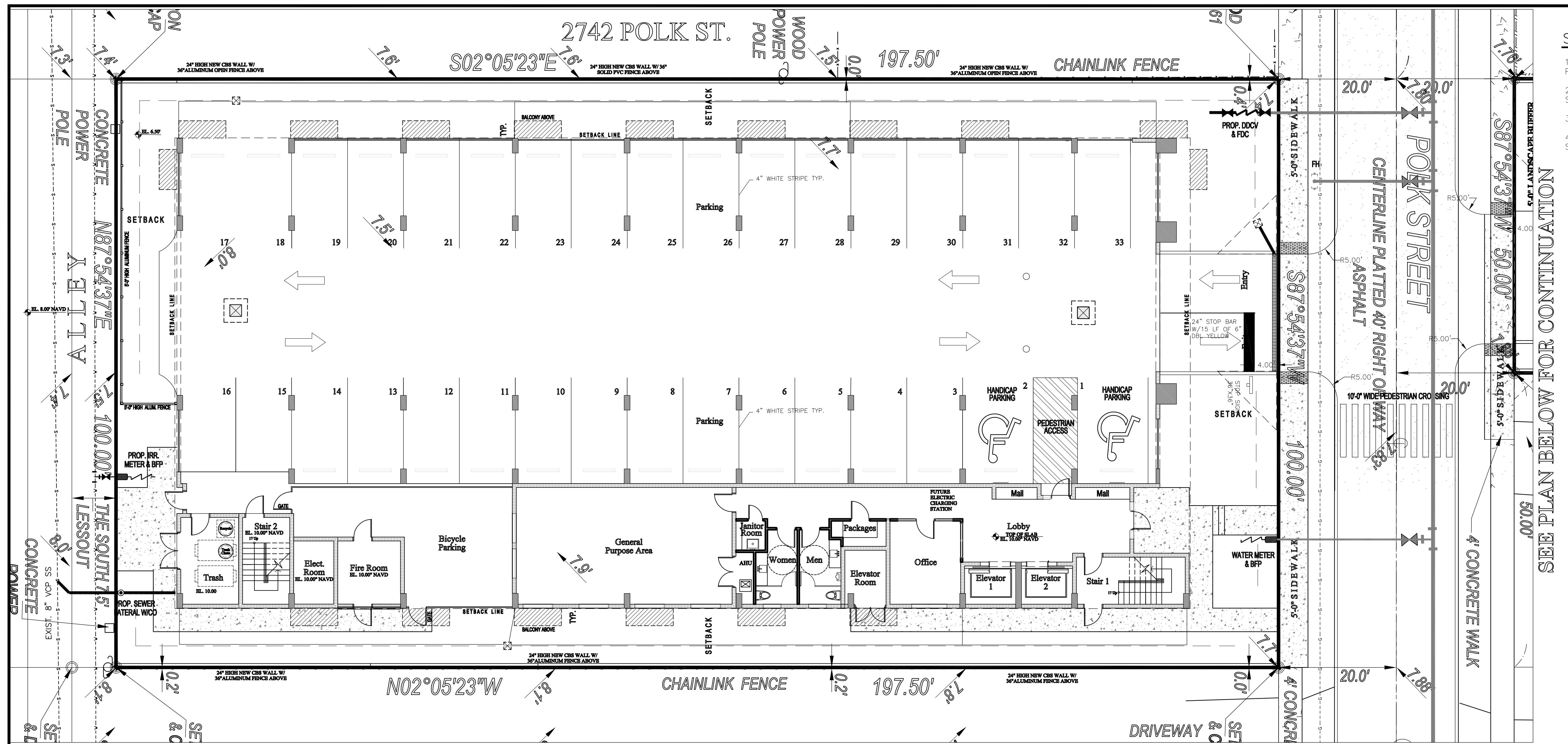
GEA PROJECT NO.: 21046a
DATE: 06-07-2021
SCALE: AS SHOWN
DESIGNED BY: R.B.J.
CHECKED BY: R.B.J.
APPROVED BY: R.B.J.

SHEET TITLE
NOTES & SECTIONS

GATOR ENGINEERING & ASSOCIATES, INC.
11380 TEMPLE STREET
COOPER CITY, FL 33320
TEL: (954) 342-5805 FAX: (954) 424-5804
CERTIFICATE OF AUTHORIZATION NUMBER 30230

SEAL

DATE: REC'D. 06/07/2021
FILE NO.: 38550



SIGNING & PAVEMENT MARKINGS NOTES:

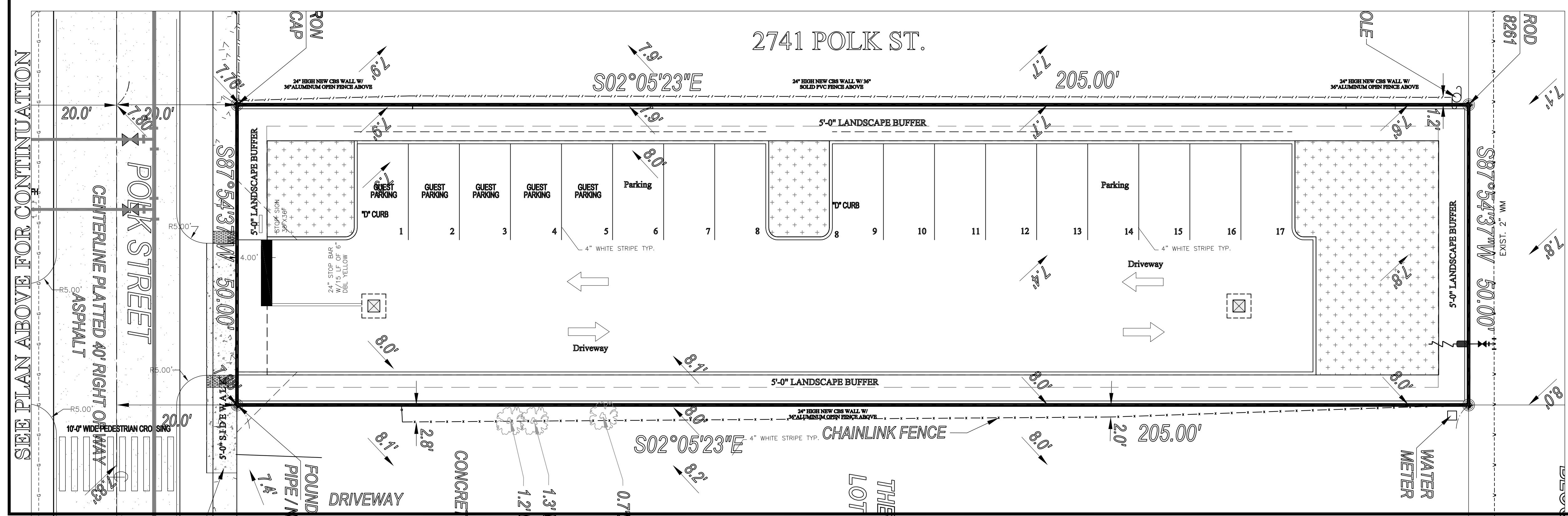
1. INSTALL SIGNS, STRIPPING AND WHEEL STOPS AS INDICATED ON PLANS.
2. ALL STOP BARS SHALL BE THERMOPLASTIC. ALL OTHER STRIPPING SHALL BE FDOT REFLECTIVE PAINT.
3. ALL SIGNS SHALL BE HIGH INTENSITY.
4. SEE DETAILS ON SHEET 6 OF 7 FOR HANDICAP AND REGULAR STALL STRIPING.



TEL: (954) 434-5905 FAX: (954) 434-5904
CERTIFICATE OF AUTHORIZATION NUMBER 302230

CIVIL PLAN

SCALE: 1"=10'



SEE PLAN ABOVE FOR CONTINUATION

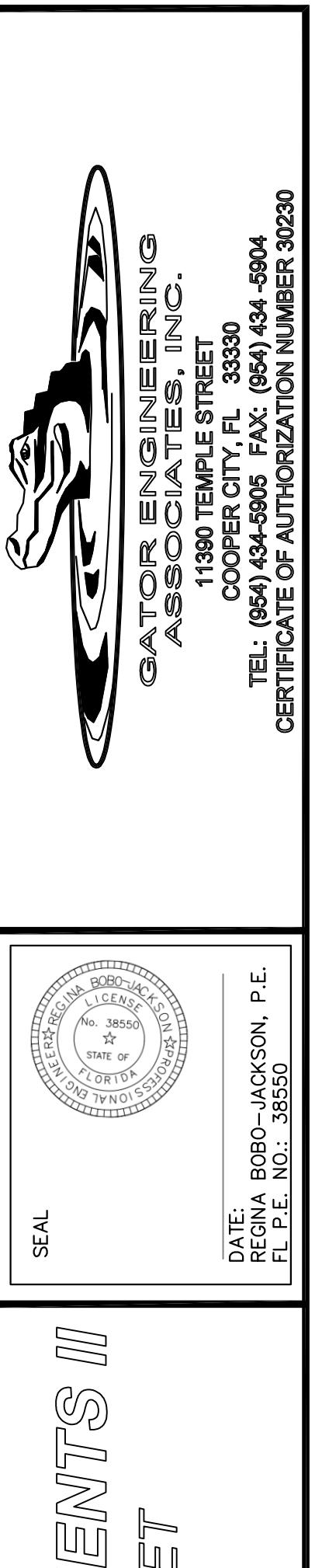
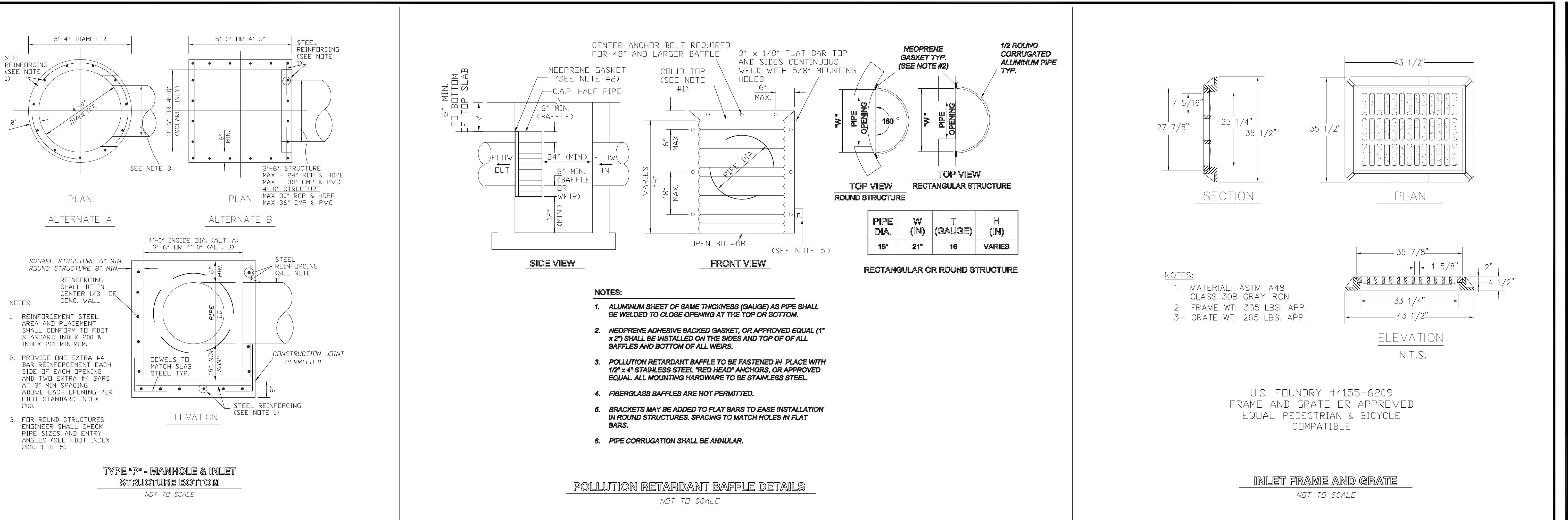
GEA PROJECT NO.: 21046a
DATE: 06-07-2021
SCALE: AS SHOWN
DESIGNED BY: R.B.J.
DRAWN BY: L.B.
CHECKED BY: R.B.J
APPROVED BY: R.B.J

SHEET TITLE

SIGNING & PAVEMENT MARKING PLAN

1 7

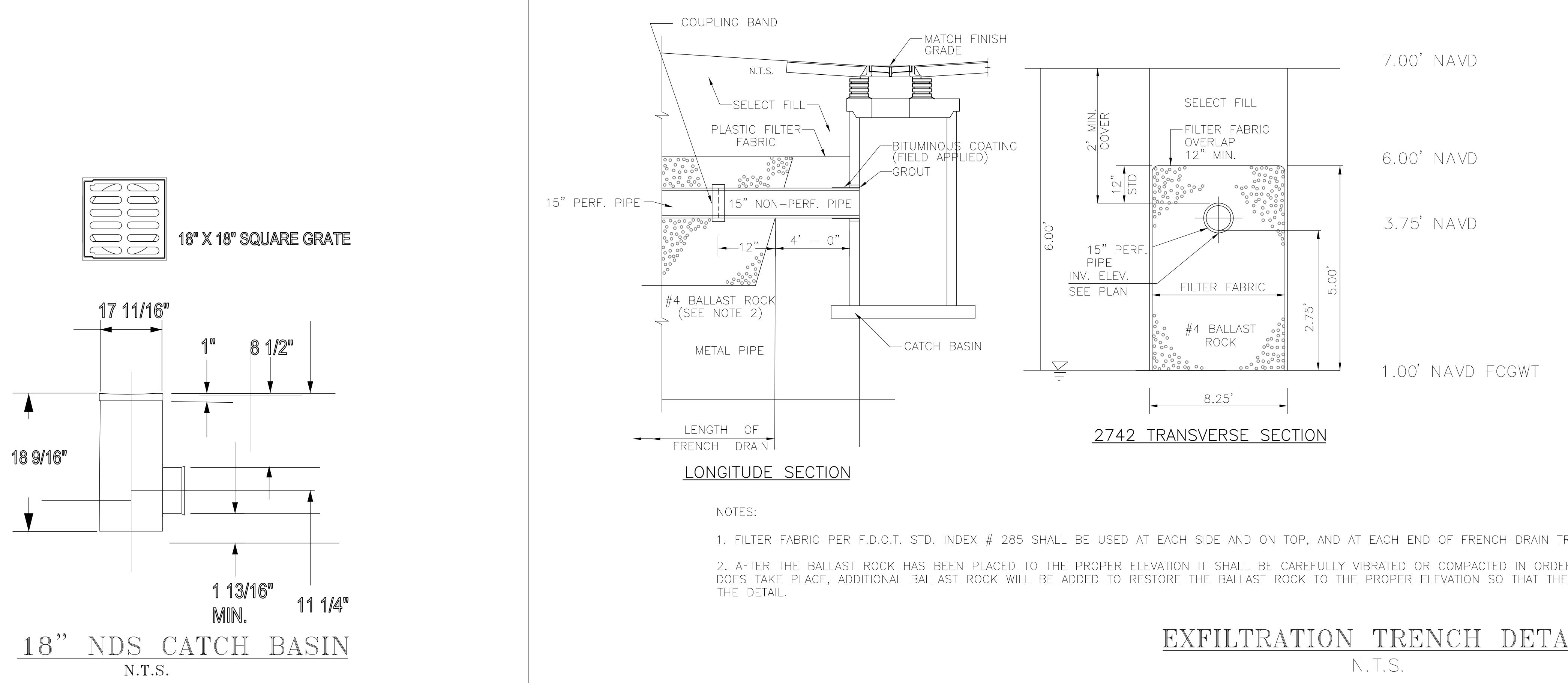
C4 OF 7

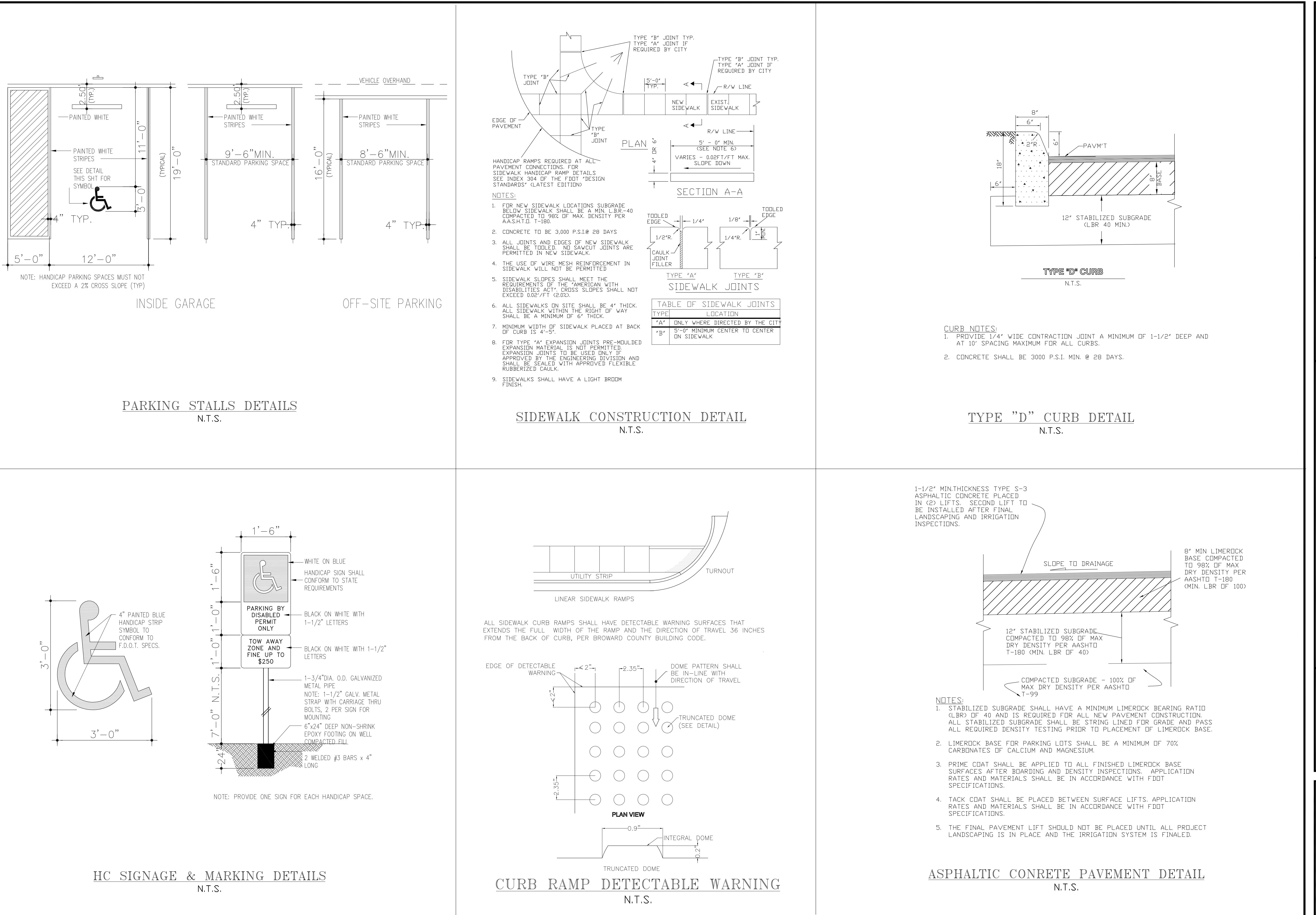


POLK STREET APARTMENTS //
2742 & 2741 POLK STREET
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APPROVED BY: R.B.J.
SHEET TITLE
CIVIL
DETAILS
C5 of 7





GATOR ENGINEERING
ASSOCIATES, INC.
11380 TEMPLE STREET
COOPER CITY, FL 33320
TEL: (954) 342-5805 FAX: (954) 424-5804
CERTIFICATE OF AUTHORIZATION NUMBER 302230

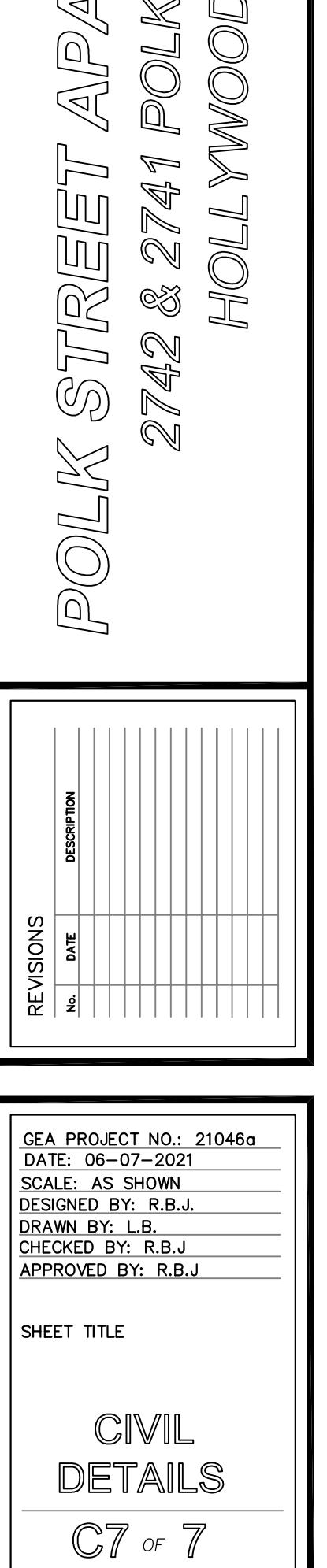
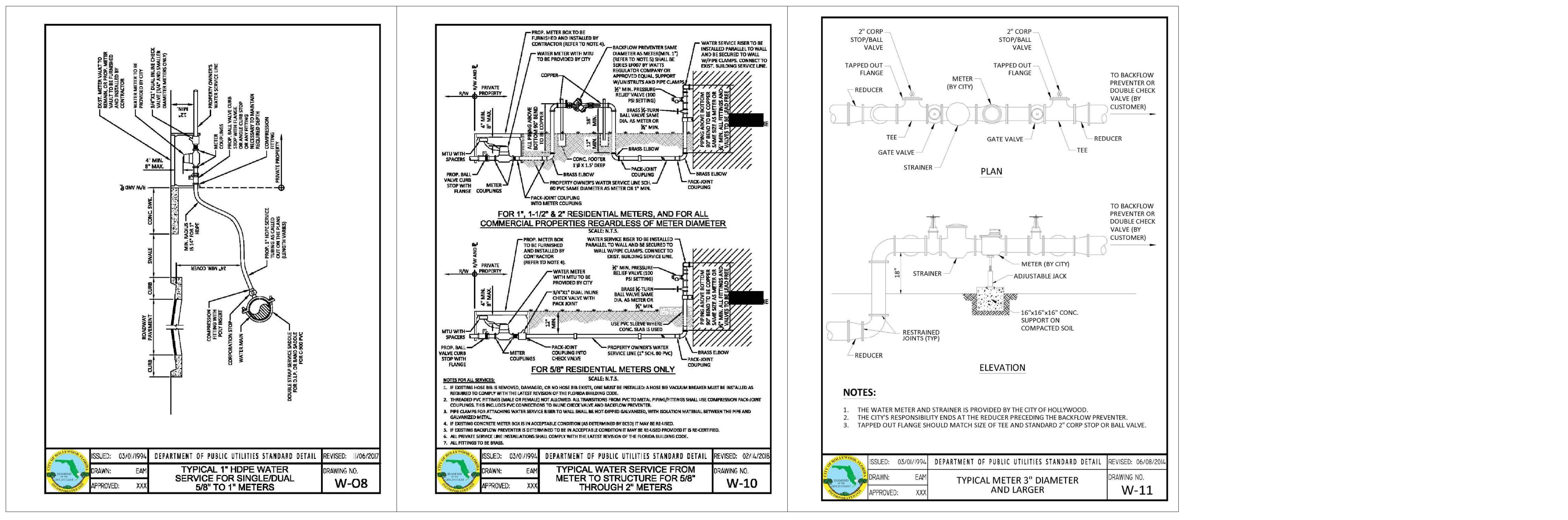
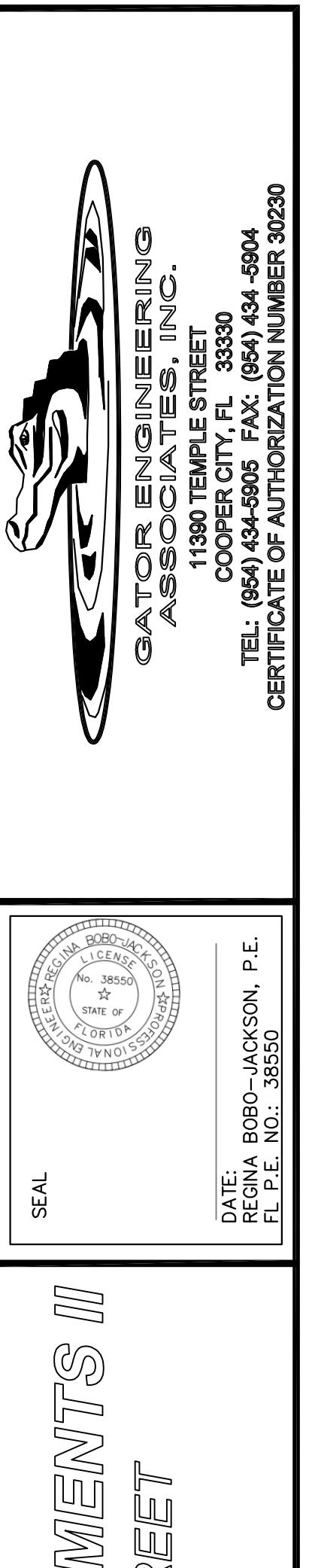
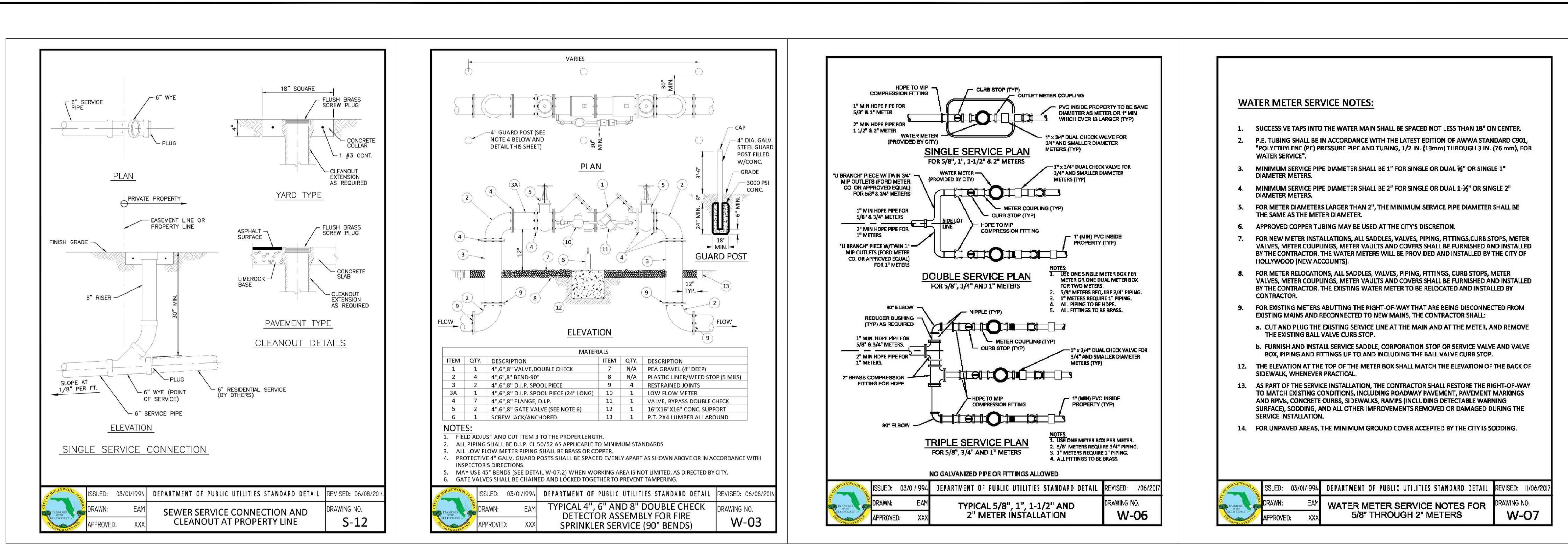
SEAL
DATE: 06-07-2021
RECEIVED BY: R.B.J.
FILE NO.: 38550

POLK STREET APARTMENTS //
2742 & 2741 POLK STREET
HOLLYWOOD, FL

REVISIONS
No. DATE
DESCRIPTION

GEA PROJECT NO.: 21046a
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CHECKED BY: R.B.J.
APPROVED BY: R.B.J.

SHEET TITLE
CIVIL DETAILS
C6 of 7



Polk Street Apartments

Hollywood, Florida

TRAFFIC STUDY

prepared for:
S&B Ent, LLC

KBP CONSULTING, INC.

December 2020
Updated August 2021

Polk Street Apartments

Hollywood, Florida

Traffic Study

December 2020
Updated August 2021

Prepared for:
S&B Ent, LLC

Prepared by:
KBP Consulting, Inc.
8400 N. University Drive, Suite 309
Tamarac, Florida 33321
Phone: (954) 560-7103

Karl B. Peterson, P.E.
Florida Registration Number 49897
KBP Consulting, Inc.
8400 N. University Drive, Suite 309
Tamarac, Florida 33321
CA # 29939

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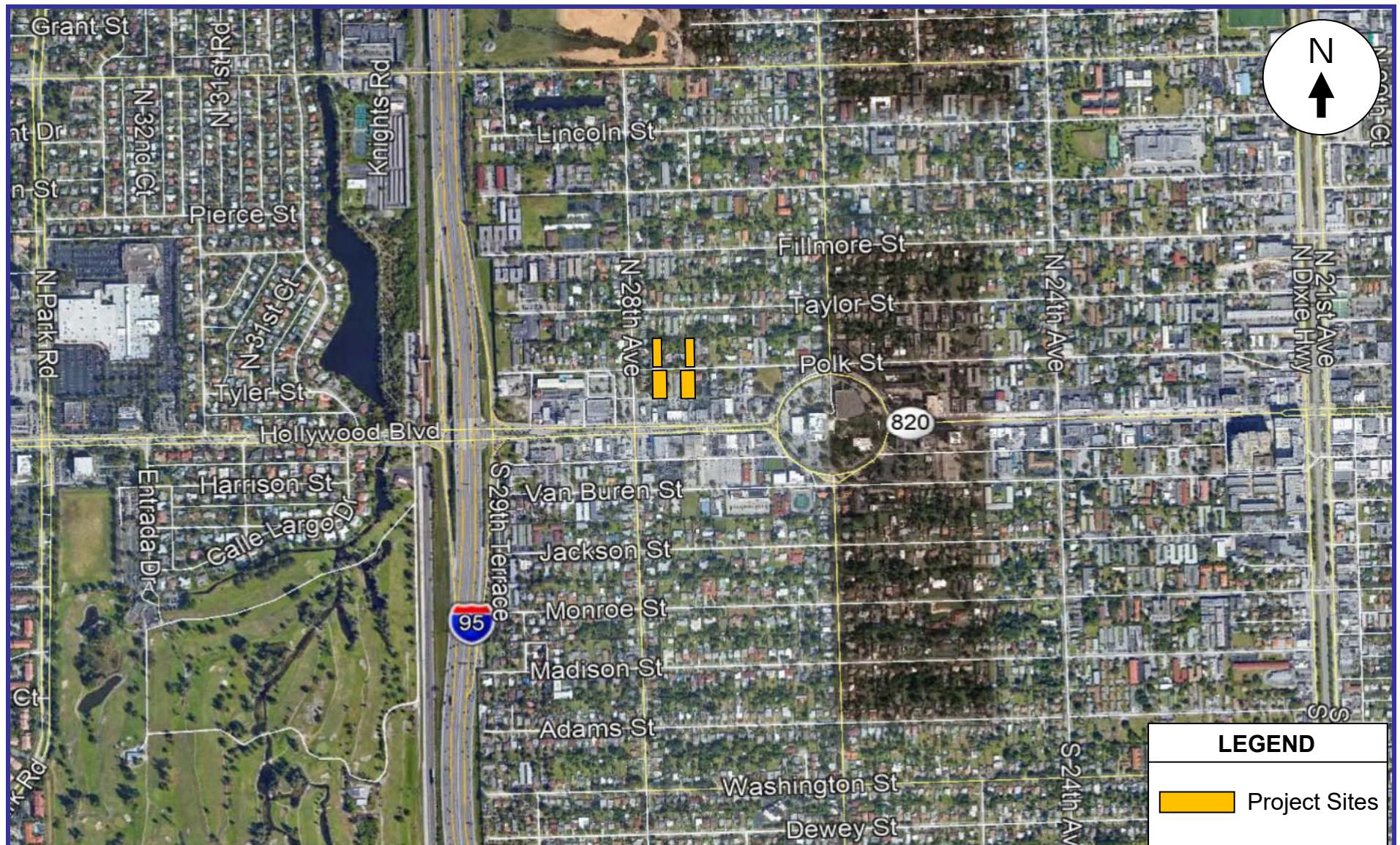
INTRODUCTION

A residential development to be known as Polk Street Apartments is proposed on several parcels of land along Polk Street between N. 26th Avenue and N. 28th Avenue in Hollywood, Broward County, Florida. The locations of the project sites are illustrated graphically in Figure 1 on the following page.

KBP Consulting, Inc. has been retained by S&B Ent, LLC to prepare a traffic study in connection with this proposed redevelopment project. This study addresses the trip generation characteristics associated with the proposed residential development, the projected peak period turning movement volumes at the project access driveways on Polk Street, traffic impacts at the nearby intersections, and the likely traffic circulation patterns to and from the site.

This traffic study is divided into nine (9) sections, as listed below:

1. Inventory
2. Existing Conditions
3. Traffic Counts
4. Trip Generation
5. Trip Distribution and Traffic Assignment
6. Traffic Circulation Patterns
7. Traffic Impact Analyses
8. Supplemental Analyses
9. Summary & Conclusions



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Project Location Map

FIGURE 1
Polk Street Apartments
Hollywood, Florida

INVENTORY

Existing Land Uses and Access

The subject project is comprised of two (2) separate sites and four (4) separate parcels of land with a total land area of approximately +/- 1.38 acres. The eastern site (2718-2720 and 2723 Polk Street) has a land area of 0.69 acre and consists of three (3) low-rise multifamily residential dwelling units. The Folio ID Numbers for this portion of the development are as follows:

- 5142 16 02 3370
- 5142 16 02 3590

The western site (2741-2742 Polk Street) also has a land area of 0.69 acre and consists of two (2) single family residential dwelling units. The Folio ID Numbers for this portion of the development are as follows:

- 5142 16 02 3350
- 5142 16 02 3630

Vehicular access to these parcels is provided along Polk Street which is a two-lane local roadway with no on-street parking.

Proposed Land Uses and Access

Multifamily residential developments are proposed on both the eastern and the western sites. Both sites will be developed with 44 dwelling units in a four-story building (mid-rise format) to be located on the south side of Polk Street. There will be 33 parking spaces on the ground floor and an additional 16 parking spaces in a surface parking lot on the north side of Polk Street. Vehicular access to these sites will be provided by full access driveways on Polk Street. The parcels on the south side of Polk Street will also have emergency vehicle access only to the alleyway to the south. Appendix A contains the preliminary site plans for the project.

EXISTING CONDITIONS

This section of the report addresses the transportation system located in the immediate vicinity of the subject site.

Roadway System

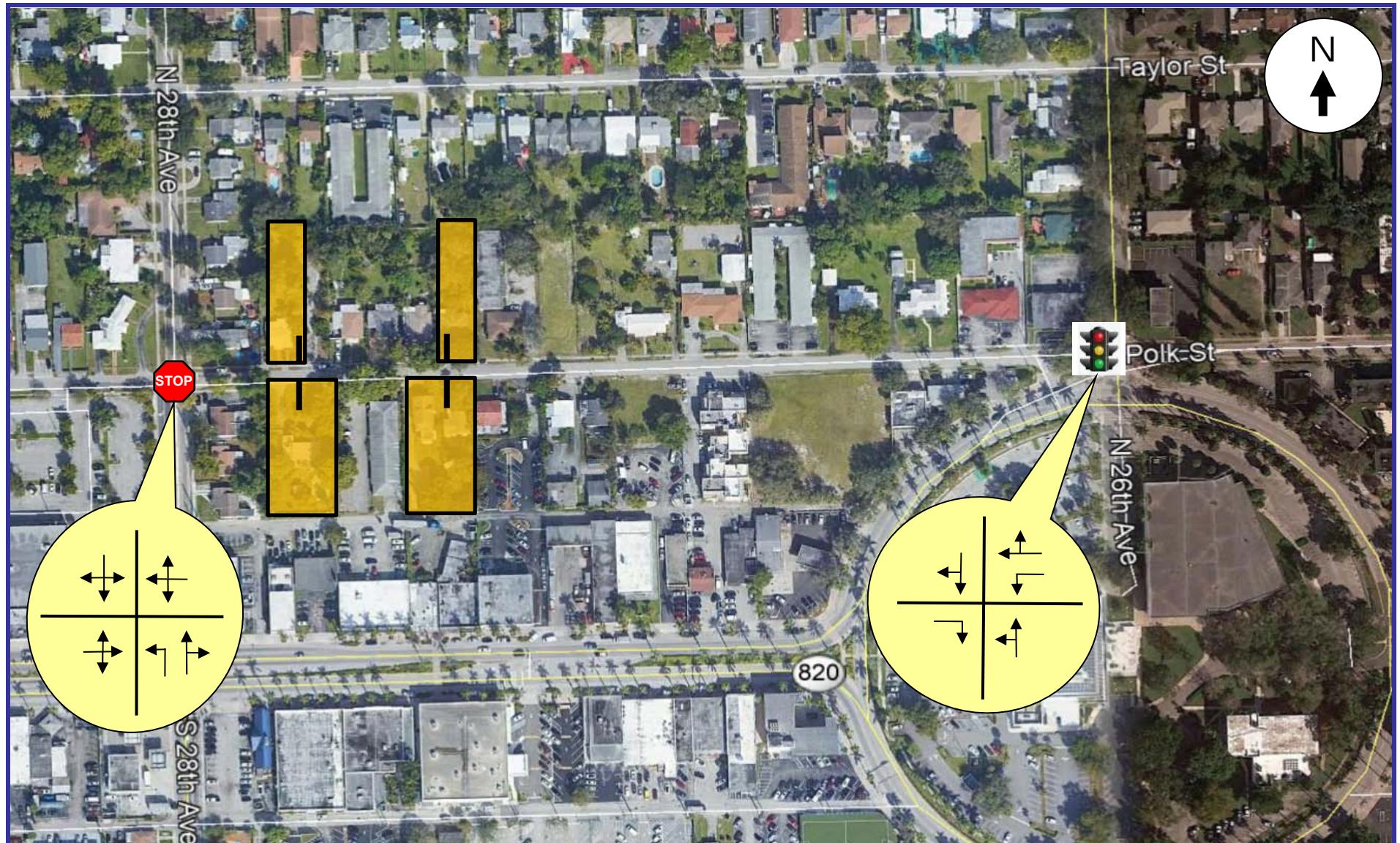
Within the limits of the project study area, Polk Street is a two-lane local roadway oriented in the east-west direction. This roadway connects N. 26th Avenue to the east and N. 28th Avenue to the west. Hollywood Boulevard (State Road 820) is located one block to the south of Polk Street. This roadway is a major east-west state-maintained arterial roadway. This facility provides regional access and has interchanges with Interstate 95 and the Florida's Turnpike. The immediately surrounding roadway network is characterized by an extensive grid system of two-lane / two-way local roadways. The project's utilization of this grid network is elaborated upon in subsequent sections of this report.

Study Intersections

Two (2) intersections were identified as the locations to be evaluated as part of this traffic impact analysis. These intersections are:

- Polk Street and N. 26th Avenue
- Polk Street and N. 28th Avenue

Figure 2 on the following page depicts the existing lane geometry of these two (2) intersections identified for analysis purposes.



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Existing Lane Geometry

FIGURE 2
Polk Street Apartments
Hollywood, Florida

TRAFFIC COUNTS

KBP Consulting, Inc., in association with Traffic Survey Specialists, Inc., collected traffic data at the following intersections:

- Polk Street and N. 26th Avenue
- Polk Street and N. 28th Avenue

The intersection turning movement counts were collected on Tuesday, July 13, 2021, during the AM peak period (7:00 AM to 9:00 AM) and the PM peak period (4:00 PM to 6:00 PM). Appendix B contains the traffic data as collected in the field.

These counts have been adjusted by a peak season conversion factor of 1.06 to reflect average peak season conditions. (Please see Appendix C for the 2019 peak season factor category report published by the Florida Department of Transportation (FDOT) for Broward County. A review of the 2020 report reveals inconsistencies likely attributed to the COVID-19 conditions.) The resulting average peak season adjusted turning movement counts are presented in Figures 3 and 4 on the following pages.



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Existing AM Peak Hour Traffic Counts

Source: Traffic Survey Specialists, Inc. (7/13/21)
Adjusted for Average Peak Season Conditions

FIGURE 3
Polk Street Apartments
Hollywood, Florida



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Existing PM Peak Hour Traffic Counts

Source: Traffic Survey Specialists, Inc. (7/13/21)
Adjusted for Average Peak Season Conditions

FIGURE 4
Polk Street Apartments
Hollywood, Florida

TRIP GENERATION

A trip generation analysis has been conducted for the proposed Polk Street Apartments project. The analysis was performed using the trip generation rates and equations published in the Institute of Transportation Engineer's (ITE) *Trip Generation Manual (10th Edition)*. The trip generation analysis was undertaken for daily, AM peak hour and PM peak hour conditions. According to the ITE report, the most appropriate "land use" categories and corresponding trip generation rates and equations for the existing and proposed development are as follows:

SINGLE-FAMILY DETACHED HOUSING – ITE LAND USE #210

- Weekday: $T = 9.44 (X)$
where T = number of trips and X = number of dwelling units
- AM Peak Hour: $T = 0.74 (X)$ (25% in / 75% out)
- PM Peak Hour: $T = 0.99 (X)$ (63% in / 37% out)

MULTIFAMILY HOUSING (LOW-RISE) – ITE LAND USE #220

- Weekday: $T = 7.32 (X)$
where T = number of trips and X = number of dwelling units
- AM Peak Hour: $T = 0.46 (X)$ (23% in / 77% out)
- PM Peak Hour: $T = 0.56 (X)$ (63% in / 37% out)

MULTIFAMILY HOUSING (MID-RISE) – ITE LAND USE #221

- Weekday: $T = 5.45 (X) - 1.75$
where T = number of trips and X = number of dwelling units
- AM Peak Hour: $\ln(T) = 0.98 \ln(X) - 0.98$ (26% in / 74% out)
- PM Peak Hour: $\ln(T) = 0.96 \ln(X) - 0.63$ (61% in / 39% out)

Utilizing the above-listed trip generation rates and equations from the referenced ITE manual, a trip generation analysis was undertaken for the existing and proposed development. The results of this effort are documented in Table 1 on the following page and applicable excerpts from the referenced ITE manual are presented in Appendix D.

Table 1
Trip Generation Summary
Polk Street Apartments - Hollywood, Florida

Land Use	Size	Daily Trips	AM Peak Hour Trips			PM Peak Hour Trips		
			In	Out	Total	In	Out	Total
<i>Existing</i>								
Multifamily Housing (Low-Rise)	3 DU	22	0	1	1	1	1	2
Single Family Detached Housing	2 DU	19	0	1	1	1	1	2
	Total	41	0	2	2	2	2	4
<i>Proposed</i>								
Multifamily Housing (Mid-Rise) - East	44 DU	238	4	11	15	12	8	20
Multifamily Housing (Mid-Rise) - West	44 DU	238	4	11	15	12	8	20
	Total	476	8	22	30	24	16	40
	Difference (Proposed - Existing)	435	8	20	28	22	14	36

Compiled by: KBP Consulting, Inc. (July 2021).

Source: Institute of Transportation Engineers (ITE) Trip Generation Manual (10th Edition).

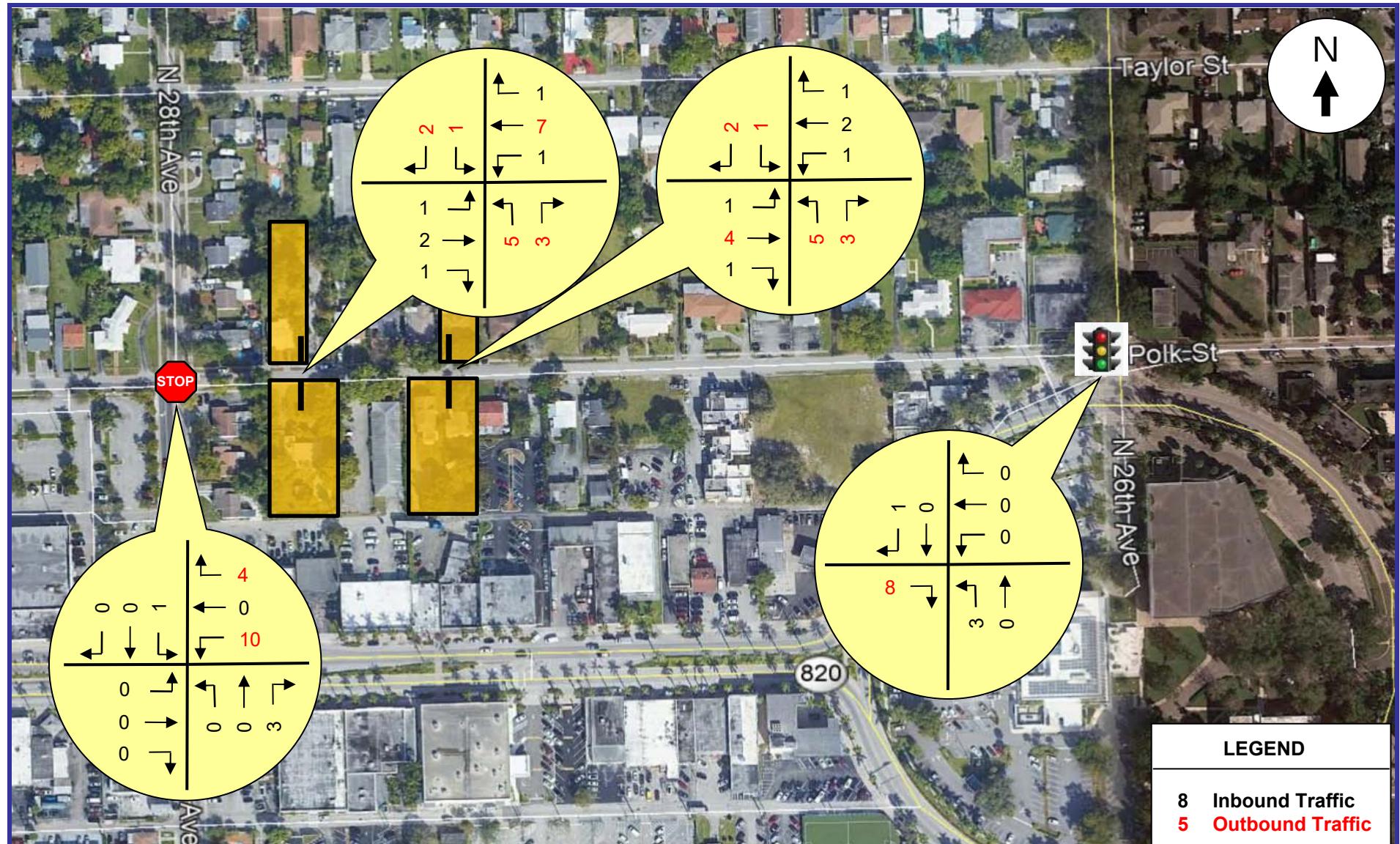
As indicated in Table 1 above, the proposed residential development is anticipated to generate approximately 476 daily vehicle trips, 30 AM peak hour vehicle trips (8 inbound and 22 outbound) and approximately 40 vehicle trips (24 inbound and 16 outbound) during the typical afternoon peak hour. When considering the existing single and multifamily residences on the subject sites this represents an increase of 435 daily vehicle trips, an increase of 28 AM peak hour vehicle trips, and an increase of 36 PM peak hour vehicle trips.

TRIP DISTRIBUTION AND TRAFFIC ASSIGNMENT

The trip distribution and traffic assignment for the proposed Polk Street Apartments residential development was developed based upon knowledge of the study area, examination of the surrounding roadway network characteristics, review of current traffic volumes, and existing land use patterns. The AM and PM peak hour traffic generated by the project was assigned to the project driveways and the intersections at Polk Street and N. 26th Avenue and at Polk Street and N. 28th Avenue. This information is summarized in Figures 5 and 6 on the following pages.

The general directional distributions are as follows:

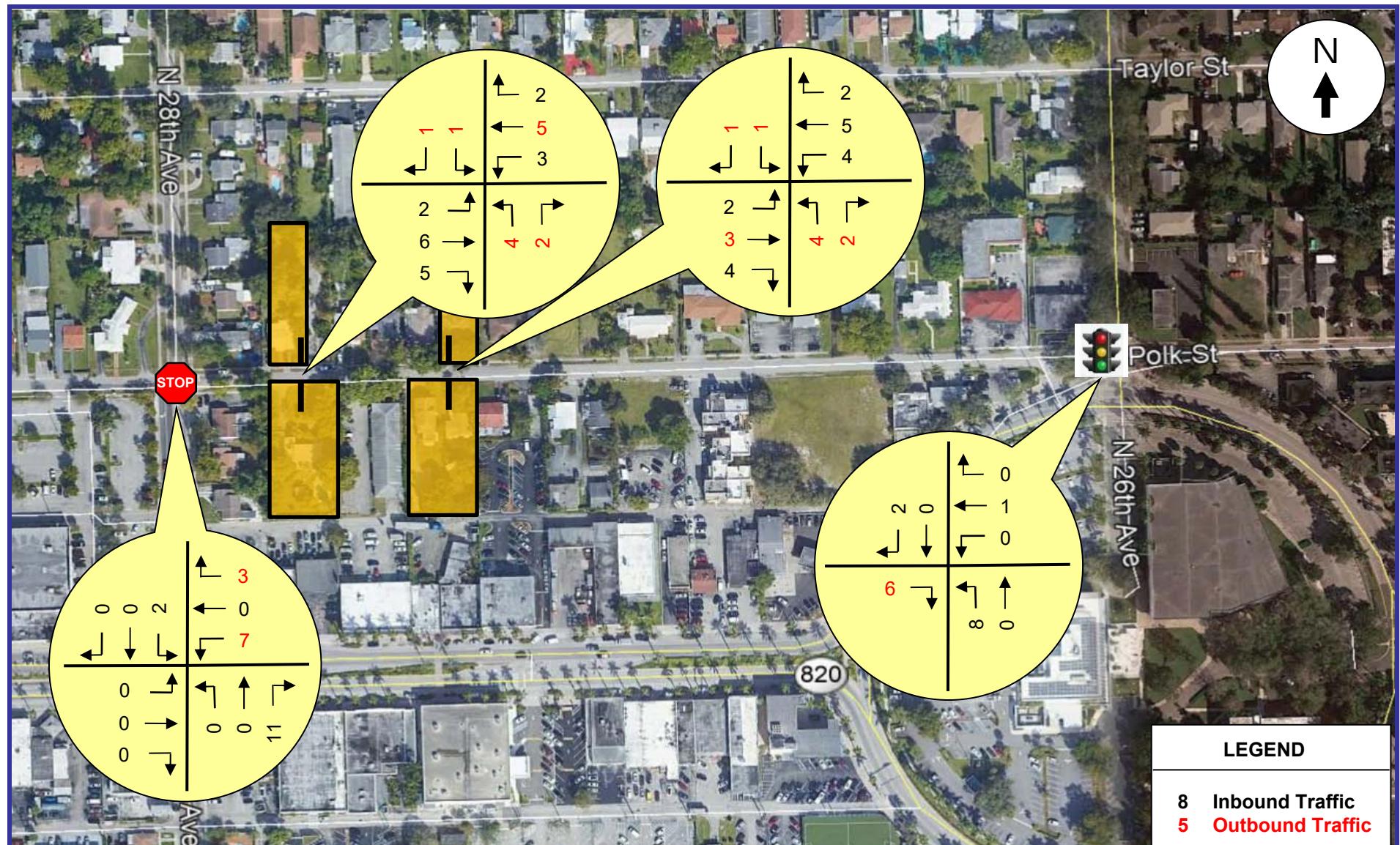
- 40% to and from the west via Hollywood Boulevard
- 30% to and from the east via Hollywood Boulevard
- 10% to and from the north via N. 26th Avenue
- 10% to and from the south via S. 26th Avenue
- 5% to and from the north via N. 28th Avenue
- 5% to and from the south via S. 28th Avenue



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Project Traffic Assignment AM Peak Hour

FIGURE 5
Polk Street Apartments
Hollywood, Florida



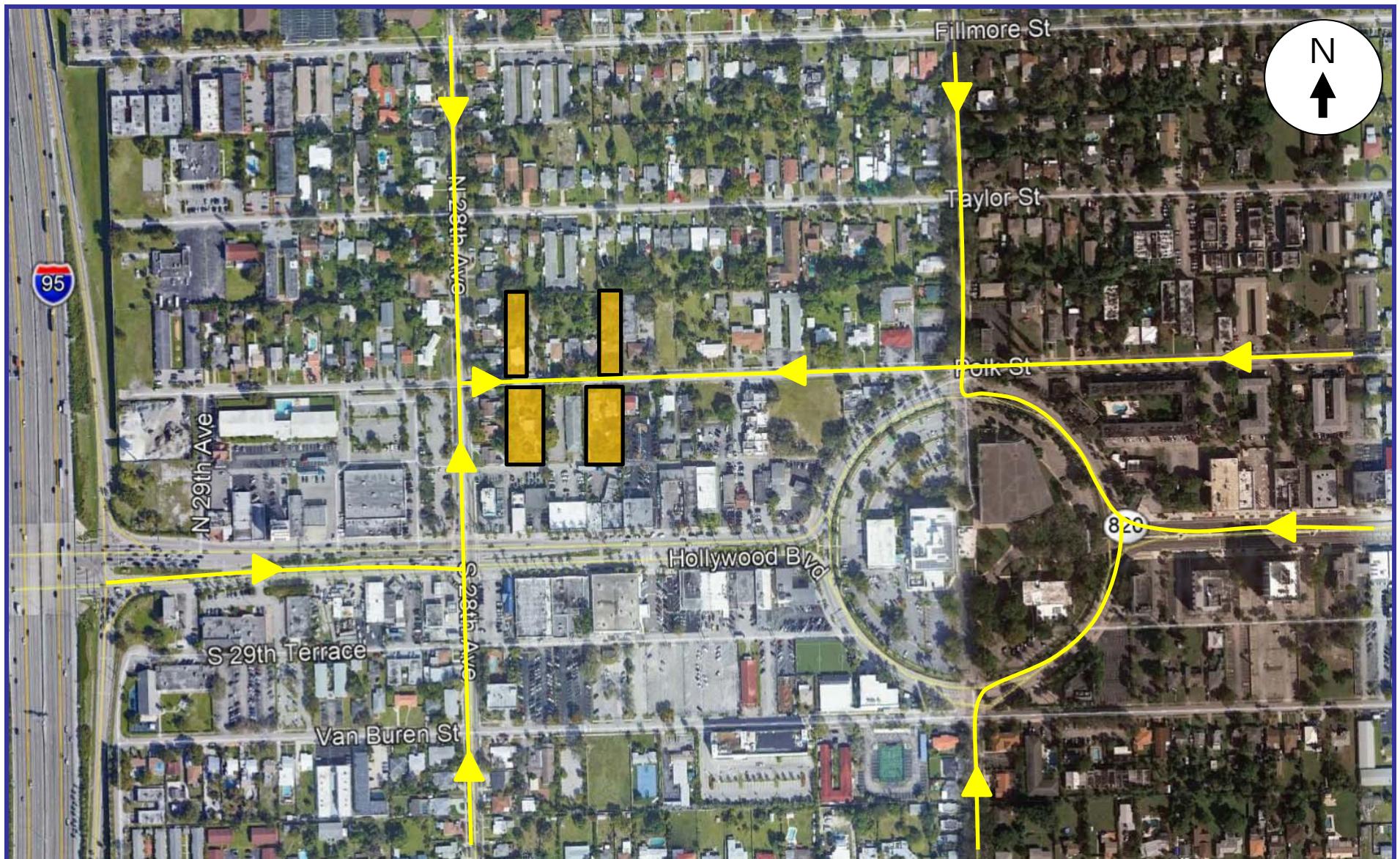
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Project Traffic Assignment PM Peak Hour

FIGURE 6
Polk Street Apartments
Hollywood, Florida

TRAFFIC CIRCULATION PATTERNS

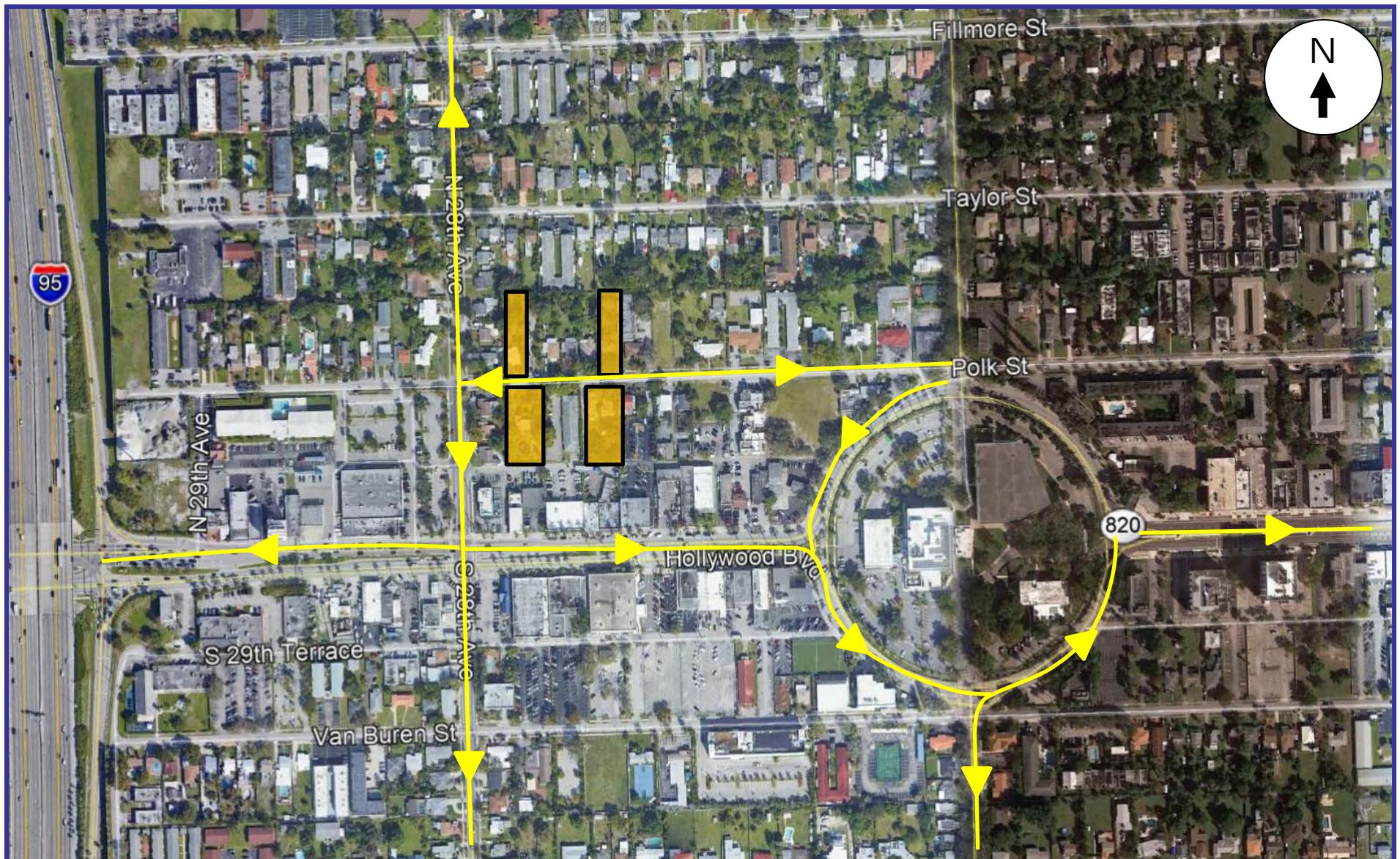
Since the intersection of Polk Street, N. 26th Avenue and Hollywood Boulevard has several turning movement restrictions, a more detailed evaluation and development of the likely traffic circulation patterns has been performed. Fortunately, the entire study area is characterized by an extensive grid roadway network that accommodates movements to and from all directions. The most likely inbound and outbound travel patterns are depicted graphically in Figures 7 and 8 on the following pages.



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Future Inbound Travel Patterns

FIGURE 7
Polk Street Apartments
Hollywood, Florida



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Future Outbound Travel Patterns

FIGURE 8
Polk Street Apartments
Hollywood, Florida

TRAFFIC IMPACT ANALYSES

This section of the study is divided into two (2) primary parts. The first part of this section involves the development of the future build-out year (2023) traffic volumes for the study area. The second part of this section includes level-of-service analyses for existing and future conditions.

Future Conditions Traffic Volumes

Future, build-out year (2023) traffic volumes were developed for the project study area in the following manner:

- **Average Peak Season Conversion Factor:** As referenced previously in this report, traffic data collected on Tuesday, July 13, 2021, was reviewed with respect to average peak season conditions. Based on FDOT's Peak Season Factor Category report (see Appendix C), the adjustment factor for data collected during this time period is 1.06.
- **Historic Traffic Growth:** Research relative to the background traffic growth in the area was conducted. Historic traffic count data (i.e. the past 5 years between 2014 and 2019) was obtained from the FDOT and is presented in Appendix E of this report. Traffic growth on Hollywood Boulevard east of I-95 was considered for this analysis. The referenced data indicates that the subject corridor has exhibited moderate growth of traffic volumes between 2014 and 2019. For the purposes of this analysis, a 3.0% annual growth rate has been applied.

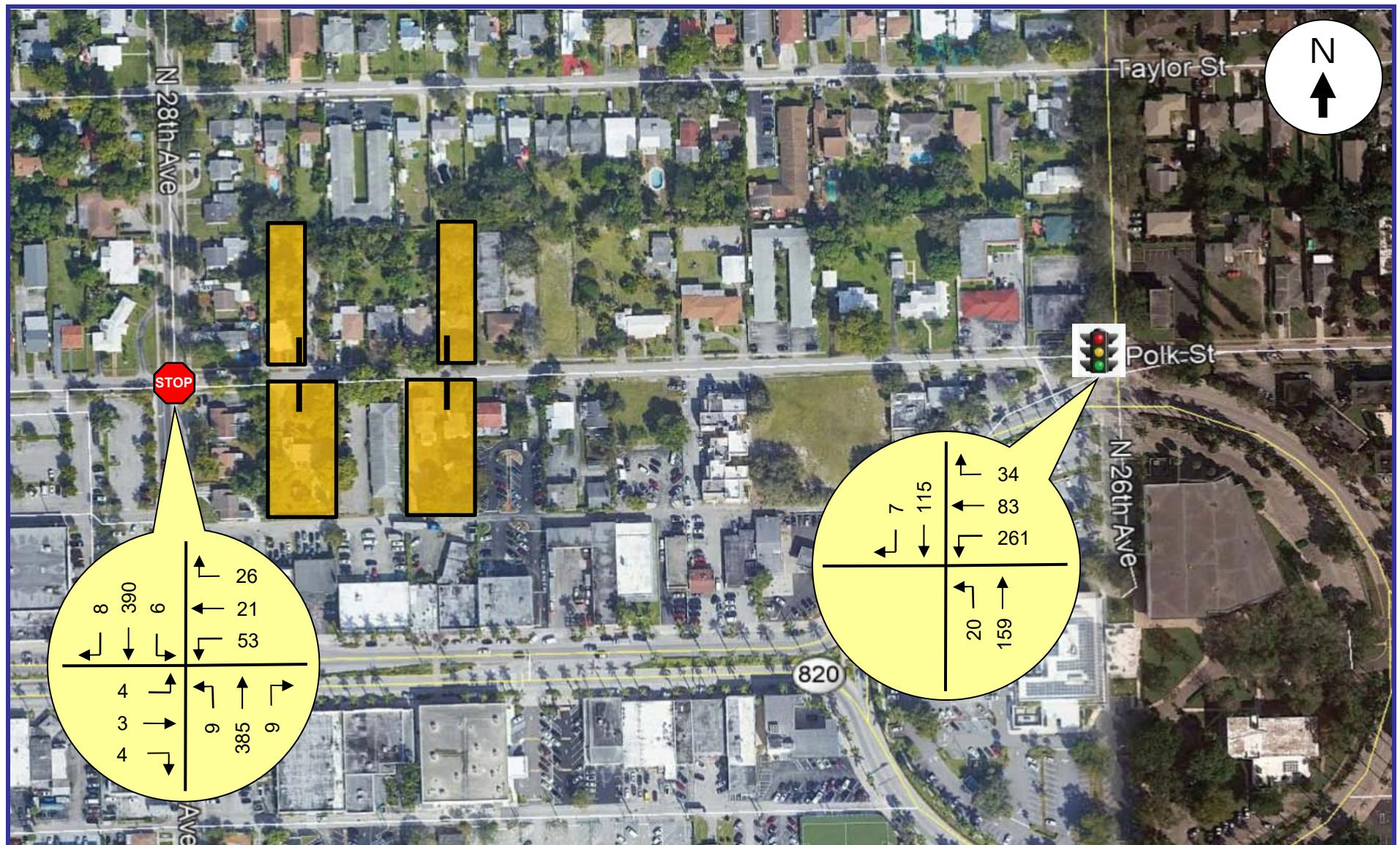
The future traffic calculations (peak season adjustments, background traffic growth, and the traffic associated with the Polk Street Apartments project) for the study intersections are contained in Appendix F in tabular format. Figures 9 and 10 include future background traffic only (without the additional Polk Street Apartments traffic) and Figures 11 and 12 include the additional traffic anticipated to be generated by the proposed development.



KBP CONSULTING, INC.

Future (2023) Background (w/out Project) AM Peak Hour Traffic Volumes

FIGURE 9
Polk Street Apartments
Hollywood, Florida



KBP CONSULTING, INC.

Future (2023) Background (w/out Project) PM Peak Hour Traffic Volumes

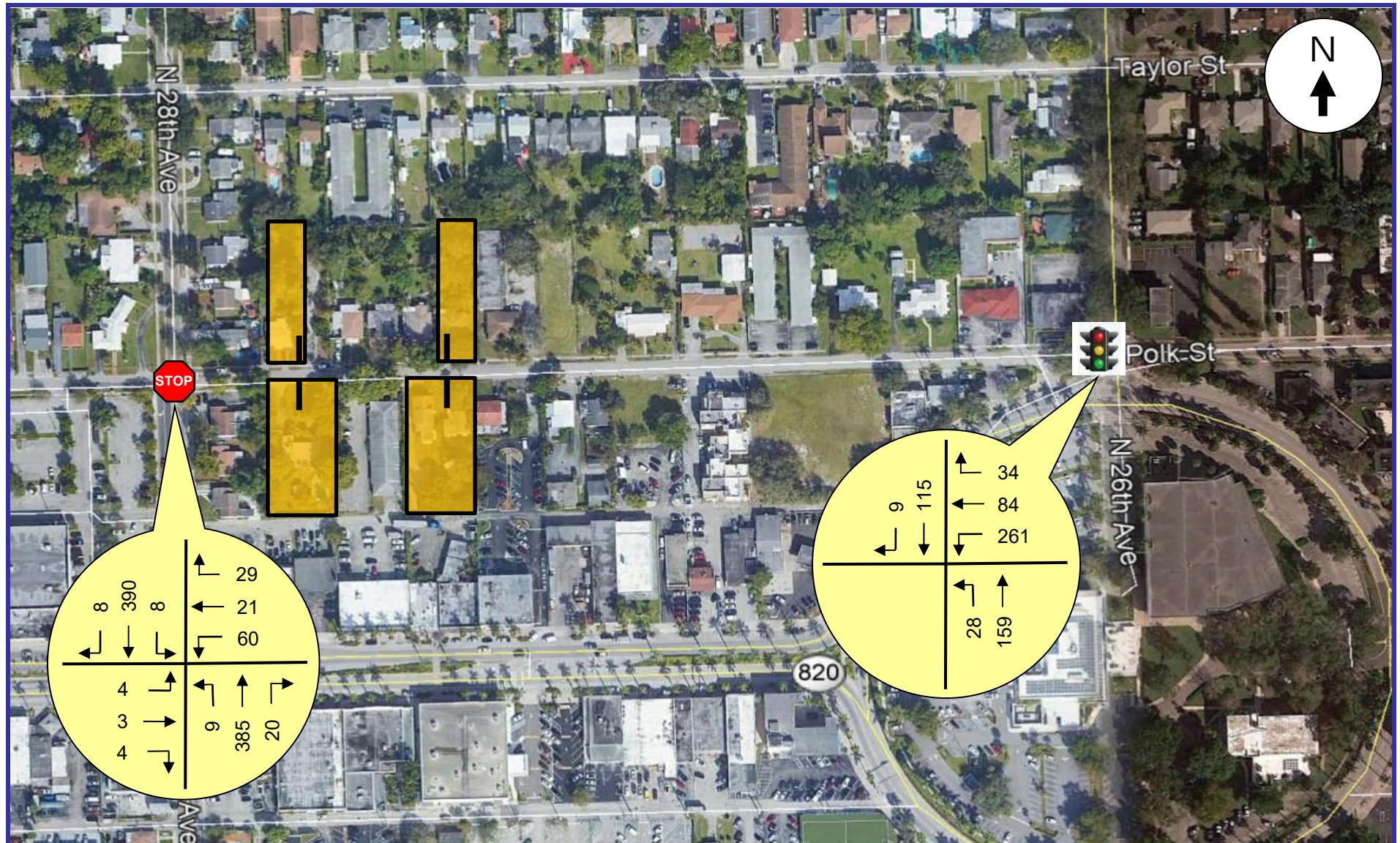
FIGURE 10
Polk Street Apartments
Hollywood, Florida



KBP
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Future (2023) Total (w/ Project) AM Peak Hour Traffic Volumes

FIGURE 11
Polk Street Apartments
Hollywood, Florida



KBP
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Future (2023) Total (w/ Project) PM Peak Hour Traffic Volumes

FIGURE 12
Polk Street Apartments
Hollywood, Florida

Level of Service (LOS) Analyses – Intersections

Intersection capacity / level of service (LOS) analyses were conducted for both of the study intersections. These analyses were undertaken following the capacity / level of service procedures outlined in the 2010 Highway Capacity Manual (HCM) using the latest Synchro software. The results of these capacity analyses are summarized in Table 2 below.

Table 2 Intersection Levels of Service Polk Street Apartments - Hollywood, Florida						
Intersection / Movement	Existing (2021) Conditions		Future (2023) Conditions Without Project Traffic		Future (2023) Conditions With Project Traffic	
	AM Peak Hour	PM Peak Hour	AM Peak Hour	PM Peak Hour	AM Peak Hour	PM Peak Hour
Signalized Intersection						
Polk Street & N. 26th Avenue	B (14.0)	B (13.2)	B (14.3)	B (13.4)	B (14.4)	B (13.6)
- Northbound Approach	C (23.5)	C (24.9)	C (23.4)	C (25.0)	C (23.5)	C (25.3)
- Southbound Approach	C (27.6)	C (22.2)	C (28.0)	C (21.9)	C (28.1)	C (21.5)
- Westbound Approach	A (3.8)	A (4.8)	A (4.0)	A (5.0)	A (4.0)	A (5.3)
Unsignalized Intersection						
Polk Street & N. 28th Avenue	A (8.2)	A (8.1)	A (8.3)	A (8.2)	A (8.3)	A (8.2)
- Northbound Left-Turn	A (7.9)	A (8.1)	A (7.9)	A (8.2)	A (7.9)	A (8.2)
- Southbound Left-Turn	C (16.0)	C (15.7)	C (17.1)	C (16.6)	C (17.3)	C (16.9)
- Eastbound Approach	C (15.6)	C (19.9)	C (16.5)	C (21.9)	C (17.2)	C (23.3)

Source: Highway Capacity Manual and SYNCHRO.

Legend: D (37.7) = LOS (Average Delay - Seconds / Vehicle)

Signalized Intersection

- **Polk Street and N. 26th Avenue** – This intersection currently operates at LOS “B” in the AM and PM peak hours. In the buildout year of 2023 both with and without the project traffic, these LOS conditions remain unchanged. In other words, the LOS does not degrade as a result of this project. Furthermore, it is noted that all approaches to this intersection operate at an acceptable LOS.

Unsignalized Intersection

- **Polk Street and N. 28th Avenue** – The northbound and southbound left-turn movements currently operate at LOS “A” in the AM and PM peak hours. And the stop-controlled approaches (eastbound and westbound) operate at LOS “C” in the AM and PM peak hours. In the buildout year of 2023 both with and without the project traffic, these LOS conditions remain unchanged.

The signal timing data for the Polk Street and N. 26th Avenue intersection has been obtained from Broward County Traffic Engineering and is presented in Appendix G. And the Synchro analysis printouts of the study intersections are contained in Appendix H.

SUPPLEMENTAL ANALYSES

Based upon conversations with City staff, several additional traffic operating parameters within the Polk Street corridor between N. 26th Avenue and N. 28th Avenue were observed and documented. These parameters are presented below.

Speed Data

The City of Hollywood has conducted a review and analysis of the traffic speeds and volumes within the Polk Street corridor. This has been done based upon observed speeding and cut-through traffic. A summary of the results of this analysis is presented in Appendix I. The posted speed limit of this corridor is 30 miles per hour (mph). According to the data collected by the City, the 85th percentile speed in this corridor is 38 mph and the daily volume is 1,116 vehicles with approximately 77% of this traffic traveling in a westbound direction. The results of this evaluation suggest that a flattop speed table should be installed 440 feet west of the brick crosswalk at N. 26th Avenue and a speed table should also be installed 440 feet to the east of the centerline of N. 28th Avenue.

Similar data was collected by the project team between Sunday, July 11, 2021, and Tuesday, July 13, 2021. The weekday volumes are generally consistent with that reported by the City. And the 85th percentile speed of approximately 39 mph is similar to that reported by the City. (This data is presented in Appendix J.) Based upon the similarity of the data, it is apparent that the proposed traffic calming measures are reasonable. As such, the Polk Street Apartments project team will coordinate with the City to ensure that the proposed traffic calming measures are compatible with the project and the proposed crosswalks.

Westbound Vehicle Queuing at N. 28th Avenue

Given that the Polk Street corridor is a known cut-through corridor, City staff requested that a queuing evaluation of the westbound approach at N. 28th Avenue be conducted during the peak hours. This data is presented in Appendix K. This analysis reveals a maximum observed queue in the AM peak period of two (2) vehicles. In the PM peak period, the maximum observed queue was four (4) vehicles which occurred between 5:00 and 5:30.

Based upon a vehicle length of 22 feet, the maximum queue would be equivalent to 88 feet. Given that these observations were conducted in the month of July, it is likely that the vehicle queues will be longer during the peak season. With a 50% increase to account for peak season conditions, a queue of six (6) vehicles would yield a queue length of approximately 132 feet. The proposed driveway and pedestrian crosswalk for the western site are located approximately 165 feet east of N. 28th Avenue. As such, it appears that these elements will be rarely blocked by westbound vehicles queued at the intersection of Polk Street and N. 28th Avenue.

Pedestrian Access & Safety

As noted on the previously referenced site plans presented in Appendix A, a portion of the required parking supply for both sites will be located on the opposite side (i.e. north side) of Polk Street. This condition will necessitate pedestrian crossings of Polk Street for some residents and their guests. In order to accommodate this activity raised pedestrian crosswalks with appropriate signing and marking are proposed for each site. For the eastern site (2718 / 2720 / 2723 Polk Street) the proposed crosswalk will be located approximately 365 feet east of the Polk Street and N. 28th Avenue intersection. And for the western site (2741 / 2742 Polk Street) the proposed crosswalk will be located approximately 160 feet east of the Polk Street and N. 28th Avenue intersection. The resulting spacing between crosswalks will be approximately 200 feet. In addition, it is noted that the placement of each of these crosswalks has considered existing driveways serving the area's residential properties and the entrances to the proposed buildings.

It is envisioned that these crosswalks will be dual purpose traffic calming elements. By implementing a raised speed table design, these crosswalks will slow the background traffic in the Polk Street corridor (between N. 26th and N. 28th Avenues). And, with the accompanying signs requiring vehicles to stop for pedestrians in the crosswalk, they will provide an enhanced safety area for pedestrians to cross Polk Street. Additionally, these elements are expected to be complementary to the planned traffic calming measures currently under consideration by the City within this corridor. Final design and implementation will require coordination with the City of Hollywood and the Broward County Traffic Engineering Division.

SUMMARY & CONCLUSIONS

A residential development to be known as Polk Street Apartments is proposed on several parcels of land along Polk Street between N. 26th Avenue and N. 28th Avenue in Hollywood, Broward County, Florida. Multifamily residential developments are proposed on an eastern site and a western site. Both sites will be developed with 44 dwelling units in a four-story building (mid-rise format) to be located on the south side of Polk Street. There will be 33 parking spaces on the ground floor and an additional 16 parking spaces in a surface parking lot on the north side of Polk Street. Vehicular access to these sites will be provided by full access driveways on Polk Street. The parcels on the south side of Polk Street will also have emergency vehicle access only to the alleyway to the south.

The Polk Street Apartments development is anticipated to generate approximately 476 daily vehicle trips, 30 AM peak hour vehicle trips (8 inbound and 22 outbound) and approximately 40 vehicle trips (24 inbound and 16 outbound) during the typical afternoon peak hour. When considering the existing single and multifamily residences on the subject sites this represents an increase of 435 daily vehicle trips, an increase of 28 AM peak hour vehicle trips, and an increase of 36 PM peak hour vehicle trips.

The intersection capacity analyses of the nearby study intersections indicate that both intersections will continue to operate at an acceptable Level of Service (LOS) and the project traffic will have a minimal (i.e. “de minimis”) impact that will not significantly degrade the overall LOS of the study intersections.

APPENDIX A

Polk Street Apartments

Site Plans

POLK STREET APARTMENTS II

2742-2741- POLK STREET HOLLYWOOD, FLORIDA

ARCHITECT'S SEAL

BRIAN BULLOCK, ARCHITECT

AR 95754

DATE

02-03-2021

DRAWN BY

RST

PROJECT NO.

BS2030

SITE PLAN

SP-1

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BY DATE

REF

1/16/2021

REV

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REV

APPENDIX B

Intersection Turning Movement Counts

Traffic Survey Specialists, Inc.

85 SE 4th Avenue, Unit 109, Delray Beach, Florida 33483
Phone (561) 272-3255

POLK STREET & N 26TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: JOHN FLOOD
SIGNALIZED

File Name : POLK26AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 1

Groups Printed- ALL VEHICLES

Start Time	N 26TH AVENUE From North				POLK STREET From East				N 26TH AVENUE From South				POLK STREET From West				Int. Total
	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	
07:00 AM	0	0	23	0	0	23	14	1	0	0	5	0	0	0	0	0	66
07:15 AM	0	0	19	3	0	34	3	0	0	0	6	0	0	0	0	0	65
07:30 AM	0	0	19	0	0	42	3	1	0	3	7	0	0	0	0	0	75
07:45 AM	0	0	25	0	0	52	7	4	0	2	15	0	0	0	0	0	105
Total	0	0	86	3	0	151	27	6	0	5	33	0	0	0	0	0	311
08:00 AM	0	0	43	0	0	54	12	7	0	0	14	0	0	0	0	0	130
08:15 AM	0	0	28	1	0	53	10	9	0	0	18	0	0	0	0	0	119
08:30 AM	0	0	34	1	0	46	8	5	0	2	20	0	0	0	0	0	116
08:45 AM	0	0	35	0	0	43	6	4	0	3	19	0	0	0	0	0	110
Total	0	0	140	2	0	196	36	25	0	5	71	0	0	0	0	0	475
04:00 PM	0	0	27	4	0	67	19	8	0	3	16	0	0	0	0	0	144
04:15 PM	0	0	33	0	0	59	15	6	0	1	29	0	0	0	0	0	143
04:30 PM	0	0	23	4	0	61	16	7	0	5	40	0	0	0	0	0	156
04:45 PM	0	0	29	0	0	51	12	3	0	1	29	0	0	0	0	0	125
Total	0	0	112	8	0	238	62	24	0	10	114	0	0	0	0	0	568
05:00 PM	0	0	28	1	0	65	22	8	0	6	34	0	0	0	0	0	164
05:15 PM	0	0	22	1	0	55	24	12	0	6	38	0	0	0	0	0	158
05:30 PM	0	0	28	1	0	46	21	8	0	4	48	0	0	0	0	0	156
05:45 PM	0	0	22	2	0	33	12	5	0	3	34	0	0	0	0	0	111
Total	0	0	100	5	0	199	79	33	0	19	154	0	0	0	0	0	589
Grand Total	0	0	438	18	0	784	204	88	0	39	372	0	0	0	0	0	1943
Apprch %	0	0	96.1	3.9	0	72.9	19	8.2	0	9.5	90.5	0	0	0	0	0	
Total %	0	0	22.5	0.9	0	40.3	10.5	4.5	0	2	19.1	0	0	0	0	0	

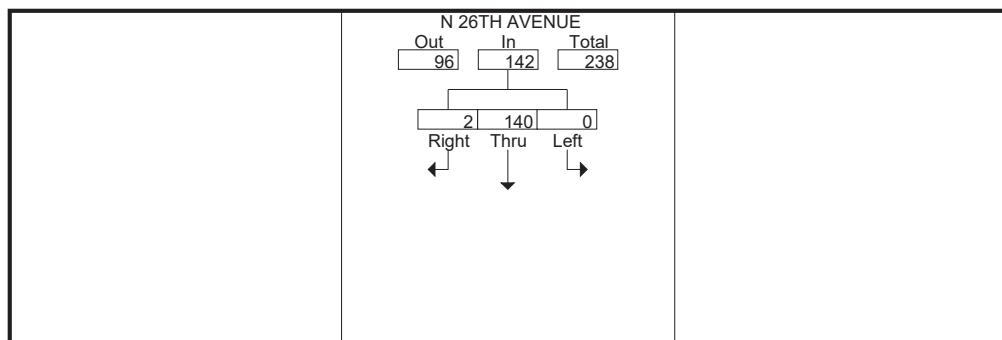
Traffic Survey Specialists, Inc.

85 SE 4th Avenue, Unit 109, Delray Beach, Florida 33483
Phone (561) 272-3255

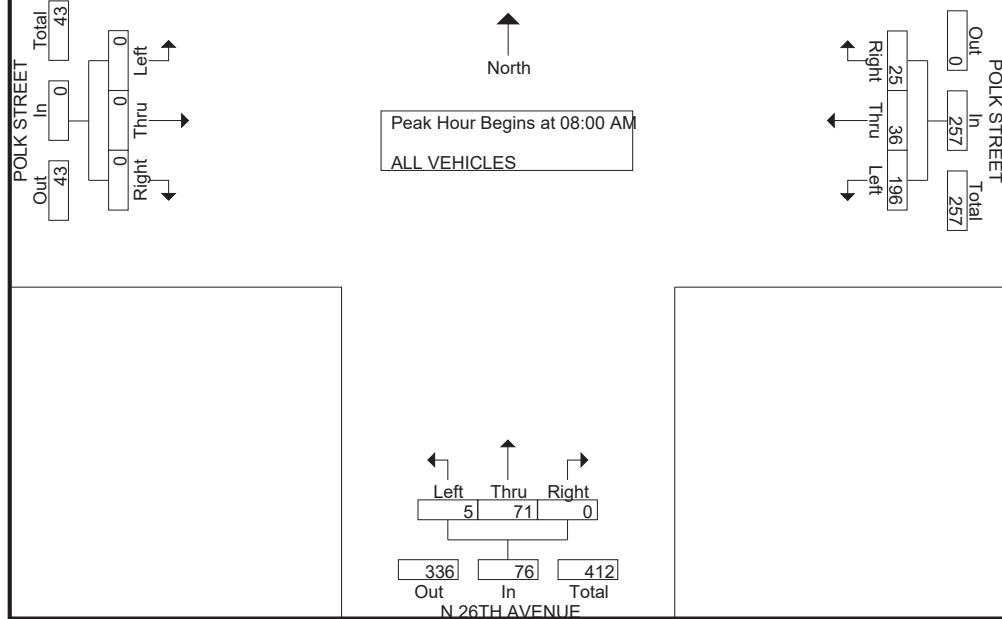
POLK STREET & N 26TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: JOHN FLOOD
SIGNALIZED

File Name : POLK26AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 2

Start Time	N 26TH AVENUE From North					POLK STREET From East					N 26TH AVENUE From South					POLK STREET From West					
	UTurn	Left	Thru	Right	App.Total	UTurn	Left	Thru	Right	App.Total	UTurn	Left	Thru	Right	App.Total	UTurn	Left	Thru	Right	App.Total	Int. Total
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:00 AM																					
08:00 AM	0	0	43	0	43	0	54	12	7	73	0	0	14	0	14	0	0	0	0	0	130
08:15 AM	0	0	28	1	29	0	53	10	9	72	0	0	18	0	18	0	0	0	0	0	119
08:30 AM	0	0	34	1	35	0	46	8	5	59	0	2	20	0	22	0	0	0	0	0	116
08:45 AM	0	0	35	0	35	0	43	6	4	53	0	3	19	0	22	0	0	0	0	0	110
Total Volume	0	0	140	2	142	0	196	36	25	257	0	5	71	0	76	0	0	0	0	0	475
% App. Total	0	0	98.6	1.4		0	76.3	14	9.7		0	6.6	93.4	0		0	0	0	0	0	
PHF	.000	.000	.814	.500	.826	.000	.907	.750	.694	.880	.000	.417	.888	.000	.864	.000	.000	.000	.000	.913	



Peak Hour Data



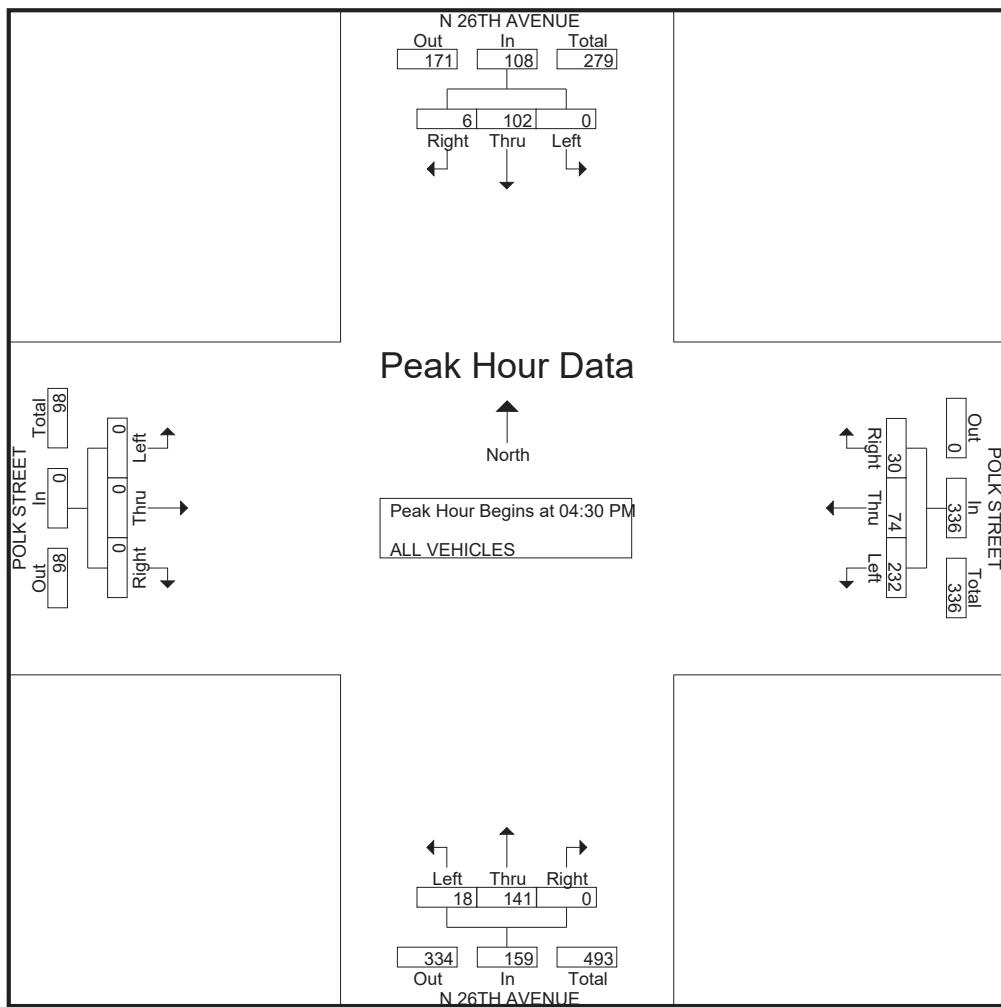
Traffic Survey Specialists, Inc.

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Phone (561) 272-3255

POLK STREET & N 26TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: JOHN FLOOD
SIGNALIZED

File Name : POLK26AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 3

Start Time	N 26TH AVENUE From North					POLK STREET From East					N 26TH AVENUE From South					POLK STREET From West					
	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	Int. Total
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:30 PM																					
04:30 PM	0	0	23	4	27	0	61	16	7	84	0	5	40	0	45	0	0	0	0	0	156
04:45 PM	0	0	29	0	29	0	51	12	3	66	0	1	29	0	30	0	0	0	0	0	125
05:00 PM	0	0	28	1	29	0	65	22	8	95	0	6	34	0	40	0	0	0	0	0	164
05:15 PM	0	0	22	1	23	0	55	24	12	91	0	6	38	0	44	0	0	0	0	0	158
Total Volume	0	0	102	6	108	0	232	74	30	336	0	18	141	0	159	0	0	0	0	0	603
% App. Total	0	0	94.4	5.6		0	69	22	8.9		0	11.3	88.7	0		0	0	0	0	0	
PHF	.000	.000	.879	.375	.931	.000	.892	.771	.625	.884	.000	.750	.881	.000	.883	.000	.000	.000	.000	.000	.919



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POLK STREET & N 26TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: JOHN FLOOD
SIGNALIZED

File Name : POLK26AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 1

Groups Printed- PEDESTRIANS & BIKES

Start Time	N 26TH AVENUE From North				POLK STREET From East				N 26TH AVENUE From South				POLK STREET From West				Int. Total
	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	
07:00 AM	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	2
07:45 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	1	0	1	0	0	0	1	0	0	0	0	0	0	0	3
08:15 AM	3	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	4
08:30 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2
Total	3	0	2	0	0	0	0	0	0	0	0	0	1	0	0	0	6
04:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
04:15 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
04:45 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
Total	2	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	3
05:00 PM	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
05:30 PM	2	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	5
05:45 PM	1	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	2
Total	6	0	0	0	3	0	0	0	1	0	0	0	0	0	0	0	10
Grand Total	11	0	3	0	5	0	0	0	2	0	0	0	1	0	0	0	22
Apprch %	78.6	0	21.4	0	100	0	0	0	100	0	0	0	100	0	0	0	
Total %	50	0	13.6	0	22.7	0	0	0	9.1	0	0	0	4.5	0	0	0	

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POLK STREET & N 28TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: MARISA CRUZ
NOT SIGNALIZED

File Name : POLK28AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 1

Groups Printed- ALL VEHICLES

Start Time	N 28TH AVENUE From North				POLK STREET From East				N 28TH AVENUE From South				POLK STREET From West				Int. Total
	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	
07:00 AM	0	1	83	0	0	1	1	1	0	0	39	1	0	1	1	0	129
07:15 AM	0	0	112	0	0	4	1	1	0	1	47	0	0	3	0	1	170
07:30 AM	0	0	105	3	0	3	1	5	0	0	44	2	0	0	0	2	165
07:45 AM	0	0	109	1	0	4	4	3	0	2	61	0	0	1	0	1	186
Total	0	1	409	4	0	12	7	10	0	3	191	3	0	5	1	4	650
08:00 AM	0	0	100	0	0	8	1	3	0	2	58	2	0	1	0	1	176
08:15 AM	0	3	95	3	0	5	1	5	0	0	65	3	0	3	0	0	183
08:30 AM	0	3	107	0	0	10	2	1	0	0	61	2	0	2	0	3	191
08:45 AM	0	1	93	2	0	4	2	4	0	1	70	4	0	8	0	0	189
Total	0	7	395	5	0	27	6	13	0	3	254	11	0	14	0	4	739
04:00 PM	0	0	68	0	0	17	4	5	0	2	80	2	0	1	0	1	180
04:15 PM	0	0	90	2	0	11	1	6	0	4	75	3	0	0	0	0	192
04:30 PM	0	2	71	0	0	14	3	7	0	5	80	0	0	2	0	2	186
04:45 PM	0	1	83	0	0	9	2	2	0	0	85	1	0	2	0	0	185
Total	0	3	312	2	0	51	10	20	0	11	320	6	0	5	0	3	743
05:00 PM	0	2	88	1	0	12	8	9	0	3	87	4	0	1	2	0	217
05:15 PM	0	2	91	3	0	18	4	4	0	4	76	2	0	0	0	1	205
05:30 PM	0	0	85	3	0	8	5	8	0	1	94	1	0	1	1	3	210
05:45 PM	0	0	66	2	0	9	0	8	0	3	85	2	0	1	0	0	176
Total	0	4	330	9	0	47	17	29	0	11	342	9	0	3	3	4	808
Grand Total	0	15	1446	20	0	137	40	72	0	28	1107	29	0	27	4	15	2940
Apprch %	0	1	97.6	1.4	0	55	16.1	28.9	0	2.4	95.1	2.5	0	58.7	8.7	32.6	
Total %	0	0.5	49.2	0.7	0	4.7	1.4	2.4	0	1	37.7	1	0	0.9	0.1	0.5	

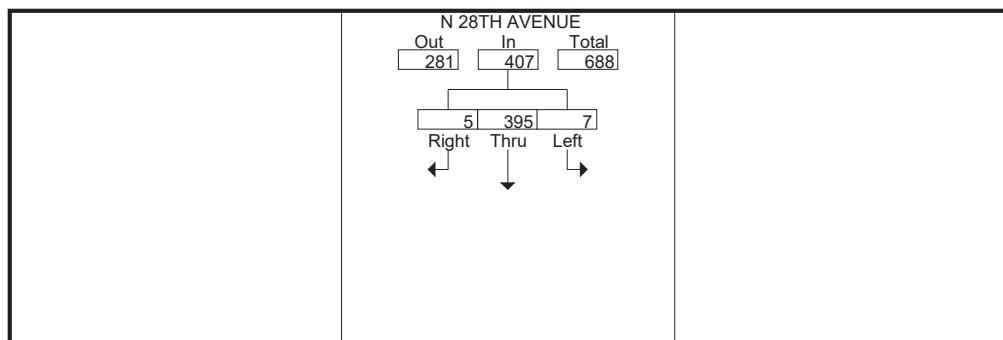
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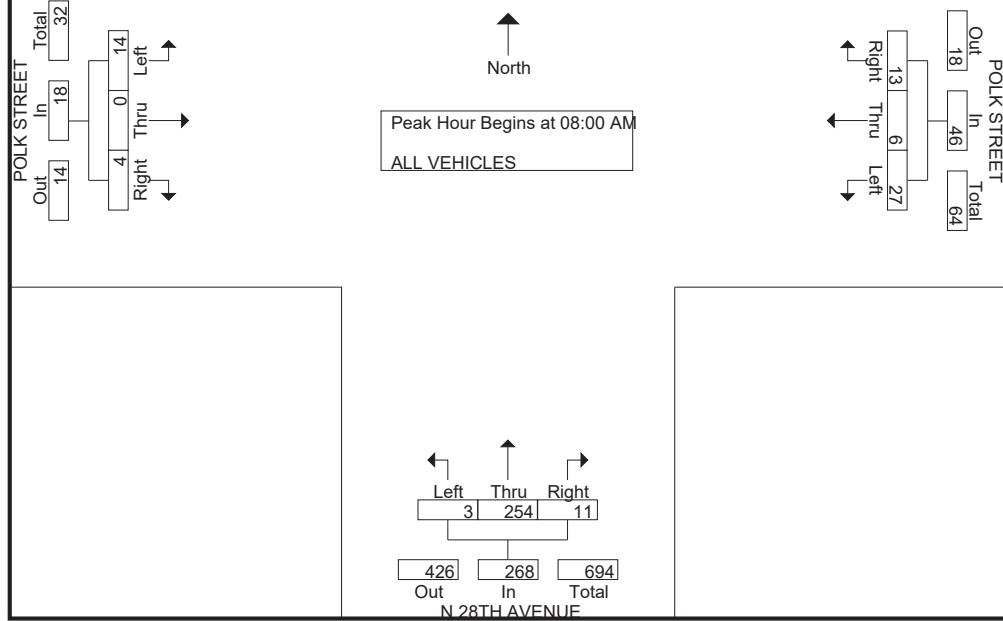
POLK STREET & N 28TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: MARISA CRUZ
NOT SIGNALIZED

File Name : POLK28AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 2

Start Time	N 28TH AVENUE From North					POLK STREET From East					N 28TH AVENUE From South					POLK STREET From West					
	UTurn	Left	Thru	Right	App.Total	UTurn	Left	Thru	Right	App.Total	UTurn	Left	Thru	Right	App.Total	UTurn	Left	Thru	Right	App.Total	Int. Total
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:00 AM																					
08:00 AM	0	0	100	0	100	0	8	1	3	12	0	2	58	2	62	0	1	0	1	2	176
08:15 AM	0	3	95	3	101	0	5	1	5	11	0	0	65	3	68	0	3	0	0	3	183
08:30 AM	0	3	107	0	110	0	10	2	1	13	0	0	61	2	63	0	2	0	3	5	191
08:45 AM	0	1	93	2	96	0	4	2	4	10	0	1	70	4	75	0	8	0	0	8	189
Total Volume	0	7	395	5	407	0	27	6	13	46	0	3	254	11	268	0	14	0	4	18	739
% App. Total	0	1.7	97.1	1.2		0	58.7	13	28.3		0	1.1	94.8	4.1		0	77.8	0	22.2		
PHF	.000	.583	.923	.417	.925	.000	.675	.750	.650	.885	.000	.375	.907	.688	.893	.000	.438	.000	.333	.563	.967



Peak Hour Data



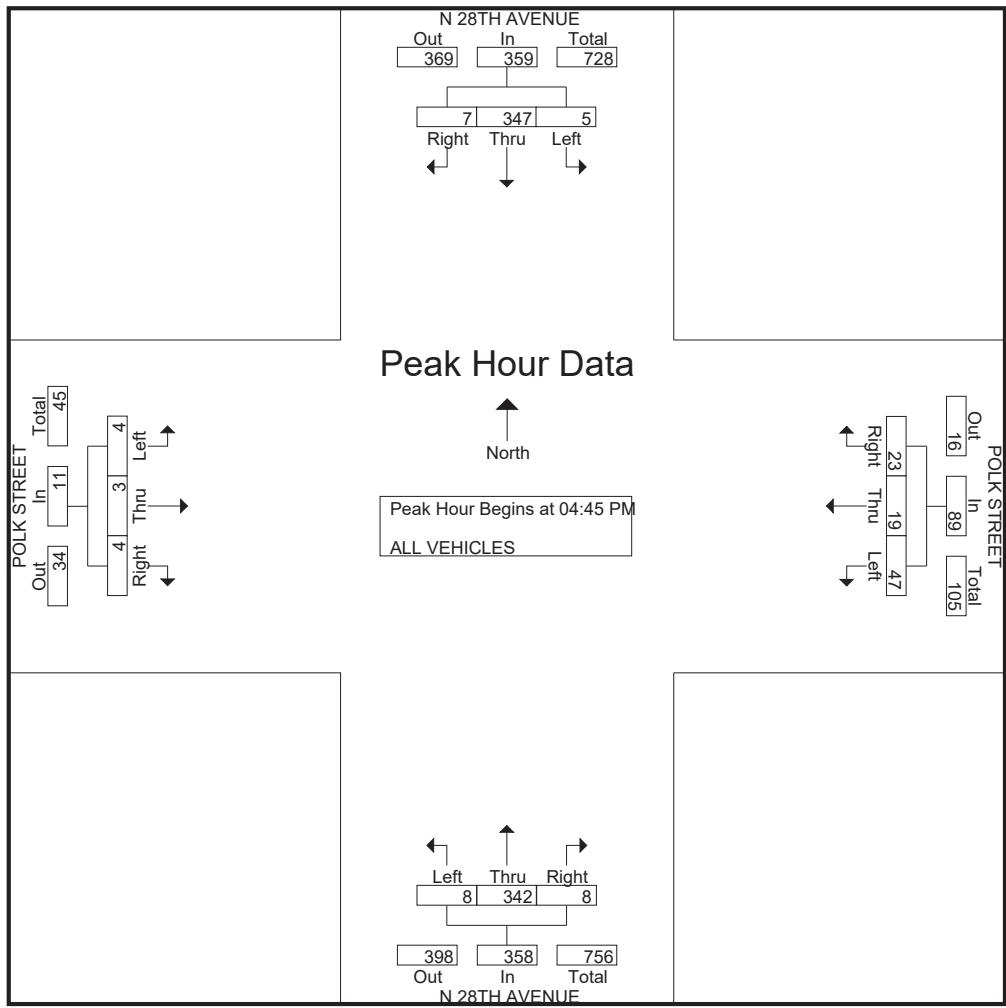
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NOT SIGNALIZED

File Name : POLK28AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 3

Start Time	N 28TH AVENUE From North				POLK STREET From East				N 28TH AVENUE From South				POLK STREET From West				Int. Total				
	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:45 PM																					
04:45 PM	0	1	83	0	84	0	9	2	2	13	0	0	85	1	86	0	2	0	0	2	185
05:00 PM	0	2	88	1	91	0	12	8	9	29	0	3	87	4	94	0	1	2	0	3	217
05:15 PM	0	2	91	3	96	0	18	4	4	26	0	4	76	2	82	0	0	0	1	1	205
05:30 PM	0	0	85	3	88	0	8	5	8	21	0	1	94	1	96	0	1	1	3	5	210
Total Volume	0	5	347	7	359	0	47	19	23	89	0	8	342	8	358	0	4	3	4	11	817
% App. Total	0	1.4	96.7	1.9		0	52.8	21.3	25.8		0	2.2	95.5	2.2		0	36.4	27.3	36.4		
PHF	.000	.625	.953	.583	.935	.000	.653	.594	.639	.767	.000	.500	.910	.500	.932	.000	.500	.375	.333	.550	.941



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POLK STREET & N 28TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: MARISA CRUZ
NOT SIGNALIZED

File Name : POLK28AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 1

Groups Printed- PEDESTRIANS & BIKES

Start Time	N 28TH AVENUE From North				POLK STREET From East				N 28TH AVENUE From South				POLK STREET From West				Int. Total
	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	
07:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	2	0	0	0	3
07:15 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
07:30 AM	1	0	1	0	0	0	1	0	0	0	0	0	1	0	0	0	4
07:45 AM	0	0	0	0	1	0	0	0	0	0	0	0	1	0	0	0	2
Total	1	0	2	0	2	0	1	0	0	0	0	0	4	0	0	0	10
08:00 AM	2	0	0	0	1	0	0	0	0	0	1	0	1	0	1	0	6
08:30 AM	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	2
08:45 AM	0	0	0	0	1	0	0	0	0	0	0	0	3	0	0	0	4
Total	2	0	0	0	2	0	0	0	0	0	2	0	5	0	1	0	12
04:15 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
04:30 PM	0	0	2	0	2	0	0	0	1	0	0	0	0	0	1	0	6
04:45 PM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
Total	0	0	2	0	4	0	1	0	1	0	0	0	0	0	1	0	9
05:00 PM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
05:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1
05:30 PM	0	0	0	0	0	0	1	0	0	0	0	0	1	0	0	0	2
05:45 PM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	3	0	4
Total	0	0	0	0	0	0	2	0	0	0	1	0	2	0	3	0	8
Grand Total	3	0	4	0	8	0	4	0	1	0	3	0	11	0	5	0	39
Apprch %	42.9	0	57.1	0	66.7	0	33.3	0	25	0	75	0	68.8	0	31.2	0	
Total %	7.7	0	10.3	0	20.5	0	10.3	0	2.6	0	7.7	0	28.2	0	12.8	0	

APPENDIX C

FDOT

Peak Season Conversion Factor Report

2019 PEAK SEASON FACTOR CATEGORY REPORT - REPORT TYPE: ALL
 CATEGORY: 8601 CEN.-W OF US1 TO SR7

MOCF: 0.97
 PSCF

WEEK	DATES	SF	
=====			
1	01/01/2019 - 01/05/2019	1.00	1.03
2	01/06/2019 - 01/12/2019	1.00	1.03
3	01/13/2019 - 01/19/2019	1.01	1.04
4	01/20/2019 - 01/26/2019	1.00	1.03
5	01/27/2019 - 02/02/2019	0.99	1.02
* 6	02/03/2019 - 02/09/2019	0.98	1.01
* 7	02/10/2019 - 02/16/2019	0.97	1.00
* 8	02/17/2019 - 02/23/2019	0.97	1.00
* 9	02/24/2019 - 03/02/2019	0.97	1.00
*10	03/03/2019 - 03/09/2019	0.96	0.99
*11	03/10/2019 - 03/16/2019	0.96	0.99
*12	03/17/2019 - 03/23/2019	0.97	1.00
*13	03/24/2019 - 03/30/2019	0.97	1.00
*14	03/31/2019 - 04/06/2019	0.97	1.00
*15	04/07/2019 - 04/13/2019	0.98	1.01
*16	04/14/2019 - 04/20/2019	0.98	1.01
*17	04/21/2019 - 04/27/2019	0.99	1.02
*18	04/28/2019 - 05/04/2019	0.99	1.02
19	05/05/2019 - 05/11/2019	1.00	1.03
20	05/12/2019 - 05/18/2019	1.00	1.03
21	05/19/2019 - 05/25/2019	1.01	1.04
22	05/26/2019 - 06/01/2019	1.01	1.04
23	06/02/2019 - 06/08/2019	1.01	1.04
24	06/09/2019 - 06/15/2019	1.02	1.05
25	06/16/2019 - 06/22/2019	1.02	1.05
26	06/23/2019 - 06/29/2019	1.02	1.05
27	06/30/2019 - 07/06/2019	1.03	1.06
28	07/07/2019 - 07/13/2019	1.03	1.06
29	07/14/2019 - 07/20/2019	1.04	1.07
30	07/21/2019 - 07/27/2019	1.03	1.06
31	07/28/2019 - 08/03/2019	1.02	1.05
32	08/04/2019 - 08/10/2019	1.02	1.05
33	08/11/2019 - 08/17/2019	1.01	1.04
34	08/18/2019 - 08/24/2019	1.02	1.05
35	08/25/2019 - 08/31/2019	1.03	1.06
36	09/01/2019 - 09/07/2019	1.03	1.06
37	09/08/2019 - 09/14/2019	1.04	1.07
38	09/15/2019 - 09/21/2019	1.05	1.08
39	09/22/2019 - 09/28/2019	1.04	1.07
40	09/29/2019 - 10/05/2019	1.02	1.05
41	10/06/2019 - 10/12/2019	1.01	1.04
42	10/13/2019 - 10/19/2019	1.00	1.03
43	10/20/2019 - 10/26/2019	1.00	1.03
44	10/27/2019 - 11/02/2019	1.00	1.03
45	11/03/2019 - 11/09/2019	1.00	1.03
46	11/10/2019 - 11/16/2019	1.00	1.03
47	11/17/2019 - 11/23/2019	1.00	1.03
48	11/24/2019 - 11/30/2019	1.00	1.03
49	12/01/2019 - 12/07/2019	1.00	1.03
50	12/08/2019 - 12/14/2019	1.00	1.03
51	12/15/2019 - 12/21/2019	1.00	1.03
52	12/22/2019 - 12/28/2019	1.00	1.03
53	12/29/2019 - 12/31/2019	1.01	1.04

* PEAK SEASON

14-FEB-2020 15:39:26

830UPD

4_8601_PKSEASON.TXT

APPENDIX D

ITE *Trip Generation Manual (10th Edition)*

Relevant Excerpts

Land Use: 210

Single-Family Detached Housing

Description

Single-family detached housing includes all single-family detached homes on individual lots. A typical site surveyed is a suburban subdivision.

Additional Data

The number of vehicles and residents had a high correlation with average weekday vehicle trip ends. The use of these variables was limited, however, because the number of vehicles and residents was often difficult to obtain or predict. The number of dwelling units was generally used as the independent variable of choice because it was usually readily available, easy to project, and had a high correlation with average weekday vehicle trip ends.

This land use included data from a wide variety of units with different sizes, price ranges, locations, and ages. Consequently, there was a wide variation in trips generated within this category. Other factors, such as geographic location and type of adjacent and nearby development, may also have had an effect on the site trip generation.

Single-family detached units had the highest trip generation rate per dwelling unit of all residential uses because they were the largest units in size and had more residents and more vehicles per unit than other residential land uses; they were generally located farther away from shopping centers, employment areas, and other trip attractors than other residential land uses; and they generally had fewer alternative modes of transportation available because they were typically not as concentrated as other residential land uses.

Time-of-day distribution data for this land use are presented in Appendix A. For the six general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:00 and 5:00 p.m., respectively. For the two sites with Saturday data, the overall highest vehicle volume was counted between 3:00 and 4:00 p.m. For the one site with Sunday data, the overall highest vehicle volume was counted between 10:15 and 11:15 a.m.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in California, Connecticut, Delaware, Illinois, Indiana, Maryland, Minnesota, Montana, New Jersey, North Carolina, Ohio, Oregon, Pennsylvania, South Carolina, South Dakota, Tennessee, Vermont, and Virginia.

Source Numbers

100, 105, 114, 126, 157, 167, 177, 197, 207, 211, 217, 267, 275, 293, 300, 319, 320, 356, 357, 367, 384, 387, 407, 435, 522, 550, 552, 579, 598, 601, 603, 614, 637, 711, 716, 720, 728, 735, 868, 903, 925, 936

Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 159

Avg. Num. of Dwelling Units: 264

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate

9.44

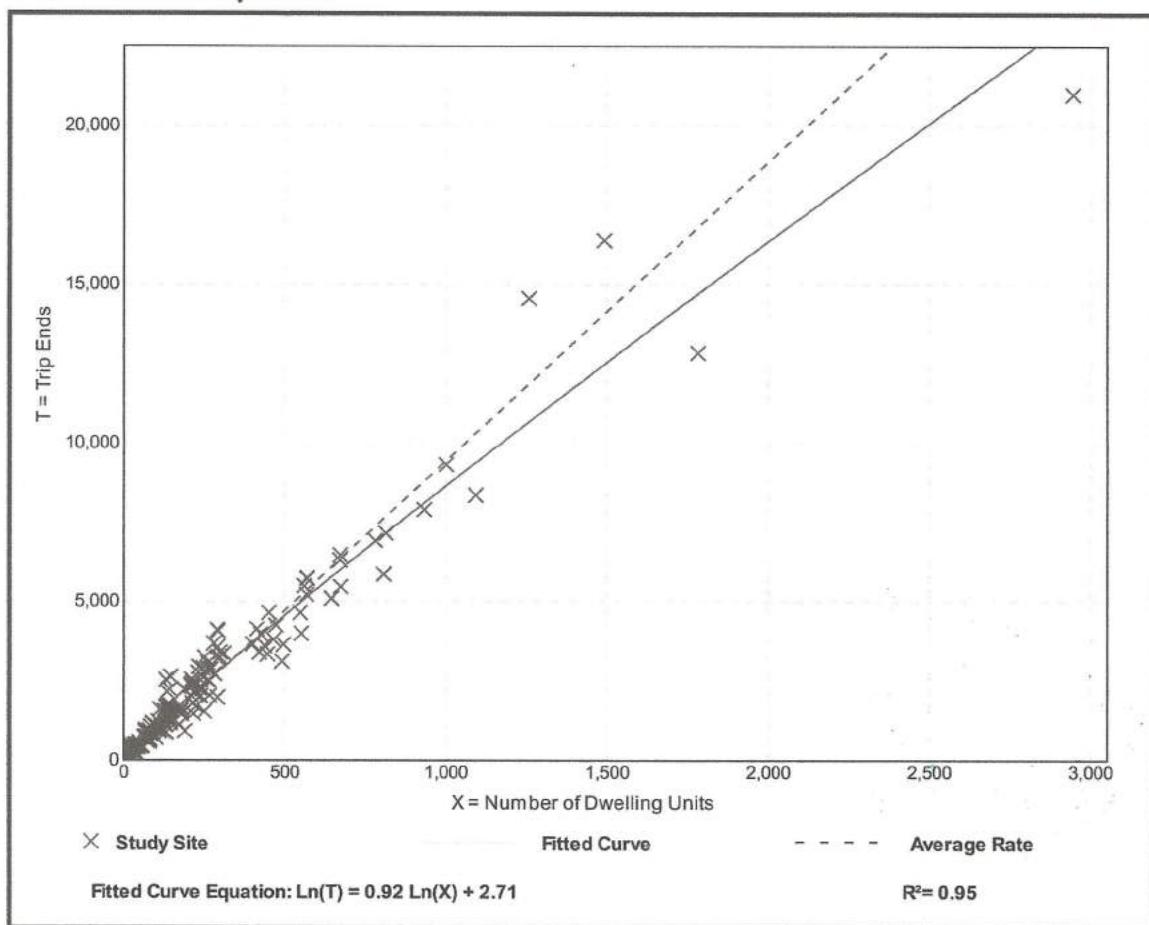
Range of Rates

4.81 - 19.39

Standard Deviation

2.10

Data Plot and Equation



Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 173

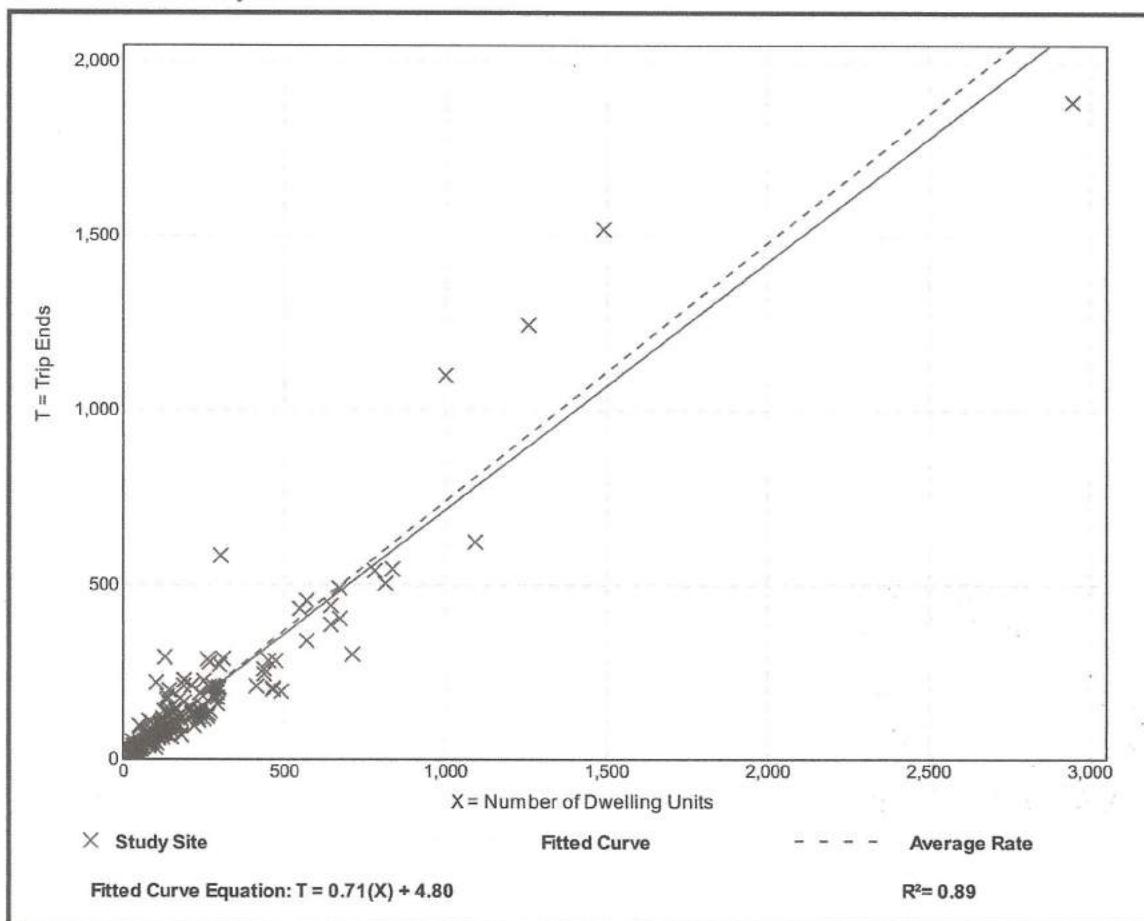
Avg. Num. of Dwelling Units: 219

Directional Distribution: 25% entering, 75% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.74	0.33 - 2.27	0.27

Data Plot and Equation



Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 190

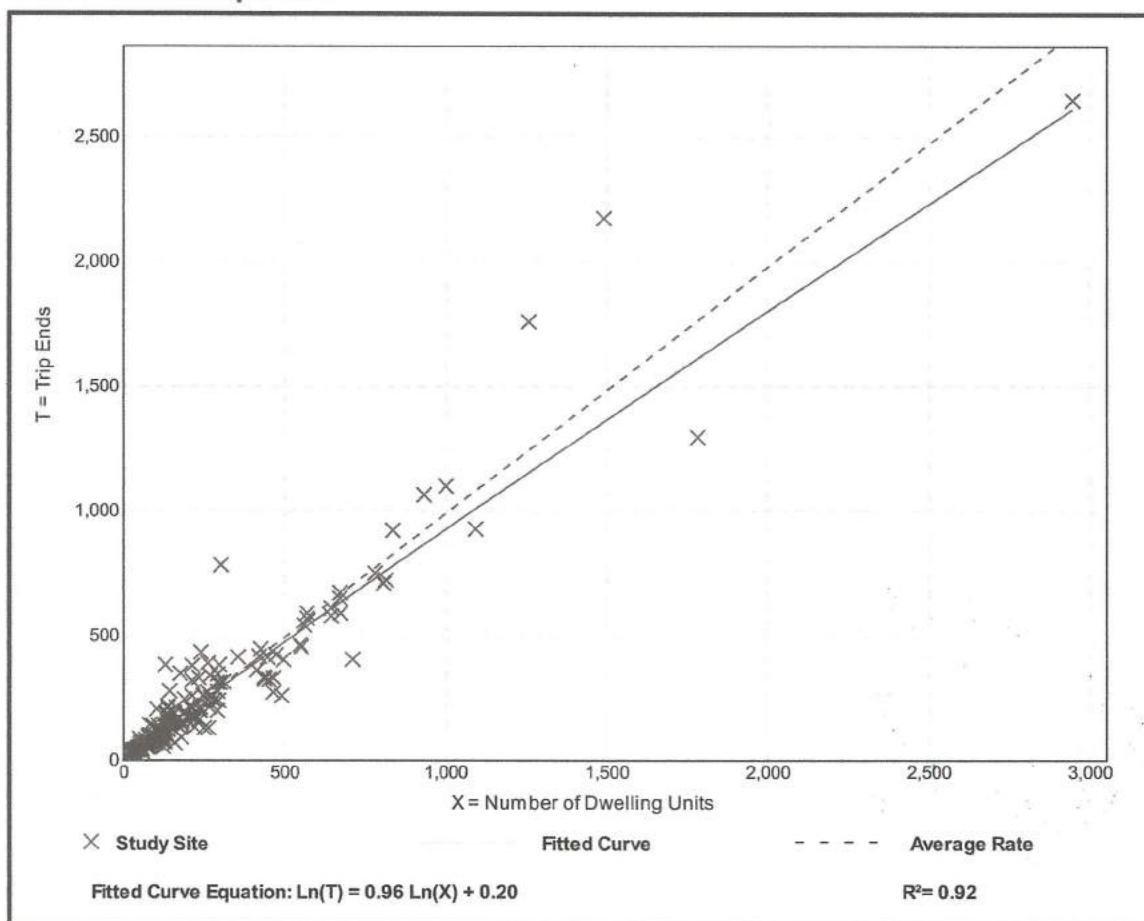
Avg. Num. of Dwelling Units: 242

Directional Distribution: 63% entering, 37% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.99	0.44 - 2.98	0.31

Data Plot and Equation



Land Use: 220

Multifamily Housing (Low-Rise)

Description

Low-rise multifamily housing includes apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have one or two levels (floors). Multifamily housing (mid-rise) (Land Use 221), multifamily housing (high-rise) (Land Use 222), and off-campus student apartment (Land Use 225) are related land uses.

Additional Data

In prior editions of *Trip Generation Manual*, the low-rise multifamily housing sites were further divided into rental and condominium categories. An investigation of vehicle trip data found no clear differences in trip making patterns between the rental and condominium sites within the ITE database. As more data are compiled for future editions, this land use classification can be reinvestigated.

For the three sites for which both the number of residents and the number of occupied dwelling units were available, there were an average of 2.72 residents per occupied dwelling unit.

For the two sites for which the numbers of both total dwelling units and occupied dwelling units were available, an average of 96.2 percent of the total dwelling units were occupied.

This land use included data from a wide variety of units with different sizes, price ranges, locations, and ages. Consequently, there was a wide variation in trips generated within this category. Other factors, such as geographic location and type of adjacent and nearby development, may also have had an effect on the site trip generation.

Time-of-day distribution data for this land use are presented in Appendix A. For the 10 general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:45 and 5:45 p.m., respectively. For the one site with Saturday data, the overall highest vehicle volume was counted between 9:45 and 10:45 a.m. For the one site with Sunday data, the overall highest vehicle volume was counted between 11:45 a.m. and 12:45 p.m.

For the one dense multi-use urban site with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:00 and 8:00 a.m. and 6:15 and 7:15 p.m., respectively.

For the three sites for which data were provided for both occupied dwelling units and residents, there was an average of 2.72 residents per occupied dwelling unit.

The average numbers of person trips per vehicle trip at the five general urban/suburban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.13 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.21 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in British Columbia (CAN), California, District of Columbia, Florida, Georgia, Illinois, Indiana, Maine, Maryland, Minnesota, New Jersey, New York, Ontario, Oregon, Pennsylvania, South Dakota, Tennessee, Texas, Utah, Virginia, and Washington.

✓ It is expected that the number of bedrooms and number of residents are likely correlated to the number of trips generated by a residential site. Many of the studies included in this land use did not indicate the total number of bedrooms. To assist in the future analysis of this land use, it is important that this information be collected and included in trip generation data submissions.

Source Numbers

168, 187, 188, 204, 211, 300, 305, 306, 319, 320, 321, 357, 390, 412, 418, 525, 530, 571, 579, 583, 864, 868, 869, 870, 896, 903, 918, 946, 947, 948, 951

Multifamily Housing (Low-Rise) (220)

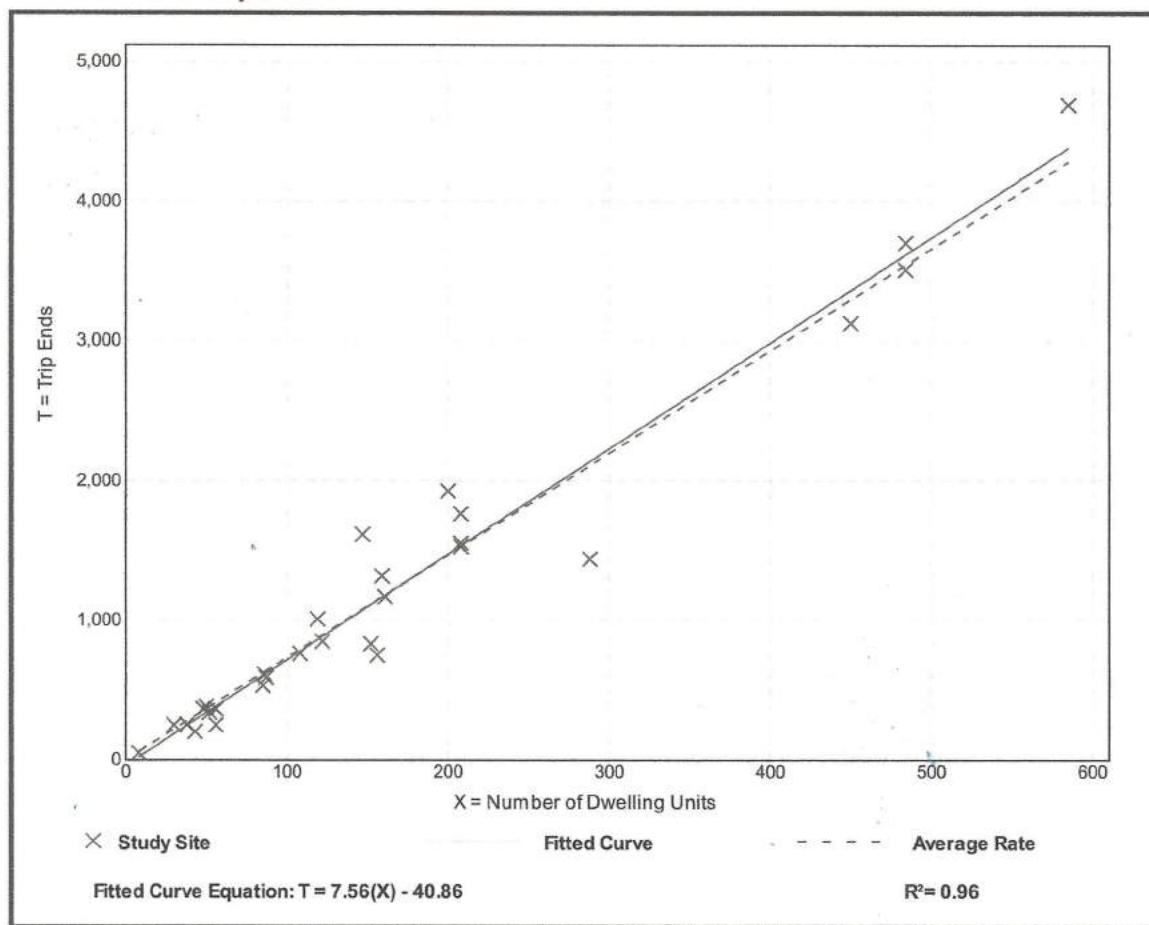
Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 29
Avg. Num. of Dwelling Units: 168
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
7.32	4.45 - 10.97	1.31

Data Plot and Equation



Multifamily Housing (Low-Rise) (220)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday

Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 42

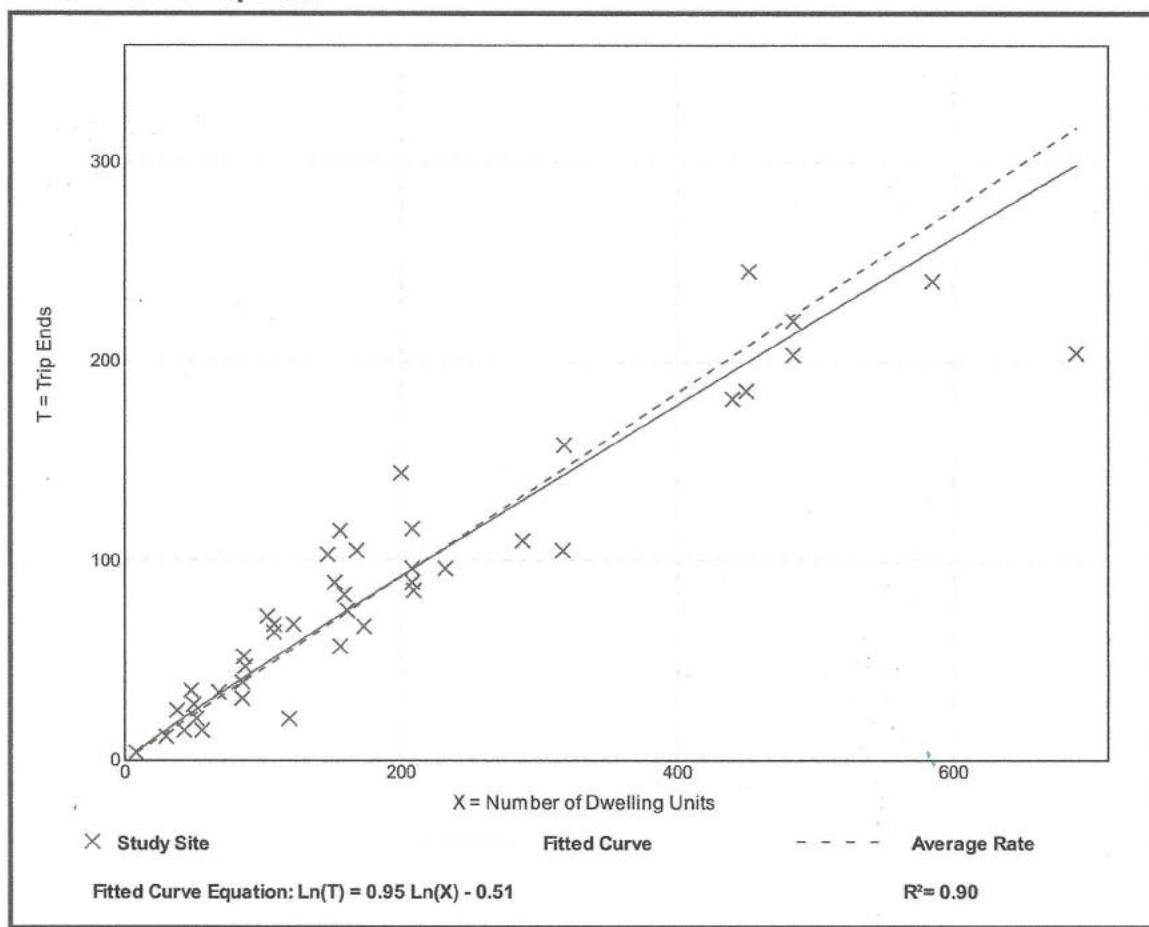
Avg. Num. of Dwelling Units: 199

Directional Distribution: 23% entering, 77% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.46	0.18 - 0.74	0.12

Data Plot and Equation



Multifamily Housing (Low-Rise) (220)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 50

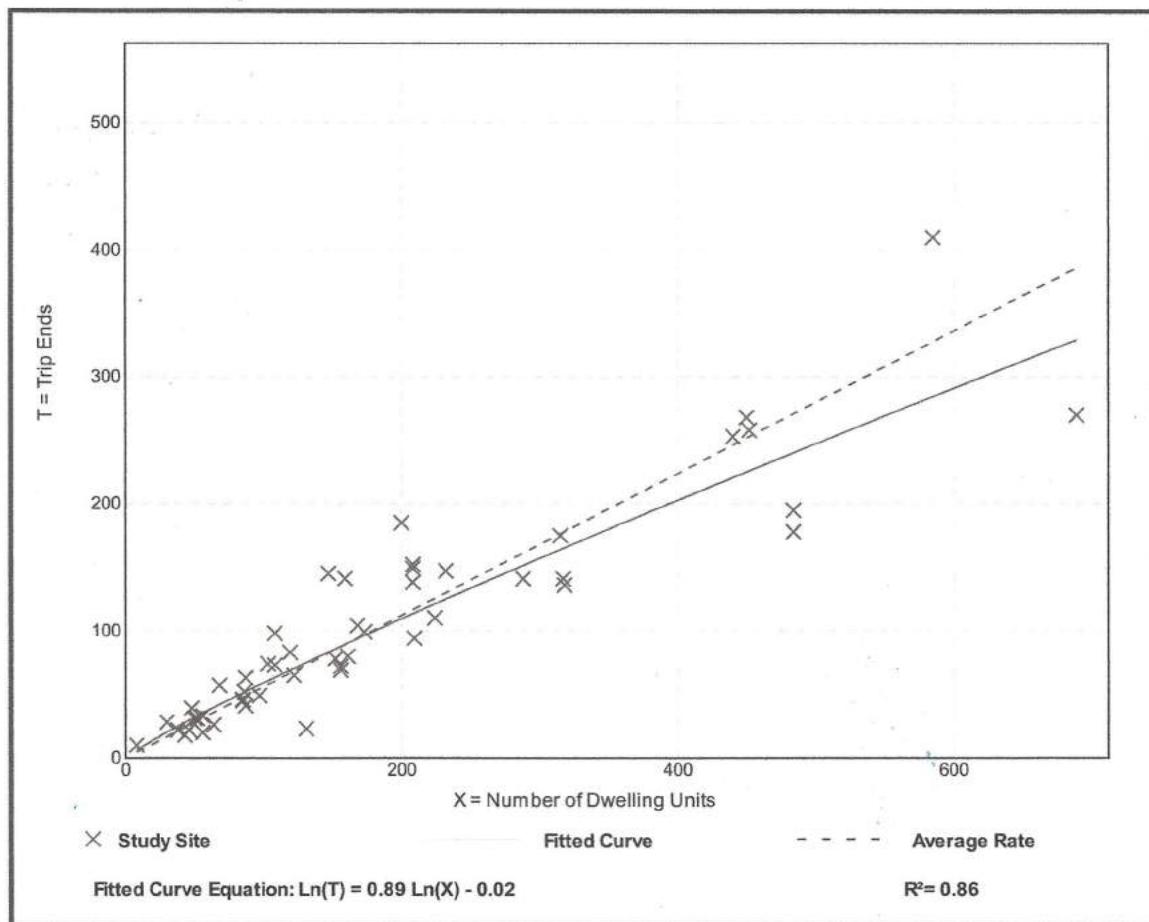
Avg. Num. of Dwelling Units: 187

Directional Distribution: 63% entering, 37% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.56	0.18 - 1.25	0.16

Data Plot and Equation



Land Use: 221

Multifamily Housing (Mid-Rise)

Description

Mid-rise multifamily housing includes apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have between three and 10 levels (floors). Multifamily housing (low-rise) (Land Use 220), multifamily housing (high-rise) (Land Use 222), off-campus student apartment (Land Use 225), and mid-rise residential with 1st-floor commercial (Land Use 231) are related land uses.

Additional Data

In prior editions of *Trip Generation Manual*, the mid-rise multifamily housing sites were further divided into rental and condominium categories. An investigation of vehicle trip data found no clear differences in trip making patterns between the rental and condominium sites within the ITE database. As more data are compiled for future editions, this land use classification can be reinvestigated.

For the six sites for which both the number of residents and the number of occupied dwelling units were available, there were an average of 2.46 residents per occupied dwelling unit.

For the five sites for which the numbers of both total dwelling units and occupied dwelling units were available, an average of 95.7 percent of the total dwelling units were occupied.

Time-of-day distribution data for this land use are presented in Appendix A. For the eight general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:00 and 8:00 a.m. and 4:45 and 5:45 p.m., respectively.

For the four dense multi-use urban sites with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:15 and 5:15 p.m., respectively. For the three center city core sites with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 6:45 and 7:45 a.m. and 5:00 and 6:00 p.m., respectively.

For the six sites for which data were provided for both occupied dwelling units and residents, there was an average of 2.46 residents per occupied dwelling unit.

For the five sites for which data were provided for both occupied dwelling units and total dwelling units, an average of 95.7 percent of the units were occupied.

The average numbers of person trips per vehicle trip at the five center city core sites at which both person trip and vehicle trip data were collected were as follows:

- 1.84 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.94 during Weekday, AM Peak Hour of Generator
- 2.07 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.59 during Weekday, PM Peak Hour of Generator

The average numbers of person trips per vehicle trip at the 32 dense multi-use urban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.90 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.90 during Weekday, AM Peak Hour of Generator
- 2.00 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.08 during Weekday, PM Peak Hour of Generator

The average numbers of person trips per vehicle trip at the 13 general urban/suburban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.56 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.88 during Weekday, AM Peak Hour of Generator
- 1.70 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.07 during Weekday, PM Peak Hour of Generator

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), British Columbia (CAN), California, Delaware, District of Columbia, Florida, Georgia, Illinois, Maryland, Massachusetts, Minnesota, New Hampshire, New Jersey, Ontario, Oregon, Pennsylvania, South Carolina, South Dakota, Tennessee, Utah, Virginia, and Wisconsin.

Source Numbers

168, 188, 204, 305, 306, 321, 357, 390, 436, 525, 530, 579, 638, 818, 857, 866, 901, 904, 910, 912, 918, 934, 936, 939, 944, 947, 948, 949, 959, 963, 964, 966, 967, 969, 970

Multifamily Housing (Mid-Rise) (221)

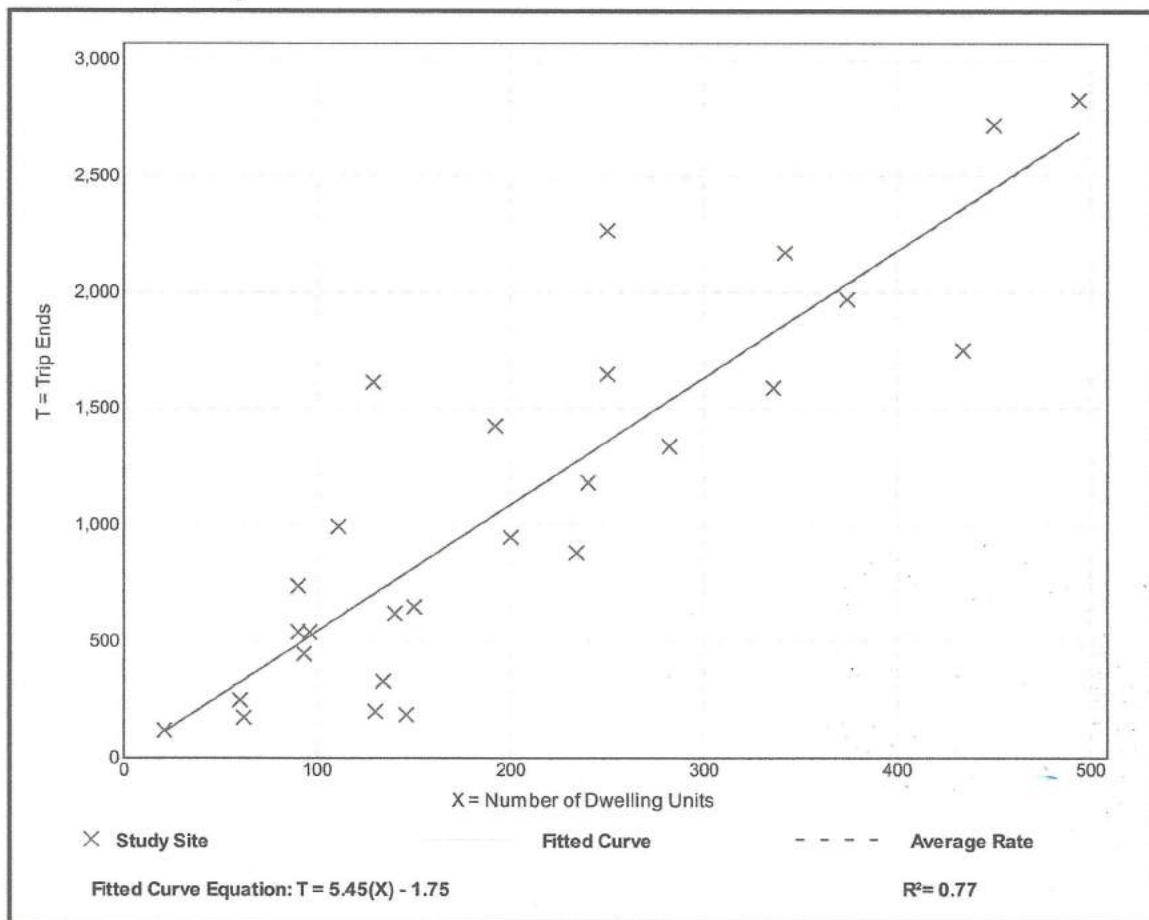
Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 27
Avg. Num. of Dwelling Units: 205
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
5.44	1.27 - 12.50	2.03

Data Plot and Equation



Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 53

Avg. Num. of Dwelling Units: 207

Directional Distribution: 26% entering, 74% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate

0.36

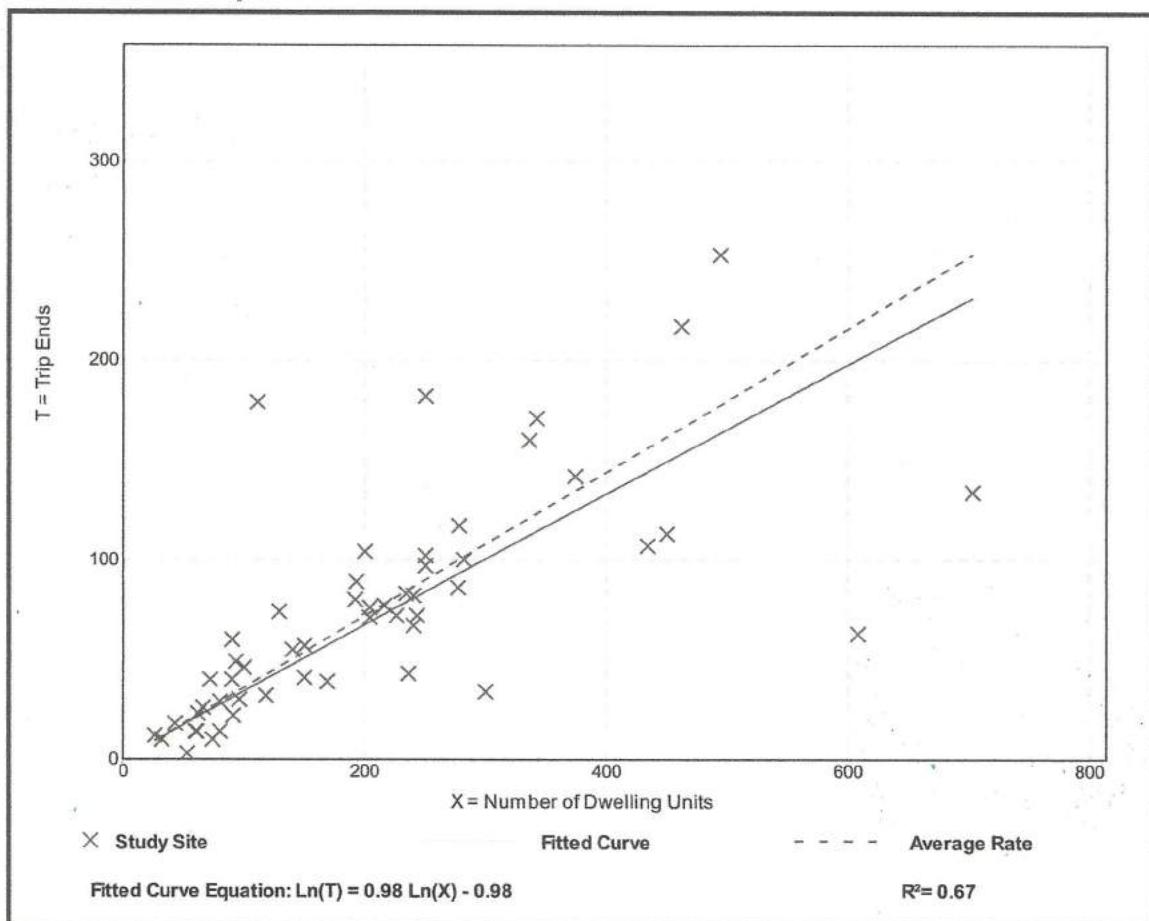
Range of Rates

0.06 - 1.61

Standard Deviation

0.19

Data Plot and Equation



Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 60

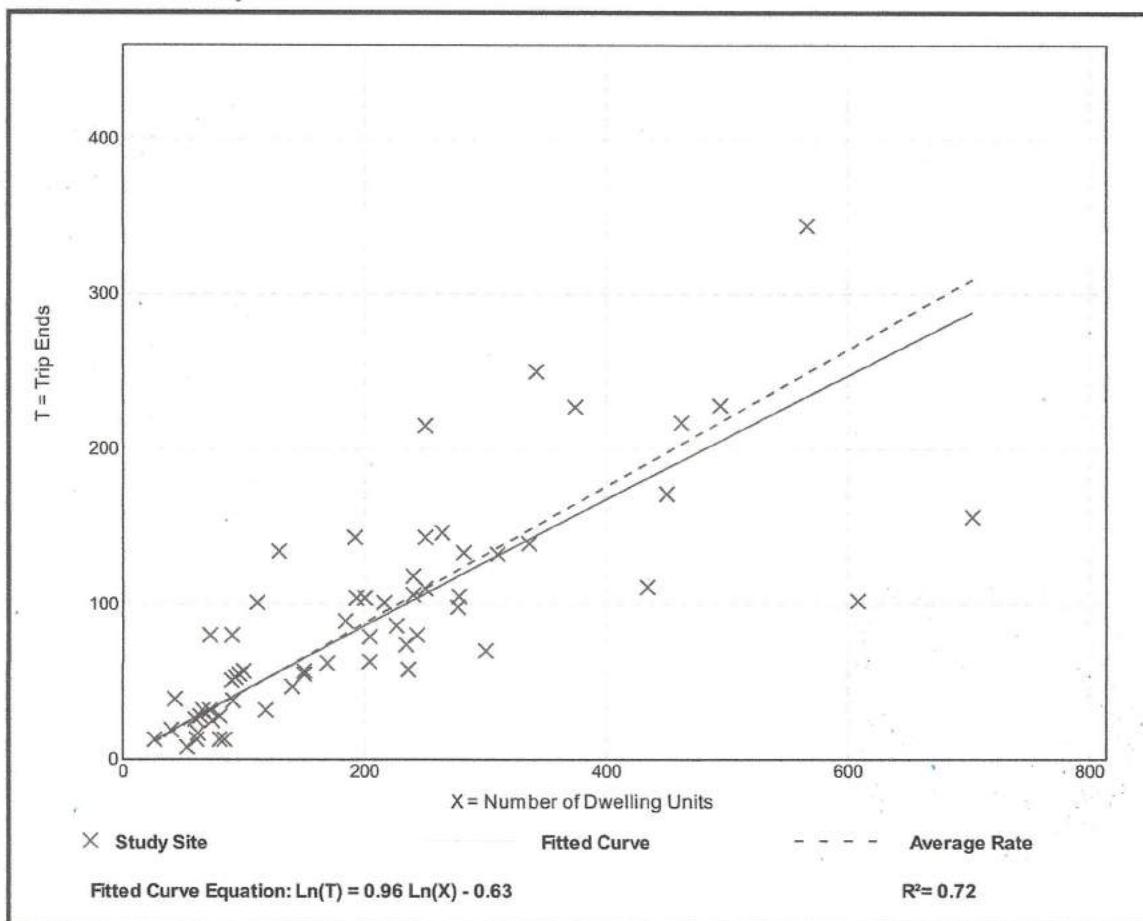
Avg. Num. of Dwelling Units: 208

Directional Distribution: 61% entering, 39% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.44	0.15 - 1.11	0.19

Data Plot and Equation



APPENDIX E

Growth Rate Analysis

FLORIDA DEPARTMENT OF TRANSPORTATION
 TRANSPORTATION STATISTICS OFFICE
 2020 HISTORICAL AADT REPORT

COUNTY: 86 - BROWARD

SITE: 0248 - SR 820/HOLLYWOOD BLVD - E OF SR 9/I-95

YEAR	AADT	DIRECTION 1	DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2020	47500 C	E 23000	W 24500	9.00	53.90	7.30
2019	55500 C	E 27500	W 28000	9.00	54.60	7.80
2018	53000 C	E 26500	W 26500	9.00	54.50	7.80
2017	42000 C	E 21000	W 21000	9.00	51.90	7.80
2016	49000 C	E 24000	W 25000	9.00	54.10	3.20
2015	43500 C	E 21000	W 22500	9.00	54.00	3.20
2014	48000 C	E 23500	W 24500	9.00	54.20	2.80
2013	46000 C	E 23000	W 23000	9.00	53.60	1.90
2012	54000 C	E 26000	W 28000	9.00	52.20	2.00
2011	49000 C	E 23500	W 25500	9.00	52.50	4.60
2010	50500 C	E 25500	W 25000	8.35	52.69	4.40
2009	51000 C	E 25000	W 26000	8.53	53.89	3.10
2008	49500 C	E 26000	W 23500	8.81	54.16	2.20
2007	50000 C	E 25500	W 24500	8.63	55.75	2.20
2006	49000 C	E 23500	W 25500	8.40	55.34	2.20
2005	46500 C	E 23000	W 23500	8.20	51.70	1.50

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

Polk Street Apartments

Hollywood, FL

Growth Rate Analysis

Site #860248 - SR 820 / Hollywood Boulevard - East of I-95

Year	Volume	Growth Rate
2014	48,000	
2019	55,500	2.95%

APPENDIX F

Future Traffic Volumes

Spreadsheets

FUTURE TURNING MOVEMENT VOLUME ANALYSIS

Polk Street and N. 26th Avenue AM Peak Hour

Description	N. 26th Avenue Northbound			N. 26th Avenue Southbound			Polk Street Eastbound			Polk Street Westbound		
	Left	Through	Right	Left	Through	Right	Left	Through	Right	Left	Through	Right
Existing Traffic (7/13/2021) Season Adjustment Factor	5 1.06	71 1.06	0 1.06	0 1.06	140 1.06	2 1.06	0 1.06	0 1.06	0 1.06	196 1.06	36 1.06	25 1.06
2021 Peak Season Traffic	5	75	0	0	148	2	0	0	0	208	38	27
Annual Growth Rate	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%
2023 Background Traffic	6	80	0	0	157	2	0	0	0	220	40	28
Polk Street Apartments	3					1						
2023 Total Traffic	9	80	0	0	157	3	0	0	0	220	40	28

FUTURE TURNING MOVEMENT VOLUME ANALYSIS

Polk Street and N. 26th Avenue PM Peak Hour

Description	N. 26th Avenue Northbound			N. 26th Avenue Southbound			Polk Street Eastbound			Polk Street Westbound		
	Left	Through	Right	Left	Through	Right	Left	Through	Right	Left	Through	Right
Existing Traffic (7/13/2021) Season Adjustment Factor	18 1.06	141 1.06	0 1.06	0 1.06	102 1.06	6 1.06	0 1.06	0 1.06	0 1.06	232 1.06	74 1.06	30 1.06
2021 Peak Season Traffic	19	149	0	0	108	6	0	0	0	246	78	32
Annual Growth Rate	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%
2023 Background Traffic	20	159	0	0	115	7	0	0	0	261	83	34
Polk Street Apartments	8				2						1	
2023 Total Traffic	28	159	0	0	115	9	0	0	0	261	84	34

FUTURE TURNING MOVEMENT VOLUME ANALYSIS

Polk Street and N. 28th Avenue AM Peak Hour

Description	N. 28th Avenue Northbound			N. 28th Avenue Southbound			Polk Street Eastbound			Polk Street Westbound		
	Left	Through	Right	Left	Through	Right	Left	Through	Right	Left	Through	Right
Existing Traffic (7/13/2021) Season Adjustment Factor	3 1.06	254 1.06	11 1.06	7 1.06	395 1.06	5 1.06	14 1.06	0 1.06	4 1.06	27 1.06	6 1.06	13 1.06
2021 Peak Season Traffic	3	269	12	7	419	5	15	0	4	29	6	14
Annual Growth Rate	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%
2023 Background Traffic	3	286	12	8	444	6	16	0	4	30	7	15
Polk Street Apartments			3		1					10		4
2023 Total Traffic	3	286	15	9	444	6	16	0	4	40	7	19

FUTURE TURNING MOVEMENT VOLUME ANALYSIS

Polk Street and N. 28th Avenue PM Peak Hour

Description	N. 28th Avenue Northbound			N. 28th Avenue Southbound			Polk Street Eastbound			Polk Street Westbound		
	Left	Through	Right	Left	Through	Right	Left	Through	Right	Left	Through	Right
Existing Traffic (7/13/2021) Season Adjustment Factor	8 1.06	342 1.06	8 1.06	5 1.06	347 1.06	7 1.06	4 1.06	3 1.06	4 1.06	47 1.06	19 1.06	23 1.06
2021 Peak Season Traffic	8	363	8	5	368	7	4	3	4	50	20	24
Annual Growth Rate	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%
2023 Background Traffic	9	385	9	6	390	8	4	3	4	53	21	26
Polk Street Apartments			11		2					7		3
2023 Total Traffic	9	385	20	8	390	8	4	3	4	60	21	29

APPENDIX G

Signal Timing Data

Station : 3165 - Hollywood Blvd & N 26 Ave/Polk St (Standard File)

Phase	1	2 (WR)	3 (SR)	4	5	6	7	8	9	10	11	12	13	14	15	16
Walk		7	7	7												
Ped Clearance		6	23	6												
Min Green		10	6	6												
Gap Ext		3	2	2												
Max1		45	25	20												
Max2																
Yellow Clr	4	4	4	4	4	4	4	4	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5
Red Clr		1	2	1					1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5
Red Revert																
Added Initial																
Max Initial																
Time Before Reduce																
Cars Before Reduce																
Time To Reduce																
Reduce By																
Min Gap																
Dynamic Max Limit																
Dynamic Max Step																
Enable	ON	ON	ON													
Auto Flash Entry				ON												
Auto Flash Exit	ON															
Non-Actuated 1						ON										
Non-Actuated 2																
Lock Call											ON	ON	ON	ON	ON	ON
Min Recall	ON															
Max Recall																
Ped Recall																
Soft Recall																
Dual Entry																
Sim Gap Enable	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON
Guar Passage																
Rest In Walk		ON														
Cond Service																
Add Init Calc																

Preemption

Channel	1	2	3	4	5	6
Lock Input	ON	ON	ON	ON	ON	ON
Override Auto Flash					ON	ON
Override Higher Preempt					ON	ON
Flash in Dwell						
Link to Preempt						
Delay						
Min Duration						
Min Green	6	6	6	6		
Min Walk						
Ped Clear						
Track Green						
Min Dwell	8	8	8	8		
Max Presence	180	180	180	180		
Track Veh 1						
Track Veh 2						
Track Veh 3						
Track Veh 4						
Dwell Cyc Veh 1	3	2		4		
Dwell Cyc Veh 2						
Dwell Cyc Veh 3						
Dwell Cyc Veh 4						
Dwell Cyc Veh 5						
Dwell Cyc Veh 6						
Dwell Cyc Veh 7						
Dwell Cyc Veh 8						
Dwell Cyc Veh 9						
Dwell Cyc Veh 10						
Dwell Cyc Veh 11						
Dwell Cyc Veh 12						
Dwell Cyc Ped1						
Dwell Cyc Ped2						
Dwell Cyc Ped3						
Dwell Cyc Ped4						
Dwell Cyc Ped5						
Dwell Cyc Ped6						
Dwell vPed7						

Preempt LP

Channel	1	2	3	4
Min				
Max				
Enable				
Lock Mode	MAX	MAX	MAX	MAX
Coord in Preempt				
No Skip				
Priority P1				
Priority P2				
Priority P3				
Priority P4				
Lock				
Headway				
Group Lock				
Queue Jump				
Free Mode				
Alt Table				

Dwell Cyc Ped8						
Exit 1	4	3		2		
Exit 2						
Exit 3						
Exit 4						

Prepared By

Date Implemented

Reviewed By

Traffic Engineer

Station : 3165 - Hollywood Blvd & N 26 Ave/Polk St (Standard File)

Coordination

Station : 3165 - Hollywood Blvd & N 26 Ave/Polk St (Standard File)

Scheduler

User Comments:



BROWARD COUNTY TRAFFIC ENGINEERING
ACTUATED TRAFFIC SIGNAL TIMING SHEET

Intersection Number	3165		Initial Operation Date	UNKNOWN				
Controller Type	2070 LN		System Number	3165				
Modification Number	12		Modification Date	05/22/2012				
Drawing/Project No	DES. GRP. 1		FPL Grid Number	87572211001				
Intersection	HOLLYWOOD BLVD. (SR 820) and N 26 AVENUE/POLK STREET							
Municipality	HOLLYWOOD							
Controller Phase	1	2	3	4	5	6	7	8
Face Number	2,2R		4,8	2A				
Direction	WB		N/S	WB PED				
Initial Green(MIN)	10		6	6				
Vehicle Ext.(GAP)	3.0		2.0	2.0				
Maximum Green I	45		25	20				
Maximum Green II								
Yellow Clearance	4.0		4.0	4.0				
All Red Clearance	1.0		2.0	1.0				
Phase Recall	MIN		OFF	OFF				
Detector Delay								
Walk	7		7	7				
Pedestrian Clearance	6		23	6				
Permissive								
Flash Operation	YELLOW		RED	YELLOW				

Attachment

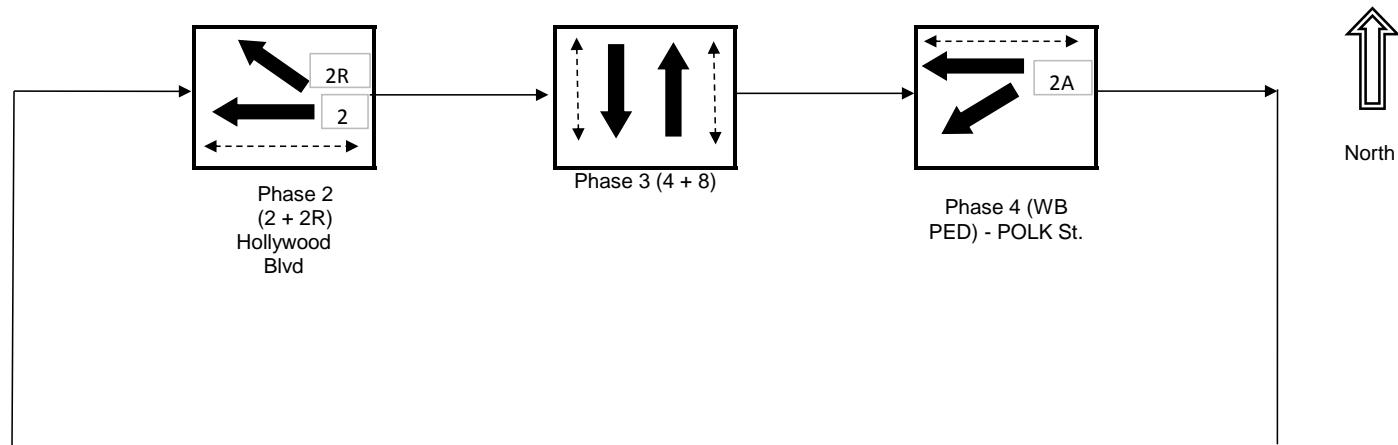
NOTES:

1. DUAL ENTRY HARDWIRED NORTH/SOUTH.
2. MOD. 12 UPDATES ATMS.NOW PARAMETERS, NO CHANGE IN TIMING.

Submitted By _____

Approved By _____

Sequence Of Operation (3165) - Hollywood Blvd and N. 26 Ave / Polk St
Hollywood



APPENDIX H

SYNCHRO Output

Existing (2021) SYNCHRO Output

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection												
Int Delay, s/veh	1.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	+	+	+	+	+	+	+	+	+	+	+	+
Traffic Vol, veh/h	15	0	4	29	6	14	3	269	12	7	419	5
Future Vol, veh/h	15	0	4	29	6	14	3	269	12	7	419	5
Conflicting Peds, #/hr	2	0	0	0	0	2	5	0	2	2	0	5
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	97	97	97	97	97	97	97	97	97	97	97	97
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	15	0	4	30	6	14	3	277	12	7	432	5
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	755	751	440	742	747	287	442	0	0	291	0	0
Stage 1	454	454	-	291	291	-	-	-	-	-	-	-
Stage 2	301	297	-	451	456	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	325	340	617	332	341	752	1118	-	-	1271	-	-
Stage 1	586	569	-	717	672	-	-	-	-	-	-	-
Stage 2	708	668	-	588	568	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	310	334	614	327	335	749	1113	-	-	1269	-	-
Mov Cap-2 Maneuver	310	334	-	327	335	-	-	-	-	-	-	-
Stage 1	582	562	-	713	669	-	-	-	-	-	-	-
Stage 2	685	665	-	580	561	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	16		15.6		0.1		0.1					
HCM LOS	C		C									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR				
Capacity (veh/h)	1113	-	-	346	391	1269	-	-				
HCM Lane V/C Ratio	0.003	-	-	0.057	0.129	0.006	-	-				
HCM Control Delay (s)	8.2	-	-	16	15.6	7.9	0	-				
HCM Lane LOS	A	-	-	C	C	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.2	0.4	0	-	-				

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations					
Traffic Volume (vph)	208	38	5	75	148
Future Volume (vph)	208	38	5	75	148
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases	2		4		
Detector Phase	2	2	4	4	8
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	20.0	20.0	20.0
Total Split (s)	40.0	40.0	20.0	20.0	20.0
Total Split (%)	66.7%	66.7%	33.3%	33.3%	33.3%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	42.7	42.7		9.7	9.7
Actuated g/C Ratio	0.71	0.71		0.16	0.16
v/c Ratio	0.18	0.06		0.30	0.55
Control Delay	5.0	3.3		23.7	29.6
Queue Delay	0.0	0.0		0.0	0.0
Total Delay	5.0	3.3		23.7	29.6
LOS	A	A		C	C
Approach Delay		4.6		23.7	29.6
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 0 (0%), Referenced to phase 2:WBT, Start of Yellow

Natural Cycle: 45

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.55

Intersection Signal Delay: 15.0

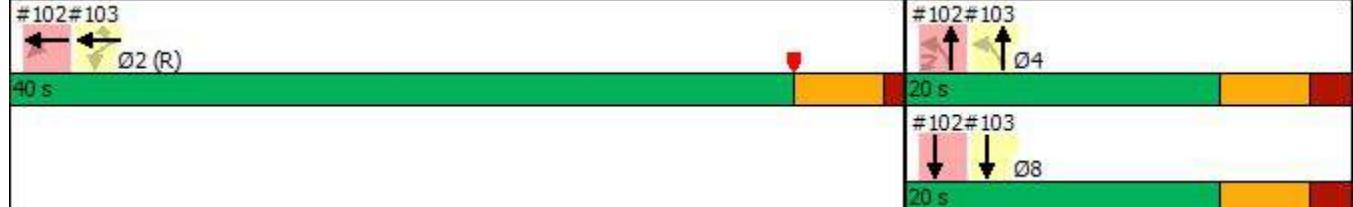
Intersection LOS: B

Intersection Capacity Utilization 29.2%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	229	72	87	165
v/c Ratio	0.18	0.06	0.30	0.55
Control Delay	5.0	3.3	23.7	29.6
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	5.0	3.3	23.7	29.6
Queue Length 50th (ft)	27	4	28	56
Queue Length 95th (ft)	63	18	58	99
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1259	1250	421	433
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.18	0.06	0.21	0.38

Intersection Summary

HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street



Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	208	38	27	5	75	148	2
Future Volume (vph)	0	208	38	27	5	75	148	2
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor	1.00	1.00			1.00	1.00		
Frpb, ped/bikes	1.00	1.00			1.00	1.00		
Flpb, ped/bikes	1.00	1.00			1.00	1.00		
Fr _t	1.00	0.94			1.00	1.00		
Fl _t Protected	0.95	1.00			1.00	1.00		
Satd. Flow (prot)	1770	1746			1857	1859		
Fl _t Permitted	0.95	1.00			0.97	1.00		
Satd. Flow (perm)	1770	1746			1805	1859		
Peak-hour factor, PHF	0.91	0.91	0.91	0.91	0.91	0.91	0.91	
Adj. Flow (vph)	0	229	42	30	5	82	163	2
RTOR Reduction (vph)	0	0	10	0	0	0	0	0
Lane Group Flow (vph)	0	229	62	0	0	87	165	0
Confl. Peds. (#/hr)								1
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2		4				
Actuated Green, G (s)	40.5	40.5			8.5	8.5		
Effective Green, g (s)	40.5	40.5			8.5	8.5		
Actuated g/C Ratio	0.68	0.68			0.14	0.14		
Clearance Time (s)	5.0	5.0			6.0	6.0		
Vehicle Extension (s)	2.0	2.0		2.0	2.0			
Lane Grp Cap (vph)	1194	1178			255	263		
v/s Ratio Prot		0.04			c0.09			
v/s Ratio Perm	c0.13				0.05			
v/c Ratio	0.19	0.05			0.34	0.63		
Uniform Delay, d1	3.6	3.3			23.2	24.3		
Progression Factor	1.00	1.00			1.00	1.00		
Incremental Delay, d2	0.4	0.1			0.3	3.3		
Delay (s)	4.0	3.4			23.5	27.6		
Level of Service	A	A			C	C		
Approach Delay (s)		3.8			23.5	27.6		
Approach LOS		A			C	C		
Intersection Summary								
HCM 2000 Control Delay	14.0				HCM 2000 Level of Service		B	
HCM 2000 Volume to Capacity ratio	0.27							
Actuated Cycle Length (s)	60.0				Sum of lost time (s)		11.0	
Intersection Capacity Utilization	29.2%				ICU Level of Service		A	
Analysis Period (min)	15							
c Critical Lane Group								

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection												
Int Delay, s/veh	2.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	+	+	+	+	+	+	+	+	+	+	+	+
Traffic Vol, veh/h	4	3	4	50	20	24	8	363	8	5	368	7
Future Vol, veh/h	4	3	4	50	20	24	8	363	8	5	368	7
Conflicting Peds, #/hr	0	0	0	0	0	0	2	0	0	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	4	3	4	53	21	26	9	386	9	5	391	7
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	839	820	397	817	819	391	400	0	0	395	0	0
Stage 1	407	407	-	409	409	-	-	-	-	-	-	-
Stage 2	432	413	-	408	410	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	285	310	652	295	310	658	1159	-	-	1164	-	-
Stage 1	621	597	-	619	596	-	-	-	-	-	-	-
Stage 2	602	594	-	620	595	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	256	305	651	288	305	658	1157	-	-	1164	-	-
Mov Cap-2 Maneuver	256	305	-	288	305	-	-	-	-	-	-	-
Stage 1	615	592	-	614	591	-	-	-	-	-	-	-
Stage 2	553	589	-	609	590	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	15.7		19.9		0.2		0.1					
HCM LOS	C		C		A		A					
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR				
Capacity (veh/h)	1157	-	-	348	341	1164	-	-				
HCM Lane V/C Ratio	0.007	-	-	0.034	0.293	0.005	-	-				
HCM Control Delay (s)	8.1	-	-	15.7	19.9	8.1	0	-				
HCM Lane LOS	A	-	-	C	C	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.1	1.2	0	-	-				

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations					
Traffic Volume (vph)	246	78	19	149	108
Future Volume (vph)	246	78	19	149	108
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases	2		4		
Detector Phase	2	2	4	4	8
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	25.0	25.0	25.0
Total Split (s)	35.0	35.0	25.0	25.0	25.0
Total Split (%)	58.3%	58.3%	41.7%	41.7%	41.7%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	38.5	38.5		10.5	10.5
Actuated g/C Ratio	0.64	0.64		0.18	0.18
v/c Ratio	0.24	0.10		0.59	0.38
Control Delay	5.9	4.1		30.2	24.2
Queue Delay	0.0	0.0		0.0	0.0
Total Delay	5.9	4.1		30.3	24.2
LOS	A	A		C	C
Approach Delay		5.4		30.3	24.2
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 42 (70%), Referenced to phase 2:WBTL, Start of Yellow

Natural Cycle: 50

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.59

Intersection Signal Delay: 15.3

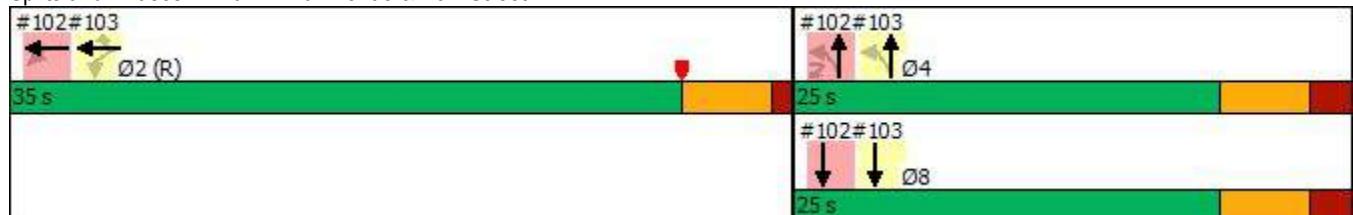
Intersection LOS: B

Intersection Capacity Utilization 38.4%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	267	120	183	124
v/c Ratio	0.24	0.10	0.59	0.38
Control Delay	5.9	4.1	30.2	24.2
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	5.9	4.1	30.3	24.2
Queue Length 50th (ft)	34	10	62	40
Queue Length 95th (ft)	78	31	106	74
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1135	1145	556	585
Starvation Cap Reductn	0	0	21	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.24	0.10	0.34	0.21

Intersection Summary

HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street



Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	246	78	32	19	149	108	6
Future Volume (vph)	0	246	78	32	19	149	108	6
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor		1.00	1.00			1.00	1.00	
Frpb, ped/bikes		1.00	0.99			1.00	1.00	
Flpb, ped/bikes		1.00	1.00			1.00	1.00	
Fr _t		1.00	0.96			1.00	0.99	
Fl _t Protected		0.95	1.00			0.99	1.00	
Satd. Flow (prot)		1770	1767			1852	1849	
Fl _t Permitted		0.95	1.00			0.94	1.00	
Satd. Flow (perm)		1770	1767			1757	1849	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	0	267	85	35	21	162	117	7
RTOR Reduction (vph)	0	0	13	0	0	0	0	0
Lane Group Flow (vph)	0	267	107	0	0	183	124	0
Confl. Peds. (#/hr)				3				
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2			4			
Actuated Green, G (s)		38.5	38.5			10.5	10.5	
Effective Green, g (s)		38.5	38.5			10.5	10.5	
Actuated g/C Ratio		0.64	0.64			0.18	0.18	
Clearance Time (s)		5.0	5.0			6.0	6.0	
Vehicle Extension (s)		2.0	2.0			2.0	2.0	
Lane Grp Cap (vph)		1135	1133			307	323	
v/s Ratio Prot			0.06				0.07	
v/s Ratio Perm		c0.15				c0.10		
v/c Ratio		0.24	0.09			0.60	0.38	
Uniform Delay, d1		4.5	4.1			22.8	21.9	
Progression Factor		1.00	1.00			1.00	1.00	
Incremental Delay, d2		0.5	0.2			2.1	0.3	
Delay (s)		5.0	4.3			24.9	22.2	
Level of Service		A	A			C	C	
Approach Delay (s)			4.8			24.9	22.2	
Approach LOS			A			C	C	
Intersection Summary								
HCM 2000 Control Delay		13.2			HCM 2000 Level of Service		B	
HCM 2000 Volume to Capacity ratio		0.31						
Actuated Cycle Length (s)		60.0			Sum of lost time (s)		11.0	
Intersection Capacity Utilization		38.4%			ICU Level of Service		A	
Analysis Period (min)		15						
c Critical Lane Group								

Future (2023) Background SYNCHRO Output

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection															
Int Delay, s/veh	1.5														
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR			
Lane Configurations	+	+	+	+	+	+	+	+	+	+	+	+			
Traffic Vol, veh/h	16	0	4	30	7	15	3	286	12	8	444	6			
Future Vol, veh/h	16	0	4	30	7	15	3	286	12	8	444	6			
Conflicting Peds, #/hr	2	0	0	0	0	2	5	0	2	2	0	5			
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free			
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None			
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-			
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-			
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-			
Peak Hour Factor	97	97	97	97	97	97	97	97	97	97	97	97			
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2			
Mvmt Flow	16	0	4	31	7	15	3	295	12	8	458	6			
Major/Minor	Minor2	Minor1			Major1			Major2							
Conflicting Flow All	802	797	466	788	794	305	469	0	0	309	0	0			
Stage 1	482	482	-	309	309	-	-	-	-	-	-	-			
Stage 2	320	315	-	479	485	-	-	-	-	-	-	-			
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-			
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-			
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-			
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-			
Pot Cap-1 Maneuver	302	319	597	309	321	735	1093	-	-	1252	-	-			
Stage 1	565	553	-	701	660	-	-	-	-	-	-	-			
Stage 2	692	656	-	568	552	-	-	-	-	-	-	-			
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-			
Mov Cap-1 Maneuver	286	313	594	303	315	732	1088	-	-	1250	-	-			
Mov Cap-2 Maneuver	286	313	-	303	315	-	-	-	-	-	-	-			
Stage 1	560	545	-	697	657	-	-	-	-	-	-	-			
Stage 2	667	653	-	559	544	-	-	-	-	-	-	-			
Approach	EB			WB			NB			SB					
HCM Control Delay, s	17.1			16.5			0.1			0.1					
HCM LOS	C			C			A			A					
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR							
Capacity (veh/h)	1088	-	-	319	367	1250	-	-							
HCM Lane V/C Ratio	0.003	-	-	0.065	0.146	0.007	-	-							
HCM Control Delay (s)	8.3	-	-	17.1	16.5	7.9	0	-							
HCM Lane LOS	A	-	-	C	C	A	A	-							
HCM 95th %tile Q(veh)	0	-	-	0.2	0.5	0	-	-							

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations					
Traffic Volume (vph)	220	40	6	80	157
Future Volume (vph)	220	40	6	80	157
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases	2		4		
Detector Phase	2	2	4	4	8
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	20.0	20.0	20.0
Total Split (s)	40.0	40.0	20.0	20.0	20.0
Total Split (%)	66.7%	66.7%	33.3%	33.3%	33.3%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	42.4	42.4		10.0	10.0
Actuated g/C Ratio	0.71	0.71		0.17	0.17
v/c Ratio	0.19	0.06		0.32	0.57
Control Delay	5.2	3.4		23.8	29.7
Queue Delay	0.0	0.0		0.0	0.0
Total Delay	5.2	3.4		23.8	29.7
LOS	A	A		C	C
Approach Delay		4.8		23.8	29.7
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 0 (0%), Referenced to phase 2:WBTL, Start of Yellow

Natural Cycle: 45

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.57

Intersection Signal Delay: 15.3

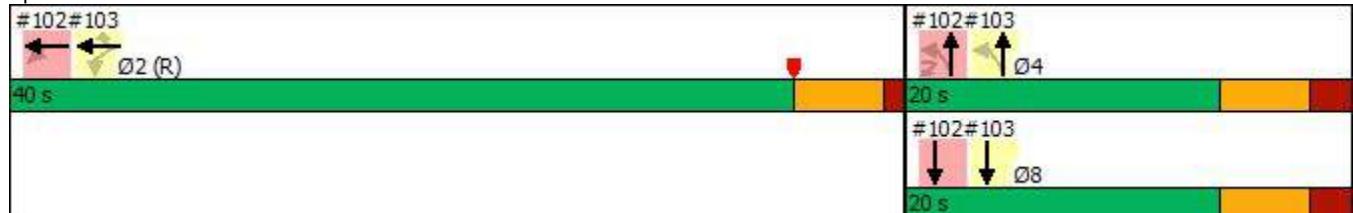
Intersection LOS: B

Intersection Capacity Utilization 30.5%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	242	75	95	175
v/c Ratio	0.19	0.06	0.32	0.57
Control Delay	5.2	3.4	23.8	29.7
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	5.2	3.4	23.8	29.7
Queue Length 50th (ft)	29	5	31	59
Queue Length 95th (ft)	68	20	62	103
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1251	1244	416	433
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.19	0.06	0.23	0.40

Intersection Summary

HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street



Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	220	40	28	6	80	157	2
Future Volume (vph)	0	220	40	28	6	80	157	2
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor		1.00	1.00			1.00	1.00	
Frpb, ped/bikes		1.00	1.00			1.00	1.00	
Flpb, ped/bikes		1.00	1.00			1.00	1.00	
Fr _t		1.00	0.94			1.00	1.00	
Fl _t Protected		0.95	1.00			1.00	1.00	
Satd. Flow (prot)		1770	1747			1856	1859	
Fl _t Permitted		0.95	1.00			0.96	1.00	
Satd. Flow (perm)		1770	1747			1787	1859	
Peak-hour factor, PHF	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Adj. Flow (vph)	0	242	44	31	7	88	173	2
RTOR Reduction (vph)	0	0	10	0	0	0	0	0
Lane Group Flow (vph)	0	242	65	0	0	95	175	0
Confl. Peds. (#/hr)								1
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2			4			
Actuated Green, G (s)		40.2	40.2			8.8	8.8	
Effective Green, g (s)		40.2	40.2			8.8	8.8	
Actuated g/C Ratio		0.67	0.67			0.15	0.15	
Clearance Time (s)		5.0	5.0			6.0	6.0	
Vehicle Extension (s)		2.0	2.0			2.0	2.0	
Lane Grp Cap (vph)		1185	1170			262	272	
v/s Ratio Prot			0.04			c0.09		
v/s Ratio Perm		c0.14				0.05		
v/c Ratio		0.20	0.06			0.36	0.64	
Uniform Delay, d1		3.8	3.4			23.1	24.1	
Progression Factor		1.00	1.00			1.00	1.00	
Incremental Delay, d2		0.4	0.1			0.3	3.9	
Delay (s)		4.2	3.5			23.4	28.0	
Level of Service		A	A			C	C	
Approach Delay (s)			4.0			23.4	28.0	
Approach LOS			A			C	C	
Intersection Summary								
HCM 2000 Control Delay		14.3			HCM 2000 Level of Service		B	
HCM 2000 Volume to Capacity ratio		0.28						
Actuated Cycle Length (s)		60.0			Sum of lost time (s)		11.0	
Intersection Capacity Utilization		30.5%			ICU Level of Service		A	
Analysis Period (min)		15						
c Critical Lane Group								

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection												
Int Delay, s/veh	2.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	+	+	+	+	+	+	+	+	+	+	+	+
Traffic Vol, veh/h	4	3	4	53	21	26	9	385	9	6	390	8
Future Vol, veh/h	4	3	4	53	21	26	9	385	9	6	390	8
Conflicting Peds, #/hr	0	0	0	0	0	0	2	0	0	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	4	3	4	56	22	28	10	410	10	6	415	9
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	894	874	422	870	873	415	426	0	0	420	0	0
Stage 1	434	434	-	435	435	-	-	-	-	-	-	-
Stage 2	460	440	-	435	438	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	262	288	632	272	289	637	1133	-	-	1139	-	-
Stage 1	600	581	-	600	580	-	-	-	-	-	-	-
Stage 2	581	578	-	600	579	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	232	283	631	265	284	637	1131	-	-	1139	-	-
Mov Cap-2 Maneuver	232	283	-	265	284	-	-	-	-	-	-	-
Stage 1	593	576	-	595	575	-	-	-	-	-	-	-
Stage 2	529	573	-	589	574	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	16.6		21.9		0.2		0.1					
HCM LOS	C		C		A		A					
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR				
Capacity (veh/h)	1131	-	-	322	318	1139	-	-				
HCM Lane V/C Ratio	0.008	-	-	0.036	0.335	0.006	-	-				
HCM Control Delay (s)	8.2	-	-	16.6	21.9	8.2	0	-				
HCM Lane LOS	A	-	-	C	C	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.1	1.4	0	-	-				

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations			20		
Traffic Volume (vph)	261	83	20	159	115
Future Volume (vph)	261	83	20	159	115
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases	2		4		
Detector Phase	2	2	4	4	8
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	25.0	25.0	25.0
Total Split (s)	35.0	35.0	25.0	25.0	25.0
Total Split (%)	58.3%	58.3%	41.7%	41.7%	41.7%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	38.1	38.1		10.9	10.9
Actuated g/C Ratio	0.64	0.64		0.18	0.18
v/c Ratio	0.25	0.11		0.61	0.40
Control Delay	6.2	4.2		30.3	24.1
Queue Delay	0.0	0.0		0.1	0.0
Total Delay	6.2	4.2		30.4	24.1
LOS	A	A		C	C
Approach Delay		5.6		30.4	24.1
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 42 (70%), Referenced to phase 2:WBTL, Start of Yellow

Natural Cycle: 50

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.61

Intersection Signal Delay: 15.5

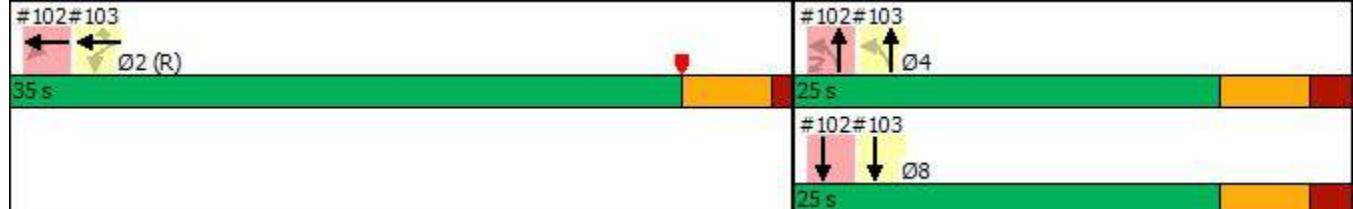
Intersection LOS: B

Intersection Capacity Utilization 44.6%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	284	127	195	133
v/c Ratio	0.25	0.11	0.61	0.40
Control Delay	6.2	4.2	30.3	24.1
Queue Delay	0.0	0.0	0.1	0.0
Total Delay	6.2	4.2	30.4	24.1
Queue Length 50th (ft)	37	11	66	43
Queue Length 95th (ft)	86	33	111	78
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1123	1135	556	585
Starvation Cap Reductn	0	0	25	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.25	0.11	0.37	0.23

Intersection Summary

HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street



Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	261	83	34	20	159	115	7
Future Volume (vph)	0	261	83	34	20	159	115	7
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor	1.00	1.00			1.00	1.00		
Frpb, ped/bikes	1.00	0.99			1.00	1.00		
Flpb, ped/bikes	1.00	1.00			1.00	1.00		
Fr _t	1.00	0.96			1.00	0.99		
Fl _t Protected	0.95	1.00			0.99	1.00		
Satd. Flow (prot)	1770	1767			1852	1848		
Fl _t Permitted	0.95	1.00			0.94	1.00		
Satd. Flow (perm)	1770	1767			1757	1848		
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	
Adj. Flow (vph)	0	284	90	37	22	173	125	8
RTOR Reduction (vph)	0	0	14	0	0	0	0	0
Lane Group Flow (vph)	0	284	113	0	0	195	133	0
Confl. Peds. (#/hr)				3				
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2			4			
Actuated Green, G (s)	38.1	38.1			10.9	10.9		
Effective Green, g (s)	38.1	38.1			10.9	10.9		
Actuated g/C Ratio	0.64	0.64			0.18	0.18		
Clearance Time (s)	5.0	5.0			6.0	6.0		
Vehicle Extension (s)	2.0	2.0			2.0	2.0		
Lane Grp Cap (vph)	1123	1122			319	335		
v/s Ratio Prot		0.06			0.07			
v/s Ratio Perm	c0.16				c0.11			
v/c Ratio	0.25	0.10			0.61	0.40		
Uniform Delay, d1	4.8	4.3			22.6	21.7		
Progression Factor	1.00	1.00			1.00	1.00		
Incremental Delay, d2	0.5	0.2			2.4	0.3		
Delay (s)	5.3	4.5			25.0	21.9		
Level of Service	A	A			C	C		
Approach Delay (s)		5.0			25.0	21.9		
Approach LOS		A			C	C		
Intersection Summary								
HCM 2000 Control Delay	13.4				HCM 2000 Level of Service	B		
HCM 2000 Volume to Capacity ratio	0.33							
Actuated Cycle Length (s)	60.0				Sum of lost time (s)	11.0		
Intersection Capacity Utilization	44.6%				ICU Level of Service	A		
Analysis Period (min)	15							
c Critical Lane Group								

Future (2023) Total SYNCHRO Output

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection												
Int Delay, s/veh	1.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	+	+	+	+	+	+	+	+	+	+	+	+
Traffic Vol, veh/h	16	0	4	41	7	20	3	286	15	9	444	6
Future Vol, veh/h	16	0	4	41	7	20	3	286	15	9	444	6
Conflicting Peds, #/hr	2	0	0	0	0	2	5	0	2	2	0	5
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	97	97	97	97	97	97	97	97	97	97	97	97
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	16	0	4	42	7	21	3	295	15	9	458	6
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	809	802	466	792	798	307	469	0	0	312	0	0
Stage 1	484	484	-	311	311	-	-	-	-	-	-	-
Stage 2	325	318	-	481	487	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	299	317	597	307	319	733	1093	-	-	1248	-	-
Stage 1	564	552	-	699	658	-	-	-	-	-	-	-
Stage 2	687	654	-	566	550	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	281	311	594	301	313	730	1088	-	-	1246	-	-
Mov Cap-2 Maneuver	281	311	-	301	313	-	-	-	-	-	-	-
Stage 1	559	544	-	696	655	-	-	-	-	-	-	-
Stage 2	657	651	-	556	542	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	17.3		17.2		0.1		0.2					
HCM LOS	C		C									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR				
Capacity (veh/h)	1088	-	-	314	366	1246	-	-				
HCM Lane V/C Ratio	0.003	-	-	0.066	0.192	0.007	-	-				
HCM Control Delay (s)	8.3	-	-	17.3	17.2	7.9	0	-				
HCM Lane LOS	A	-	-	C	C	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.2	0.7	0	-	-				

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations					
Traffic Volume (vph)	220	40	9	80	157
Future Volume (vph)	220	40	9	80	157
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases	2			4	
Detector Phase	2	2	4	4	8
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	20.0	20.0	20.0
Total Split (s)	40.0	40.0	20.0	20.0	20.0
Total Split (%)	66.7%	66.7%	33.3%	33.3%	33.3%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	42.4	42.4		10.0	10.0
Actuated g/C Ratio	0.71	0.71		0.17	0.17
v/c Ratio	0.19	0.06		0.34	0.57
Control Delay	5.2	3.4		24.1	29.7
Queue Delay	0.0	0.0		0.0	0.0
Total Delay	5.2	3.4		24.1	29.7
LOS	A	A		C	C
Approach Delay		4.8		24.1	29.7
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 0 (0%), Referenced to phase 2:WBTL, Start of Yellow

Natural Cycle: 45

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.57

Intersection Signal Delay: 15.4

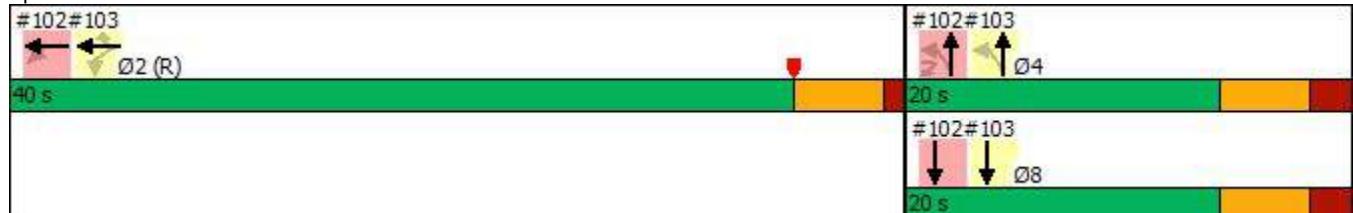
Intersection LOS: B

Intersection Capacity Utilization 33.1%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	242	75	98	176
v/c Ratio	0.19	0.06	0.34	0.57
Control Delay	5.2	3.4	24.1	29.7
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	5.2	3.4	24.1	29.7
Queue Length 50th (ft)	29	5	32	59
Queue Length 95th (ft)	68	20	63	104
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1250	1243	409	433
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.19	0.06	0.24	0.41

Intersection Summary

HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street



Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	220	40	28	9	80	157	3
Future Volume (vph)	0	220	40	28	9	80	157	3
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor	1.00	1.00			1.00	1.00		
Frpb, ped/bikes	1.00	1.00			1.00	1.00		
Flpb, ped/bikes	1.00	1.00			1.00	1.00		
Fr _t	1.00	0.94			1.00	1.00		
Fl _t Protected	0.95	1.00			0.99	1.00		
Satd. Flow (prot)	1770	1747			1853	1858		
Fl _t Permitted	0.95	1.00			0.94	1.00		
Satd. Flow (perm)	1770	1747			1755	1858		
Peak-hour factor, PHF	0.91	0.91	0.91	0.91	0.91	0.91	0.91	
Adj. Flow (vph)	0	242	44	31	10	88	173	3
RTOR Reduction (vph)	0	0	10	0	0	0	0	0
Lane Group Flow (vph)	0	242	65	0	0	98	176	0
Confl. Peds. (#/hr)								1
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2		4				
Actuated Green, G (s)	40.2	40.2			8.8	8.8		
Effective Green, g (s)	40.2	40.2			8.8	8.8		
Actuated g/C Ratio	0.67	0.67			0.15	0.15		
Clearance Time (s)	5.0	5.0			6.0	6.0		
Vehicle Extension (s)	2.0	2.0			2.0	2.0		
Lane Grp Cap (vph)	1185	1170			257	272		
v/s Ratio Prot		0.04			c0.09			
v/s Ratio Perm	c0.14				0.06			
v/c Ratio	0.20	0.06			0.38	0.65		
Uniform Delay, d1	3.8	3.4			23.1	24.1		
Progression Factor	1.00	1.00			1.00	1.00		
Incremental Delay, d2	0.4	0.1			0.3	3.9		
Delay (s)	4.2	3.5			23.5	28.1		
Level of Service	A	A			C	C		
Approach Delay (s)		4.0			23.5	28.1		
Approach LOS		A			C	C		
Intersection Summary								
HCM 2000 Control Delay	14.4				HCM 2000 Level of Service		B	
HCM 2000 Volume to Capacity ratio	0.28							
Actuated Cycle Length (s)	60.0				Sum of lost time (s)		11.0	
Intersection Capacity Utilization	33.1%				ICU Level of Service		A	
Analysis Period (min)	15							
c Critical Lane Group								

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection												
Int Delay, s/veh	3.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	+	+	+	+	+	+	+	+	+	+	+	+
Traffic Vol, veh/h	4	3	4	60	21	29	9	385	21	8	390	8
Future Vol, veh/h	4	3	4	60	21	29	9	385	21	8	390	8
Conflicting Peds, #/hr	0	0	0	0	0	0	2	0	0	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	4	3	4	64	22	31	10	410	22	9	415	9
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	908	892	422	882	885	421	426	0	0	432	0	0
Stage 1	440	440	-	441	441	-	-	-	-	-	-	-
Stage 2	468	452	-	441	444	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	256	281	632	267	284	632	1133	-	-	1128	-	-
Stage 1	596	578	-	595	577	-	-	-	-	-	-	-
Stage 2	575	570	-	595	575	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	225	275	631	259	278	632	1131	-	-	1128	-	-
Mov Cap-2 Maneuver	225	275	-	259	278	-	-	-	-	-	-	-
Stage 1	589	571	-	590	572	-	-	-	-	-	-	-
Stage 2	521	565	-	582	568	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	16.9		23.3		0.2		0.2					
HCM LOS	C		C		A		A					
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR				
Capacity (veh/h)	1131	-	-	314	312	1128	-	-				
HCM Lane V/C Ratio	0.008	-	-	0.037	0.375	0.008	-	-				
HCM Control Delay (s)	8.2	-	-	16.9	23.3	8.2	0	-				
HCM Lane LOS	A	-	-	C	C	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.1	1.7	0	-	-				

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations					
Traffic Volume (vph)	261	84	29	159	115
Future Volume (vph)	261	84	29	159	115
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases		2		4	
Detector Phase		2	2	4	4
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	25.0	25.0	25.0
Total Split (s)	35.0	35.0	25.0	25.0	25.0
Total Split (%)	58.3%	58.3%	41.7%	41.7%	41.7%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	37.6	37.6		11.4	11.4
Actuated g/C Ratio	0.63	0.63		0.19	0.19
v/c Ratio	0.26	0.11		0.63	0.39
Control Delay	6.5	4.4		30.6	23.3
Queue Delay	0.0	0.0		0.1	0.0
Total Delay	6.5	4.4		30.7	23.3
LOS	A	A		C	C
Approach Delay		5.9		30.7	23.3
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 42 (70%), Referenced to phase 2:WBTL, Start of Yellow

Natural Cycle: 50

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.63

Intersection Signal Delay: 15.8

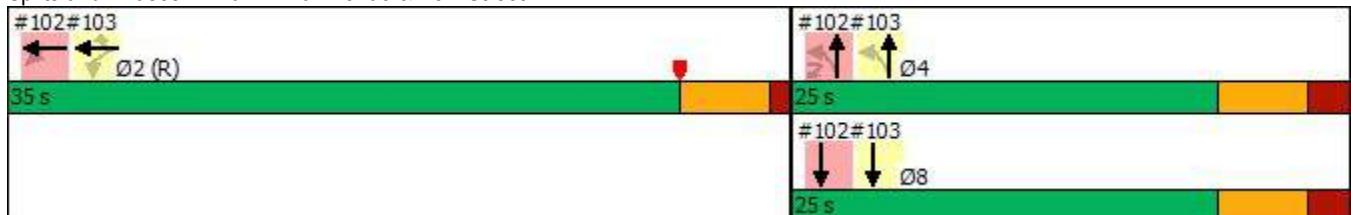
Intersection LOS: B

Intersection Capacity Utilization 45.2%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	284	128	205	135
v/c Ratio	0.26	0.11	0.63	0.39
Control Delay	6.5	4.4	30.6	23.3
Queue Delay	0.0	0.0	0.1	0.0
Total Delay	6.5	4.4	30.7	23.3
Queue Length 50th (ft)	39	11	69	43
Queue Length 95th (ft)	89	35	115	78
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1109	1122	542	583
Starvation Cap Reductn	0	0	27	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.26	0.11	0.40	0.23

Intersection Summary

HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street



Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	261	84	34	29	159	115	9
Future Volume (vph)	0	261	84	34	29	159	115	9
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor	1.00	1.00			1.00	1.00		
Frpb, ped/bikes	1.00	0.99			1.00	1.00		
Flpb, ped/bikes	1.00	1.00			1.00	1.00		
Fr _t	1.00	0.96			1.00	0.99		
Fl _t Protected	0.95	1.00			0.99	1.00		
Satd. Flow (prot)	1770	1768			1848	1844		
Fl _t Permitted	0.95	1.00			0.92	1.00		
Satd. Flow (perm)	1770	1768			1712	1844		
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	0	284	91	37	32	173	125	10
RTOR Reduction (vph)	0	0	14	0	0	0	0	0
Lane Group Flow (vph)	0	284	114	0	0	205	135	0
Confl. Peds. (#/hr)				3				
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2			4			
Actuated Green, G (s)	37.6	37.6			11.4	11.4		
Effective Green, g (s)	37.6	37.6			11.4	11.4		
Actuated g/C Ratio	0.63	0.63			0.19	0.19		
Clearance Time (s)	5.0	5.0			6.0	6.0		
Vehicle Extension (s)	2.0	2.0			2.0	2.0		
Lane Grp Cap (vph)	1109	1107			325	350		
v/s Ratio Prot		0.06			0.07			
v/s Ratio Perm	c0.16				c0.12			
v/c Ratio	0.26	0.10			0.63	0.39		
Uniform Delay, d1	5.0	4.5			22.4	21.2		
Progression Factor	1.00	1.00			1.00	1.00		
Incremental Delay, d2	0.6	0.2			2.9	0.3		
Delay (s)	5.5	4.7			25.3	21.5		
Level of Service	A	A			C	C		
Approach Delay (s)		5.3			25.3	21.5		
Approach LOS		A			C	C		
Intersection Summary								
HCM 2000 Control Delay	13.6				HCM 2000 Level of Service	B		
HCM 2000 Volume to Capacity ratio	0.34							
Actuated Cycle Length (s)	60.0				Sum of lost time (s)	11.0		
Intersection Capacity Utilization	45.2%				ICU Level of Service	A		
Analysis Period (min)	15							
c Critical Lane Group								

APPENDIX I

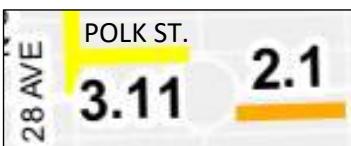
Polk Street Evaluation

SEGMENT DESCRIPTION

Date Prepared:	3/11/2021	Segment ID No.:	3.11	Speed Limit:	30 mph by State statute
STREET:	Polk St.	Traffic Count ID No.:	3.11.1 043	Posted:	No
From:	N. 26th Ave.	Jurisdiction:	City	Existing Daily Volume (Raw):	1,116
To:	N 28th Ave.	Functional Class:	Local	85th %ile Speed:	38
Length:	1,365 feet	Existing TC Devices:	No	Warrant Score:	15.9

Segment Map:

Segment lies just northwest of City Hall circle.
Traffic count was taken midblock.



Existing Intersection Controls:

West end: 2-way STOP on Polk St.
East end: Traffic signal at N. 28th Ave.

Legend

Intersection Control Type

- 2 way stop EW
- 2 way stop NS
- 3 way Stop
- 4 way stop
- Dead-End Treatment
- Other
- Stop Sign
- Traffic Circle Stop
- Traffic Circle Yield
- Traffic Signal
- Yield



Existing Traffic Calming Devices:

None on Polk St.
Speed humps on Taylor St. one block to the north are at 300 feet/500 feet/520 feet spacings.

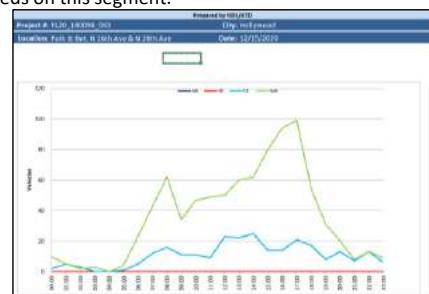
Typical Segment Image:



Midblock looking east

TRAFFIC CALMING ANALYSIS NOTES

1. Citizen comment: Need speed bumps 77% of traffic is westbound, as eastbound traffic is directed to westbound on City Hall circle at N. 26th Ave.
2. After review of options, speed tables are the practical approach to moderate the excessive travel speeds on this segment.
3. It is considered that 2 speed tables, as on Taylor St. to the north, would be sufficient.



RECOMMENDED ACTION

1. Install new standard flattop speed table 440 feet west of brick crosswalk at N. 26th Ave.
2. Install new speed table approx. 410 feet east of centerline of N. 28th Avenue.

These recommendations will place traffic calming devices at 410 feet/515 feet/440 feet spacings along this segment from west to east.



APPENDIX J

Polk Street – Speed and Volume Data

Traffic Survey Specialists, Inc.
Daily Eastbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
00:00 - 00:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15 - 00:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:30 - 00:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:45 - 00:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:45 - 01:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 - 02:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45 - 03:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30 - 04:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30 - 05:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15 - 06:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:30 - 06:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:45 - 06:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 - 07:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:15 - 07:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30 - 07:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:45 - 07:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 - 08:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 - 08:29	0	2	0	0	0	0	0	0	0	0	0	0	0	2
08:30 - 08:44	0	0	0	2	1	0	0	0	0	0	0	0	0	3
08:45 - 08:59	0	0	0	0	2	0	0	0	0	0	0	0	0	2
09:00 - 09:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:15 - 09:29	0	0	1	1	1	0	0	0	0	0	0	0	0	3
09:30 - 09:44	0	0	2	0	1	0	0	0	0	0	0	0	0	3
09:45 - 09:59	0	0	1	2	1	0	0	0	0	0	0	0	0	4
10:00 - 10:14	0	0	0	0	0	1	0	0	0	0	0	0	0	1
10:15 - 10:29	0	1	1	0	2	0	0	0	0	0	0	0	0	4
10:30 - 10:44	0	1	0	1	1	0	0	0	0	0	0	0	0	3
10:45 - 10:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 - 11:14	0	0	2	0	0	0	0	0	1	0	0	0	0	3
11:15 - 11:29	0	0	1	2	0	0	0	0	0	0	0	0	0	3
11:30 - 11:44	0	1	0	0	1	0	0	0	1	0	0	0	0	3
11:45 - 11:59	0	1	0	0	0	1	0	0	0	0	0	0	0	2
12:00 - 12:14	0	0	1	1	0	0	0	0	0	0	0	0	0	2
12:15 - 12:29	0	2	2	1	2	1	0	0	0	0	0	0	0	8
12:30 - 12:44	0	0	1	0	2	0	0	0	0	0	0	0	0	3

Traffic Survey Specialists, Inc.
Daily Eastbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
12:45 - 12:59	0	2	0	0	1	0	0	0	0	0	0	0	0	3
13:00 - 13:14	0	0	3	1	1	0	0	0	0	0	0	0	0	5
13:15 - 13:29	0	1	2	1	0	0	0	0	0	0	0	0	0	4
13:30 - 13:44	1	0	0	0	0	1	1	0	0	0	0	0	0	3
13:45 - 13:59	2	0	1	0	0	0	0	0	0	0	0	0	0	3
14:00 - 14:14	0	0	1	1	0	1	1	0	0	0	0	0	0	4
14:15 - 14:29	0	1	1	1	1	0	0	0	0	0	0	0	0	4
14:30 - 14:44	0	0	3	2	0	0	0	0	0	0	0	0	0	5
14:45 - 14:59	0	0	0	1	1	1	0	0	0	0	0	0	0	3
15:00 - 15:14	0	0	0	3	0	0	1	0	0	0	0	0	0	4
15:15 - 15:29	0	0	0	0	2	0	0	0	0	0	0	0	0	2
15:30 - 15:44	0	1	0	1	0	1	0	0	0	0	0	0	0	3
15:45 - 15:59	0	0	1	1	0	1	0	0	0	0	0	0	0	3
16:00 - 16:14	0	1	0	0	0	0	0	0	0	0	0	0	0	1
16:15 - 16:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:30 - 16:44	0	0	2	1	0	0	0	0	0	0	0	0	0	3
16:45 - 16:59	0	1	0	1	1	0	0	0	0	0	0	0	0	3
17:00 - 17:14	0	0	2	0	0	0	0	0	0	0	0	0	0	2
17:15 - 17:29	0	0	1	1	1	0	0	0	0	0	0	0	0	3
17:30 - 17:44	1	0	0	0	0	0	0	0	0	0	0	0	0	1
17:45 - 17:59	0	0	2	2	1	0	0	0	0	0	0	0	0	5
18:00 - 18:14	0	0	0	0	0	1	0	0	0	0	0	0	0	1
18:15 - 18:29	0	0	1	0	1	0	1	1	0	0	0	0	0	4
18:30 - 18:44	0	1	1	0	0	0	0	0	0	0	0	0	0	2
18:45 - 18:59	0	0	0	1	1	1	0	0	0	0	0	0	0	3
19:00 - 19:14	0	1	0	0	0	1	0	0	0	0	0	0	0	2
19:15 - 19:29	1	1	3	0	0	0	0	0	0	0	0	0	0	5
19:30 - 19:44	0	1	0	1	0	1	0	0	0	0	0	0	0	3
19:45 - 19:59	0	0	3	1	1	0	0	0	0	0	0	0	0	5
20:00 - 20:14	0	0	0	2	0	0	1	0	0	0	0	0	0	3
20:15 - 20:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
20:30 - 20:44	0	0	0	3	0	1	0	0	1	0	0	0	0	5
20:45 - 20:59	1	0	3	1	1	0	0	0	0	0	0	0	0	6
21:00 - 21:14	0	1	0	1	0	0	0	0	0	0	0	0	0	2
21:15 - 21:29	0	2	0	1	0	0	0	0	0	0	0	0	0	3
21:30 - 21:44	0	0	0	2	1	1	1	0	0	0	0	0	0	5
21:45 - 21:59	0	0	0	0	0	1	0	0	0	0	0	0	0	1
22:00 - 22:14	1	0	0	0	0	0	0	0	0	0	0	0	0	1
22:15 - 22:29	1	0	4	0	1	1	0	0	0	0	0	0	0	7
22:30 - 22:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:45 - 22:59	0	0	0	0	1	1	0	0	0	0	0	0	0	2
23:00 - 23:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:15 - 23:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
23:30 - 23:44	0	0	0	2	0	1	0	0	0	0	0	0	0	3
23:45 - 23:59	1	1	0	0	0	0	0	0	0	0	0	0	0	2
Totals	9	23	47	42	30	18	6	1	3	0	0	0	0	179
Percent of Total	5.0	12.8	26.3	23.5	16.8	10.1	3.4	0.6	1.7	0.0	0.0	0.0	0.0	100
Percent of AM	0.0	16.7	22.2	22.2	27.8	5.6	0.0	0.0	5.6	0.0	0.0	0.0	0.0	100
Percent of PM	6.3	11.9	27.3	23.8	14.0	11.2	4.2	0.7	0.7	0.0	0.0	0.0	0.0	100

Traffic Survey Specialists, Inc.
Daily Eastbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	8.7 MPH	Ten Mile Pace:	20 to 29 MPH	85th Percentile:	35.2 MPH
Mean Speed:	27.3 MPH	Percent in Ten Mile Pace:	49.7%	15th Percentile:	19.5 MPH
Median Speed:	26.4 MPH			90th Percentile:	38.2 MPH
Modal Speed:	23.0 MPH			95th Percentile:	40.5 MPH

Traffic Survey Specialists, Inc.
Daily Eastbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
00:00 - 00:14	0	0	0	0	1	0	0	0	0	0	0	0	0	1
00:15 - 00:29	0	0	0	0	0	0	0	1	0	0	0	0	0	1
00:30 - 00:44	0	1	0	0	0	1	0	0	0	1	0	0	0	3
00:45 - 00:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
01:00 - 01:14	0	1	0	1	0	0	0	0	0	0	0	0	0	2
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	0	0	1	0	0	0	0	0	0	0	0	0	0	1
01:45 - 01:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 - 02:14	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:30 - 03:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45 - 03:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30 - 04:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30 - 05:44	1	0	0	0	0	0	1	0	0	0	0	0	0	2
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15 - 06:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:30 - 06:44	0	0	2	0	0	0	0	0	0	0	0	0	0	2
06:45 - 06:59	1	0	0	1	1	0	0	0	0	0	0	0	0	3
07:00 - 07:14	0	0	0	2	0	0	0	0	0	0	0	0	0	2
07:15 - 07:29	0	0	0	0	0	1	0	0	0	0	0	0	0	1
07:30 - 07:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:45 - 07:59	0	0	2	1	0	0	1	0	0	0	0	0	0	4
08:00 - 08:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 - 08:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
08:30 - 08:44	0	0	0	1	4	3	0	0	0	0	0	0	0	8
08:45 - 08:59	0	0	0	0	1	1	0	0	0	0	0	0	0	2
09:00 - 09:14	0	0	0	0	2	0	0	0	0	0	0	0	0	2
09:15 - 09:29	1	0	2	0	1	0	0	0	0	0	0	0	0	4
09:30 - 09:44	0	0	0	2	1	0	0	0	0	0	0	0	0	3
09:45 - 09:59	1	0	1	1	1	0	0	0	0	0	0	0	0	4
10:00 - 10:14	0	0	2	1	2	1	0	0	0	0	0	0	0	6
10:15 - 10:29	0	3	1	0	0	1	1	0	0	0	0	0	0	6
10:30 - 10:44	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:45 - 10:59	1	0	1	0	0	0	0	1	0	0	0	0	0	3
11:00 - 11:14	0	0	0	0	1	0	0	0	0	0	0	0	0	1
11:15 - 11:29	1	0	5	1	1	0	0	0	0	0	0	0	0	8
11:30 - 11:44	1	1	2	1	0	0	0	0	0	1	0	0	0	6
11:45 - 11:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
12:00 - 12:14	1	0	0	0	0	0	0	0	0	0	0	0	0	1
12:15 - 12:29	0	0	1	0	1	0	0	0	0	0	0	0	0	2
12:30 - 12:44	0	2	1	0	0	0	1	0	0	0	0	0	0	4

Traffic Survey Specialists, Inc.
Daily Eastbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
12:45 - 12:59	0	1	1	0	0	0	0	0	0	0	0	0	0	2
13:00 - 13:14	0	0	1	2	0	0	0	0	0	0	0	0	0	3
13:15 - 13:29	0	0	1	1	1	0	0	0	0	0	0	0	0	3
13:30 - 13:44	0	1	1	0	0	0	1	0	0	0	0	0	0	3
13:45 - 13:59	1	0	2	3	1	0	0	0	0	0	0	0	0	7
14:00 - 14:14	0	1	3	2	0	0	0	0	0	0	0	0	0	6
14:15 - 14:29	0	2	0	1	1	1	0	0	0	0	0	0	0	5
14:30 - 14:44	0	0	2	0	1	0	0	0	0	0	0	0	0	3
14:45 - 14:59	1	1	1	1	0	1	1	0	0	0	0	0	0	6
15:00 - 15:14	0	0	1	1	0	0	0	0	0	0	0	0	0	2
15:15 - 15:29	0	2	1	0	0	1	0	0	0	0	0	0	0	4
15:30 - 15:44	0	2	0	0	0	1	1	0	0	0	0	0	0	4
15:45 - 15:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
16:00 - 16:14	0	2	1	0	1	0	0	0	0	0	0	0	0	4
16:15 - 16:29	1	1	1	1	0	1	0	0	0	0	0	0	0	5
16:30 - 16:44	0	0	0	2	0	0	0	0	0	0	0	0	0	2
16:45 - 16:59	1	1	0	1	1	0	0	0	0	0	0	0	0	4
17:00 - 17:14	1	1	0	1	0	1	0	0	0	0	0	0	0	4
17:15 - 17:29	2	1	1	1	0	1	1	0	0	0	0	0	0	7
17:30 - 17:44	0	0	2	0	1	0	0	0	0	0	0	0	0	3
17:45 - 17:59	1	0	1	0	3	1	0	0	0	0	0	0	0	6
18:00 - 18:14	0	1	0	0	1	0	0	0	0	0	0	0	0	2
18:15 - 18:29	0	0	0	2	0	0	0	0	0	0	0	0	0	2
18:30 - 18:44	0	0	0	2	1	2	0	0	0	0	0	0	0	5
18:45 - 18:59	0	1	1	1	2	0	0	0	0	0	0	0	0	5
19:00 - 19:14	0	2	2	0	0	1	0	0	0	0	0	0	0	5
19:15 - 19:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
19:30 - 19:44	1	0	1	0	0	0	0	1	0	0	0	0	0	3
19:45 - 19:59	0	0	2	2	1	1	0	0	0	0	0	0	0	6
20:00 - 20:14	0	0	1	1	0	0	0	0	0	0	0	0	0	2
20:15 - 20:29	0	0	1	1	2	0	0	0	0	0	0	0	0	4
20:30 - 20:44	0	1	1	0	0	0	0	0	0	0	0	0	0	2
20:45 - 20:59	0	0	0	1	0	1	0	0	0	0	0	0	0	2
21:00 - 21:14	0	0	1	1	0	0	0	0	0	1	0	0	0	3
21:15 - 21:29	0	1	0	0	1	0	0	0	0	0	0	0	0	2
21:30 - 21:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
21:45 - 21:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:00 - 22:14	0	0	0	0	0	1	0	0	0	0	0	0	0	1
22:15 - 22:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
22:30 - 22:44	0	1	3	1	0	0	0	0	0	0	0	0	0	5
22:45 - 22:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
23:00 - 23:14	0	0	1	1	1	0	0	0	0	0	0	0	0	3
23:15 - 23:29	0	1	0	0	0	0	0	0	0	0	0	0	0	1
23:30 - 23:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:45 - 23:59	0	0	0	0	2	0	0	0	0	0	0	0	0	2
Totals	19	34	57	45	39	22	8	3	0	3	0	0	0	230
Percent of Total	8.3	14.8	24.8	19.6	17.0	9.6	3.5	1.3	0.0	1.3	0.0	0.0	0.0	100
Percent of AM	11.0	9.8	24.4	17.1	19.5	9.8	3.7	2.4	0.0	2.4	0.0	0.0	0.0	100
Percent of PM	6.8	17.6	25.0	20.9	15.5	9.5	3.4	0.7	0.0	0.7	0.0	0.0	0.0	100

Traffic Survey Specialists, Inc.
Daily Eastbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.7 MPH	Ten Mile Pace:	20 to 29 MPH	85th Percentile:	35.3 MPH
Mean Speed:	26.6 MPH	Percent in Ten Mile Pace:	44.3%	15th Percentile:	17.6 MPH
Median Speed:	25.6 MPH			90th Percentile:	38.4 MPH
Modal Speed:	23.0 MPH			95th Percentile:	41.5 MPH

Traffic Survey Specialists, Inc.
Daily Eastbound Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
00:00 - 00:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15 - 00:29	0	1	0	0	0	0	0	0	0	0	0	0	0	1
00:30 - 00:44	0	0	0	0	0	0	0	1	0	0	0	0	0	1
00:45 - 00:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	0	0	0	0	1	0	0	0	0	0	0	0	0	1
01:45 - 01:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
02:00 - 02:14	0	1	0	0	0	0	0	0	0	0	0	0	0	1
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:45 - 02:59	0	0	0	0	0	0	1	0	0	0	0	0	0	1
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45 - 03:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:30 - 04:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30 - 05:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15 - 06:29	0	0	1	1	0	0	0	0	0	0	0	0	0	2
06:30 - 06:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:45 - 06:59	0	0	0	2	0	0	0	0	0	0	0	0	0	2
07:00 - 07:14	0	0	1	0	1	1	0	0	0	0	0	0	0	3
07:15 - 07:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30 - 07:44	0	0	0	3	0	0	0	0	0	0	0	0	0	3
07:45 - 07:59	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 - 08:14	0	1	0	0	1	0	0	0	0	0	0	0	0	2
08:15 - 08:29	1	2	0	0	1	0	0	0	0	0	0	0	0	4
08:30 - 08:44	0	0	1	2	1	1	0	0	0	0	0	0	0	5
08:45 - 08:59	0	0	1	1	1	0	2	0	0	0	0	0	0	5
09:00 - 09:14	0	0	0	1	1	0	0	0	0	0	0	0	0	2
09:15 - 09:29	1	1	0	1	0	0	0	0	0	0	0	0	0	3
09:30 - 09:44	0	0	1	1	1	0	0	0	0	0	0	0	0	3
09:45 - 09:59	0	1	1	1	1	0	0	0	0	0	0	0	0	4
10:00 - 10:14	0	0	2	2	1	3	0	0	0	0	0	0	0	8
10:15 - 10:29	1	0	0	0	0	1	0	0	0	0	0	0	0	2
10:30 - 10:44	2	0	2	0	4	0	0	0	0	0	0	0	0	8
10:45 - 10:59	1	1	1	1	0	1	0	0	0	0	0	0	0	5
11:00 - 11:14	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:15 - 11:29	0	0	2	2	1	0	0	0	0	0	0	0	0	5
11:30 - 11:44	2	0	2	2	1	0	0	0	0	0	0	0	0	7
11:45 - 11:59	0	0	3	2	0	0	0	0	0	0	0	0	0	5
12:00 - 12:14	0	0	1	0	0	1	2	0	0	0	0	0	0	4
12:15 - 12:29	2	0	1	1	0	0	0	2	0	0	0	0	0	6
12:30 - 12:44	1	0	0	1	0	0	0	0	0	0	0	0	0	2

Traffic Survey Specialists, Inc.
Daily Eastbound Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
12:45 - 12:59	0	0	0	1	1	0	0	0	0	0	0	0	0	2
13:00 - 13:14	0	0	1	3	0	0	0	0	0	0	0	0	0	4
13:15 - 13:29	1	0	1	0	2	0	0	0	0	0	0	0	0	4
13:30 - 13:44	0	0	1	2	0	1	1	0	0	0	0	0	0	5
13:45 - 13:59	1	0	0	1	0	1	0	0	0	0	0	0	0	3
14:00 - 14:14	0	0	3	0	3	0	0	0	0	0	0	0	0	6
14:15 - 14:29	1	1	0	2	1	1	0	0	0	0	0	0	0	6
14:30 - 14:44	0	2	3	0	1	0	1	0	0	0	0	0	0	7
14:45 - 14:59	0	0	1	2	0	0	0	0	0	0	0	0	0	3
15:00 - 15:14	0	0	2	0	0	1	0	0	0	0	0	0	0	3
15:15 - 15:29	0	0	1	0	0	2	0	0	0	0	0	0	0	3
15:30 - 15:44	0	0	1	2	0	0	0	0	0	0	0	0	0	3
15:45 - 15:59	1	0	1	1	0	0	0	0	0	0	0	0	0	3
16:00 - 16:14	0	0	1	0	0	0	0	0	0	0	0	0	0	1
16:15 - 16:29	0	1	0	1	0	0	0	0	0	0	0	0	0	2
16:30 - 16:44	0	0	0	2	1	1	0	0	0	0	0	0	0	4
16:45 - 16:59	0	0	2	0	0	0	0	0	0	0	0	0	0	2
17:00 - 17:14	0	1	1	0	0	0	1	1	0	0	0	0	0	4
17:15 - 17:29	1	1	1	1	0	0	0	0	0	0	0	0	0	4
17:30 - 17:44	0	0	1	0	0	0	0	0	0	0	0	0	0	1
17:45 - 17:59	0	0	0	2	0	0	0	0	0	0	0	0	0	2
18:00 - 18:14	0	1	1	2	0	0	0	0	0	0	0	0	0	4
18:15 - 18:29	0	0	0	1	1	0	0	0	0	0	0	0	0	2
18:30 - 18:44	0	1	2	0	0	2	0	0	0	0	0	0	0	5
18:45 - 18:59	1	1	0	0	0	0	2	0	0	0	0	0	0	4
19:00 - 19:14	0	0	0	1	1	0	0	0	0	0	0	0	0	2
19:15 - 19:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
19:30 - 19:44	0	1	0	2	0	2	0	0	0	0	0	0	0	5
19:45 - 19:59	0	0	0	2	1	1	0	0	0	0	0	0	0	4
20:00 - 20:14	0	0	0	0	1	0	0	0	0	0	0	0	0	1
20:15 - 20:29	0	0	1	0	0	0	1	0	0	0	0	0	0	2
20:30 - 20:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
20:45 - 20:59	0	1	0	1	1	1	0	0	0	0	0	0	0	4
21:00 - 21:14	1	0	3	0	0	0	0	0	0	0	0	0	0	4
21:15 - 21:29	0	0	0	1	1	0	0	0	0	0	0	0	0	2
21:30 - 21:44	0	0	3	1	1	0	0	0	0	0	0	0	0	5
21:45 - 21:59	1	1	0	0	2	0	0	0	0	0	0	0	0	4
22:00 - 22:14	1	1	0	1	0	0	0	0	0	0	0	0	0	3
22:15 - 22:29	0	1	0	0	1	0	0	0	0	0	0	0	0	2
22:30 - 22:44	0	0	0	1	1	0	1	0	0	0	0	0	0	3
22:45 - 22:59	0	0	3	1	1	0	0	0	0	0	0	0	0	5
23:00 - 23:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:15 - 23:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
23:30 - 23:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:45 - 23:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
Totals	23	24	55	59	38	21	14	2	0	0	0	0	0	236
Percent of Total	9.7	10.2	23.3	25.0	16.1	8.9	5.9	0.8	0.0	0.0	0.0	0.0	0.0	100
Percent of AM	12.4	11.2	20.2	25.8	18.0	7.9	3.4	1.1	0.0	0.0	0.0	0.0	0.0	100
Percent of PM	8.2	9.5	25.2	24.5	15.0	9.5	7.5	0.7	0.0	0.0	0.0	0.0	0.0	100

Traffic Survey Specialists, Inc.
Daily Eastbound Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.2 MPH	Ten Mile Pace:	20 to 29 MPH	85th Percentile:	35.3 MPH
Mean Speed:	26.7 MPH	Percent in Ten Mile Pace:	48.3%	15th Percentile:	18.0 MPH
Median Speed:	26.6 MPH			90th Percentile:	38.7 MPH
Modal Speed:	28.0 MPH			95th Percentile:	41.6 MPH

Traffic Survey Specialists, Inc.
Daily Westbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
00:00 - 00:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15 - 00:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:30 - 00:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:45 - 00:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:45 - 01:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 - 02:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45 - 03:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30 - 04:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30 - 05:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15 - 06:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:30 - 06:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:45 - 06:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 - 07:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:15 - 07:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30 - 07:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:45 - 07:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 - 08:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 - 08:29	0	1	1	0	0	0	2	0	0	0	0	0	0	4
08:30 - 08:44	0	0	1	0	4	1	0	0	0	0	0	0	0	6
08:45 - 08:59	0	0	1	0	0	1	0	0	0	0	0	0	0	2
09:00 - 09:14	0	1	1	0	1	1	1	0	0	0	0	0	0	5
09:15 - 09:29	0	1	0	0	3	3	1	0	0	0	0	0	0	8
09:30 - 09:44	0	1	0	1	1	3	2	1	0	0	0	0	0	9
09:45 - 09:59	0	1	0	0	2	2	0	0	0	0	0	0	0	5
10:00 - 10:14	0	1	2	2	3	2	0	0	0	0	0	0	0	10
10:15 - 10:29	0	0	1	0	6	2	0	1	0	0	0	0	0	10
10:30 - 10:44	2	0	0	1	2	1	0	1	0	0	0	0	0	7
10:45 - 10:59	1	1	1	0	2	2	3	0	0	0	0	0	0	10
11:00 - 11:14	0	0	1	1	4	0	1	0	0	0	0	0	0	7
11:15 - 11:29	0	0	2	4	2	0	0	1	0	0	0	0	0	9
11:30 - 11:44	0	1	0	1	6	3	0	1	0	0	0	0	0	12
11:45 - 11:59	0	0	1	1	3	0	0	0	0	0	0	0	0	5
12:00 - 12:14	0	0	0	1	4	1	1	0	0	0	0	0	0	7
12:15 - 12:29	0	0	1	2	3	2	1	0	0	0	0	0	0	9
12:30 - 12:44	0	1	0	1	4	3	1	0	0	0	0	0	0	10

Traffic Survey Specialists, Inc.
Daily Westbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
12:45 - 12:59	0	0	2	0	0	0	1	0	0	0	0	0	0	3
13:00 - 13:14	0	0	2	4	3	1	2	0	0	0	0	0	0	12
13:15 - 13:29	0	1	0	1	1	3	0	1	0	0	0	0	0	7
13:30 - 13:44	0	0	0	0	1	0	0	0	0	0	0	0	0	1
13:45 - 13:59	0	1	0	1	1	4	0	0	0	0	0	0	0	7
14:00 - 14:14	0	2	0	0	2	3	1	1	3	0	0	0	0	12
14:15 - 14:29	0	1	0	3	1	5	0	0	0	0	0	0	0	10
14:30 - 14:44	0	2	2	4	2	2	1	0	0	0	0	0	0	13
14:45 - 14:59	0	0	0	1	1	2	1	0	0	0	0	0	0	5
15:00 - 15:14	0	1	1	2	2	5	0	0	0	0	0	0	0	11
15:15 - 15:29	0	0	0	2	1	1	1	1	0	0	0	0	0	6
15:30 - 15:44	0	0	1	0	3	1	1	0	1	0	0	0	0	7
15:45 - 15:59	0	0	0	4	0	2	1	1	0	0	0	0	0	8
16:00 - 16:14	1	0	0	0	4	1	0	0	0	0	0	0	0	6
16:15 - 16:29	0	0	3	4	4	3	2	0	0	0	0	0	0	16
16:30 - 16:44	0	0	1	2	3	2	1	0	0	0	0	0	0	9
16:45 - 16:59	0	0	4	4	3	4	0	0	0	0	0	0	0	15
17:00 - 17:14	0	1	0	4	3	1	0	0	0	0	0	0	0	9
17:15 - 17:29	0	0	3	0	4	2	0	0	0	0	0	0	0	9
17:30 - 17:44	1	2	0	2	2	1	0	0	0	1	0	0	0	9
17:45 - 17:59	0	0	1	1	5	2	1	0	0	0	0	0	0	10
18:00 - 18:14	0	0	0	1	4	3	3	1	0	0	0	0	0	12
18:15 - 18:29	0	0	1	5	1	1	1	1	0	1	0	0	0	11
18:30 - 18:44	0	1	1	3	3	0	0	0	0	0	0	0	0	8
18:45 - 18:59	0	1	2	4	2	4	1	1	1	0	0	0	0	16
19:00 - 19:14	0	0	3	2	2	3	1	0	0	0	0	0	0	11
19:15 - 19:29	2	1	3	1	3	1	1	0	0	0	0	0	0	12
19:30 - 19:44	0	1	0	1	4	1	1	0	0	0	0	0	0	8
19:45 - 19:59	0	1	1	1	4	2	1	0	0	0	0	0	0	10
20:00 - 20:14	1	0	1	2	2	2	1	0	0	0	0	0	0	9
20:15 - 20:29	0	0	1	1	2	5	1	0	0	0	0	0	0	10
20:30 - 20:44	0	0	1	2	1	1	0	1	0	0	0	0	0	6
20:45 - 20:59	0	0	2	1	3	0	0	0	0	0	0	0	0	6
21:00 - 21:14	2	1	3	2	0	0	0	1	0	0	0	0	0	9
21:15 - 21:29	1	0	1	1	3	0	0	0	0	0	0	0	0	6
21:30 - 21:44	0	0	1	0	1	0	2	0	0	0	1	0	0	5
21:45 - 21:59	0	1	1	1	2	1	0	1	0	0	0	0	0	7
22:00 - 22:14	1	0	0	2	2	0	1	0	0	0	0	0	0	6
22:15 - 22:29	1	0	1	2	0	0	0	0	0	0	0	0	0	4
22:30 - 22:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
22:45 - 22:59	0	1	0	2	4	0	0	0	0	0	0	0	0	7
23:00 - 23:14	0	0	1	1	0	0	0	0	1	0	0	0	0	3
23:15 - 23:29	0	1	0	2	0	1	0	0	0	0	0	0	0	4
23:30 - 23:44	0	0	1	0	1	1	0	0	0	0	0	0	0	3
23:45 - 23:59	0	0	1	2	0	1	0	0	0	0	0	0	0	4
Totals	13	30	59	93	140	99	40	15	6	2	1	0	0	498
Percent of Total	2.6	6.0	11.8	18.7	28.1	19.9	8.0	3.0	1.2	0.4	0.2	0.0	0.0	100
Percent of AM	2.8	7.3	11.0	10.1	35.8	19.3	9.2	4.6	0.0	0.0	0.0	0.0	0.0	100
Percent of PM	2.6	5.7	12.1	21.1	26.0	20.1	7.7	2.6	1.5	0.5	0.3	0.0	0.0	100

Traffic Survey Specialists, Inc.
Daily Westbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	8.8 MPH	Ten Mile Pace:	30 to 39 MPH	85th Percentile:	40.3 MPH
Mean Speed:	32.0 MPH	Percent in Ten Mile Pace:	48.0%	15th Percentile:	23.2 MPH
Median Speed:	32.3 MPH			90th Percentile:	42.1 MPH
Modal Speed:	33.0 MPH			95th Percentile:	45.8 MPH

Traffic Survey Specialists, Inc.
Daily Westbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
00:00 - 00:14	0	1	0	1	0	0	1	0	0	0	0	0	0	3
00:15 - 00:29	0	0	0	1	0	1	1	0	0	0	0	0	0	3
00:30 - 00:44	1	0	0	0	0	0	0	0	0	0	0	0	0	1
00:45 - 00:59	0	0	0	0	0	1	2	0	0	0	0	0	0	3
01:00 - 01:14	0	1	0	0	0	0	0	0	0	0	0	0	0	1
01:15 - 01:29	0	0	0	0	0	1	1	0	0	0	0	0	0	2
01:30 - 01:44	0	0	0	0	0	0	1	0	0	1	0	0	0	2
01:45 - 01:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
02:00 - 02:14	1	0	1	0	0	0	0	0	0	0	0	0	0	2
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	1	0	1	0	0	0	0	0	0	0	0	0	0	2
03:30 - 03:44	0	0	0	1	0	1	0	0	0	0	0	0	0	2
03:45 - 03:59	1	0	0	0	2	0	1	0	0	0	0	0	0	4
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	1	1	0	0	0	0	0	2
04:30 - 04:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:45 - 04:59	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 - 05:14	0	0	0	0	0	0	0	0	1	0	0	0	0	1
05:15 - 05:29	0	0	0	0	0	1	0	0	0	0	0	0	0	1
05:30 - 05:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	1	0	1	1	0	1	0	0	0	0	0	0	4
06:15 - 06:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
06:30 - 06:44	0	0	0	1	2	0	1	0	0	0	0	0	0	4
06:45 - 06:59	0	1	1	3	1	2	0	0	0	0	0	0	0	8
07:00 - 07:14	0	0	1	0	0	1	1	0	0	0	0	0	0	3
07:15 - 07:29	1	0	3	1	0	0	0	0	0	1	0	0	0	6
07:30 - 07:44	0	0	2	1	3	0	1	0	0	0	0	0	0	7
07:45 - 07:59	1	0	2	1	1	1	0	0	0	0	0	0	0	6
08:00 - 08:14	0	0	3	2	2	2	0	0	0	0	0	0	0	9
08:15 - 08:29	0	2	1	2	4	0	1	1	0	0	0	0	0	11
08:30 - 08:44	0	0	1	3	3	2	1	0	0	0	0	0	0	10
08:45 - 08:59	0	0	0	2	3	4	0	0	0	0	0	0	0	9
09:00 - 09:14	0	0	1	3	4	0	0	0	0	0	0	0	0	8
09:15 - 09:29	0	0	1	1	1	4	0	0	0	0	0	0	0	7
09:30 - 09:44	0	0	5	3	4	0	0	1	0	0	0	0	0	13
09:45 - 09:59	0	1	2	3	1	2	1	0	0	0	0	0	0	10
10:00 - 10:14	0	0	1	3	2	1	0	0	0	0	0	0	0	7
10:15 - 10:29	0	2	1	5	0	1	0	0	0	0	1	0	0	10
10:30 - 10:44	0	0	0	0	3	1	1	0	1	0	0	0	0	6
10:45 - 10:59	0	1	2	2	1	1	1	1	0	0	0	0	0	9
11:00 - 11:14	0	0	1	3	3	1	0	1	0	0	0	0	0	9
11:15 - 11:29	0	0	3	1	5	0	2	0	0	0	0	0	0	11
11:30 - 11:44	0	0	2	3	5	2	0	0	0	0	0	0	0	12
11:45 - 11:59	1	0	5	1	1	0	0	0	0	0	0	0	0	8
12:00 - 12:14	0	0	1	5	7	2	3	0	0	0	0	0	0	18
12:15 - 12:29	0	1	1	2	4	3	1	0	0	0	0	0	0	12
12:30 - 12:44	0	1	2	2	1	2	0	0	0	0	0	0	0	8

Traffic Survey Specialists, Inc.
Daily Westbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
12:45 - 12:59	0	1	2	6	1	0	0	0	0	0	0	0	0	10
13:00 - 13:14	0	0	2	0	0	0	0	0	0	0	0	0	0	2
13:15 - 13:29	1	0	0	1	0	0	1	0	0	0	0	0	0	3
13:30 - 13:44	0	0	4	3	0	2	0	0	0	0	0	0	0	9
13:45 - 13:59	0	1	2	2	2	1	1	0	0	0	0	0	0	9
14:00 - 14:14	1	1	2	3	1	1	0	0	0	0	0	0	0	9
14:15 - 14:29	0	0	5	2	3	0	1	0	0	0	0	0	0	11
14:30 - 14:44	0	1	2	4	1	1	0	0	0	0	0	0	0	9
14:45 - 14:59	2	0	2	0	0	0	0	1	0	0	0	0	0	5
15:00 - 15:14	0	1	3	1	5	0	1	0	0	0	0	0	0	11
15:15 - 15:29	1	0	0	3	3	2	4	0	0	0	0	0	0	13
15:30 - 15:44	0	1	1	5	6	3	2	0	0	0	0	0	0	18
15:45 - 15:59	0	0	0	0	4	2	0	0	0	0	0	0	0	6
16:00 - 16:14	0	0	1	3	5	3	3	0	0	0	0	0	0	15
16:15 - 16:29	1	1	1	2	4	3	1	1	0	0	0	0	0	14
16:30 - 16:44	0	1	2	1	3	4	1	0	0	0	0	0	0	12
16:45 - 16:59	2	0	1	3	3	4	2	0	0	0	0	0	0	15
17:00 - 17:14	0	0	1	8	14	4	0	0	0	0	0	0	0	27
17:15 - 17:29	1	0	4	10	5	6	4	0	1	0	0	0	0	31
17:30 - 17:44	0	2	0	5	11	6	3	0	0	0	0	0	0	27
17:45 - 17:59	0	0	2	1	1	2	2	0	0	0	0	0	0	8
18:00 - 18:14	0	0	0	2	6	5	2	0	0	1	0	0	0	16
18:15 - 18:29	0	0	1	3	4	2	2	1	0	0	0	0	0	13
18:30 - 18:44	0	0	2	5	4	0	0	1	0	0	0	0	0	12
18:45 - 18:59	0	0	2	2	4	2	1	0	0	0	0	0	0	11
19:00 - 19:14	0	0	0	2	4	3	1	1	0	0	0	0	0	11
19:15 - 19:29	0	0	1	2	1	1	0	0	0	0	0	0	0	5
19:30 - 19:44	0	1	2	1	0	4	0	1	1	0	0	0	0	10
19:45 - 19:59	0	0	2	0	1	1	0	0	0	0	0	0	0	4
20:00 - 20:14	0	0	1	2	1	0	1	0	0	0	0	0	0	5
20:15 - 20:29	0	0	1	1	2	1	0	0	0	0	0	0	0	5
20:30 - 20:44	0	0	1	2	1	1	0	0	0	0	0	0	0	5
20:45 - 20:59	0	1	0	1	4	0	0	0	0	0	0	0	0	6
21:00 - 21:14	0	0	2	2	0	1	0	0	0	0	0	0	0	5
21:15 - 21:29	0	0	0	2	1	2	1	0	0	0	0	0	0	6
21:30 - 21:44	0	1	0	0	1	1	0	0	0	0	0	0	0	3
21:45 - 21:59	0	0	0	0	1	0	0	1	0	0	0	0	0	2
22:00 - 22:14	0	0	1	0	1	1	1	0	0	1	0	0	0	5
22:15 - 22:29	1	0	1	1	1	0	0	0	0	0	0	0	0	4
22:30 - 22:44	0	0	1	1	2	0	0	0	0	0	0	0	0	4
22:45 - 22:59	0	1	0	0	1	1	0	0	0	0	0	0	0	3
23:00 - 23:14	0	3	0	0	0	0	0	1	0	0	0	0	0	4
23:15 - 23:29	1	1	2	0	1	0	1	0	0	0	0	0	0	6
23:30 - 23:44	0	0	0	0	1	0	0	0	0	0	0	0	0	1
23:45 - 23:59	0	1	0	1	0	1	1	0	0	0	0	0	0	4
Totals	19	32	102	151	179	108	60	13	4	4	1	0	0	673
Percent of Total	2.8	4.8	15.2	22.4	26.6	16.0	8.9	1.9	0.6	0.6	0.1	0.0	0.0	100
Percent of AM	3.6	5.0	18.6	22.2	24.0	13.6	8.6	2.3	0.9	0.9	0.5	0.0	0.0	100
Percent of PM	2.4	4.6	13.5	22.6	27.9	17.3	9.1	1.8	0.4	0.4	0.0	0.0	0.0	100

Traffic Survey Specialists, Inc.
Daily Westbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	8.6 MPH	Ten Mile Pace:	25 to 34 MPH	85th Percentile:	39.9 MPH
Mean Speed:	31.3 MPH	Percent in Ten Mile Pace:	49.0%	15th Percentile:	22.9 MPH
Median Speed:	31.1 MPH			90th Percentile:	41.4 MPH
Modal Speed:	33.0 MPH			95th Percentile:	44.8 MPH

Traffic Survey Specialists, Inc.
Daily Westbound Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
00:00 - 00:14	0	0	0	0	1	0	0	0	0	0	1	0	0	2
00:15 - 00:29	0	0	1	0	1	0	0	0	1	0	0	0	0	3
00:30 - 00:44	0	0	0	0	0	1	1	1	0	0	0	0	0	3
00:45 - 00:59	0	1	0	0	1	0	0	0	0	0	0	0	0	2
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:45 - 01:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 - 02:14	0	0	1	0	0	0	0	0	0	0	0	0	0	1
02:15 - 02:29	0	0	0	0	1	0	0	0	0	0	0	0	0	1
02:30 - 02:44	0	0	1	0	0	1	0	0	0	0	0	0	0	2
02:45 - 02:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 - 03:14	0	0	0	0	0	1	0	0	0	0	0	0	0	1
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:45 - 03:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
04:00 - 04:14	1	0	0	1	1	0	0	0	0	0	0	0	0	3
04:15 - 04:29	1	0	0	0	0	0	0	0	0	0	1	0	0	2
04:30 - 04:44	0	1	0	1	0	0	0	0	0	0	0	0	0	2
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	1	0	0	0	0	0	0	0	0	1
05:30 - 05:44	0	0	0	0	1	1	0	0	0	0	0	0	0	2
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	2	1	0	0	0	0	0	0	0	0	0	3
06:15 - 06:29	1	1	2	2	1	3	0	0	0	0	0	0	0	10
06:30 - 06:44	2	0	0	2	1	1	0	1	0	0	0	0	0	7
06:45 - 06:59	0	0	1	2	1	2	0	0	0	0	0	0	0	6
07:00 - 07:14	0	1	1	0	1	0	0	0	0	0	0	0	0	3
07:15 - 07:29	1	0	2	1	4	0	1	0	0	0	0	0	0	9
07:30 - 07:44	1	2	1	0	1	2	1	0	0	0	0	0	0	8
07:45 - 07:59	0	0	1	0	3	2	1	0	0	0	0	0	0	7
08:00 - 08:14	0	0	1	2	6	1	0	1	2	0	0	0	0	13
08:15 - 08:29	0	4	1	2	1	2	0	1	0	0	0	0	0	11
08:30 - 08:44	0	0	0	0	3	5	2	1	0	0	0	0	0	11
08:45 - 08:59	0	0	1	0	5	3	0	0	0	0	0	0	0	9
09:00 - 09:14	0	1	0	4	3	2	0	1	0	0	0	0	0	11
09:15 - 09:29	0	0	0	1	1	1	1	0	1	1	0	0	0	6
09:30 - 09:44	0	0	4	2	4	3	1	1	0	0	0	0	0	15
09:45 - 09:59	2	2	3	2	2	0	0	0	0	0	0	0	0	11
10:00 - 10:14	0	0	2	2	3	2	0	0	0	0	0	0	0	9
10:15 - 10:29	0	1	0	3	4	0	0	0	0	0	0	0	0	8
10:30 - 10:44	0	1	0	1	1	0	1	1	0	0	0	0	0	5
10:45 - 10:59	1	2	1	1	2	1	2	0	0	0	0	0	0	10
11:00 - 11:14	0	1	3	1	1	0	1	0	0	0	0	0	0	7
11:15 - 11:29	1	0	0	0	8	0	0	0	1	0	0	0	0	10
11:30 - 11:44	2	0	1	4	0	1	0	0	0	0	0	0	0	8
11:45 - 11:59	1	0	0	6	0	3	0	0	0	0	0	0	0	10
12:00 - 12:14	0	0	0	6	1	0	1	0	1	0	0	0	0	9
12:15 - 12:29	1	0	0	4	1	4	2	0	0	0	0	0	0	12
12:30 - 12:44	1	1	1	1	1	3	0	0	0	0	0	0	0	8

Traffic Survey Specialists, Inc.
Daily Westbound Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
12:45 - 12:59	0	0	4	7	8	2	1	0	0	0	0	0	0	22
13:00 - 13:14	0	1	1	1	0	3	1	2	0	0	0	0	0	9
13:15 - 13:29	0	0	0	3	6	2	0	1	0	0	0	0	0	12
13:30 - 13:44	2	0	3	1	4	3	1	0	0	0	0	0	0	14
13:45 - 13:59	0	1	0	2	2	5	1	1	0	0	0	0	0	12
14:00 - 14:14	0	2	1	4	3	3	1	0	0	0	0	0	0	14
14:15 - 14:29	1	0	1	3	9	3	0	2	0	0	0	0	0	19
14:30 - 14:44	0	1	1	2	2	1	3	0	0	0	0	0	0	10
14:45 - 14:59	0	0	4	3	2	4	0	1	0	0	0	0	0	14
15:00 - 15:14	0	0	2	1	0	3	3	2	0	0	0	0	0	11
15:15 - 15:29	0	0	2	4	5	3	2	1	0	0	0	0	0	17
15:30 - 15:44	0	1	2	4	10	5	2	0	0	0	0	0	0	24
15:45 - 15:59	0	1	3	2	6	3	2	0	0	0	0	0	0	17
16:00 - 16:14	0	1	3	3	5	6	4	1	0	0	0	0	0	23
16:15 - 16:29	1	0	0	6	4	2	3	0	0	0	0	0	0	16
16:30 - 16:44	0	3	3	6	9	4	1	0	0	0	0	0	0	26
16:45 - 16:59	0	0	0	1	5	3	4	0	0	0	0	0	0	13
17:00 - 17:14	0	0	0	10	6	7	1	0	0	0	0	0	1	25
17:15 - 17:29	0	1	2	0	14	7	1	0	0	0	0	0	0	25
17:30 - 17:44	0	0	1	5	7	4	1	0	0	0	0	0	0	18
17:45 - 17:59	0	0	2	4	7	4	1	0	0	0	0	0	0	18
18:00 - 18:14	0	1	1	2	6	3	2	0	0	0	0	0	0	15
18:15 - 18:29	0	1	3	3	7	6	0	0	0	0	0	0	0	20
18:30 - 18:44	1	1	0	3	4	3	1	1	0	0	0	0	0	14
18:45 - 18:59	1	1	1	2	1	1	1	0	0	0	0	0	0	8
19:00 - 19:14	2	1	0	2	1	1	0	0	0	0	0	0	0	7
19:15 - 19:29	0	0	2	9	2	2	0	0	0	0	0	0	0	15
19:30 - 19:44	0	1	1	2	2	0	0	0	0	0	0	0	0	6
19:45 - 19:59	1	0	0	1	3	1	0	1	1	0	0	0	0	8
20:00 - 20:14	0	1	1	1	1	1	0	0	0	0	0	0	0	5
20:15 - 20:29	0	0	0	1	4	0	0	0	0	0	0	0	0	5
20:30 - 20:44	0	1	0	3	0	1	0	0	0	0	0	0	0	5
20:45 - 20:59	0	0	0	2	1	1	0	0	0	0	0	0	0	4
21:00 - 21:14	0	1	0	1	2	0	1	0	0	0	0	0	0	5
21:15 - 21:29	1	0	0	1	0	0	0	0	0	0	0	0	0	2
21:30 - 21:44	1	0	0	1	0	1	0	0	0	0	0	0	0	3
21:45 - 21:59	2	0	0	1	1	1	0	0	0	0	0	0	0	5
22:00 - 22:14	0	1	1	0	0	1	0	0	0	0	0	0	0	3
22:15 - 22:29	1	1	0	1	0	0	1	0	0	0	0	0	0	4
22:30 - 22:44	0	0	0	1	1	0	0	0	0	0	0	0	0	2
22:45 - 22:59	0	1	1	0	1	0	1	0	0	0	0	0	0	4
23:00 - 23:14	0	0	0	1	0	3	0	0	0	0	0	0	0	4
23:15 - 23:29	0	1	1	0	0	0	1	0	0	1	0	0	0	4
23:30 - 23:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:45 - 23:59	0	0	1	1	1	0	0	0	0	0	0	0	0	3
Totals	31	45	79	163	219	148	56	21	7	2	2	0	1	774
Percent of Total	4.0	5.8	10.2	21.1	28.3	19.1	7.2	2.7	0.9	0.3	0.3	0.0	0.1	100
Percent of AM	6.4	8.1	12.8	17.4	27.2	16.2	5.1	3.4	2.1	0.4	0.4	0.9	0.0	100
Percent of PM	3.0	4.8	9.1	22.6	28.8	20.4	8.2	2.4	0.4	0.2	0.0	0.0	0.2	100

Traffic Survey Specialists, Inc.
Daily Westbound Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.1 MPH	Ten Mile Pace:	25 to 34 MPH	85th Percentile:	39.9 MPH
Mean Speed:	31.5 MPH	Percent in Ten Mile Pace:	49.4%	15th Percentile:	23.0 MPH
Median Speed:	31.9 MPH			90th Percentile:	41.2 MPH
Modal Speed:	33.0 MPH			95th Percentile:	45.3 MPH

Traffic Survey Specialists, Inc.
Daily Total Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
00:00 - 00:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15 - 00:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:30 - 00:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:45 - 00:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:45 - 01:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 - 02:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45 - 03:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30 - 04:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30 - 05:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15 - 06:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:30 - 06:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:45 - 06:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 - 07:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:15 - 07:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30 - 07:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:45 - 07:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 - 08:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 - 08:29	0	3	1	0	0	0	2	0	0	0	0	0	0	6
08:30 - 08:44	0	0	1	2	5	1	0	0	0	0	0	0	0	9
08:45 - 08:59	0	0	1	0	2	1	0	0	0	0	0	0	0	4
09:00 - 09:14	0	1	1	0	1	1	1	0	0	0	0	0	0	5
09:15 - 09:29	0	1	1	1	4	3	1	0	0	0	0	0	0	11
09:30 - 09:44	0	1	2	1	2	3	2	1	0	0	0	0	0	12
09:45 - 09:59	0	1	1	2	3	2	0	0	0	0	0	0	0	9
10:00 - 10:14	0	1	2	2	3	3	0	0	0	0	0	0	0	11
10:15 - 10:29	0	1	2	0	8	2	0	1	0	0	0	0	0	14
10:30 - 10:44	2	1	0	2	3	1	0	1	0	0	0	0	0	10
10:45 - 10:59	1	1	1	0	2	2	3	0	0	0	0	0	0	10
11:00 - 11:14	0	0	3	1	4	0	1	0	1	0	0	0	0	10
11:15 - 11:29	0	0	3	6	2	0	0	1	0	0	0	0	0	12
11:30 - 11:44	0	2	0	1	7	3	0	1	1	0	0	0	0	15
11:45 - 11:59	0	1	1	1	3	1	0	0	0	0	0	0	0	7
12:00 - 12:14	0	0	1	2	4	1	1	0	0	0	0	0	0	9
12:15 - 12:29	0	2	3	3	5	3	1	0	0	0	0	0	0	17
12:30 - 12:44	0	1	1	1	6	3	1	0	0	0	0	0	0	13

Traffic Survey Specialists, Inc.
Daily Total Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
12:45 - 12:59	0	2	2	0	1	0	1	0	0	0	0	0	0	6
13:00 - 13:14	0	0	5	5	4	1	2	0	0	0	0	0	0	17
13:15 - 13:29	0	2	2	2	1	3	0	1	0	0	0	0	0	11
13:30 - 13:44	1	0	0	0	1	1	1	0	0	0	0	0	0	4
13:45 - 13:59	2	1	1	1	1	4	0	0	0	0	0	0	0	10
14:00 - 14:14	0	2	1	1	2	4	2	1	3	0	0	0	0	16
14:15 - 14:29	0	2	1	4	2	5	0	0	0	0	0	0	0	14
14:30 - 14:44	0	2	5	6	2	2	1	0	0	0	0	0	0	18
14:45 - 14:59	0	0	0	2	2	3	1	0	0	0	0	0	0	8
15:00 - 15:14	0	1	1	5	2	5	1	0	0	0	0	0	0	15
15:15 - 15:29	0	0	0	2	3	1	1	1	0	0	0	0	0	8
15:30 - 15:44	0	1	1	1	3	2	1	0	1	0	0	0	0	10
15:45 - 15:59	0	0	1	5	0	3	1	1	0	0	0	0	0	11
16:00 - 16:14	1	1	0	0	4	1	0	0	0	0	0	0	0	7
16:15 - 16:29	0	0	3	4	4	3	2	0	0	0	0	0	0	16
16:30 - 16:44	0	0	3	3	3	2	1	0	0	0	0	0	0	12
16:45 - 16:59	0	1	4	5	4	4	0	0	0	0	0	0	0	18
17:00 - 17:14	0	1	2	4	3	1	0	0	0	0	0	0	0	11
17:15 - 17:29	0	0	4	1	5	2	0	0	0	0	0	0	0	12
17:30 - 17:44	2	2	0	2	2	1	0	0	0	1	0	0	0	10
17:45 - 17:59	0	0	3	3	6	2	1	0	0	0	0	0	0	15
18:00 - 18:14	0	0	0	1	4	4	3	1	0	0	0	0	0	13
18:15 - 18:29	0	0	2	5	2	1	2	2	0	1	0	0	0	15
18:30 - 18:44	0	2	2	3	3	0	0	0	0	0	0	0	0	10
18:45 - 18:59	0	1	2	5	3	5	1	1	1	0	0	0	0	19
19:00 - 19:14	0	1	3	2	2	4	1	0	0	0	0	0	0	13
19:15 - 19:29	3	2	6	1	3	1	1	0	0	0	0	0	0	17
19:30 - 19:44	0	2	0	2	4	2	1	0	0	0	0	0	0	11
19:45 - 19:59	0	1	4	2	5	2	1	0	0	0	0	0	0	15
20:00 - 20:14	1	0	1	4	2	2	2	0	0	0	0	0	0	12
20:15 - 20:29	0	0	1	1	2	5	1	0	0	0	0	0	0	10
20:30 - 20:44	0	0	1	5	1	2	0	1	1	0	0	0	0	11
20:45 - 20:59	1	0	5	2	4	0	0	0	0	0	0	0	0	12
21:00 - 21:14	2	2	3	3	0	0	0	1	0	0	0	0	0	11
21:15 - 21:29	1	2	1	2	3	0	0	0	0	0	0	0	0	9
21:30 - 21:44	0	0	1	2	2	1	3	0	0	0	1	0	0	10
21:45 - 21:59	0	1	1	1	2	2	0	1	0	0	0	0	0	8
22:00 - 22:14	2	0	0	2	2	0	1	0	0	0	0	0	0	7
22:15 - 22:29	2	0	5	2	1	1	0	0	0	0	0	0	0	11
22:30 - 22:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
22:45 - 22:59	0	1	0	2	5	1	0	0	0	0	0	0	0	9
23:00 - 23:14	0	0	1	1	0	0	0	0	1	0	0	0	0	3
23:15 - 23:29	0	1	1	2	0	1	0	0	0	0	0	0	0	5
23:30 - 23:44	0	0	1	2	1	2	0	0	0	0	0	0	0	6
23:45 - 23:59	1	1	1	2	0	1	0	0	0	0	0	0	0	6
Totals	22	53	106	135	170	117	46	16	9	2	1	0	0	677
Percent of Total	3.2	7.8	15.7	19.9	25.1	17.3	6.8	2.4	1.3	0.3	0.1	0.0	0.0	100
Percent of AM	2.1	9.7	13.8	13.1	33.8	15.9	6.9	3.4	1.4	0.0	0.0	0.0	0.0	100
Percent of PM	3.6	7.3	16.2	21.8	22.7	17.7	6.8	2.1	1.3	0.4	0.2	0.0	0.0	100

Traffic Survey Specialists, Inc.
Daily Total Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.0 MPH	Ten Mile Pace:	25 to 34 MPH	85th Percentile:	39.6 MPH
Mean Speed:	30.7 MPH	Percent in Ten Mile Pace:	45.1%	15th Percentile:	21.5 MPH
Median Speed:	30.8 MPH			90th Percentile:	40.8 MPH
Modal Speed:	33.0 MPH			95th Percentile:	45.2 MPH

Traffic Survey Specialists, Inc.
Daily Total Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
00:00 - 00:14	0	1	0	1	1	0	1	0	0	0	0	0	0	4
00:15 - 00:29	0	0	0	1	0	1	1	1	0	0	0	0	0	4
00:30 - 00:44	1	1	0	0	0	1	0	0	0	1	0	0	0	4
00:45 - 00:59	0	0	0	1	0	1	2	0	0	0	0	0	0	4
01:00 - 01:14	0	2	0	1	0	0	0	0	0	0	0	0	0	3
01:15 - 01:29	0	0	0	0	0	1	1	0	0	0	0	0	0	2
01:30 - 01:44	0	0	1	0	0	0	1	0	0	1	0	0	0	3
01:45 - 01:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
02:00 - 02:14	2	0	1	0	0	0	0	0	0	0	0	0	0	3
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	1	1	1	0	0	0	0	0	0	0	0	0	0	3
03:30 - 03:44	0	0	0	1	0	1	0	0	0	0	0	0	0	2
03:45 - 03:59	1	0	0	0	2	0	1	0	0	0	0	0	0	4
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	1	1	0	0	0	0	0	2
04:30 - 04:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:45 - 04:59	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 - 05:14	0	0	0	0	0	0	0	0	1	0	0	0	0	1
05:15 - 05:29	0	0	0	0	0	1	0	0	0	0	0	0	0	1
05:30 - 05:44	1	0	0	0	0	0	1	0	0	0	0	0	0	2
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	1	0	1	1	0	1	0	0	0	0	0	0	4
06:15 - 06:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
06:30 - 06:44	0	0	2	1	2	0	1	0	0	0	0	0	0	6
06:45 - 06:59	1	1	1	4	2	2	0	0	0	0	0	0	0	11
07:00 - 07:14	0	0	1	2	0	1	1	0	0	0	0	0	0	5
07:15 - 07:29	1	0	3	1	0	1	0	0	0	1	0	0	0	7
07:30 - 07:44	0	1	2	1	3	0	1	0	0	0	0	0	0	8
07:45 - 07:59	1	0	4	2	1	1	1	0	0	0	0	0	0	10
08:00 - 08:14	0	0	3	2	2	2	0	0	0	0	0	0	0	9
08:15 - 08:29	0	2	2	2	4	0	1	1	0	0	0	0	0	12
08:30 - 08:44	0	0	1	4	7	5	1	0	0	0	0	0	0	18
08:45 - 08:59	0	0	0	2	4	5	0	0	0	0	0	0	0	11
09:00 - 09:14	0	0	1	3	6	0	0	0	0	0	0	0	0	10
09:15 - 09:29	1	0	3	1	2	4	0	0	0	0	0	0	0	11
09:30 - 09:44	0	0	5	5	5	0	0	1	0	0	0	0	0	16
09:45 - 09:59	1	1	3	4	2	2	1	0	0	0	0	0	0	14
10:00 - 10:14	0	0	3	4	4	2	0	0	0	0	0	0	0	13
10:15 - 10:29	0	5	2	5	0	2	1	0	0	0	1	0	0	16
10:30 - 10:44	1	0	0	0	3	1	1	0	1	0	0	0	0	7
10:45 - 10:59	1	1	3	2	1	1	1	2	0	0	0	0	0	12
11:00 - 11:14	0	0	1	3	4	1	0	1	0	0	0	0	0	10
11:15 - 11:29	1	0	8	2	6	0	2	0	0	0	0	0	0	19
11:30 - 11:44	1	1	4	4	5	2	0	0	0	1	0	0	0	18
11:45 - 11:59	1	0	5	2	1	0	0	0	0	0	0	0	0	9
12:00 - 12:14	1	0	1	5	7	2	3	0	0	0	0	0	0	19
12:15 - 12:29	0	1	2	2	5	3	1	0	0	0	0	0	0	14
12:30 - 12:44	0	3	3	2	1	2	1	0	0	0	0	0	0	12

Traffic Survey Specialists, Inc.
Daily Total Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
12:45 - 12:59	0	2	3	6	1	0	0	0	0	0	0	0	0	12
13:00 - 13:14	0	0	3	2	0	0	0	0	0	0	0	0	0	5
13:15 - 13:29	1	0	1	2	1	0	1	0	0	0	0	0	0	6
13:30 - 13:44	0	1	5	3	0	2	1	0	0	0	0	0	0	12
13:45 - 13:59	1	1	4	5	3	1	1	0	0	0	0	0	0	16
14:00 - 14:14	1	2	5	5	1	1	0	0	0	0	0	0	0	15
14:15 - 14:29	0	2	5	3	4	1	1	0	0	0	0	0	0	16
14:30 - 14:44	0	1	4	4	2	1	0	0	0	0	0	0	0	12
14:45 - 14:59	3	1	3	1	0	1	1	1	0	0	0	0	0	11
15:00 - 15:14	0	1	4	2	5	0	1	0	0	0	0	0	0	13
15:15 - 15:29	1	2	1	3	3	3	4	0	0	0	0	0	0	17
15:30 - 15:44	0	3	1	5	6	4	3	0	0	0	0	0	0	22
15:45 - 15:59	0	0	0	0	5	2	0	0	0	0	0	0	0	7
16:00 - 16:14	0	2	2	3	6	3	3	0	0	0	0	0	0	19
16:15 - 16:29	2	2	2	3	4	4	1	1	0	0	0	0	0	19
16:30 - 16:44	0	1	2	3	3	4	1	0	0	0	0	0	0	14
16:45 - 16:59	3	1	1	4	4	4	2	0	0	0	0	0	0	19
17:00 - 17:14	1	1	1	9	14	5	0	0	0	0	0	0	0	31
17:15 - 17:29	3	1	5	11	5	7	5	0	1	0	0	0	0	38
17:30 - 17:44	0	2	2	5	12	6	3	0	0	0	0	0	0	30
17:45 - 17:59	1	0	3	1	4	3	2	0	0	0	0	0	0	14
18:00 - 18:14	0	1	0	2	7	5	2	0	0	1	0	0	0	18
18:15 - 18:29	0	0	1	5	4	2	2	1	0	0	0	0	0	15
18:30 - 18:44	0	0	2	7	5	2	0	1	0	0	0	0	0	17
18:45 - 18:59	0	1	3	3	6	2	1	0	0	0	0	0	0	16
19:00 - 19:14	0	2	2	2	4	4	1	1	0	0	0	0	0	16
19:15 - 19:29	0	0	1	2	1	1	0	0	0	0	0	0	0	5
19:30 - 19:44	1	1	3	1	0	4	0	2	1	0	0	0	0	13
19:45 - 19:59	0	0	4	2	2	2	0	0	0	0	0	0	0	10
20:00 - 20:14	0	0	2	3	1	0	1	0	0	0	0	0	0	7
20:15 - 20:29	0	0	2	2	4	1	0	0	0	0	0	0	0	9
20:30 - 20:44	0	1	2	2	1	1	0	0	0	0	0	0	0	7
20:45 - 20:59	0	1	0	2	4	1	0	0	0	0	0	0	0	8
21:00 - 21:14	0	0	3	3	0	1	0	0	0	1	0	0	0	8
21:15 - 21:29	0	1	0	2	2	2	1	0	0	0	0	0	0	8
21:30 - 21:44	0	1	0	0	1	1	0	0	0	0	0	0	0	3
21:45 - 21:59	0	0	0	0	1	0	0	1	0	0	0	0	0	2
22:00 - 22:14	0	0	1	0	1	2	1	0	0	1	0	0	0	6
22:15 - 22:29	1	0	2	1	1	0	0	0	0	0	0	0	0	5
22:30 - 22:44	0	1	4	2	2	0	0	0	0	0	0	0	0	9
22:45 - 22:59	0	1	0	1	1	1	0	0	0	0	0	0	0	4
23:00 - 23:14	0	3	1	1	1	0	0	1	0	0	0	0	0	7
23:15 - 23:29	1	2	2	0	1	0	1	0	0	0	0	0	0	7
23:30 - 23:44	0	0	0	0	1	0	0	0	0	0	0	0	0	1
23:45 - 23:59	0	1	0	1	2	1	1	0	0	0	0	0	0	6
Totals	38	66	159	196	218	130	68	16	4	7	1	0	0	903
Percent of Total	4.2	7.3	17.6	21.7	24.1	14.4	7.5	1.8	0.4	0.8	0.1	0.0	0.0	100
Percent of AM	5.6	6.3	20.1	20.8	22.8	12.5	7.3	2.3	0.7	1.3	0.3	0.0	0.0	100
Percent of PM	3.5	7.8	16.3	22.2	24.8	15.3	7.7	1.5	0.3	0.5	0.0	0.0	0.0	100

Traffic Survey Specialists, Inc.
Daily Total Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.1 MPH	Ten Mile Pace:	25 to 34 MPH	85th Percentile:	39.2 MPH
Mean Speed:	30.1 MPH	Percent in Ten Mile Pace:	45.8%	15th Percentile:	21.2 MPH
Median Speed:	30.8 MPH			90th Percentile:	40.5 MPH
Modal Speed:	33.0 MPH			95th Percentile:	44.4 MPH

Traffic Survey Specialists, Inc.
Daily Total Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
00:00 - 00:14	0	0	0	0	1	0	0	0	0	0	1	0	0	2
00:15 - 00:29	0	1	1	0	1	0	0	0	1	0	0	0	0	4
00:30 - 00:44	0	0	0	0	0	1	1	2	0	0	0	0	0	4
00:45 - 00:59	0	1	0	0	1	0	0	0	0	0	0	0	0	2
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	1	0	0	0	1	0	0	0	0	0	0	0	0	2
01:45 - 01:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
02:00 - 02:14	0	1	1	0	0	0	0	0	0	0	0	0	0	2
02:15 - 02:29	0	0	0	0	1	0	0	0	0	0	0	0	0	1
02:30 - 02:44	1	0	1	0	0	1	0	0	0	0	0	0	0	3
02:45 - 02:59	0	0	0	0	0	0	1	0	0	0	0	0	0	1
03:00 - 03:14	0	0	0	0	0	1	0	0	0	0	0	0	0	1
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:45 - 03:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
04:00 - 04:14	1	0	0	1	1	0	0	0	0	0	0	0	0	3
04:15 - 04:29	1	1	0	0	0	0	0	0	0	0	1	0	0	3
04:30 - 04:44	0	1	0	1	0	0	0	0	0	0	0	0	0	2
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	1	0	0	0	0	0	0	0	0	1
05:30 - 05:44	0	0	0	0	1	1	0	0	0	0	0	0	0	2
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	2	1	0	0	0	0	0	0	0	0	0	3
06:15 - 06:29	1	1	3	3	1	3	0	0	0	0	0	0	0	12
06:30 - 06:44	2	0	0	2	1	1	0	1	0	0	0	0	0	7
06:45 - 06:59	0	0	1	4	1	2	0	0	0	0	0	0	0	8
07:00 - 07:14	0	1	2	0	2	1	0	0	0	0	0	0	0	6
07:15 - 07:29	1	0	2	1	4	0	1	0	0	0	0	0	0	9
07:30 - 07:44	1	2	1	3	1	2	1	0	0	0	0	0	0	11
07:45 - 07:59	1	1	1	0	3	2	1	0	0	0	0	0	0	9
08:00 - 08:14	0	1	1	2	7	1	0	1	2	0	0	0	0	15
08:15 - 08:29	1	6	1	2	2	2	0	1	0	0	0	0	0	15
08:30 - 08:44	0	0	1	2	4	6	2	1	0	0	0	0	0	16
08:45 - 08:59	0	0	2	1	6	3	2	0	0	0	0	0	0	14
09:00 - 09:14	0	1	0	5	4	2	0	1	0	0	0	0	0	13
09:15 - 09:29	1	1	0	2	1	1	1	0	1	1	0	0	0	9
09:30 - 09:44	0	0	5	3	5	3	1	1	0	0	0	0	0	18
09:45 - 09:59	2	3	4	3	3	0	0	0	0	0	0	0	0	15
10:00 - 10:14	0	0	4	4	4	5	0	0	0	0	0	0	0	17
10:15 - 10:29	1	1	0	3	4	1	0	0	0	0	0	0	0	10
10:30 - 10:44	2	1	2	1	5	0	1	1	0	0	0	0	0	13
10:45 - 10:59	2	3	2	2	2	2	0	0	0	0	0	0	0	15
11:00 - 11:14	1	1	3	1	1	0	1	0	0	0	0	0	0	8
11:15 - 11:29	1	0	2	2	9	0	0	0	1	0	0	0	0	15
11:30 - 11:44	4	0	3	6	1	1	0	0	0	0	0	0	0	15
11:45 - 11:59	1	0	3	8	0	3	0	0	0	0	0	0	0	15
12:00 - 12:14	0	0	1	6	1	1	3	0	1	0	0	0	0	13
12:15 - 12:29	3	0	1	5	1	4	4	0	0	0	0	0	0	18
12:30 - 12:44	2	1	1	2	1	3	0	0	0	0	0	0	0	10

Traffic Survey Specialists, Inc.
Daily Total Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1- 15	15- 20	20- 25	25- 30	30- 35	35- 40	40- 45	45- 50	50- 55	55- 60	60- 65	65- 70	70- 100	Total
12:45 - 12:59	0	0	4	8	9	2	1	0	0	0	0	0	0	24
13:00 - 13:14	0	1	2	4	0	3	1	2	0	0	0	0	0	13
13:15 - 13:29	1	0	1	3	8	2	0	1	0	0	0	0	0	16
13:30 - 13:44	2	0	4	3	4	4	2	0	0	0	0	0	0	19
13:45 - 13:59	1	1	0	3	2	6	1	1	0	0	0	0	0	15
14:00 - 14:14	0	2	4	4	6	3	1	0	0	0	0	0	0	20
14:15 - 14:29	2	1	1	5	10	4	0	2	0	0	0	0	0	25
14:30 - 14:44	0	3	4	2	3	1	4	0	0	0	0	0	0	17
14:45 - 14:59	0	0	5	5	2	4	0	1	0	0	0	0	0	17
15:00 - 15:14	0	0	4	1	0	4	3	2	0	0	0	0	0	14
15:15 - 15:29	0	0	3	4	5	5	2	1	0	0	0	0	0	20
15:30 - 15:44	0	1	3	6	10	5	2	0	0	0	0	0	0	27
15:45 - 15:59	1	1	4	3	6	3	2	0	0	0	0	0	0	20
16:00 - 16:14	0	1	4	3	5	6	4	1	0	0	0	0	0	24
16:15 - 16:29	1	1	0	7	4	2	3	0	0	0	0	0	0	18
16:30 - 16:44	0	3	3	8	10	5	1	0	0	0	0	0	0	30
16:45 - 16:59	0	0	2	1	5	3	4	0	0	0	0	0	0	15
17:00 - 17:14	0	1	1	10	6	7	2	1	0	0	0	0	1	29
17:15 - 17:29	1	2	3	1	14	7	1	0	0	0	0	0	0	29
17:30 - 17:44	0	0	2	5	7	4	1	0	0	0	0	0	0	19
17:45 - 17:59	0	0	2	6	7	4	1	0	0	0	0	0	0	20
18:00 - 18:14	0	2	2	4	6	3	2	0	0	0	0	0	0	19
18:15 - 18:29	0	1	3	4	8	6	0	0	0	0	0	0	0	22
18:30 - 18:44	1	2	2	3	4	5	1	1	0	0	0	0	0	19
18:45 - 18:59	2	2	1	2	1	1	3	0	0	0	0	0	0	12
19:00 - 19:14	2	1	0	3	2	1	0	0	0	0	0	0	0	9
19:15 - 19:29	0	0	2	9	2	2	0	0	0	0	0	0	0	15
19:30 - 19:44	0	2	1	4	2	2	0	0	0	0	0	0	0	11
19:45 - 19:59	1	0	0	3	4	2	0	1	1	0	0	0	0	12
20:00 - 20:14	0	1	1	1	2	1	0	0	0	0	0	0	0	6
20:15 - 20:29	0	0	1	1	4	0	1	0	0	0	0	0	0	7
20:30 - 20:44	0	1	0	3	0	1	0	0	0	0	0	0	0	5
20:45 - 20:59	0	1	0	3	2	2	0	0	0	0	0	0	0	8
21:00 - 21:14	1	1	3	1	2	0	1	0	0	0	0	0	0	9
21:15 - 21:29	1	0	0	2	1	0	0	0	0	0	0	0	0	4
21:30 - 21:44	1	0	3	2	1	1	0	0	0	0	0	0	0	8
21:45 - 21:59	3	1	0	1	3	1	0	0	0	0	0	0	0	9
22:00 - 22:14	1	2	1	1	0	1	0	0	0	0	0	0	0	6
22:15 - 22:29	1	2	0	1	1	0	1	0	0	0	0	0	0	6
22:30 - 22:44	0	0	0	2	2	0	1	0	0	0	0	0	0	5
22:45 - 22:59	0	1	4	1	2	0	1	0	0	0	0	0	0	9
23:00 - 23:14	0	0	0	1	0	3	0	0	0	0	0	0	0	4
23:15 - 23:29	0	1	2	0	0	0	1	0	0	1	0	0	0	5
23:30 - 23:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:45 - 23:59	0	0	1	1	2	0	0	0	0	0	0	0	0	4
Totals	54	69	134	222	257	169	70	23	7	2	2	0	1	1010
Percent of Total	5.3	6.8	13.3	22.0	25.4	16.7	6.9	2.3	0.7	0.2	0.2	0.0	0.1	100
Percent of AM	8.0	9.0	14.8	19.8	24.7	13.9	4.6	2.8	1.5	0.3	0.6	0.0	0.0	100
Percent of PM	4.1	5.8	12.5	23.0	25.8	18.1	8.0	2.0	0.3	0.1	0.0	0.0	0.1	100

Traffic Survey Specialists, Inc.
Daily Total Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.4 MPH	Ten Mile Pace:	25 to 34 MPH	85th Percentile:	39.3 MPH
Mean Speed:	30.4 MPH	Percent in Ten Mile Pace:	47.4%	15th Percentile:	21.3 MPH
Median Speed:	30.6 MPH			90th Percentile:	40.3 MPH
Modal Speed:	33.0 MPH			95th Percentile:	44.6 MPH

APPENDIX K

Polk Street Queueing Data

Traffic Survey Specialists, Inc. 85 SE 4th Avenue, Unit 109, Delray Beach, Florida 33483

Maximum Observed Queue of Westbound Traffic at Polk Street & N 28th Avenue

Hollywood, Florida July 13th, 2021

Observed By: Luis Palomino Westbound Polk Street at N 28th Avenue

7:00 AM 0

7:15 AM 0

7:30 AM 0

7:45 AM 0

8:00 AM 2

8:15 AM 0

8:30 AM 2

8:45 AM 0

4:00 PM 3

4:15 PM 2

4:30 PM 3

4:45 PM 2

5:00 PM 4

5:15 PM 4

5:30 PM 2

5:45 PM 3