

ATTACHMENT A

Application Package

PLANNING DIVISION



2600 Hollywood Boulevard Room 315
Hollywood, FL 33022

File No. (internal use only): _____

GENERAL APPLICATION



Tel: (954) 921-3471

Fax: (954) 921-3347

This application must be completed in full and submitted with all documents to be placed on a Board or Committee's agenda.

The applicant is responsible for obtaining the appropriate checklist for each type of application.

Applicant(s) or their authorized legal agent must be present at all Board or Committee meetings.

At least one set of the submitted plans for each application must be signed and sealed (i.e. Architect or Engineer).

Documents and forms can be accessed on the City's website at

<http://www.hollywoodfl.org/DocumentCenter/Home/View/21>



APPLICATION TYPE (CHECK ONE):

- ☐ Technical Advisory Committee ☐ Historic Preservation Board
☐ City Commission ☒ Planning and Development Board

Date of Application: _____

Location Address: 2741 & 2742 Polk St Hollywood FL 33020

Lot(s): Lot 11 & West 1/2 of Lot 12 Block(s): Block 31 & Block 32 Subdivision: Hollywood Little Ranches

Folio Number(s): 514216023350, 514216023630

Zoning Classification: Trans Core TC-1 /MC-1 North Lot Land Use Classification: Reg Act Center - RAC

Existing Property Use: Single Family Sq Ft/Number of Units: 2

Is the request the result of a violation notice? () Yes (✓) No If yes, attach a copy of violation.

Has this property been presented to the City before? If yes, check all that apply and provide File Number(s) and Resolution(s): File No. 20-DP-41

- ☐ Economic Roundtable ☒ Technical Advisory Committee ☐ Historic Preservation Board
☐ City Commission ☐ Planning and Development

Explanation of Request: Proposed construction of 45 apartment unit project all 1 bedroom with parking

Number of units/rooms: ● 44 Sq Ft: 650 sqft per Unit

Value of Improvement: \$4 Million Estimated Date of Completion: 15 Months after Permit

Will Project be Phased? () Yes (✓) No If Phased, Estimated Completion of Each Phase

Name of Current Property Owner: S&B ENT LLC

Address of Property Owner: 17555 COLLINS AVE APT 1606 SUNNY ISLES BEACH, FL 33160-2166

Telephone: 917-912-0851 Fax: _____ Email Address: benz1018@gmail.com

Name of Consultant Representative/Tenant (circle one): Tom Benedict - Architect

Address: 1525 NW 3rd St. Suite 1 Deerfield Beach FL 33442 Telephone: 954-570-9500

Fax: 954-570-9550 Email Address: tom@tbbg.net

Date of Purchase: 09/20/2019 Is there an option to purchase the Property? Yes () No (✓)

If Yes, Attach Copy of the Contract.

List Anyone Else Who Should Receive Notice of the Hearing: _____

Address: _____

Email Address: _____



File No. (internal use only): _____

2600 Hollywood Boulevard Room 315
Hollywood, FL 33022

GENERAL APPLICATION

CERTIFICATION OF COMPLIANCE WITH APPLICABLE REGULATIONS

The applicant/owner(s) signature certifies that he/she has been made aware of the criteria, regulations and guidelines applicable to the request. This information can be obtained in Room 315 of City Hall or on our website at www.hollywoodfl.org. The owner(s) further certifies that when required by applicable law, including but not limited to the City's Zoning and Land Development Regulations, they will post the site with a sign provided by the Office of Planning and Development Services. The owner(s) will photograph the sign the day of posting and submit photographs to the Office of Planning and Development Services as required by applicable law. Failure to post the sign will result in violation of State and Municipal Notification Requirements and Laws.

(I)(We) certify that (I) (we) understand and will comply with the provisions and regulations of the City's Zoning and Land Development Regulations, Design Guidelines, Design Guidelines for Historic Properties and City's Comprehensive Plan as they apply to this project. (I)(We) further certify that the above statements and drawings made on any paper or plans submitted herewith are true to the best of (my)(our) knowledge. (I)(We) understand that the application and attachments become part of the official public records of the City and are not returnable.

Signature of Current Owner: _____

Date: 1/15/21PRINT NAME: Ben Rasabi S&B ENT LLC

Date: _____

Signature of Consultant/Representative: _____

Date: 1-7-2021PRINT NAME: Thomas Benedict - ArchitectDate: 1-7-2021

Signature of Tenant: _____

Date: _____

PRINT NAME: _____

Date: _____

Current Owner Power of Attorney

I am the current owner of the described real property and that I am aware of the nature and effect the request for the development plans _____ to my property, which is hereby made by me or I am hereby authorizing _____ to be my legal representative before the tech. advisory (~~Board and/or~~ Committee) relative to all matters concerning this application.

Sworn to and subscribed before me
this 15 day of January

Notary Public

State of Florida

My Commission Expires: 3/11/23 (Check One) ☐ Personally known to me; OR ☒ Produced Identification Driver License

Elianni Marin
Commission # GG310447
Expires: March 11, 2023
Bonded Thru Aaron Notary

Signature of Current Owner

Print Name

July 26, 2021

Subject: CRITERIA STATEMENT: DESIGN CRITERIA

POLK STREET APARTMENTS I & II

Request: Site Plan Approval by the Planning and Development Board (PDB)

TAC # 20-DP-40 & 20-DP-41

This is a request for approval of our proposed project located on two sites: 2718/20 & 2742 Polk Street with additional parking located at 2723 & 2741 Polk Street, respectively. Each construction site is proposing 44 one bedroom apartment units in identical buildings that are the mirror image of one another. The total unit count will be 88 units. Unit size is 650 SF.

The following is a summary of the elements proposed to conform to the Design Criteria requirements of the City of Hollywood.

1) Architecture

This project proposes 3 stories of 1 bedroom, 1 1/2 bath rental apartments over street level parking with ground floor arrival, Lobby, two elevators, stair and support spaces including rest rooms. The roof level offers a gym, rest rooms, covered Lanai and swimming pool with 2 spas.

After preliminary review and several meetings regarding the design of this project, the architecture has been revised to more closely meet our understanding of City criteria for the vision of future development. Architectural features introduced (different from our first submission) include metal awnings, additional balconies with horizontal aluminum railing and most significantly pitched metal roofs. These elements assist in the visual expression of the residential character of the higher density housing anticipated in this neighborhood.

The base of the proposed structure is to be clad in porcelain with recessed glass entry door surrounded by sidelights and automobile garage entry both facing Polk Street. The 3 levels above this base are presented in smooth stucco with substantial areas of glass and railed balconies again facing Polk Street. The upper gym, recreation deck, pool and spa level is recessed further from Polk Street allowing the front building profile to be predominately 3 levels above parking capped by a pitched roof, as seen from the north-side sidewalk area adjacent to Polk Street. The recreation level is intended to be seen from the street although set back to lower the edge profile.

Material choices and construction methods will include Green Building Practices. Further evidence of this will be identified by our Green building consultant as we move forward with the construction document preparation.

2) Compatibility

The proposed project is located on Polk Street east of 28th Avenue. The proposed architecture is intended to be as consistent with the intent of the RAC zoning for this neighborhood as possible. Considerable discussion regarding building mass and the impact of the applied floor area ratio has resulted in our proposed design. The existing neighborhood is identified by older one story single family homes some of which are being used for business/ commercial activities and is an area of transition to new development.

This project is among the first to be proposed within the changed zoning within this area and will be an incentive for future new development some of which may be by this owner/developer.

3) Scale/Massing

To assist in diminishing the impact of this project, the setback proposed from Polk Street has been increased to 20' at the ground floor. 15' is required. The second through 4th floor of the building is setback 15' with balconies to articulate the facade. The front entry door arrival area is setback 24' and remains in that plane for the height of the front elevation. The interior stair enclosure could have been pushed within the front setback but was kept at the setback to offer greater landscaped space. The upper level gym / recreation space has been pushed back 24' to 34', further animating the facade.

4) Landscaping

Landscaping for this project has been carefully coordinated with the City's landscape department to ensure the variety of native and preferred plant types are integrated into the design. All efforts to maintain existing trees were made. The existing trees and their mitigation and the new trees and planting have been thoroughly documented, discussed and approved by the City landscape department.



THE MERCHANT STRATEGY

May 7, 2021

City of Hollywood
Zoning and Land Development
PO Box 229045
Hollywood FL 33022-9045

Re: S & B ENT, LLC
Polk Street Apartments Project - File Nos.: 20-DP-40 and 20-DP-41

Dear Sir/Madam:

The Merchant Strategy was retained by S & B Ent., LLC, to handle the public outreach/participation for this project. Pursuant to the City of Hollywood's Zoning and Land Development Regulations, Ordinance 5.1, we held a public participation meeting on Tuesday, April 6, 2021, at 6:00 pm, via Zoom.

In preparation for this meeting, we mailed invitations to all property owners, businesses, and certified/registered civic and neighborhood associations within 500 feet of the proposed project. We also placed signage directly at the project locations to further advise residents and/or stakeholders of the meeting as well as emailing the certified/registered civic and neighborhood associations so they could provide their residents a link for the Zoom meeting.

We are enclosing the completed Public Participation Meeting Certification Package which includes the following:

- The attendance list and Meeting Agenda – since this was a virtual meeting, we were not able to physically have people sign in. Therefore, we tracked and noted the names of the attendees. There is a recording of the meeting which can be provided via email if requested
- PowerPoint presentation
- Meeting notes
- Meeting invitation and signage

Thank you for your consideration.

Sincerely,

Cheryl Scott
Operations Manager

Polk Street Apartments Public Meeting
April 6, 2021 at 6:00 pm

Attendees:

Karen Newman Albertson
Patricia Antrican
Theresa
10503COH
Karen Caputo
Casey Carlson
Mayor Josh Levy
10496COH
Patty
Robin S.
Brian Tetzlaff
Vice Mayor Linda Sherwood]
Carlos Naranjo
Commissioner Linda Hill Anderson
Caryl Shuham
Commissioner Traci Callari

Questions/Comments From Meeting:

From Robin S.: What street does the under building parking open onto? Does this face onto the alley or Polk street?

From Robin S.: Is there a wall around the pool area? How many feet is the setback from the pool to the edge of the building?

From Karen Newman Anderson: How many people will you rent to in these one bedroom apartments?

From Robin S: What street modifications or traffic signal changes will be made to accommodate all the extra traffic?

From Theresa: how tall are these buildings?
will the parking be landscaped?
which direction do the balconies face?

From Theresa: what is the potential price range for these units?

From Theresa: how tall is the screening wall?

From Carlos Naranja: Is the price range of units within the average price range of existing homes in the area?

From Brian Tetzlaff: Timelines for the project? as well as hours of operation for construction

From Karen: My concern is the safety crossing the street to the parking area. Since the new crosswalks on Hollywood Boulevard are new and are already NOT WORKING in front of the City Hall.

From Karen Caputo: The TAC has returned the application for a dozen second requests and about 50 areas of concern. When will your amended application be ready to be viewed by the civic association?

From Casey Carlson: I am at 2620 Taylor st. What your looking at on my screen is my sky line from my back yard. What does that mean to the single family homes and their privacy?

From Robin S: There are two additional 1/2 acre lots owned by S&B Ent on that block of Polk that is not included here. Are there plans for two more building of this type to be added as well?

From Casey Carlson: This company also bought two more properties on Polk.. Is there another building going to go up as part of this project?

From Carlos Naranjo: We need to have these units fit the existing price range of existing comparable residencies. I'm at Polk and 27th

From Robin S: Will all these people have to exit onto 28th ave or the circle? These are already really busy traffic areas.

From: Brian Tetzlaff : Agreed Robin^ wish there were additional plans to direct traffic. Even modifications to traffic lights during high traffic times. could someone from DOT or city do a traffic study to help alleviate congestion? green light length. etc..

From Theresa: can you share some other projects you have done with us?

Also from Theresa: names - cities? Adding on to her last question

From Robin S: I'm worried about adding 200 people to our little neighborhood. The circle is a nightmare and Polk is narrow.

Patty: When the train comes in the station all the traffic comes to a standstill

Your Input is Wanted on the Proposed Polk Street Apartments Project



A rendering of one of the proposed identical mirror imaged buildings

YOUR INPUT AND OPINION IS IMPORTANT

S & B Ent, LLC, is committed to proactive resident and stakeholder outreach throughout the project. Please feel free to contact our team at info@themerchantstrategy.com with any questions or concerns.

We are here to help!

YOU ARE INVITED

S&B ENT, LLC invites you to a public meeting to discuss the proposed Polk Street Apartments project located at 2718, 2720 and 2742 Polk Street with additional parking at 2741 and 2723 Polk Street. Both sites are proposing forty five 1 bedroom apartment units in identical buildings - the mirror image of each other. The total unit count will be ninety units.

What: Proposed Polk Street Apartments Project

When: Tuesday, April 6 at 6:00 pm

Where: Via Zoom

Zoom Meeting Link:

https://us02web.zoom.us/j/87562518728?pwd=dHgzdkZkREY2_eHlaNTV4a0FJeHdiQT09

Meeting ID: 875 6251 8728

Passcode: 720414

One Tap Mobile

+13126266799,,87562518728#,,,,*720414#US (Chicago)

+16465588656,,87562518728#,,,,*720414#US (New York)

Your Input is Wanted on the Proposed Polk Street Apartments Project



A rendering of one of the proposed identical mirror imaged buildings

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We are here to help!

YOU ARE INVITED

S&B ENT, LLC invites you to a public meeting to discuss the proposed Polk Street Apartments project located at 2018, 2020 and 2742 Polk Street with additional parking at 2023 and 2043 Polk Street. Both sites are proposing forty five 1 bedroom apartment units in identical buildings - the mirror image of each other. The total unit count will be ninety units.

What: Proposed Polk Street Apartments Project

When: Tuesday, April 6 2021 at 6:00 pm

Where: Via Zoom

Zoom Meeting Link:

<https://us02web.zoom.us/j/87562518728?pwd=dHgZdkZkREY2eHlaNTV4a0FJeHdiQT09>

Meeting ID: 875 6251 8728

Passcode: 720414

One Tap Mobile

+13126266799,,87562518728#,,,,*720414#US (Chicago)

+16465588656,,87562518728#,,,,*720414#US (New York)



Virtual Meeting Agenda

Polk Street Apartments Project

April 6, 2021

6:00 pm

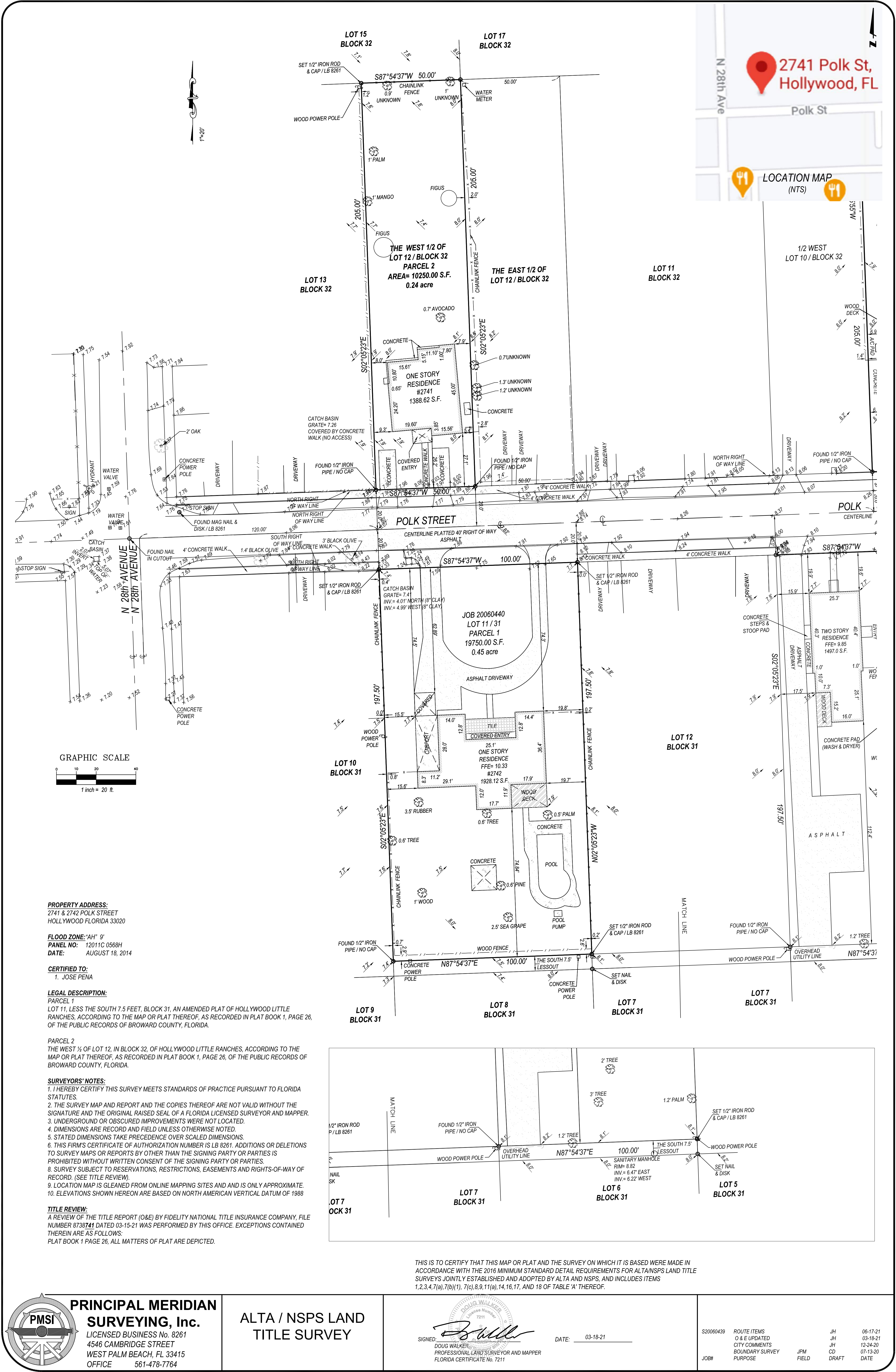
<https://us02web.zoom.us/j/87562518728?pwd=dHgZdkZkREY2eHlaNTV4a0FjeHdiQT09>

- I. Presenter Introductions – Sharon J. Merchant
 - a. Thomas Benedict, The Benedict Bullock Group, PA – Architect
 - b. Brian Bullock, The Benedict Bullock Group, PA – Architect
 - c. Sharon Merchant, The Merchant Strategy – Public Involvement

This meeting will be recorded and will be available for anyone unable to join us this evening. If you have a question after this meeting or would like access to the recorded presentation, please email Info@themerchantstrategy.com.

- II. Opening remarks – Thomas Benedict
- III. Proposed project overview, presentation and community benefits such as upgrading the neighborhood and additional tax money to the city and Presentations – Thomas Benedict will be speaking and Brian Bullock will be handling the presentation.
- IV. Q&A – Thomas Benedict
- V. Close – Sharon Merchant

This meeting will be recorded and will be available for anyone unable to join us this evening. If you have a question after this meeting or would like access to the recorded presentation, please email Info@themerchantstrategy.com.



PROPERTY ADDRESS:
2741 & 2742 POLK STREET
HOLLYWOOD FLORIDA 33020

FLOOD ZONE: AH 9'
PANEL NO: 12011C 0568H
DATE: AUGUST 18, 2014

CERTIFIED TO:
1. JOSE PEÑA

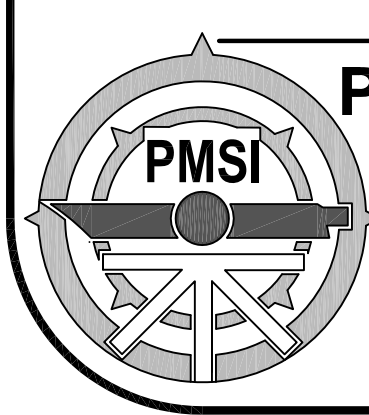
LEGAL DESCRIPTION:
PARCEL 1
LOT 11, LESS THE SOUTH 7.5 FEET, BLOCK 31, AN AMENDED PLAT OF HOLLYWOOD LITTLE RANCHES, ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 1, PAGE 26, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.

PARCEL 2
THE WEST 1/2 OF LOT 12, IN BLOCK 32, OF HOLLYWOOD LITTLE RANCHES, ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 1, PAGE 26, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.

SURVEYORS' NOTES:
1. I HEREBY CERTIFY THIS SURVEY MEETS STANDARDS OF PRACTICE PURSUANT TO FLORIDA STATUTES.
2. THE SURVEY MAP AND REPORT AND THE COPIES THEREOF ARE NOT VALID WITHOUT THE SIGNATURE AND THE ORIGINAL RAISED SEAL OF A FLORIDA LICENSED SURVEYOR AND MAPPER.
3. UNDERGROUND OR OBSCURED IMPROVEMENTS WERE NOT LOCATED.
4. DIMENSIONS ARE RECORD AND FIELD UNLESS OTHERWISE NOTED.
5. STATED DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS.
6. THIS FIRM'S CERTIFICATE OF AUTHORIZATION NUMBER IS LB 8261. ADDITIONS OR DELETIONS TO SURVEY MAPS OR REPORTS BY OTHER THAN THE SIGNING PARTY OR PARTIES IS PROHIBITED WITHOUT WRITTEN CONSENT OF THE SIGNING PARTY OR PARTIES.
7. SURVEY SUBJECT TO RESERVATIONS, RESTRICTIONS, EASEMENTS AND RIGHTS-OF-WAY OF RECORD. (SEE TITLE REVIEW).
8. LOCATION MAP IS GLEANED FROM ONLINE MAPPING SITES AND AND IS ONLY APPROXIMATE.
9. ELEVATIONS SHOWN HEREON ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988

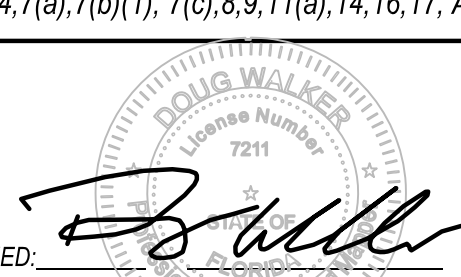
TITLE REVIEW:
A REVIEW OF THE TITLE REPORT (O&E) BY FIDELITY NATIONAL TITLE INSURANCE COMPANY, FILE NUMBER 8738741 DATED 03-15-21 WAS PERFORMED BY THIS OFFICE. EXCEPTIONS CONTAINED THEREIN ARE AS FOLLOWS:
PLAT BOOK 1 PAGE 26, ALL MATTERS OF PLAT ARE DEPICTED.


THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1.2, 3.4, 7(a), 7(b)(1), 7(c), 8.9, 11(a), 14, 16, 17, AND 18 OF TABLE 'A' THEREOF.



PRINCIPAL MERIDIAN SURVEYING, Inc.
LICENSED BUSINESS No. 8261
4546 CAMBRIDGE STREET
WEST PALM BEACH, FL 33415
OFFICE 561-478-7764

ALTA / NSPS LAND TITLE SURVEY



SIGNED: 
DOUG WALKER
PROFESSIONAL LAND SURVEYOR AND MAPPER
FLORIDA CERTIFICATE No. 7211

DATE: 03-18-21

S20060439 JOB#	ROUTE ITEMS	JH	06-17-21
	O & E UPDATED	JH	03-18-21
	CITY COMMENTS	JH	12-24-20
	BOUNDARY SURVEY PURPOSE	CD	07-13-20
		JPM	DATE
		FIELD	
		DRAFT	



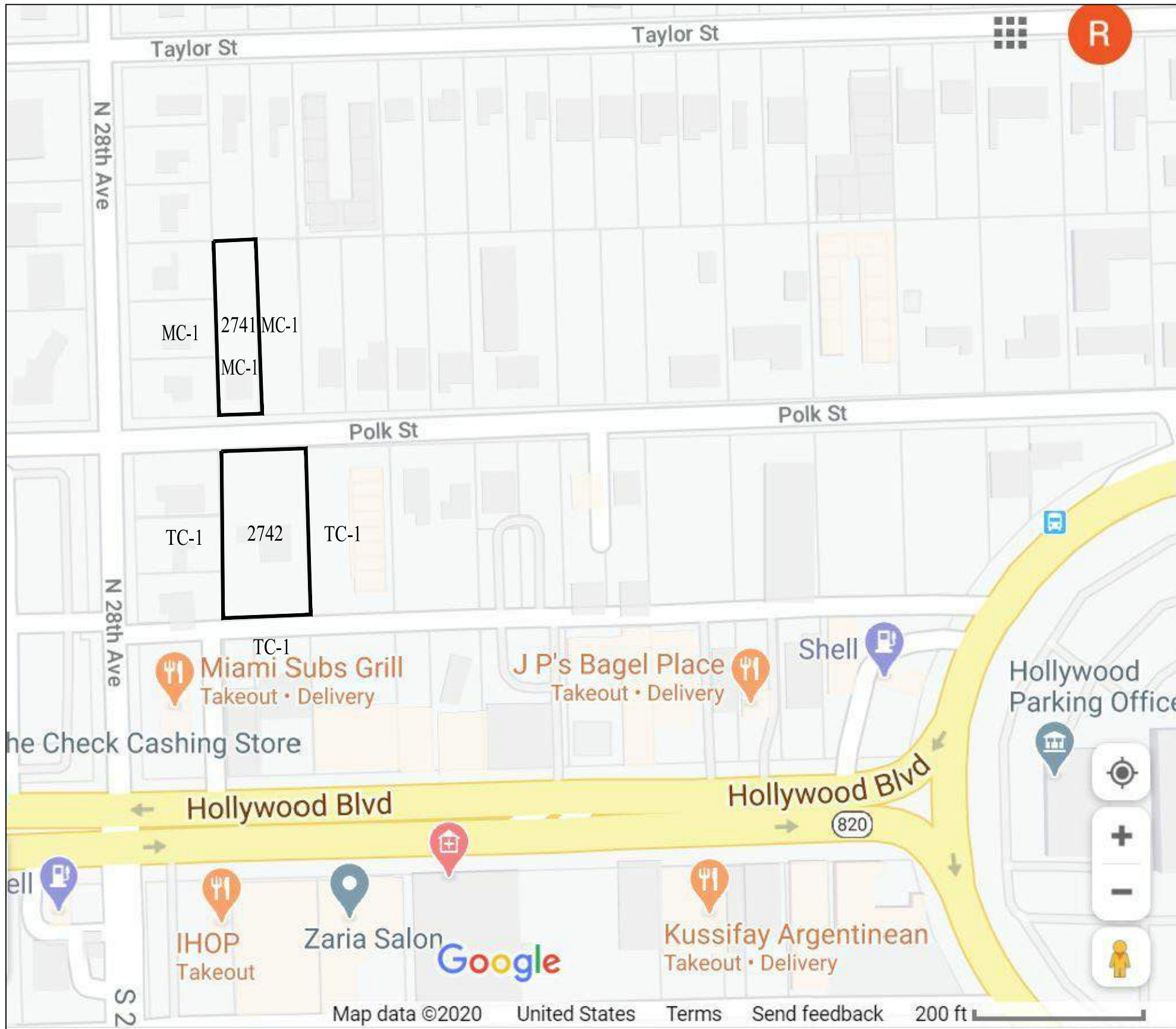
2742 POLK STREET

The
Benedict
Bullock
Group, PA
www.tbhg.net
Lic. No. AA 26003089

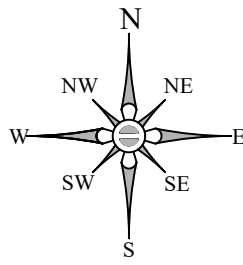


POLK STREET APARTMENTS

2742-2741- POLK STREET HOLLYWOOD, FLORIDA



LOCATION MAP



INDEX OF DRAWINGS

T-1	TITLE SHEET & LOCATION MAP
SP-1	SITE PLAN & SITE DATA TABLE
CS-1	CONTEXTUAL STREET ELEVATION
A-1	FIRST FLOOR BUILDING PLAN
A-2	SECOND FLOOR BUILDING PLAN
A-3	TYPICAL FLOOR PLAN
A-4	3RD THRU 4TH BUILDING PLAN
A-4	ROOF / POOL DECK PLAN
A-5	ELEVATIONS
A-6	ELEVATION & SECTION
TDP1	EXISTING TREE DISPOSITION PLAN
TDP2	EXISTING TREE DISPOSITION PLAN
LP1	LANDSCAPE PLAN
IR-1	IRRIGATION PLAN
C-1	EROSION & SEDIMENT CONTROL PLAN
C-2	CIVIL PLAN
C-3	NOTES & SECTIONS
C-4	SIGNING & PAVEMENT MARKING PLAN
C-5	CIVIL DETAILS
C-6	CIVIL DETAILS
C-7	CIVIL DETAILS
WS1	WATER SERVICE PLAN & NOTES
WS2	WATER SERVICE PLAN
WS3	WATER SERVICE DETAILS
WS4	WATER SERVICE DETAILS

PROJECT DATA

OCCUPANCY : R2 CONSTRUCTION : TYPE Va
SPRINKLERED

CODES AND REFERENCES :

* THE FLORIDA BUILDING CODE, 2020 EDITION*
* NATIONAL ELECTRIC CODE 2017 EDITION*

MEETING DATES

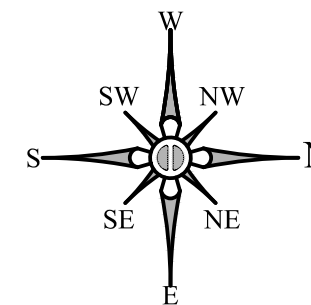
PACO MEETING	JULY 27, 2020
PRELIM TAC MEETING	SEPT 21, 2020
TAC MEETING	FEB 22, 2021
DEV. REVIEW BOARD	SEPT 21, 2021

SYMBOLS

NOTE REFERENCE	④	REFERENCE NUMBER
DOOR REFERENCE	2	DOOR NUMBER
WINDOW REFERENCE	M	WINDOW LETTER
REVISION REFERENCE	Δ	REVISION NUMBER
INTERIOR ELEVATION REFERENCE	4.4	INTERIOR WALL LETTER (KEY TO ELEVATION OR ROOM FINISH SCH.)
	4.4	SHEET NUMBER
DETAIL REFERENCE	Δ	DETAIL NUMBER SHEET NUMBER
BUILDING SECTION REFERENCE	Δ	SECTION NUMBER SHEET NUMBER
FOUNDATION REFERENCE	Δ	FOUNDATION LETTER/NUMBER

The
Benedict
Bullock
Group, PA

ARCHITECTURE PLANNING
1325 NW 38th STREET SUITE 101
HOUSTON, TEXAS 77042
(281) 770-0000 FAX (281) 770-0000
www.benedictbullockgroup.com Lic. No. 64-35000009



POLK STREET APARTMENTS II

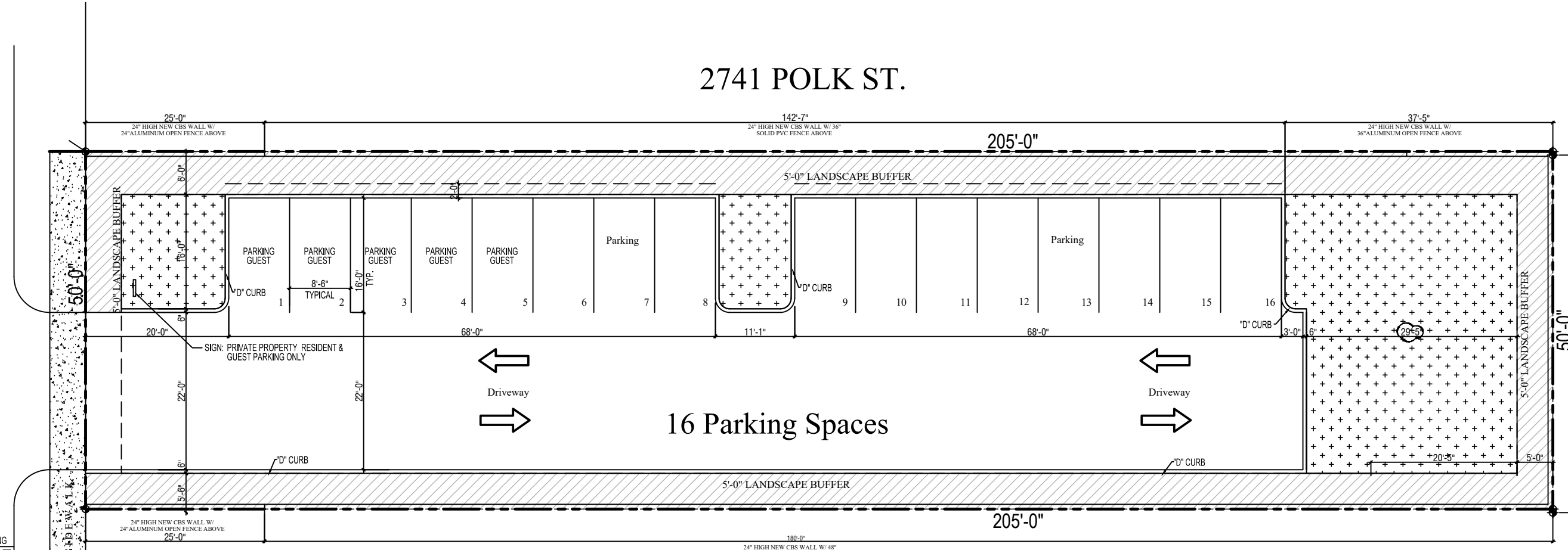
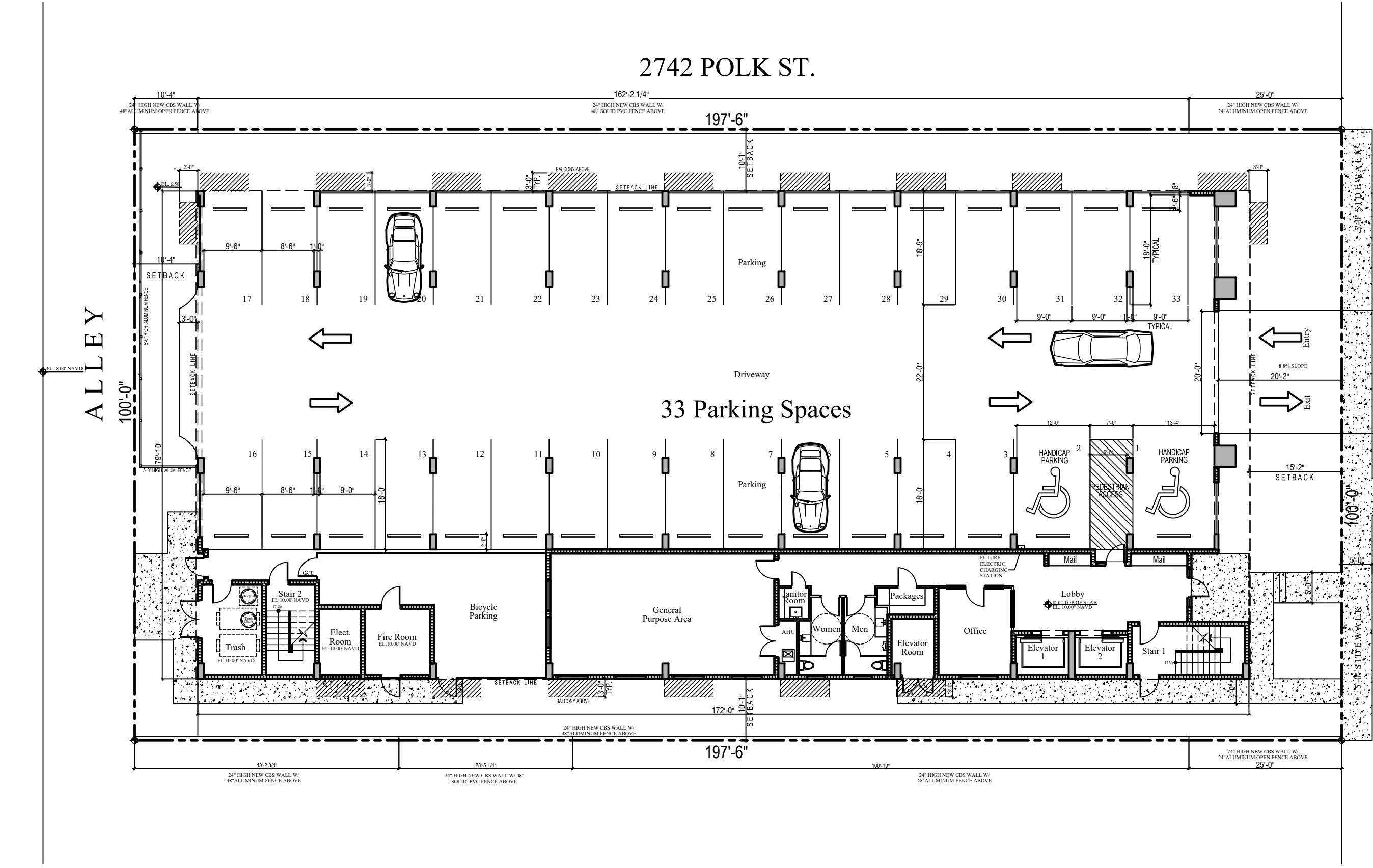
2742-2741- POLK STREET HOLLYWOOD, FLORIDA

ARCHITECT'S SEAL
BRIAN IRA BULLOCK, ARCHITECT
AR 95754

SCALE N.A.
DATE 02-03-2021
DRAWN BY RST
PROJECT NO. BS2030

TITLE SHEET

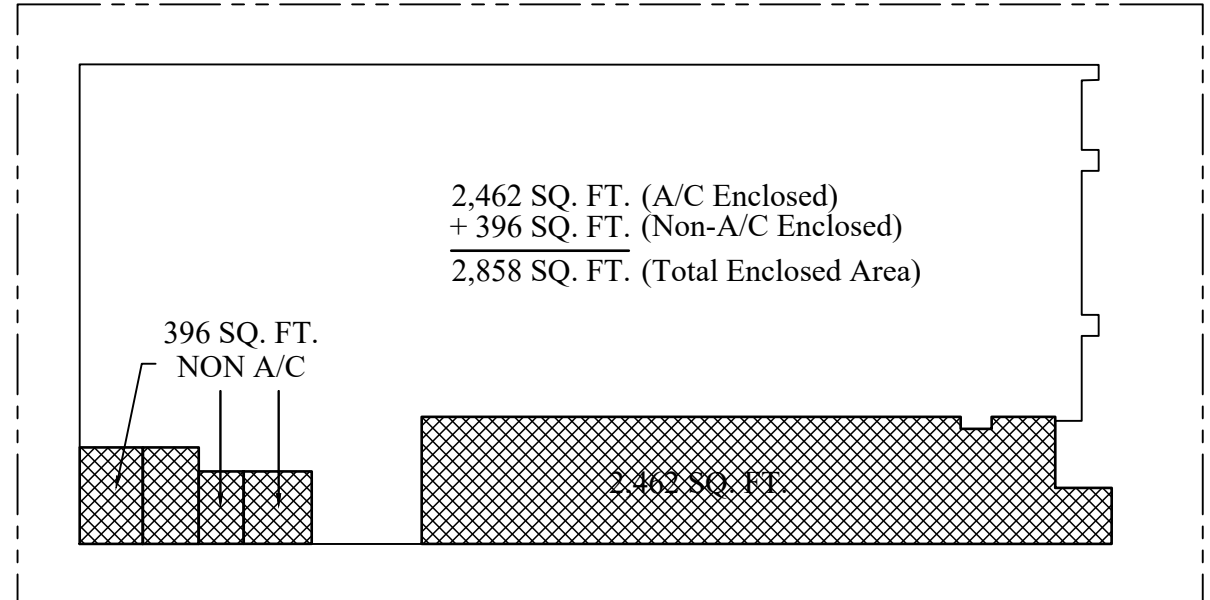
T-1



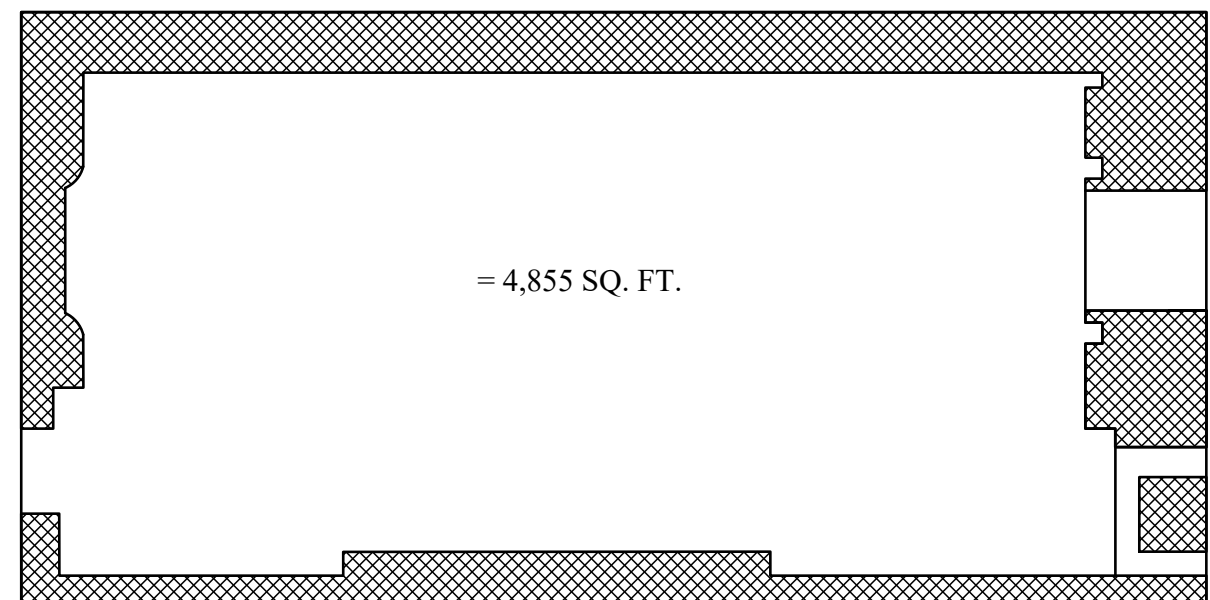
- NOTES
- ALL CHANGES TO THE DESIGN WILL REQUIRE PLANNING REVIEW AND MAYBE SUBJECT TO BOARD APPROVAL.
 - PAVEMENT MARKING SHALL COMPLY WITH THE MANUAL UNIFORM TRAFFIC CONTROL DEVICES.
 - ALL SIGNAGE SHALL BE IN COMPLIANCE WITH THE ZONING AND LANDSCAPE DEVELOPMENT REGULATION.
 - THIRD PARTY FLORIDA GREEN BUILDING COALITION CERTIFICATION TO BE PROVIDED.

SITE PLAN

SCALE 1/16"=1'-0"



FIRST FLOOR ENCLOSED AREA DIAGRAM
SCALE 1/32"=1'-0"



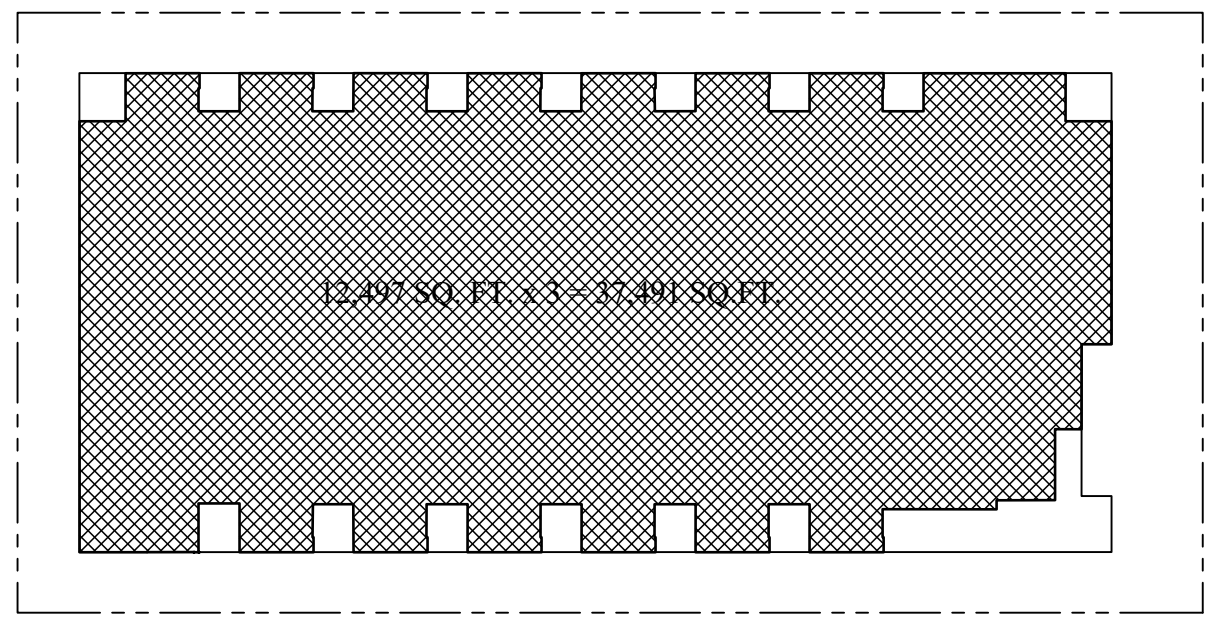
PERVIOUS DIAGRAM
SCALE 1/32"=1'-0"

F.A.R. / ENCLOSE AREA CALCULATION	
FIRST FLOOR	2,858 SQ. FT.
2ND THRU 4TH FLOOR	37,491 SQ. FT.
ROOF POOL DECK	3,169 SQ. FT.
TOTAL ENCLOSED AREA	43,518 SQ. FT.

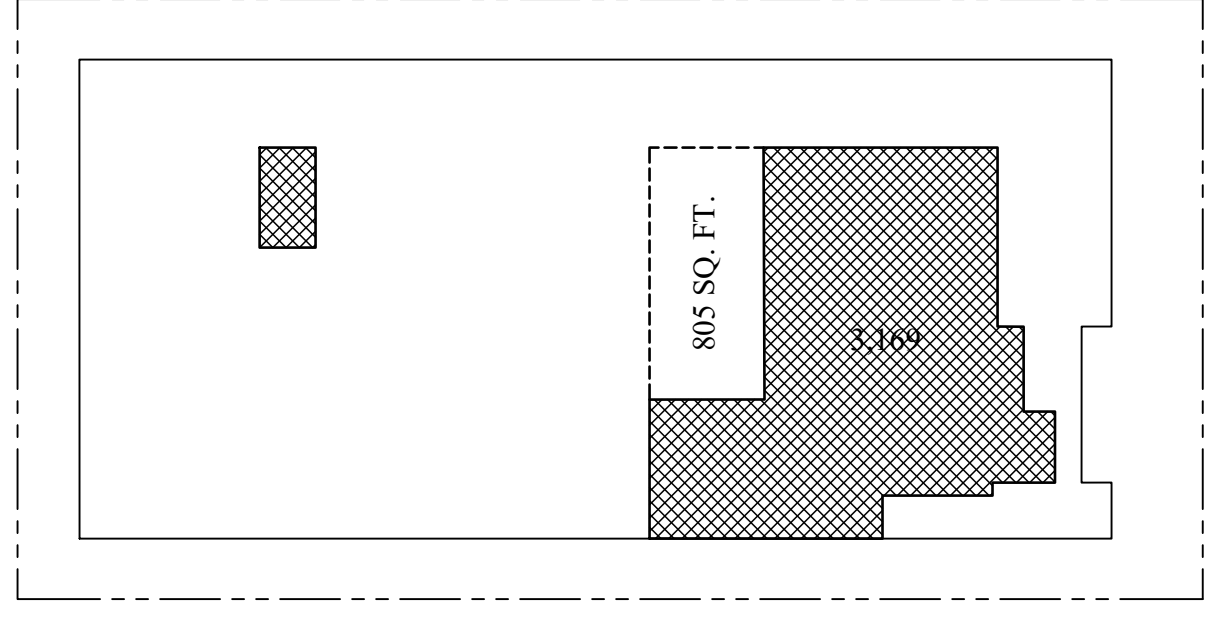
PARKING CALCULATION:	
44 UNITS x 1/U	= 44 SPACES
GUEST 1 SPACE/10U	= 4.4 SPACES
	= 48.4 SPACES
	= 49 SPACES REQUIRED

2741 POLK STREET. LANDSCAPE CALCULATION:	
VUA = SITE - BUFFER AREA x 15%	
VUA = 10,250 - 2,450 x 15%	= 1,170 SQ. FT.
PERVIOUS AREA	
LANDSCAPE REQUIRED (40%)	= 4100 SQ. FT.
LANDSCAPE AREA	= 1,665 SQ. FT.
BUFFER AREA	= 2,534 SQ. FT.
LANDSCAPE PROVIDED (40.2%)	= 4119 SQ. FT.
IMPERVIOUS AREA	
DRIVEWAY AREA	= 3742 SQ. FT.
PARKING AREA	= 2,389 SQ. FT.
HARDSCAPE PROVIDED (59.8%)	= 6,131 SQ. FT.

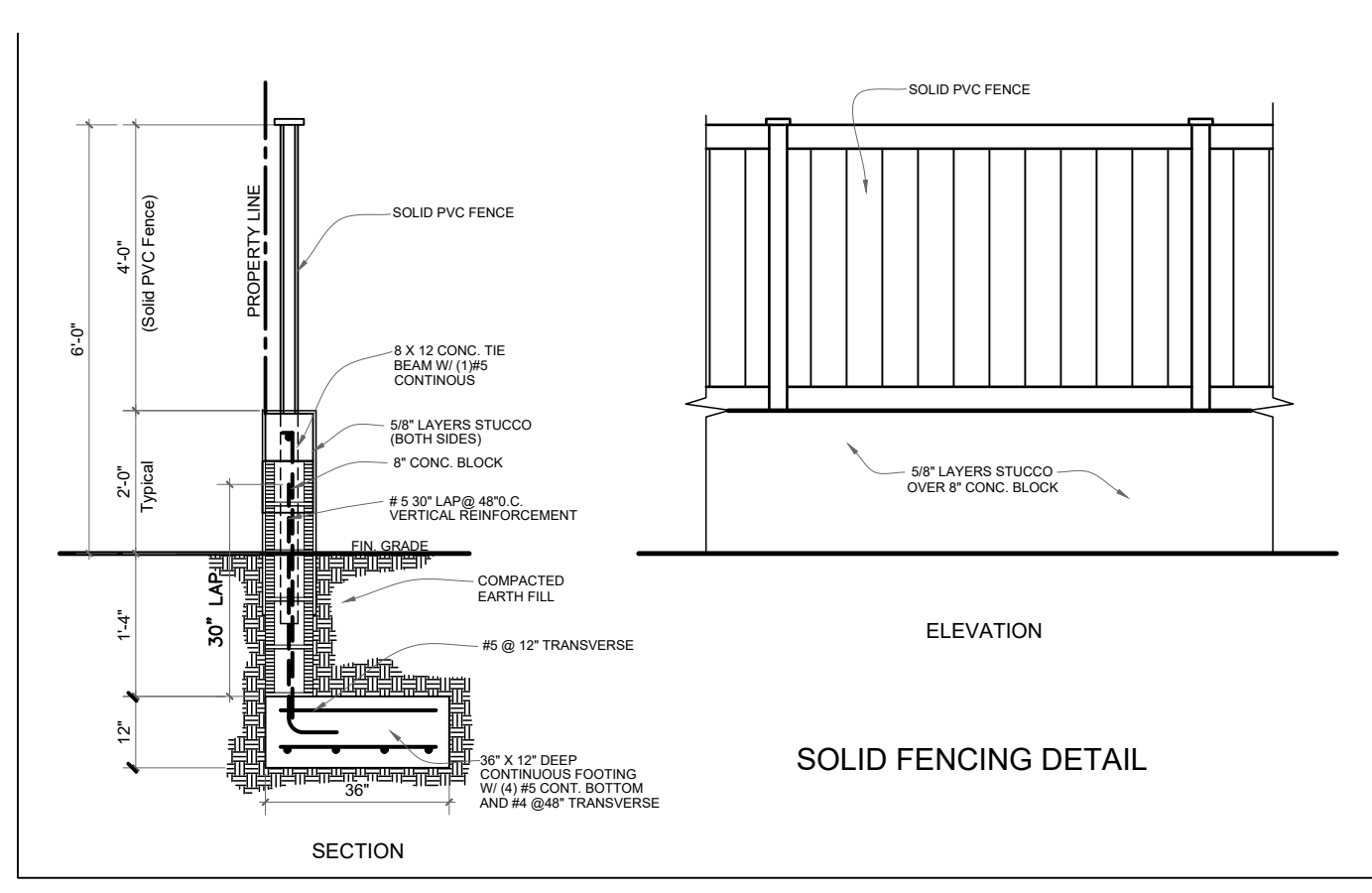
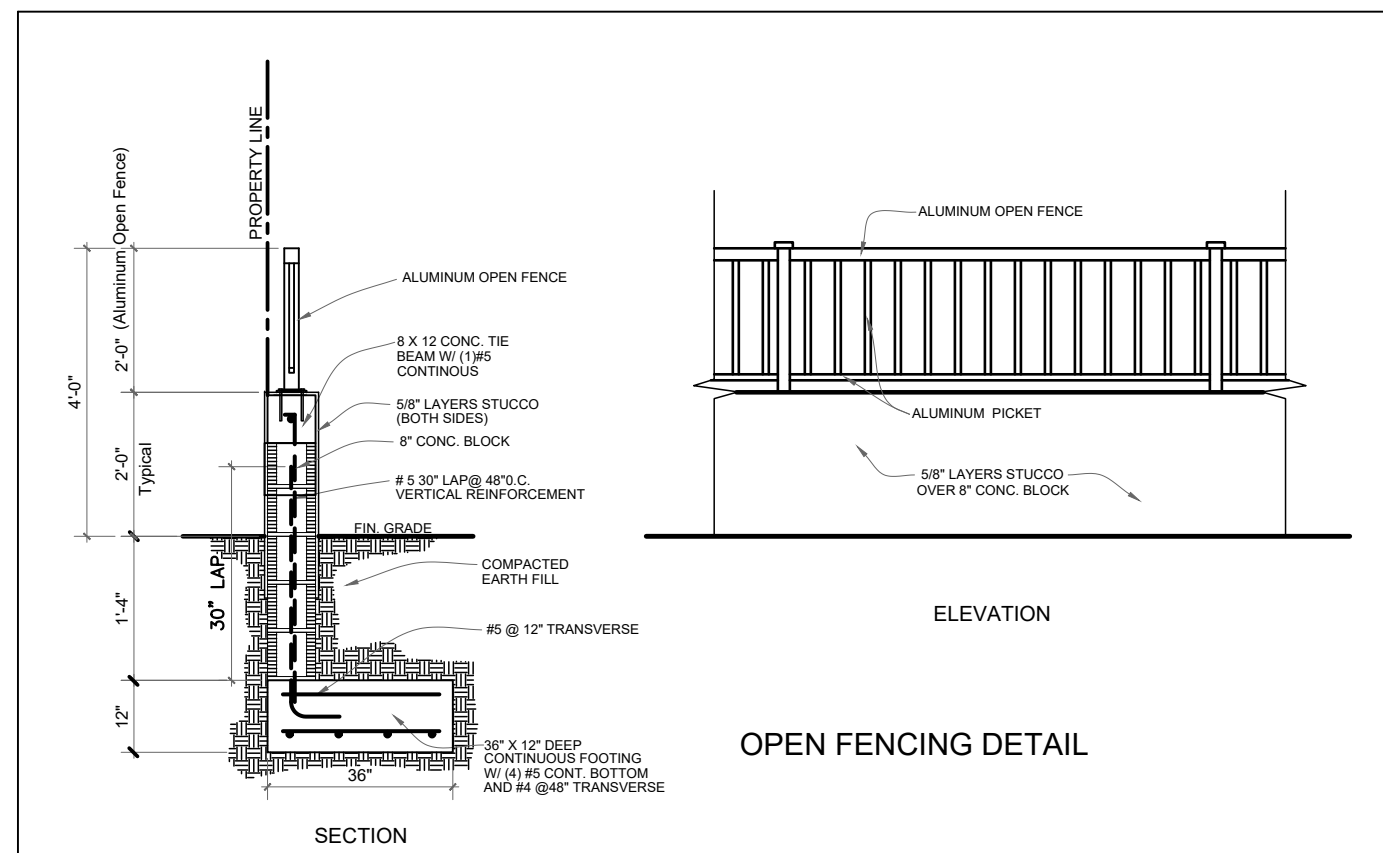
LEGAL DESCRIPTION
2741 POLK ST. THE WEST OF 1/2 LOT 12, IN BLOCK 32 OF HOLLYWOOD LITTLE RANCHES ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 1, PAGE(S) 26 OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.
2742 POLK ST. LOT 11 LESS THE SOUTH 7.5 FEET THERE OF, BLOCK 31, AN AMENDED PLAT OF HOLLYWOOD LITTLE RANCHES ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 1, PAGE(S) 26 OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.



2ND THRU 4TH FLOOR ENCLOSED AREA DIAGRAM
SCALE 1/32"=1'-0"



ROOF POOL DECK ENCLOSED AREA DIAGRAM
SCALE 1/32"=1'-0"



ZONING INFORMATION			
ZONE	REMARKS	REQUIRED/ALLOWED	PROVIDED
ZONE	TC-1 (SOUTH OF POLK STREET)		
ZONE	MC-1 (NORTH OF POLK STREET)		
LAND USE	REGIONAL ACTIVITY CENTER RAC		
LOT SIZE	BLDG. LOT - 197.5'x100', PARKING LOT 205' x 50'		
LOT AREA	BLDG. LOT - 197.5' x 100' = 19,750 SQ. FT. = 0.453 ACRES		
LOT AREA	PARKING LOT - 205' x 50' = 10,250 SQ. FT. = 0.235 ACRES		
TOTAL AREA OF LOTS	30,000 SQ. FT. = 0.689 ACRES		
F.A.R.	30,000 x 1.5 = 45,000 SQ. FT.	45,000 SQ. FT.	43,518 SQ. FT.
FRONT SETBACK		15 Ft. MIN.	15'-2" & 20'-0"
SIDE SETBACK		10 Ft. MIN.	10'-1"
REAR SETBACK		10 Ft. MIN.	10'-4"
PERVIOUS AREA	2742/2741 POLK ST.		4,855 SQ. FT. = 0.11 ACRES
IMPERVIOUS AREA	2742/2741 POLK ST.		14,895 SQ. FT. = 0.34 ACRES
PARKING	47 REGULAR PARKING SPACES + 2 HANDICAP PARKING	49 SPACES	49 SPACES
PARKING SPACE SIZES		GARAGE PARKING (9'-0" Clear x 18'-0") OPEN PARKING (8'-6" x 18'-0")	33 SPACES 16 SPACES
LIGHTING LEVEL	MAX. 0.5 FOOT-CANDLE AT ALL PROPERTY LINES		
NO. OF UNITS			44
UNIT SIZE	AIRCONDITIONED SPACE	650 SQ. FT. MIN. AVERAGE	650 SQ. FT. Interior Unit 632 SQ. FT. End Unit
BALCONY SIZE	END UNITS = 105 SQ. FT. INTERIOR UNITS = 81 SQ. FT.		
UNIT TYPE	1 BEDROOM, 1 1/2 BATH		44
AREA TABULATION			
FIRST FLOOR	GARAGE NON-A/C = 11,269 SQ. FT. A/C AREA = 2,462 SQ. FT.		13,731 SQ. FT.
TYPICAL FLOOR (2ND THRU 4TH)	A/C AREA = 12,356 SQ. FT. NON A/C AREA = 1,722 SQ. FT.		14,078 SQ. FT.
RECREATION LEVEL	A/C AREA = 2,990 SQ. FT. ROOF AREA = 13,677 SQ. FT.	30% OF ROOF = 4,013 SQ. FT.	3,964 SQ. FT. (29.0%)
TOTAL A/C AREA			42,555 SQ. FT.
TOTAL BUILDING LENGTH			172'-0"
STRUCTURE HEIGHT			40'-0"
MAX. HEIGHT OF VERTICAL ENCROACHMENT			50 Ft. 17'-0" OR TOTAL OF 57'-0"

ALL IDEAS, DESIGNS, ARRANGEMENTS, & PLANS INDICATED OR REPRESENTED BY THIS DRAWING ARE OWNED BY & THE PROPERTY OF THE ARCHITECT & WERE CREATED, EVOLVED, & DEVELOPED FOR USE ON, & IN CONNECTION WITH THE SPECIFIED PROJECT. NONE OF SUCH IDEAS, DESIGNS, ARRANGEMENTS, OR PLANS SHALL BE USED BY, OR DISCLOSED TO ANY PERSON, FIRM, OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT THE WRITTEN PERMISSION OF BRIAN RA BULLOCK, ARCHITECT. WRITTEN DIMENSIONS ON THESE DRAWINGS SHALL HAVE PRECEDENCE OVER SCALE DIMENSIONS. CONTRACTORS SHALL VERIFY & BE RESPONSIBLE FOR ALL DIMENSIONS & CONDITIONS ON THE JOB. THE ARCHITECT MUST BE NOTIFIED OF ANY VARIATIONS FROM THE DIMENSIONS AND CONDITIONS SHOWN BY THESE DRAWINGS. SHOP DETAILS OF ADEQUATE SCALE MUST BE SUBMITTED TO THE ARCHITECT FOR APPROVAL BEFORE PROCEEDING WITH FABRICATION ON ITEMS SO NOTED.

BY

DATE

REV

REVISIONS

The

Benedict

Bullock

Group, PA

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POLK STREET APARTMENTS II

2742-2741- POLK STREET HOLLYWOOD, FLORIDA

ARCHITECT'S SEAL

BRIAN RA BULLOCK

AR 95754

BRIAN BULLOCK, ARCHITECT

AR 95754

SCALE

1/16"=1'-0"

DATE

02-03-2021

DRAWN BY

RST

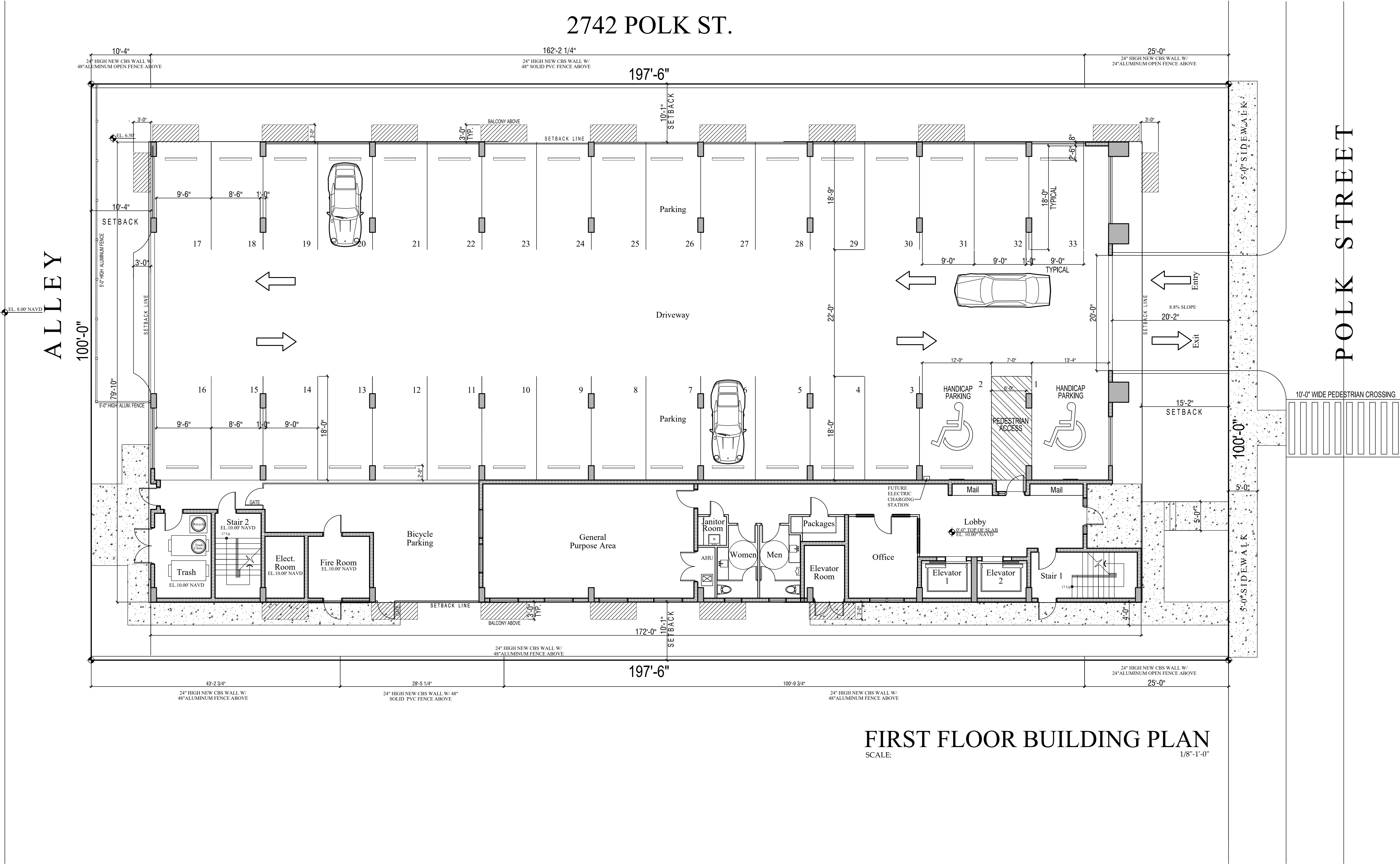
PROJECT NO.

BS2030

SITE PLAN

SP-1

DEV.BRD PERMIT 08-13-2021



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REV	REVISIONS	BY	DATE

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DORSETT, FLORIDA 33428
(850) 570-0500 FAX (850) 570-0550
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POLK STREET APARTMENTS II
2742-2741- POLK STREET HOLLYWOOD, FLORIDA

ARCHITECT'S SEAL

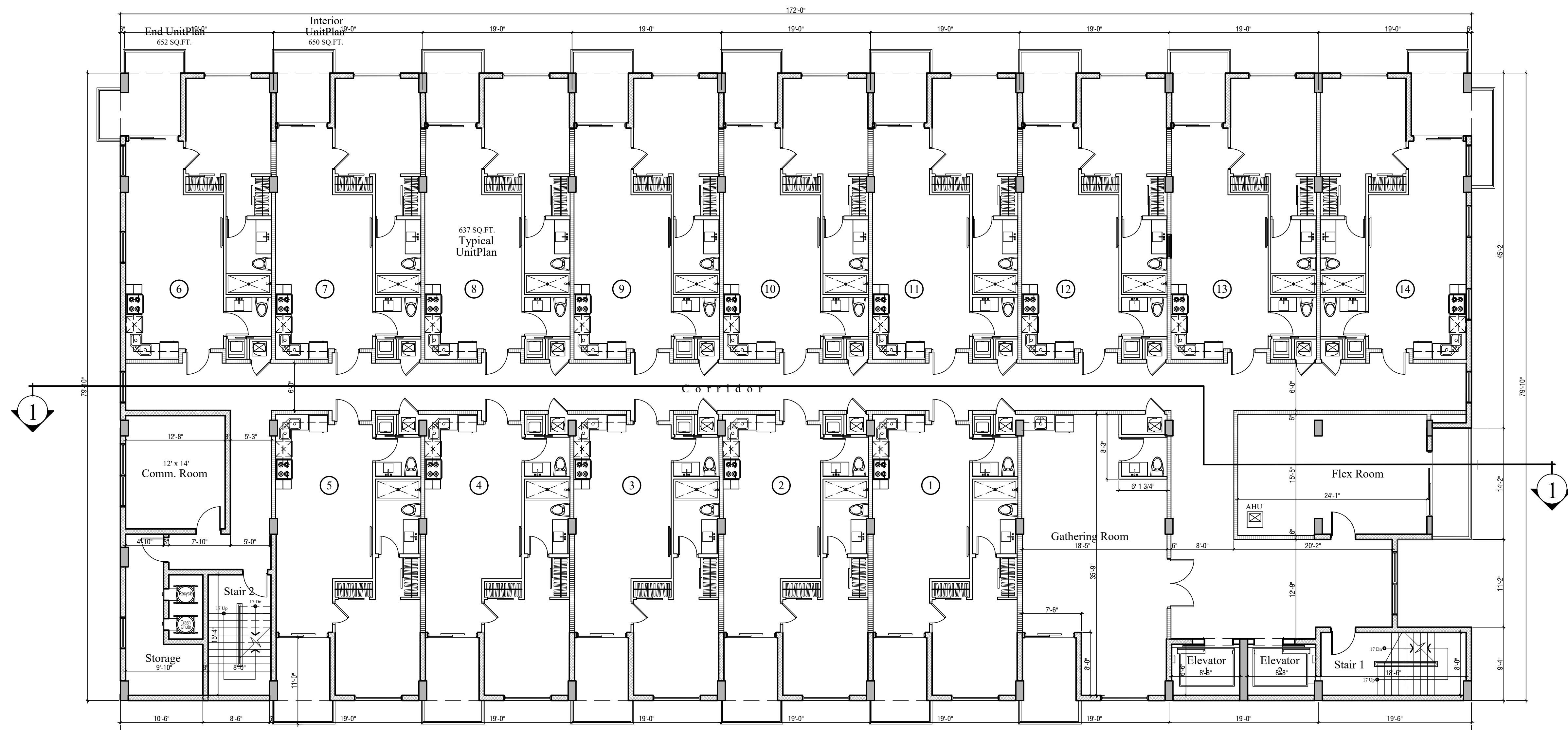
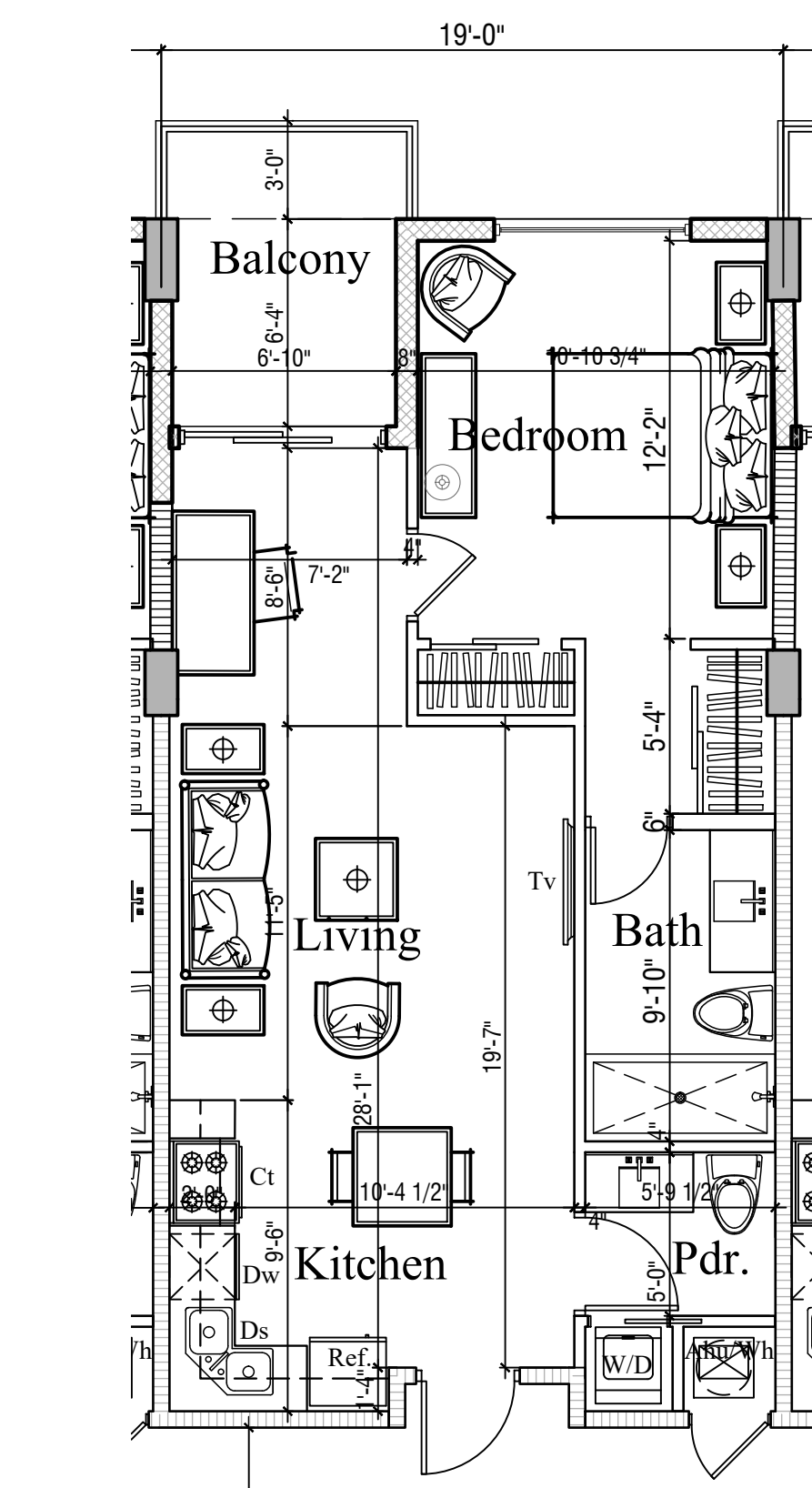
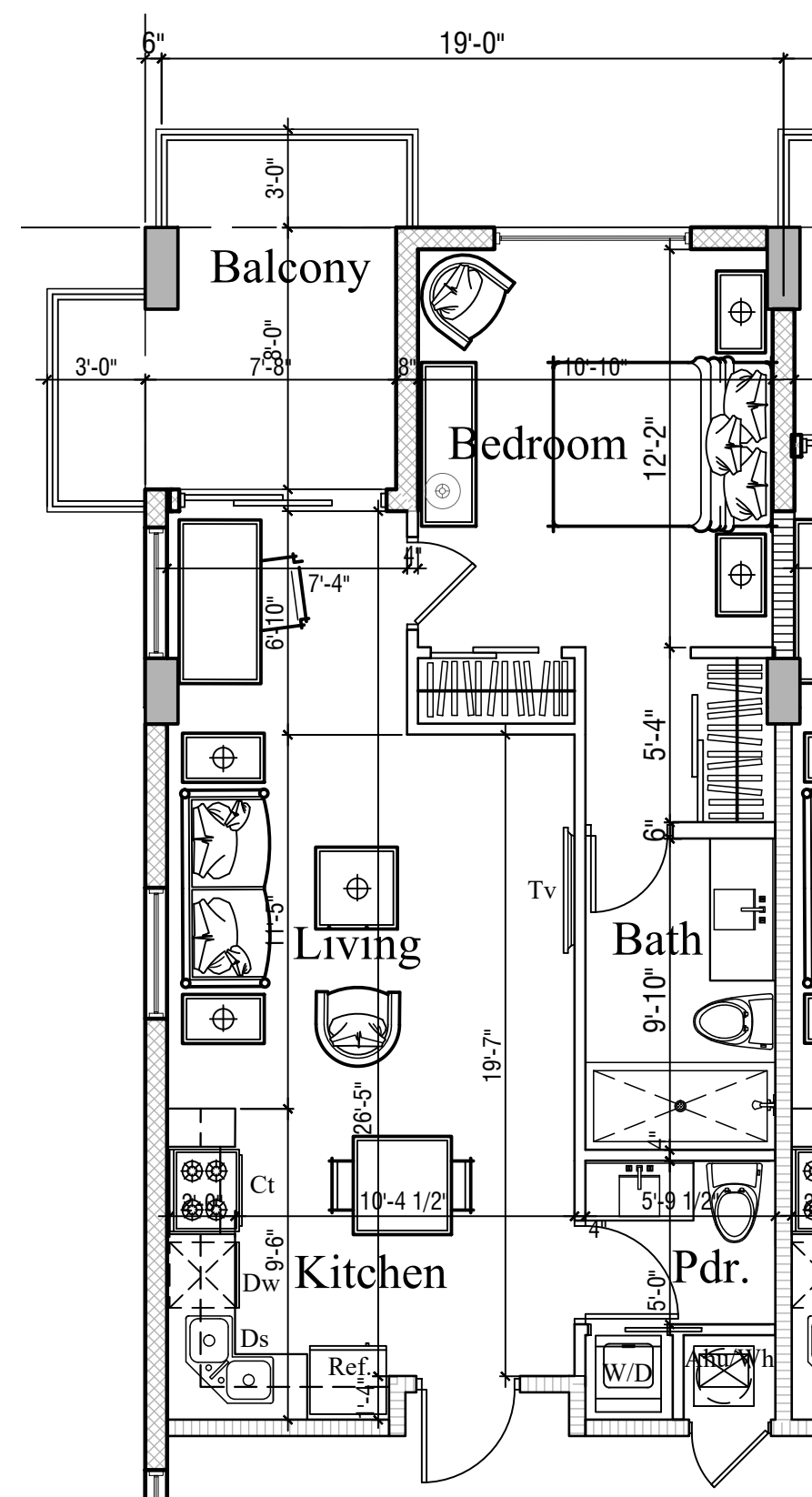
BRIAN BULLOCK, ARCHITECT
A.P. 05754

SCALE 1/8"=1'-0"
DATE 02-03-2021
DRAWN BY RST
PROJECT NO. BS2030

FIRST FLOOR
BUILDING PLAN

A-1

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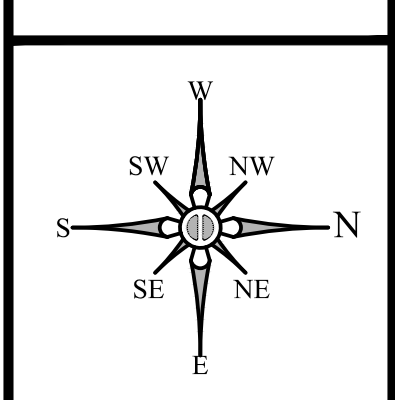
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Bullock**
Group, PA

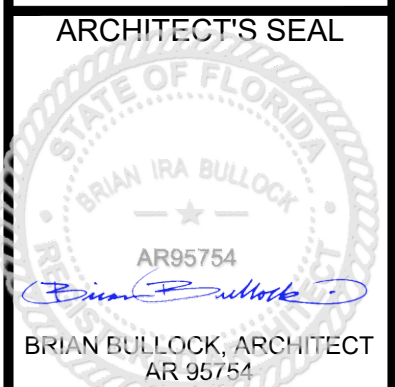
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1525 NW 58th STREET
DEERFIELD BEACH,
(561) 578-9500
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SUITE NO.1
FLORIDA 33442
FAX (561) 578-9550
Lic. No. AA, 2660389



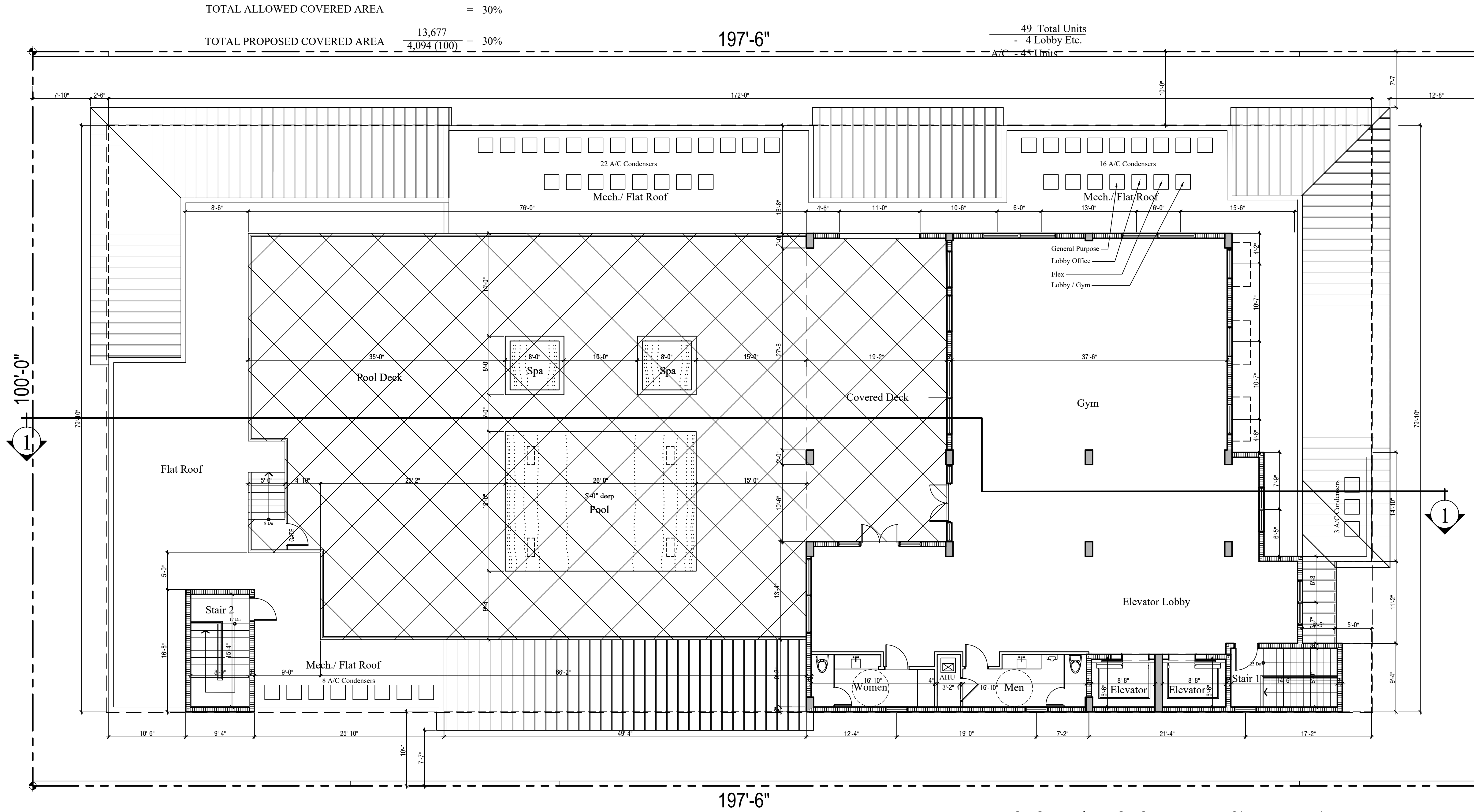
POLK STREET APARTMENTS II
2742-2741- POLK STREET HOLLYWOOD, FLORIDA



SCALE	1/8"=1'-0"
DATE	02-03-2021
DRAWN BY	RST
PROJECT NO.	BS2030

SECOND FLOOR
BUILDING PLAN &
TYP. UNIT PLAN

A-2



ROOF / POOL DECK PLAN
SCALE: 1/8"=1'-0"

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DEV.BRD PERMIT 08-13-2021

The Benedict Bullock Group, PA

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FORT LAUDERDALE, FL 33309
(954) 576-6556 FAX (954) 576-6550
www.benedictgroup.com Lic. No. AS 2005089

SW NW
SE NE
S N
E W

POLK STREET APARTMENTS II
2742-2741- POLK STREET HOLLYWOOD, FLORIDA

ARCHITECT'S SEAL

BRIAN W. BULLOCK
AR95754

BRIAN BULLOCK, ARCHITECT
AR 95754

SCALE	1/8"=1'-0"
DATE	02-03-2021
DRAWN BY	RST
PROJECT NO.	BS2030

ROOF PLAN

A-4



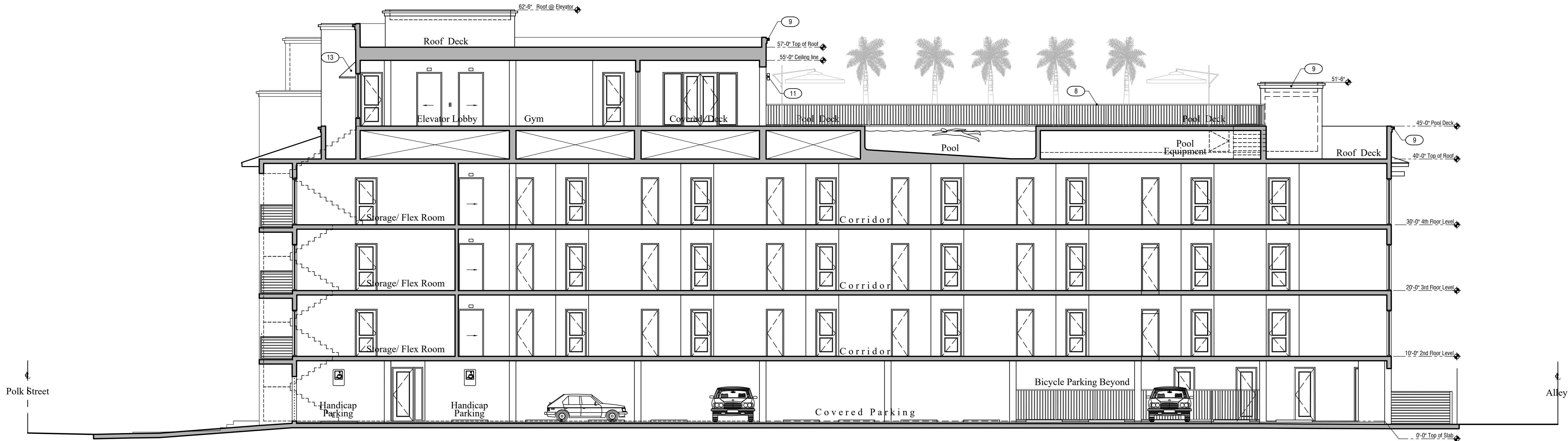
- WEST ELEVATION
SCALE: 1/8"=1'-0"

SCALE	
DATE	1/8"=1'-0" 03-03-202
DRAWN BY	RST
PROJECT NO.	BS2030

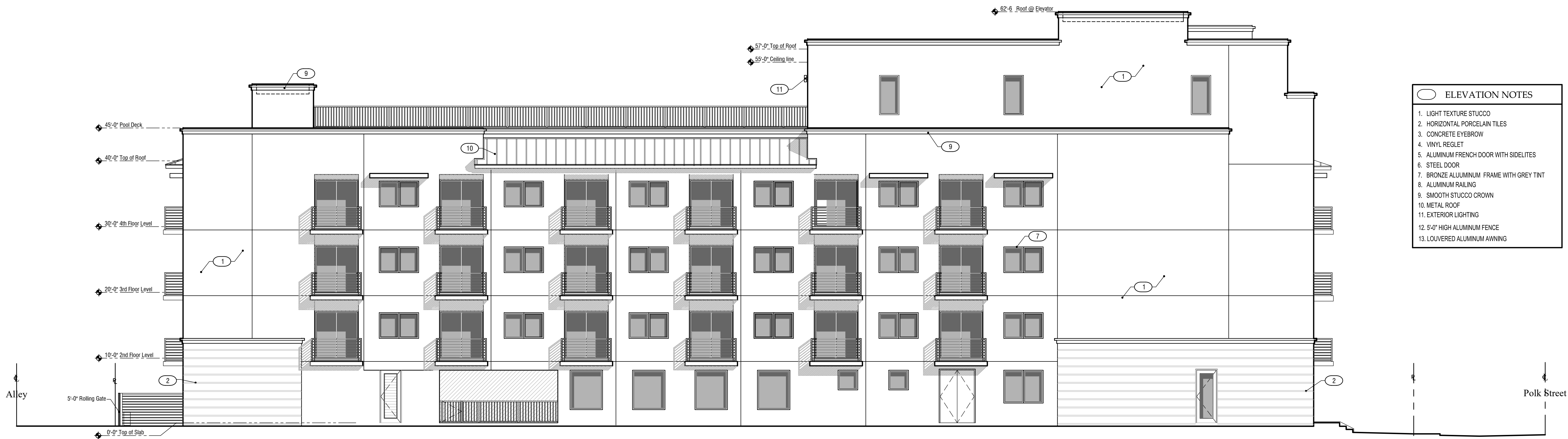
ELEVATIONS

A-5

DEV.BRD PERMIT 08-13-2021



EAST ELEVATION
SCALE: 1/8"=1'-0"



SECTION - 1
SCALE: 1/8"=1'-0"

- ELEVATION NOTES
1. LIGHT TEXTURE STUCCO
 2. HORIZONTAL PORCELAIN TILES
 3. CONCRETE EYEBROW
 4. VINYL REGLET
 5. ALUMINUM FRENCH DOOR WITH SIDELITES
 6. STEEL DOOR
 7. BRONZE ALUMINUM FRAME WITH GREY TINT
 8. ALUMINUM RAILING
 9. SMOOTH STUCCO CROWN
 10. METAL ROOF
 11. EXTERIOR LIGHTING
 12. 5'-0" HIGH ALUMINUM FENCE
 13. LOUVERED ALUMINUM AWNING

REV	REVISIONS	BY	DATE

The Benedict Bullock Group, PA

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POLK STREET APARTMENTS II
2742-2741- POLK STREET HOLLYWOOD, FLORIDA

ARCHITECT'S SEAL

BRIAN RA BULLOCK
AR 95754
BRIAN BULLOCK, ARCHITECT
AR 95754

SCALE	1/8"=1'-0"
DATE	02-03-2021
DRAWN BY	RST
PROJECT NO.	BS2030

ELEVATION & SECTION

A-6

ALL IDEAS, DESIGNS, ARRANGEMENTS, & PLANS INDICATED OR REPRESENTED BY THIS DRAWING ARE OWNED BY, & THE PROPERTY OF THE ARCHITECT & WERE CREATED, EVOLVED, & DEVELOPED FOR USE ON, & IN CONNECTION WITH THE SPECIFIED PROJECT. NONE OF SUCH IDEAS, DESIGNS, ARRANGEMENTS, OR PLANS SHALL BE USED BY, OR DISCLOSED TO ANY PERSON, FIRM, OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT THE WRITTEN PERMISSION OF BRIAN RA BULLOCK, ARCHITECT. WRITTEN DIMENSIONS ON THESE DRAWINGS SHALL HAVE PRECEDENCE OVER SCALE DIMENSIONS. CONTRACTORS SHALL VERIFY & BE RESPONSIBLE FOR ALL DIMENSIONS & CONDITIONS ON THE JOB. THE ARCHITECT MUST BE NOTIFIED OF ANY VARIATIONS FROM THE DIMENSIONS AND CONDITIONS SHOWN BY THESE DRAWINGS. SHOP DETAILS OF ADEQUATE SCALE MUST BE SUBMITTED TO THE ARCHITECT FOR APPROVAL BEFORE PROCEEDING WITH FABRICATION ON ITEMS SO NOTED.

DEV BRD PERMIT 08-13-2021

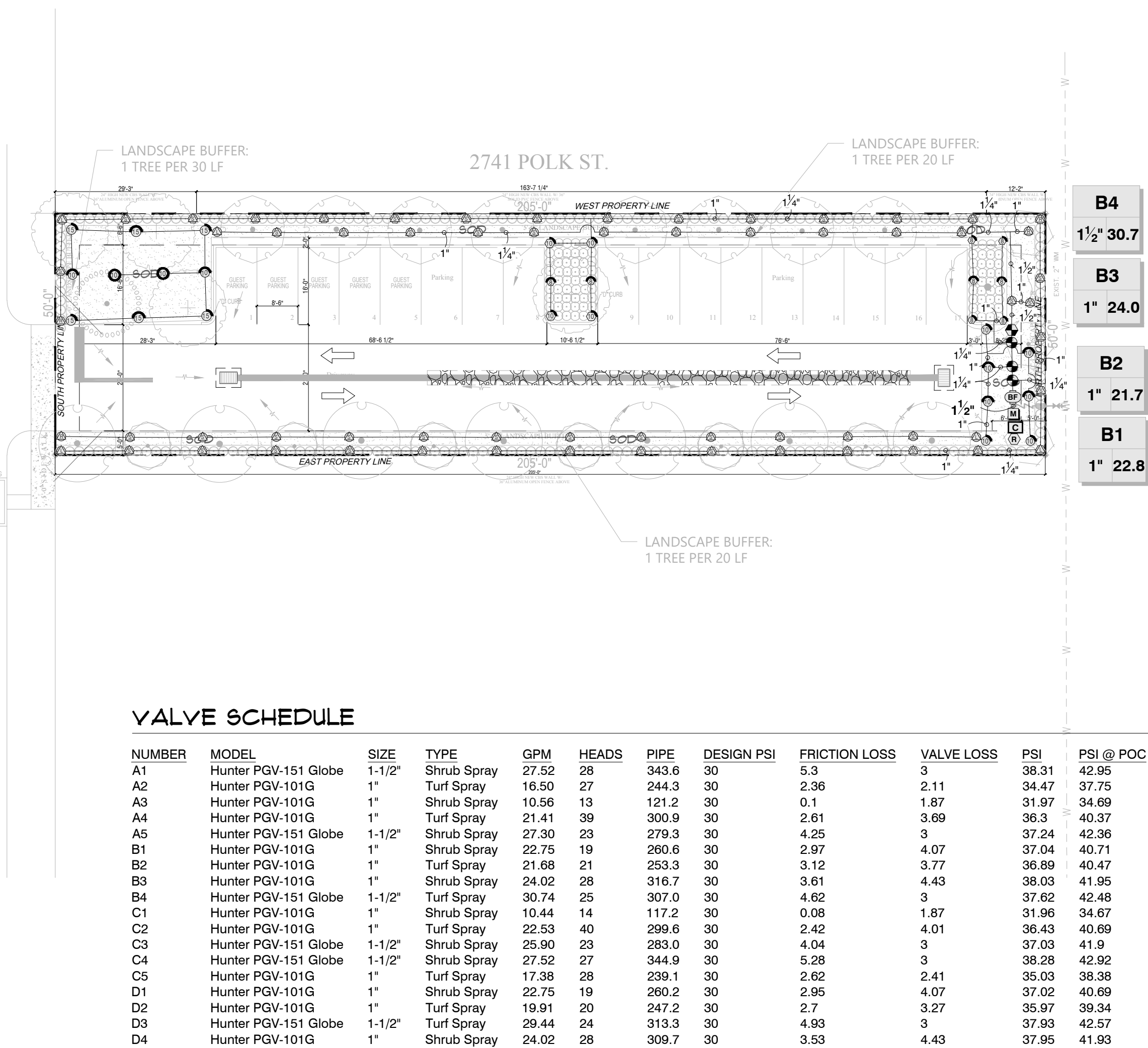


ALLEY

POLK STREET

2742 POLK ST.

2741 POLK ST.



REV.	DATE	
REVISIONS		

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1521 NW 80 STREET SUITE 501
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NUMBER	MODEL	SIZE	TYPE	GPW	HEADS	PIPE	DESIGN PSI	FRICTION LOSS	VALVE LOSS	PSI	PSI @ POC	PRECIP
A1	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	27.52	28	343.6	30	5.3	3	38.31	42.95	1.66 in/h
A2	Hunter PGV-101G	1"	Turf Spray	16.50	27	244.3	30	2.36	2.11	34.47	37.75	1.71 in/h
A3	Hunter PGV-101G	1"	Shrub Spray	10.56	13	121.2	30	0.1	1.87	31.97	34.69	1.7 in/h
A4	Hunter PGV-101G	1"	Turf Spray	21.41	39	300.9	30	2.61	3.69	36.3	40.37	1.81 in/h
A5	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	27.30	23	279.3	30	4.25	3	37.24	42.36	1.72 in/h
B1	Hunter PGV-101G	1"	Shrub Spray	22.75	19	260.6	30	2.97	4.07	37.04	40.71	1.7 in/h
B2	Hunter PGV-101G	1"	Turf Spray	21.68	21	253.3	30	3.12	3.77	36.89	40.47	1.64 in/h
B3	Hunter PGV-101G	1"	Shrub Spray	24.02	28	316.7	30	3.61	4.43	38.03	41.95	1.57 in/h
B4	Hunter PGV-151 Globe	1-1/2"	Turf Spray	30.74	25	307.0	30	4.62	3	37.62	42.48	1.81 in/h
C1	Hunter PGV-101G	1"	Shrub Spray	10.44	14	117.2	30	0.08	1.87	31.96	34.67	1.78 in/h
C3	Hunter PGV-101G	1"	Turf Spray	22.53	38	299.3	30	2.42	4.01	36.43	40.69	1.8 in/h
C4	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	25.90	23	283.0	30	4.4	3	37.03	41.9	1.65 in/h
C5	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	27.52	27	344.9	30	5.28	3	38.28	42.92	1.65 in/h
C4	Hunter PGV-101G	1"	Turf Spray	17.38	28	239.1	30	2.62	2.41	35.03	38.38	1.76 in/h
D1	Hunter PGV-101G	1"	Shrub Spray	22.75	19	260.2	30	2.95	4.07	37.02	40.69	1.72 in/h
D2	Hunter PGV-101G	1"	Turf Spray	19.91	20	247.2	30	2.7	3.27	35.97	39.34	1.59 in/h
D3	Hunter PGV-151 Globe	1-1/2"	Turf Spray	29.44	24	313.3	30	4.93	3	37.93	42.57	1.69 in/h
D4	Hunter PGV-101G	1"	Shrub Spray	24.02	28	309.7	30	3.53	4.43	37.95	41.93	1.57 in/h

P.O.C. NUMBER: 01
Water Source Information:

FLOW AVAILABLE
Water Meter Size: 1-1/2"
Flow Available: 75.00 gpm

PRESSURE AVAILABLE	
Static Pressure at POC:	60.00 PSI
Elevation Change:	5.00 ft
Service Line Size:	3"
Length of Service Line:	20 ft
Pressure Available:	57.00 psi

DESIGN ANALYSIS	
Maximum Station Flow:	27.52 gpm
Flow Available at POC:	75.00 gpm
Residual Flow Available:	47.48 gpm

Pressure Req. at Critical Station:	38.30 psi
Loss for Fittings:	0.05 psi
Loss for Main Line:	0.47 psi
Loss for POC to Valve Elevation:	0.00 psi
Loss for Backflow:	2.58 psi
Loss for Water Meter:	1.55 psi
Critical Station Pressure at POC:	42.95 psi
Pressure Available:	57.00 psi
Residual Pressure Available:	14.05 psi

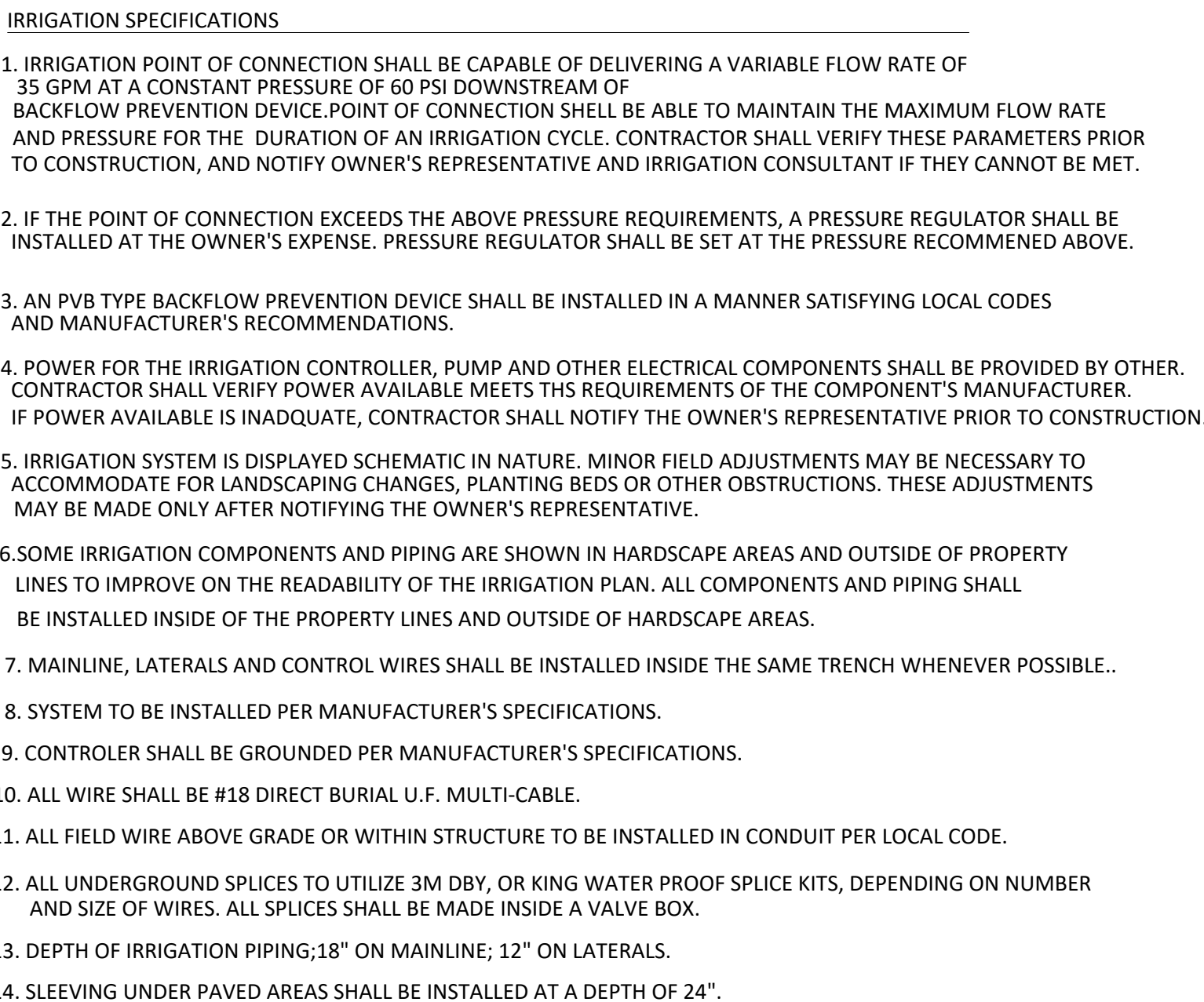
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Water Source Information:

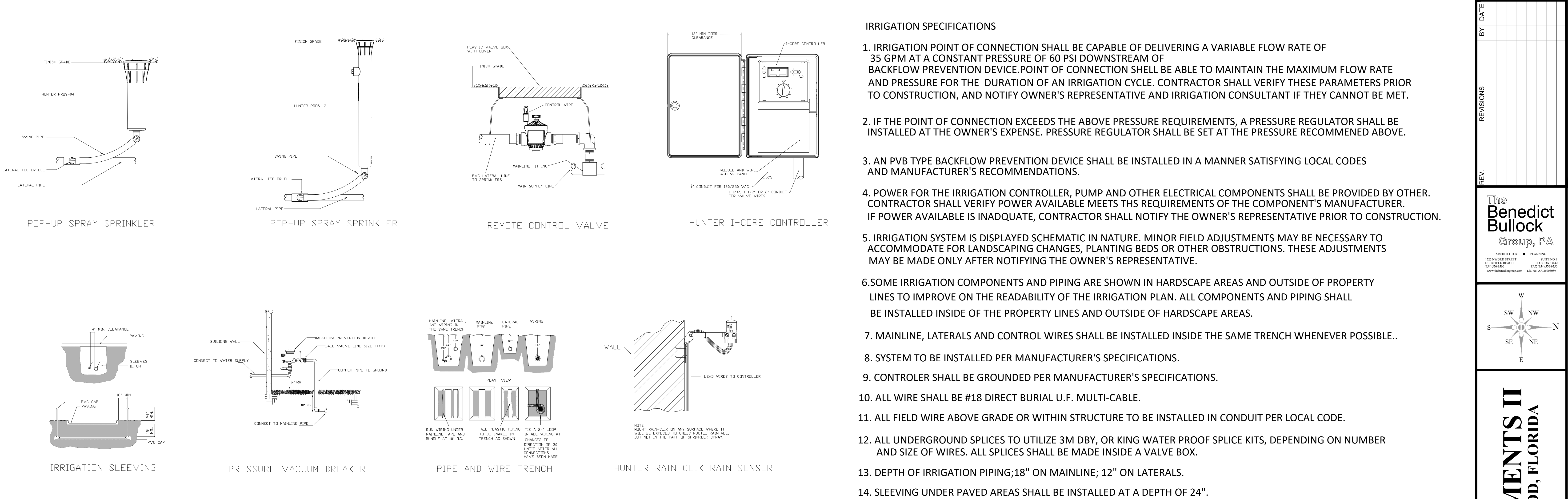
FLOW AVAILABLE
Water Meter Size: 1-1/2"
Flow Available: 75.00 gpm

PRESSURE AVAILABLE	
Static Pressure at POC:	60.00 PSI
Elevation Change:	5.00 ft
Service Line Size:	3"
Length of Service Line:	20 ft
Pressure Available:	57.00 psi

DESIGN ANALYSIS	
Maximum Station Flow:	30.74 gpm
Flow Available at POC:	75.00 gpm
Residual Flow Available:	44.26 gpm

Pressure Req. at Critical Station:	37.63 psi
Loss for Fittings:	0.03 psi
Loss for Main Line:	0.29 psi
Loss for POC to Valve Elevation:	0.00 psi
Loss for Backflow:	2.61 psi
Loss for Water Meter:	1.92 psi
Critical Station Pressure at POC:	42.48 psi
Pressure Available:	57.00 psi
Residual Pressure Available:	14.52 psi





CRITICAL ANALYSIS

P.O.C. NUMBER: 01
Water Source Information:

FLOW AVAILABLE
Water Meter Size: 1-1/2"
Flow Available: 75.00 gpm

PRESSURE AVAILABLE
Static Pressure at POC: 60.00 PSI
Elevation Change: 5.00 ft
Service Line Size: 3"
Length of Service Line: 20 ft
Pressure Available: 57.00 psi

DESIGN ANALYSIS
Maximum Station Flow: 27.52 gpm
Flow Available at POC: 75.00 gpm
Residual Flow Available: 47.48 gpm

Pressure Req. at Critical Station: 38.30 psi
Loss for Fittings: 0.05 psi
Loss for Main Line: 0.47 psi
Loss for POC to Valve Elevation: 0.00 psi
Loss for Backflow: 2.58 psi
Loss for Water Meter: 1.55 psi
Critical Station Pressure at POC: 42.95 psi
Pressure Available: 57.00 psi
Residual Pressure Available: 14.05 psi

CRITICAL ANALYSIS

P.O.C. NUMBER: 02
Water Source Information:

FLOW AVAILABLE
Water Meter Size: 1-1/2"
Flow Available: 75.00 gpm

PRESSURE AVAILABLE
Static Pressure at POC: 60.00 PSI
Elevation Change: 5.00 ft
Service Line Size: 3"
Length of Service Line: 20 ft
Pressure Available: 57.00 psi

DESIGN ANALYSIS
Maximum Station Flow: 30.74 gpm
Flow Available at POC: 75.00 gpm
Residual Flow Available: 44.26 gpm

Pressure Req. at Critical Station: 37.63 psi
Loss for Fittings: 0.03 psi
Loss for Main Line: 0.29 psi
Loss for POC to Valve Elevation: 0.00 psi
Loss for Backflow: 2.61 psi
Loss for Water Meter: 1.92 psi
Critical Station Pressure at POC: 42.48 psi
Pressure Available: 57.00 psi
Residual Pressure Available: 14.52 psi

VALVE SCHEDULE

NUMBER	MODEL	SIZE	TYPE	GPM	HEADS	PIPE	DESIGN PSI	FRICTION LOSS	VALVE LOSS	PSI	PSI @ POC	PRECIP
A1	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	27.52	28	343.6	30	5.3	3	38.31	42.95	1.66 in/h
A2	Hunter PGV-101G	1"	Turf Spray	16.50	27	244.3	30	2.36	2.11	34.47	37.75	1.71 in/h
A3	Hunter PGV-101G	1"	Shrub Spray	10.56	13	121.2	30	0.1	1.87	31.97	34.69	1.7 in/h
A4	Hunter PGV-101G	1"	Turf Spray	21.41	39	300.9	30	2.61	3.69	36.3	40.37	1.81 in/h
A5	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	27.30	23	279.3	30	4.25	3	37.24	42.36	1.72 in/h
B1	Hunter PGV-101G	1"	Shrub Spray	22.75	19	260.6	30	2.97	4.07	37.04	40.71	1.7 in/h
B2	Hunter PGV-101G	1"	Turf Spray	21.68	21	253.3	30	3.12	3.77	36.89	40.47	1.64 in/h
B3	Hunter PGV-101G	1"	Shrub Spray	24.02	28	316.7	30	3.61	4.43	38.03	41.95	1.57 in/h
B4	Hunter PGV-151 Globe	1-1/2"	Turf Spray	30.74	25	307.0	30	4.62	3	37.62	42.48	1.81 in/h
C1	Hunter PGV-101G	1"	Shrub Spray	10.44	14	117.2	30	0.08	1.87	31.96	34.67	1.78 in/h
C2	Hunter PGV-101G	1"	Turf Spray	22.53	40	299.6	30	2.42	4.01	36.43	40.69	1.8 in/h
C3	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	25.90	23	283.0	30	4.04	3	37.03	41.9	1.65 in/h
C4	Hunter PGV-151 Globe	1-1/2"	Shrub Spray	27.52	27	344.9	30	5.28	3	38.28	42.92	1.65 in/h
C5	Hunter PGV-101G	1"	Turf Spray	17.38	28	239.1	30	2.62	2.41	35.03	38.38	1.76 in/h
D1	Hunter PGV-101G	1"	Shrub Spray	22.75	19	260.2	30	2.95	4.07	37.02	40.69	1.72 in/h
D2	Hunter PGV-101G	1"	Turf Spray	19.91	20	247.2	30	2.7	3.27	35.97	39.34	1.59 in/h
D3	Hunter PGV-151 Globe	1-1/2"	Turf Spray	29.44	24	313.3	30	4.93	3	37.93	42.57	1.69 in/h
D4	Hunter PGV-101G	1"	Shrub Spray	24.02	28	309.7	30	3.53	4.43	37.95	41.93	1.57 in/h

BY DATE

REVISIONS

REV

The
Benedict
Bullock
Group, PA

ARCHITECTURE ■ PLANNING
1325 NW 38th STREET SUITE NO.1
FORTLAUDERDALE, FL 33409-1442
(954) 570-8588 FAX: (954) 570-8559
www.benedictbullockgroup.com Lic. No. AA-38003897

W
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SE E

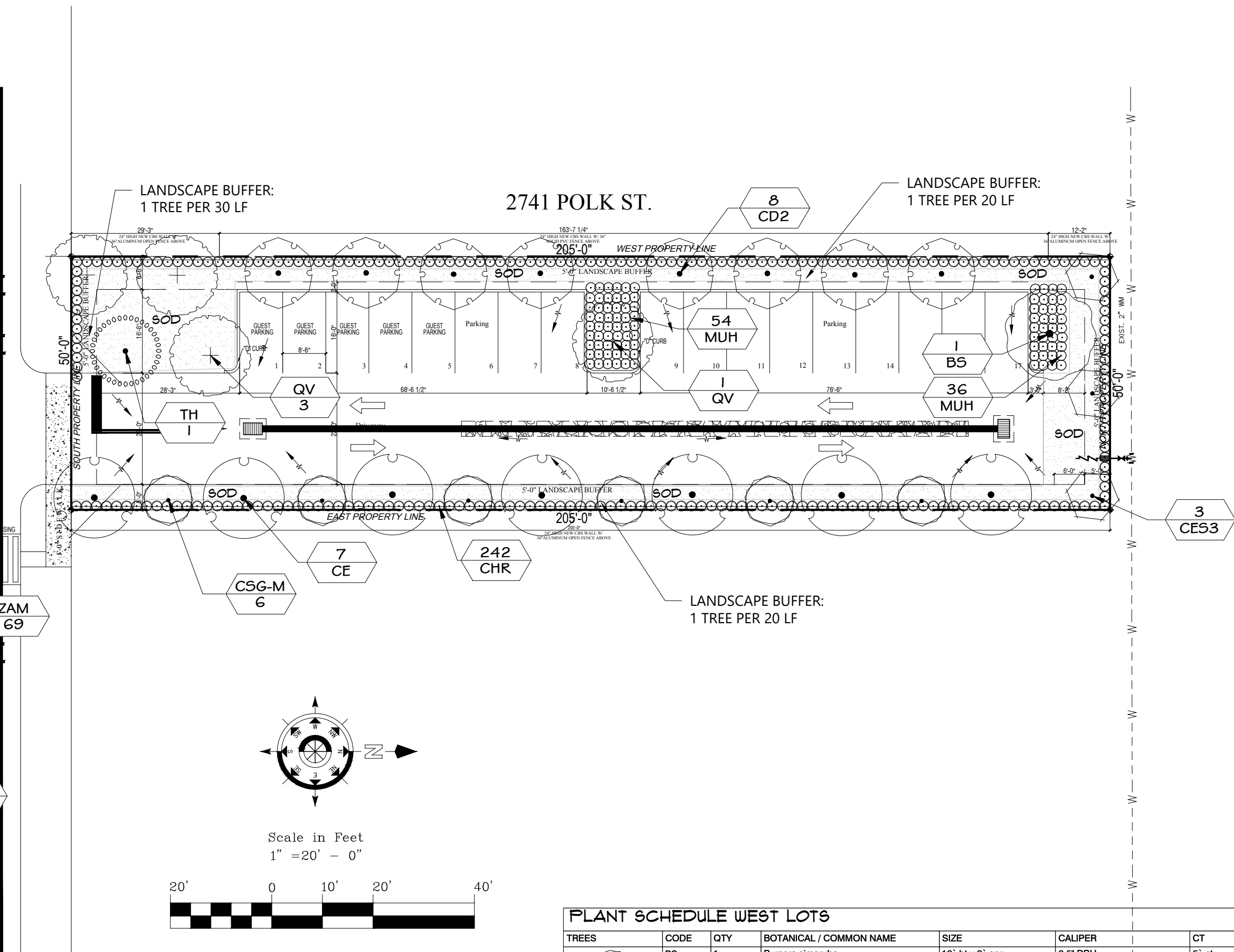
POLK STREET APARTMENTS II
2742-2741 POLK STREET HOLLYWOOD, FLORIDA

LANDSCAPE ARCHITECT'S SEAL

LYNN M BENDER
LA6666715

SCALE
DATE 06-10-2021
DRAWN BY LB
PROJECT NO. 20-536

IRRIGATION
DETAILS
IR-2
2 OF 2



MINIMUM LANDSCAPE REQUIREMENTS		PER CITY OF HOLLYWOOD, FL ZONING & LAND DEVELOPMENT REGULATIONS	PARKING LOT SITE SPEC	
ZONING CATEGORY		MC-1 (NORTH OF PICK L STREET)		
TOTAL SITE AREA		PARKING LOT = 10,250 SF (235 ACRES)		
PERVIOUS AREA			REQUIRED	PROVIDED 1009 SF
PERIMETER LANDSCAPE				
WEST - 265 LF				
	TREES	265 LF / 20 LF = 11 TREES	11 TREES	10 TREES
	SHRUBS	2' min. CONTINUOUS HEDGE *		
SOUTH - 50 LF - 20 LF DRIVE = 30 LF				
	TREES	30 LF / 20 LF = 1 TREE	1 TREE	3 TREES
	SHRUBS	2' min. CONTINUOUS HEDGE *		
EAST- 205 LF				
	TREES	205 LF / 20 LF = 11 TREES	11 TREES	7 TREES
	SHRUBS	2' min. CONTINUOUS HEDGE *		
NORTH - 50 LF				
	TREES	50 LF / 30 LF = 2 TREES	2 TREES	3 TREES
	SHRUBS	2' min. CONTINUOUS HEDGE *		
VUA LANDSCAPE				
TREES		1 TREE PER TERMINAL & INTERMEDIATE ISLAND		
LANDSCAPE AREA		LOTS <50' WIDTH MUST HAVE 8% OF PAVED AREA DEVOTED TO LANDSCAPE		
OPEN SPACE		1 TREE PER 1000 SF OF PAVED AREA	1009/1000 = 1 tree	1 tree
NATIVE		50% MIN		
PALMS		MAX. 30% OF REQ. TREES		

MITIGATION CALCULATIONS:	
1.5" ADDT PER CODE TREE =	1.5 INCHES
5" ADDT PER CODE TREE =	4 INCHES
1.5" ADDT PER CODE TREE =	10.5 INCHES
2" ADDT MITIGATION TREE =	12 INCHES
5" ADDT PER CODE TREE =	6.5 INCHES
5" ADDT PER CODE TREE =	.5 INCHES
5" ADDT PER CODE TREE =	.5 INCHES
2" ADDT MITIGATION TREE =	24 INCHES
2" ADDT PER TREE =	12 INCHES
5" ADDT PER TREE =	1 INCH
TOTAL = 72.5 INCHES USED FOR ON-SITE MITIGATION CREDITS	

IRRIGATION NOTE:
An automatic properly functioning underground irrigation system with a rain sensing cutoff device shall be in compliance with the South Florida Building Code and city Building Code requirements. The rain sensing cutoff device shall be located and installed in such a manner that the building eaves, balconies and similar overhangs do not interfere with the operation of the device. The irrigation system shall be rust free & properly maintained in good working order to provide a minimum coverage of 100% with 100% overlap.

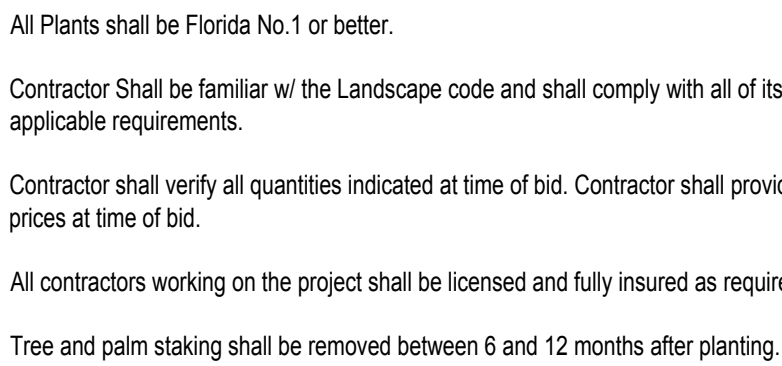
Adequate irrigation of all newly proposed landscaped areas shall be provided for the first full growing season and continue thereafter as necessary to maintain required vegetation in good and healthy condition. Irrigation systems shall conform to following standards:

- a. Irrigation systems shall be continuously maintained in working order and shall be designed so not to overlap water zones or to water impervious areas.

Vegetation shall not be planted or installed in areas that maintain shutting any public street which causes water from the system to stop onto the roadway or to strike passing vehicular traffic.

LANDSCAPE ARCHITECT CONSULTANT

LBLA, Inc.
Lynn Bender Landscape Architecture
5610 Adair Way
Lake Worth, FL 33467





EXISTING TREES (per tree survey) WITH AERIAL

EXISTING TREE DISPOSITION CHART - LOT 2742 POLK STREET (SOUTH SIDE)									
TREE #	BOTANICAL NAME	COMMON NAME	DBH (IN)	PALM HT (FT)	O.A. HT (FT)	CANOPY SPREAD (FT)	DISPOSITION / ACTION	TREE CONDITION	NOTES
1	Bursaria sinuata	Gumbo Limbo	8	-	15	10	REMOVE	FAIR	maintenance trimming to prevent damage to branches from construction
2	Cupaniopsis anacardioides	Carrotwood	12	-	28	30	REMOVE	FAIR	on property line, FLEPPC CAT 1, invasive
3	Thrinax radata	Thatch palm	3 @ 4	8	20	16	REMOVE	GOOD	PALM @ 1:1 or replace palm @ 1:1
4	Thrinax radata	Thatch palm	2 @ 4	9 & 12	18	18	REMOVE	GOOD	PALM @ 1:1 or replace palm @ 1:1
5	Bucida buceras	Black Olive	26	-	40	48	REMOVE	GOOD	13 tree has good branching and proper leader
6	Bursaria sinuata	Gumbo Limbo	10	-	14	26	REMAIN	GOOD	tree has good branching and proper leader
7	unknown fruit	Fruit Tree	8	-	16	9	REMOVE	POOR	multi trunk @ base, bush like
8	unknown fruit	Fruit Tree	9	-	18	8	REMOVE	POOR	multi trunk @ base
9	Citrus fruit	Fruit Tree	5	-	12	10	REMOVE	FAIR	open wounds throughout tree from storm damage
10	Yucca elephantipes	Palm - Yucca mix	6	8	20	12	REMOVE	POOR	N/A cluster mix
11	Coccoloba ovifera	Seagrape cluster	12	-	36	40	REMOVE	GOOD	* DBH cumulative, multi trunk @ base, maintenance trimming to prevent damage to branches from construction
12	Podocarpus gracilior	Podocarpus tree	10	-	18	18	REMOVE	GOOD	tree has nutritional needs
13	Adonis meridialis	Christmas Palm	6	18	22	6	REMOVE	POOR	PALM @ 1:1 or replace palm @ 1:1
14	Conocarpus erectus var. serotinus	Silver Buttonwood	8	-	20	12	REMOVE	GOOD	tree structural in good condition
15	Ficus spp.	Mango Tree	48	-	28	40	REMOVE	POOR	multi leaders open wounds
16	Mangifera indica	Mango Tree	17	-	25	25	REMOVE	POOR	poor branch structural open wounds
17	Bucida buceras	Black Olive	48	-	50	60	REMAIN	FAIR	Off-site, provide 5 min TPZ off PL, maintenance trimming to prevent damage to branches from construction
18	Bucida buceras	Black Olive	26	-	40	52	REMAIN	FAIR	Off-site, provide 5 min TPZ off PL, maintenance trimming to prevent damage to branches from construction
18a	Bucida buceras	Black Olive	26	-	40	50	REMAIN	FAIR	Off-site, provide 5 min TPZ off PL, maintenance trimming to prevent damage to branches from construction
18b	Bucida buceras	Black Olive	26	-	40	36	REMAIN	FAIR	Off-site, provide 5 min TPZ off PL, maintenance trimming to prevent damage to branches from construction
19	Bursaria sinuata	Gumbo Limbo	26	-	40	41	REMAIN	FAIR	Off-site, provide 5 min TPZ off PL, maintenance trimming to prevent damage to branches from construction
38	Schinus terebinthifolius	Brazilian Pepper	NA	-	-	-	REMAIN	NA	invasive, OFF-SITE
39	Schefflera actinophylla	Umbrella Tree	24	-	25	16	REMOVE	POOR	multi stems, open wounds, invasive
40	Bursaria sinuata	Gumbo Limbo	12	-	14	12	REMOVE	POOR	tree would need structural pruning to correct storm damage
									TOTAL MITIGATION INCHES REMOVED
									171

COMBINED TOTAL OF 387 INCHES OF MITIGATION REQUIRED (INCLUDING BOTH NORTH & SOUTH LOTS)

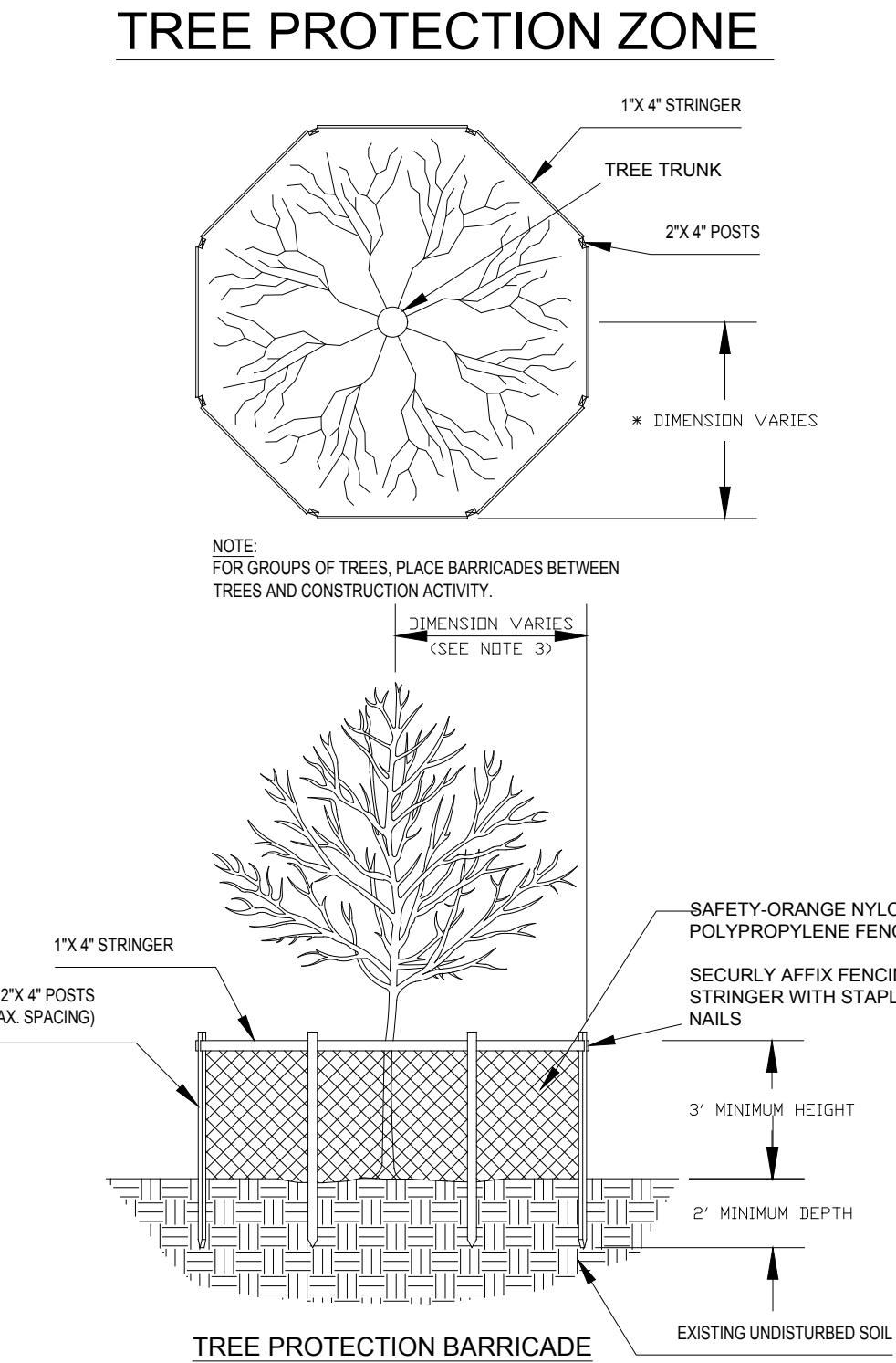
72.5 INCHES MITIGATED FOR ON-SITE - REFER TO LP-1 PLANT SCHEDULE FOR BREAKDOWN (with limited space for mitigation trees to be planted above and beyond the code required proposed trees on site, the remaining inches shall be paid to city via payment into tree trust fund &/or donation of new tree plantings on city public property.

314.5 INCHES TO BE MITIGATED FOR via payment to city into tree trust fund &/or donation of new plantings on city public property. (\$350 per a 2" DBH 12' on ht tree) (314.5 inches divided by 2" credit per tree = 157.25 x \$350 = \$55,037.50 owed for remaining mitigation)

4 PALMS TO BE MITIGATED FOR @ 1:1 OR \$350 per palm (350 x 4 = \$1400)

TOTAL MITIGATION PAYMENT = \$56,437.50

EXISTING TREE DISPOSITION CHART - LOT 2741 POLK STREET (NORTH SIDE)									
TREE #	BOTANICAL NAME	COMMON NAME	DBH (IN)	PALM HT (FT)	O.A. HT (FT)	CANOPY SPREAD (FT)	DISPOSITION / ACTION	DBH (IN) MITIGATION	NOTES
20	Bursaria sinuata	Gumbo Limbo	17	-	22	38	REMOVE	12	* double trunk - DBH cumulative inches, in fence, top 5 min.
21	Ficus benjamina	Ficus tree	67	-	55	40	REMOVE	67	(2) Solitary palms adjacent to base
22	Mangifera indica	Mango Tree	12	-	32	46	REMOVE	12	no central leader, open wounds, previous storm damage
23	Cocos nucifera	Coconut Palm	12	40	53	15	REMOVE	PALM @ 1:1	tree needs fertilization and maintenance
24	Cupaniopsis anacardioides	Carrotwood	8	-	36	29	REMOVE	8	invasive, tree needs fertilization and maintenance
25	Cupaniopsis anacardioides	Carrotwood	14	-	23	35	REMOVE	14	invasive, tree needs fertilization and maintenance
26	Ficus sp.	Ficus Tree	81	-	46	75	REMOVE	81	maintenance trimming to prevent damage to branches from construction
27	Cocos nucifera	Coconut Palm	12	1	10	15	REMOVE	12	young juvenile, minimal trunk
28	Bucida buceras	Black Olive	36	-	58	46	REMAIN	36	Off-site, provide 5 min TPZ off PL, maintenance trimming to prevent damage to branches from construction
29	Senna alexandrina	Cassia	14	-	18	18	REMOVE	16	on property line, in fence
30	Bucida buceras	Black Olive	16	-	46	22	REMAIN	16	Off-site, provide 5 min TPZ off PL, maintenance trimming to prevent damage to branches from construction
31	Persea americana	Avocado	10	-	14	15	REMOVE	10	Off-site, provide 5 min TPZ off PL, maintenance trimming to prevent damage to branches from construction
32	Bursaria sinuata	Gumbo Limbo	8	-	24	18	REMAIN	8	Off-site, provide 5 min TPZ off PL, maintenance trimming to prevent damage to branches from construction
33	Cupaniopsis anacardioides	Carrotwood	6	-	12	10	REMOVE	6	invasive, on property line
34	Cupaniopsis anacardioides	Carrotwood	10	-	24	24	REMAIN	10	invasive
35	Bursaria sinuata	Gumbo Limbo	16	-	20	28	REMAIN	16	Off-site, provide 5 min TPZ off PL, maintenance trimming to prevent damage to branches from construction
36	Bursaria sinuata	Gumbo Limbo	14	-	22	26	REMAIN	14	Off-site, provide 5 min TPZ off PL, maintenance trimming to prevent damage to branches from construction
37	Cupaniopsis anacardioides	Carrotwood	6	-	18	12	REMAIN	6	invasive, Off-site, provide 5 min TPZ off PL
									TOTAL MITIGATION INCHES REMOVED
									216

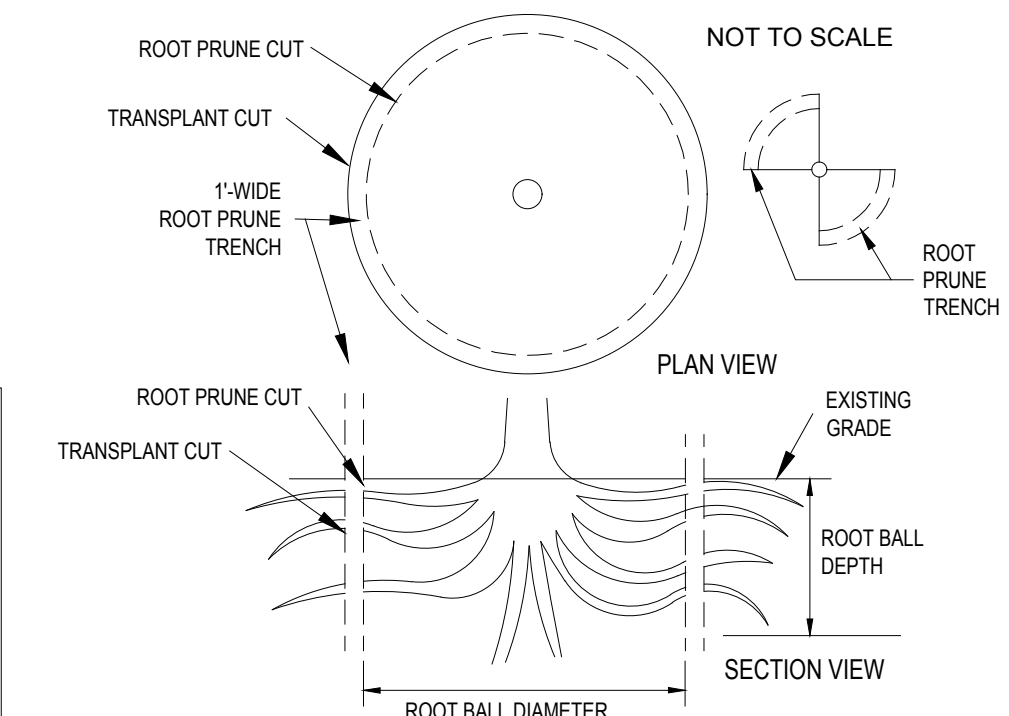


- NOTES FOR TREE PROTECTION BARRICADES:
- BARRICADE CIRCLE TO BE CENTERED ON TREE TRUNK(S). FOR GROUPS OF TREES, PLACE BARRICADES BETWEEN TREES AND CONSTRUCTION ACTIVITY.
 - NO GRADE CHANGES, TRAFFIC, STORAGE, TRENCHING OR ROOT CUTTING ALLOWED IN "TREE PROTECTION ZONE". CONTRACTOR TO REPLACE OR MITIGATE DAMAGED TREES OR VEGETATION AT CONTRACTOR'S EXPENSE PER LOCAL GOVERNMENT CODE REQUIREMENTS.
 - TREE PROTECTION BARRICADES SHALL BE LOCATED TO PROTECT EXISTING, RELOCATED AND NEW TREES IN CONSTRUCTION AREAS. THE PROTECTIVE BARRIER SHALL BE PLACED AROUND THE TREE AT A DISTANCE 8 FEET FROM THE TREE TRUNK OR AT THE CANOPY DRIP LINE, WHICHEVER IS GREATER OR AS SHOWN ON LANDSCAPE PLAN.

TREE PRUNING SPECIFICATIONS

THE CONTRACTOR SHALL PROTECT ALL TREE CANOPIES TO PREVENT LIMB BREAKAGE AND OTHER DAMAGE DURING ALL CONSTRUCTION OPERATIONS AT ALL TIMES. THE CONTRACTOR SHALL HIRE AN ISA CERTIFIED ARBORIST TO PERFORM TREE CROWN REDUCTION FOR ANY TREE CANOPY THAT WILL CONFLICT WITH CONSTRUCTION OPERATIONS. THE REQUIRED CLEARANCE PRUNING SHALL BE DONE IN ACCORDANCE WITH ANSI A300 - STANDARD PRACTICES FOR TREE, SHRUB, AND OTHER WOODY PLANT MAINTENANCE. PART 1: PRUNING. NO MORE THAN ONE-FOURTH OF A TREE'S LEAF-BEARING CANOPY SHALL BE REMOVED. ALL PRUNING CUTS SHALL BE MADE BACK TO STRONG LATERALS OR PARENT LIMBS THAT CAN ASSUME APICAL DOMINANCE. ALL PRUNING SHALL ALSO CLEAN UP DEAD, DISEASED, BROKEN OR WEAKLY ATTACHED BRANCHES, AND SHALL MAINTAIN THE NATURAL SHAPE AND STRUCTURE OF THE TREE. THE ARBORIST HIRED FOR THIS PROJECT MUST HAVE A LOCAL COUNTY TREE TRIMMER'S LICENSE.

ALL TREE ROOTS THAT MUST BE CUT TO INSTALL ADJACENT CONCRETE CURBS, UTILITIES, AND FOOTERS, ETC., OR FOR ROOT PRUNING BEFORE RELOCATION, SHALL BE CUT CLEANLY WITH SHARP BLADES TO PREVENT UNNECESSARY ROOT SPLITTING AND DAMAGE.



- NOTES:
- MARK AND CUT A 12'-WIDE "ROOT PRUNE TRENCH" AROUND THE TREE TO PROVIDE A ROOT BALL SIZE PER THE APPROPRIATE TABLE IN ITEM 3 (REFER TO SHEET LA-3, TREE RELOCATION SPEC.) USE SHARPENED SPADES FOR ROOT PRUNING. DO NOT CUT UNDER THE ROOT BALL, NOR REMOVE OLD ROOTS ON TOP OF ROOT BALL.
 - CUT THE "ROOT PRUNE TRENCH" INITIALLY ON TWO SIDES (QUARTERS) OPPOSITE EACH OTHER. THEN AFTER 1/2 OF TOTAL "WAIT TIME" HAS PASSED, CUT THE "ROOT PRUNE TRENCH" FOR THE REMAINING TWO QUARTERS.
 - FILL THE "ROOT PRUNE TRENCH" WITH A MIXTURE OF EXISTING SOIL AND 1/2 PEAT HUMUS. LEAVE A DEPRESSION TO HOLD WATER AND THEN IRRIGATE THE ROOT BALL AREA AND TRENCH TO PROMOTE FEEDER ROOT GROWTH.
 - REFER TO "TREE RELOCATION SPECIFICATIONS" ON SHEET LA-3 FOR COMPLETE SPECIFICATIONS.

TREE & PALM ROOT PRUNING

LANDSCAPE ARCHITECT CONSULTANT

LBLA, Inc.
Lynn Bender Landscape Architecture
5610 Adair Way
Lake Worth, FL 33467

811

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W

SW

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N

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E

POLK STREET APARTMENTS II

2742-2741 POLK STREET HOLLYWOOD, FLORIDA

LANDSCAPE ARCHITECT'S SEAL

LYNN M BENDER
LA6666715

SCALE

DATE 02-03-2021

DRAWN BY LB

PROJECT NO. 20-536

EXISTING TREE DISPOSITION PLAN

TDP-1

OF 2



Prepared for:
City of Hollywood Environmental Department
Prepared by:
Juan Masson
305-525-2830
usijmasson@aol.com
International Society of Arboriculture Professional Member #198726
ISA Certified Arborist FL-6184A
President, US INTERMED CORP.

To inspect all trees and evaluate their conditions. To determine any trees that require protecting species, high value, endangered and in excellent condition which would make good candidate to relocate on site. Trees are given a rating of Poor, Fair, and Good. Neighbors' trees were inspected to determine impact to adjacent property

City of Hollywood Department of Environmental Services

City of Hollywood Department of Environmental Services

City of Hollywood Department of Environmental Services

City of Hollywood Department of Environmental Services

City of Hollywood Department of Environmental Services

City of Hollywood Department of Environmental Services

City of Hollywood Department of Environmental Services

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POLK STREET APARTMENTS II
2742-2741 POLK STREET HOLLYWOOD, FLORIDA



SCALE	
DATE	04-25-2021
DRAWN BY	LB
PROJECT NO.	20-536

ARBORIST REPORT

TDP-2

OF 2

EROSION AND SEDIMENT CONTROL NOTES

1. THE CONTRACTOR IS RESPONSIBLE FOR FOLLOWING THE BEST EROSION AND SEDIMENT CONTROL PRACTICES AS OUTLINED IN THE PLANS, SPECIFICATIONS AND APPLICABLE WATER MANAGEMENT DISTRICT PERMIT(S) FOR THIS PROJECT.

2. FOR ADDITIONAL INFORMATION ON SEDIMENT AND EROSION CONTROL REFER TO "THE STATE OF FLORIDA EROSION AND SEDIMENT CONTROL DESIGNER AND REVIEWERS MANUAL" FROM THE STATE OF FLORIDA DEPARTMENT OF ENVIRONMENTAL REGULATION (FDER).

3. THIS PLAN INDICATES THE MINIMUM EROSION AND SEDIMENT CONTROL MEASURES REQUIRED FOR THIS PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR MEETING ALL APPLICABLE RULES, REGULATIONS AND WATER QUALITY GUIDELINES AND MAY NEED TO INSTALL ADDITIONAL CONTROLS.

4. ALL EXCAVATIONS AND EARTHWORK SHALL BE DONE IN A MANNER TO MINIMIZE WATER TURBIDITY AND POLLUTION. DISCHARGE SHALL BE CONTROLLED AND REROUTED THROUGH FILTERS, SILTATION DIAPERS AND SUMPS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PREVENTION, CORRECTION, CONTROL AND ABATEMENT OF EROSION AND WATER POLLUTION IN ACCORDANCE WITH CHAPTER 62-302, FLORIDA ADMINISTRATIVE CODE.

5. THE CONTRACTOR SHALL PAY FOR ANY WATER QUALITY CONTROL VIOLATIONS FROM ANY AGENCY THAT RESULTS IN FINES BEING ASSESSED TO THE OWNER BECAUSE OF THE CONTRACTOR'S FAILURE TO ELIMINATE TURBID RUNOFF FROM LEAVING THE SITE AND RAISING BACKGROUND LEVELS ABOVE EXISTING BACKGROUND LEVEL.

6. THE SITE CONTRACTOR IS RESPONSIBLE FOR REMOVING THE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER COMPLETION OF CONSTRUCTION AND ONLY WHEN AREAS HAVE BEEN STABILIZED.

7. ADDITIONAL PROTECTION - ON-SITE PROTECTION MUST BE PROVIDED THAT WILL NOT PERMIT SILT TO LEAVE THE PROJECT CONFINES DO TO UNFORSEEN CONDITIONS OR ACCIDENTS.

8. SILT FENCES SHALL BE USED ALONG THE PROPERTY LINES TO MINIMIZE OFFSITE SILTATION MITGRATION.

9. SILT FENCES SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND AT LEASE DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.

10. SHOULD THE FABRIC ON A SILT FENCE DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE AND THE BARRIER IS STILL NECESSARY, THE FABRIC SHALL BE REPLACED PROMPTLY.

11. FILER FABRIC SHALL BE INSTALL UNDER INLET GRATES AND EXTEND A MINIMUM OF 1 FOOT BEYOND EACH SIDE OF THE INLET STRUCTURE. IF MORE THAN ONE STRIP OF FABRIC IS NECESSARY, THE STRIPS SHALL BE OVERLAPPED 1 FOOT.

12. SEDIMENT DEPOSITS SHOULD BE REMOVED AFTER EACH RAINFALL AND AS NEEDED.

13. ANY DISCHARGE FROM DEWATERING ACTIVITY SHALL BE FILTERED AND CONVEYED TO THE OUTFALL IN A MANNER WHICH PREVENTS EROSION AND TRANSPORTATION OF SUSPENDED SOLIDS TO THE RECEIVING OUTFALL.

14. DEWATERING PUMPS SHALL NOT EXCEED THE CAPACITY OF THAT WHICH REQUIRES A CONSUMPTIVE USE PERMIT FROM THE APPLICABLE WATER MANAGEMENT DISTRICT.

15. ALL DISTURBED AREAS SHALL BE GRASSED, FERTILIZED, MULCHED AND MAINTAINED UNTIL A PERMAMENT VEGETATIVE COVER IS ESTABLISHED.

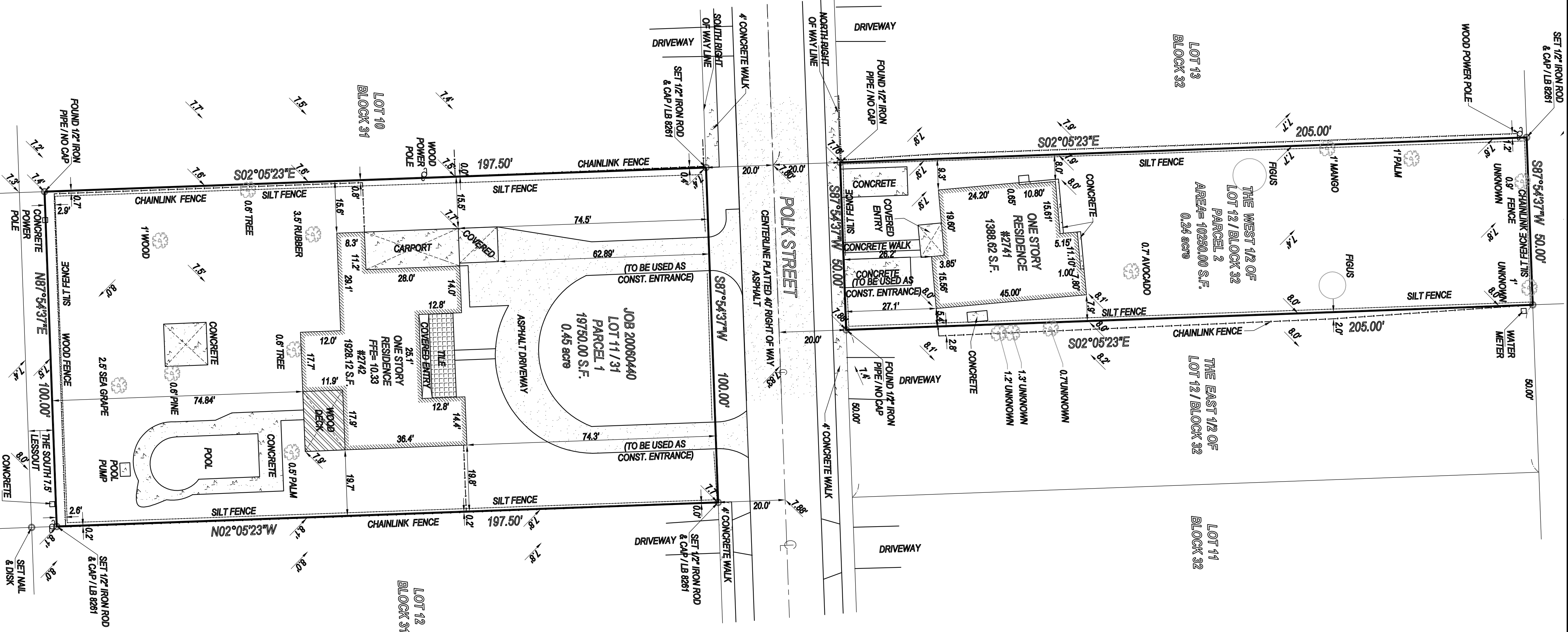
16. SOD SHALL BE PLACED IN AREAS WHICH MAY REQUIRE IMMEDIATE EROSION PROTECTION TO ENSURE WATER QUALITY STANDARDS ARE MAINTAINED.

17. ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER BARRIER ARE NO LONGER REQUIRED SHALL BE DRESSED TO CONFROM TO THE EXISTING GRADE, PREPARED AND SEEDED.

18. CONTRACTOR SHALL INSURE THAT ALL DRAINAGE STRUCTURES, PIPES, ETC. ARE CLEANED OUT AND WORKING PROPERLY AT TIME OF ACCEPTANCE.

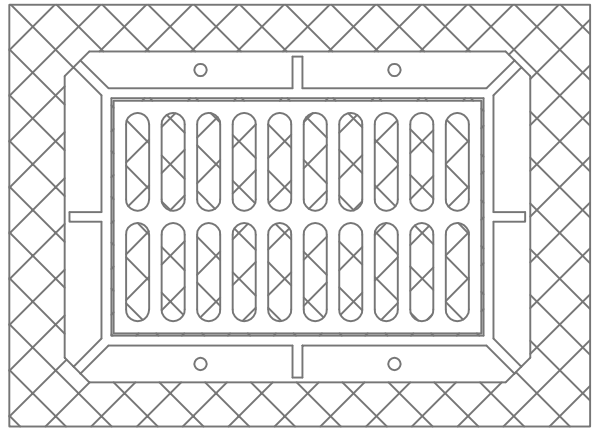
19. FLOATING TURBIDITY BARRIERS WILL BE PLACED OFF SET FROM THE SEAWALL ADJACENT TO THE PROPERTY. IF SEAGRASSES ARE PRESENT BARRIERS WILL NOT BE PLACED OVER THEM. THE FLOATING TURBIDITY BARRIERS SHALL ALSO BE INSTALLED IN A MANNER TO PREVENT MANATEE ENTANGLEMENT.

20. ALL DEATERING, EROSION, AND SEDIMENT CONTROL SHALL REMAIN IN PLACE UNTIL AFTER COMPLETION OF CONSTRUCTION AND SHALL BE REMOVED WHEN AREAS HAVE BEEN STABILIZED.

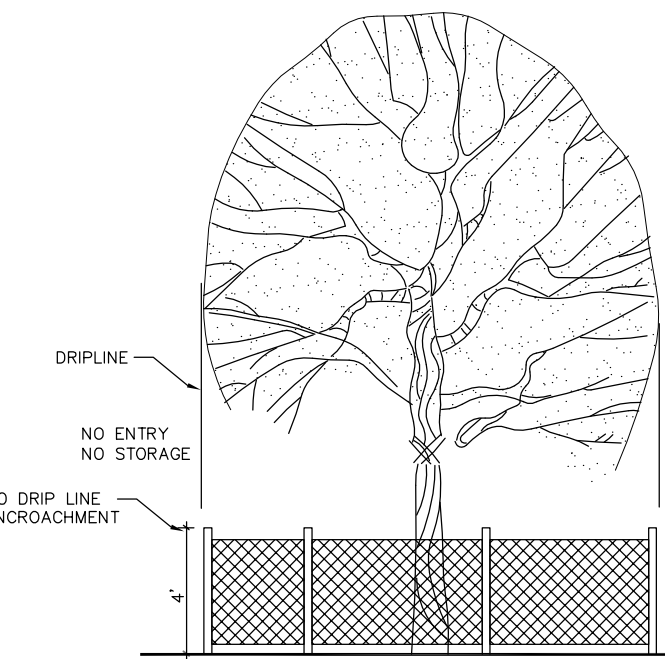


EROSION AND SEDIMENT CONTROL PLAN

SCALE: 1"=20'

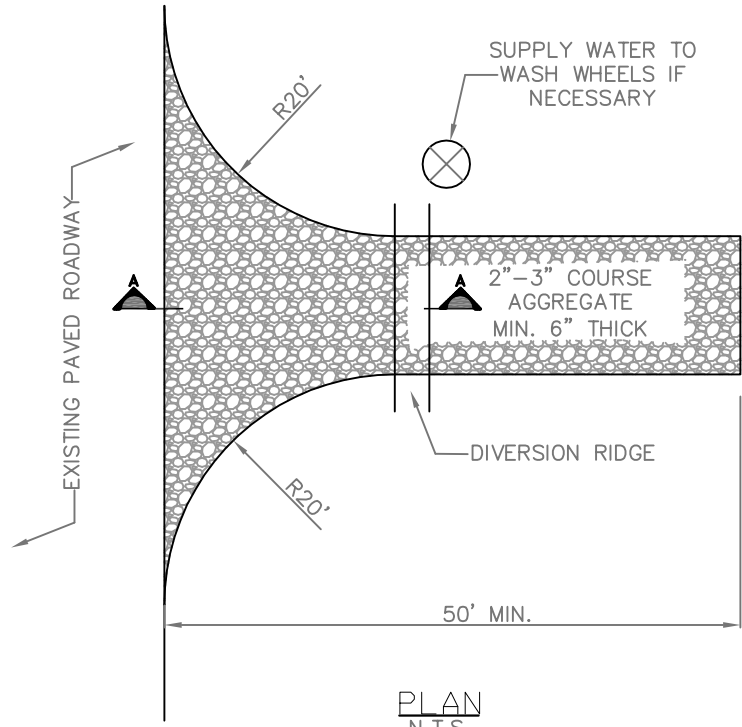


INLET PROTECTION
N.T.S.

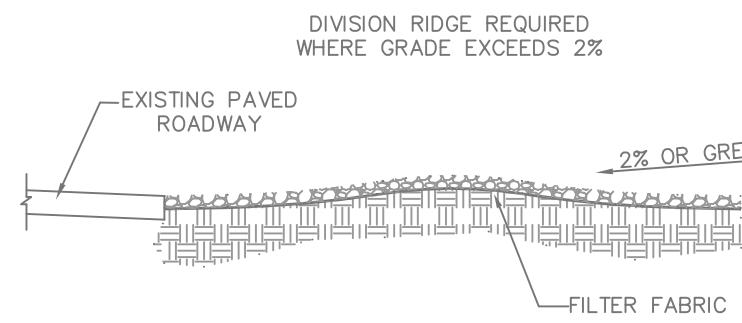


TREE PRESERVATION
BARRICADE FENCING DETAIL
N.T.S.

NOTE: INSTALL FILTER FABRIC UNDER ALL GRATES TO PREVENT SILT AND CONSTRUCTION DEBRIS FROM ENTERING THE SYSTEM.



PLAN
N.T.S.



SECTION A-A
N.T.S.

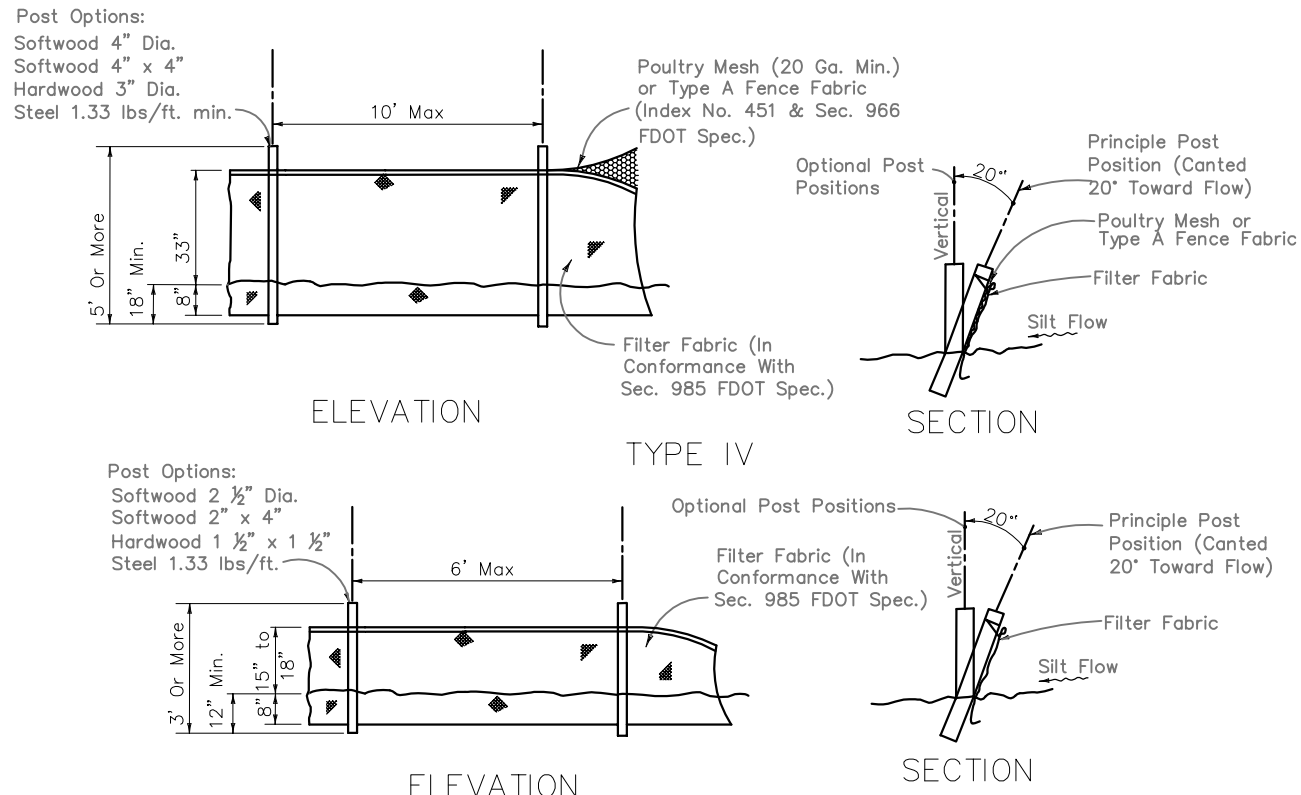
TEMPORARY GRAVEL CONSTRUCTION ENTRANCE

NOTES:

1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.

2. WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.

3. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN.



ELEVATION
TYPE III

ELEVATION
TYPE IV

ELEVATION
TYPE V

ELEVATION
TYPE VI

ELEVATION
TYPE VII

ELEVATION
TYPE VIII

ELEVATION
TYPE IX

ELEVATION
TYPE X

ELEVATION
TYPE XI

ELEVATION
TYPE XII

NOTE: FOR SILT FENCE APPLICATIONS AND NOTES, SEE SHEET 3 OF 3 FDOT INDEX No. 102.

SILT FENCE DETAIL
N.T.S.

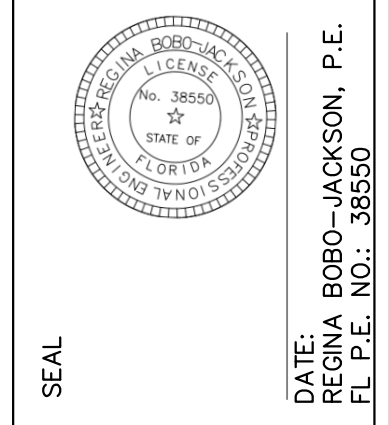


GATOR ENGINEERING, INC.

11390 TEMPLE STREET
COOPER CITY, FL 33330

TEL: (864) 434-5905 FAX: (864) 434-5904

CERTIFICATE OF AUTHORIZATION NUMBER 30230



POLK STREET APARTMENTS III
2742 & 2741 POLK STREET
HOLLYWOOD, FL

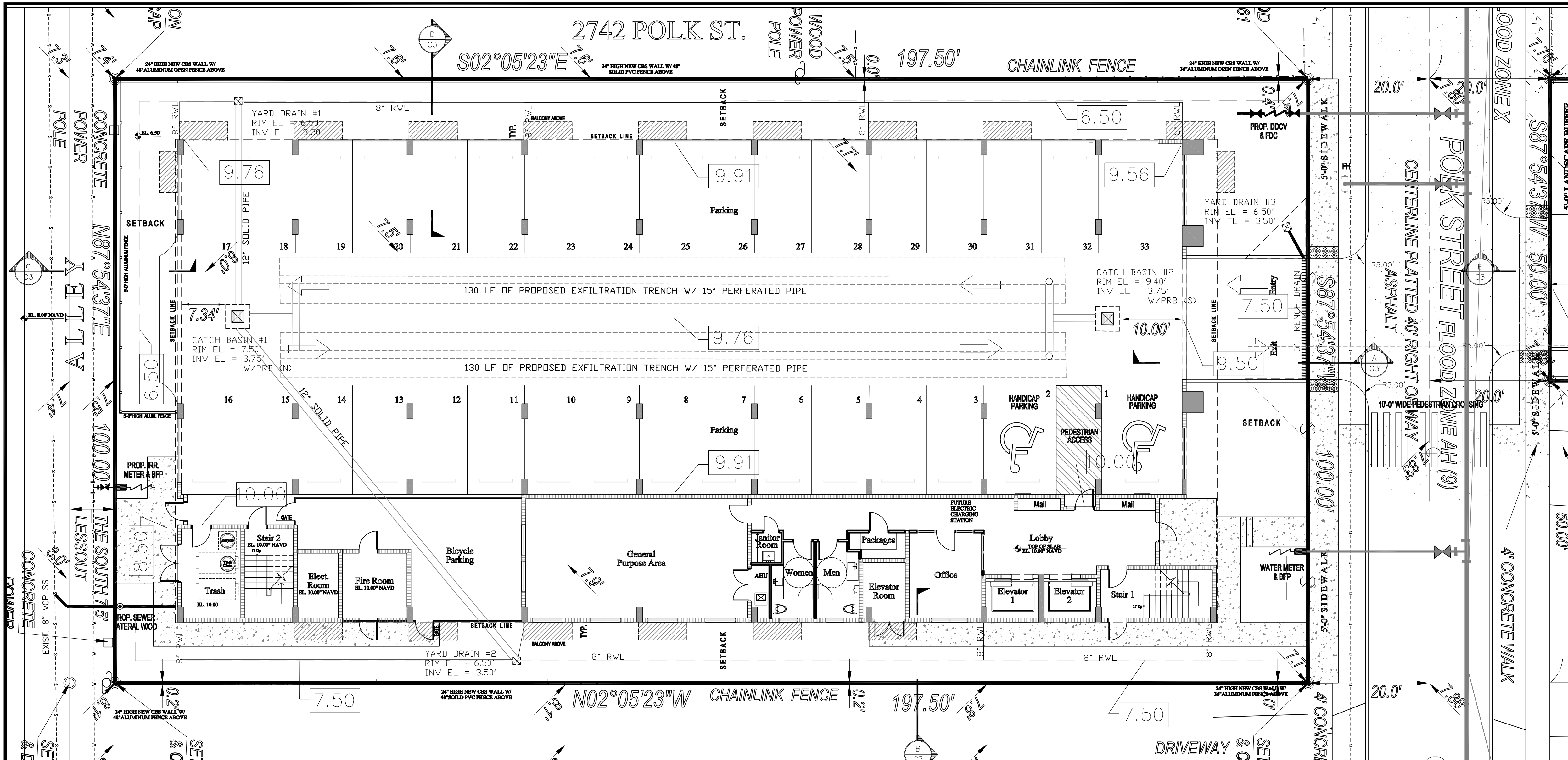
REVISIONS	DATE	DESCRIPTION
No.		

GEA PROJECT NO.: 21046a
DATE: 06-07-2021
SCALE: AS SHOWN
DESIGNED BY: R.B.J.
DRAWN BY: L.B.
CHECKED BY: R.B.J.
APPROVED BY: R.B.J.

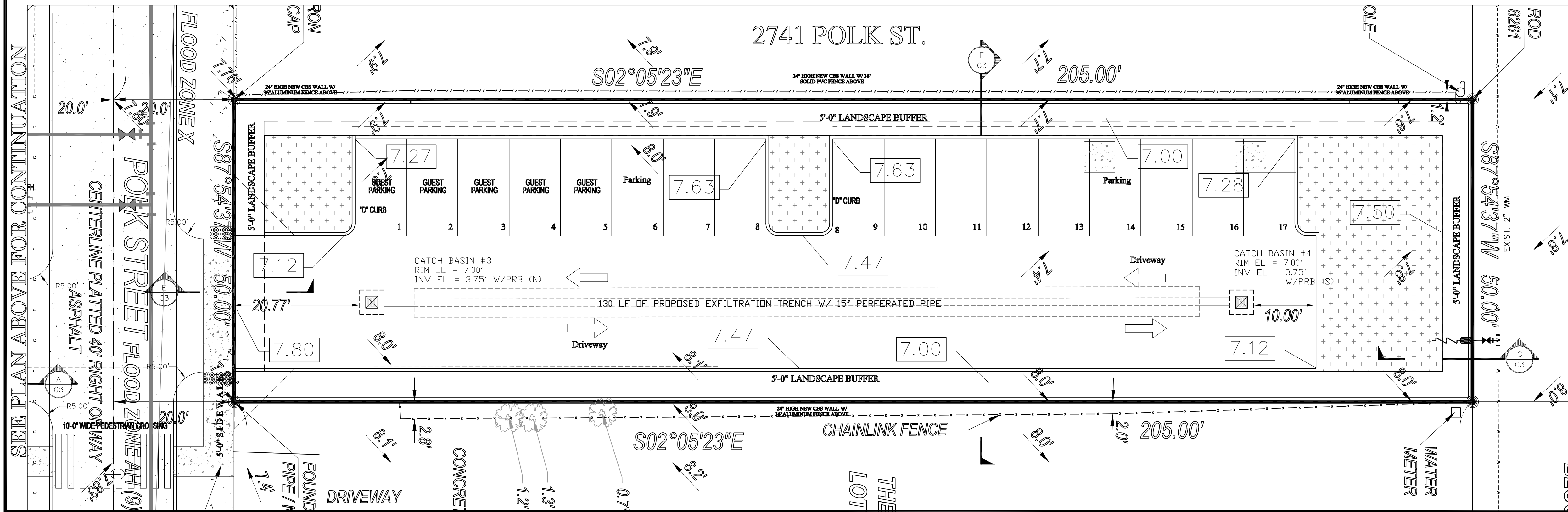
SHEET TITLE

EROSION &
SEDIMENT
CONTROL
PLAN

C1 OF 7



CIVIL PLAN
SCALE: 1"=10'



LEGAL DESCRIPTION

2742
LOT 11, LESS THE SOUTH 7 1/2 FEET THEREOF, BLOCK 31, AN AMENDED PLAT OF HOLLYWOOD LITTLE RANCHES, ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 1, PAGE(S) 26, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA, 2741
THE WEST 1/2 OF LOT 12, IN BLOCK 32, OF HOLLYWOOD LITTLE RANCHES, ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 1, PAGE 26, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.

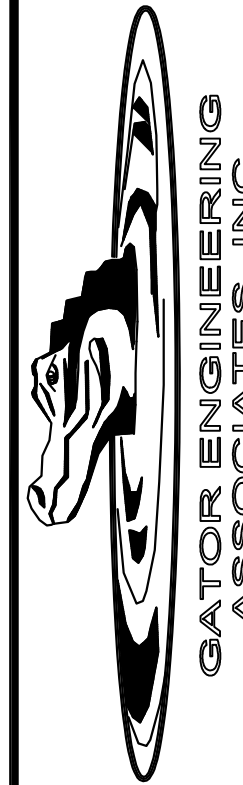
GENERAL NOTES:

1. BASE SURVEY WAS PROVIDED BY ROYAL POINT LAND SURVEYORS, INC.
2. ELEVATIONS SHOWN REFER TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (N.A.V.D.).
3. HORIZONTAL AND VERTICAL CONTROL SHALL BE PROVIDED BY THE CONTRACTOR'S SURVEYOR. LAYOUT IS THE RESPONSIBILITY OF THE CONTRACTOR.
4. IT IS THE INTENT OF THESE DRAWINGS TO BE IN ACCORDANCE WITH APPLICABLE CODES AND AUTHORITIES HAVING JURISDICTION. ANY DISCREPANCIES BETWEEN THESE DRAWINGS AND APPLICABLE CODES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE ENGINEER AND OWNER REPRESENTATIVE.
5. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE ALL UNDERGROUND UTILITIES VERIFIED AND LOCATED PRIOR TO THE START OF CONSTRUCTION. ALL TRENCH EXCAVATION SHALL PROCEED WITH EXTREME CAUTION. IN THE EVENT THAT EXISTING UTILITIES ARE DAMAGED, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR OR REPLACE SUCH DAMAGES.
6. CONTRACTOR SHALL BE RESPONSIBLE FOR RESETTling ANY DISTURBED EXISTING MANHOLES, VALVE BOXES, BLOW-OFF RISERS OR ANY OTHER POINT OF ACCESSIBILITY TO UTILITIES, AND TO MATCH ASPHALT GRADES, AS REQUIRED, WHETHER SPECIFICALLY SHOWN ON THE DRAWINGS OR NOT.
7. TO AVOID MISUNDERSTANDING AND TO INSURE COMPLIANCE WITH SPECIFICATIONS, BEFORE PURCHASING MATERIALS OR EQUIPMENT FOR HIS WORK, THE CONTRACTOR SHALL FURNISH AT LEAST FOUR COPIES OF SHOP DRAWINGS OR ILLUSTRATION SHEETS FOR APPROVAL BY THE ENGINEER. THE APPROVAL OF SHOP OR WORKING DRAWINGS BY THE ENGINEER SHALL NOT RELIEVE THE CONTRACTOR OF RESPONSIBILITY FOR ERRONEOUS OR INCONSISTENT DIMENSIONS, NOTATIONS, OMISSIONS OR OTHER ERRORS, OR FOR THE PROPER FUNCTIONING OF THE COMPLETE INSTALLATION.
8. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL GIVE TIMELY NOTIFICATION TO ALL UTILITY COMPANIES WITH FACILITIES IN THE AREA.
9. THE LOCATION OF EXISTING FACILITIES WERE PLOTTED FROM AVAILABLE RECORDS. THE CONTRACTOR SHALL FIELD LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION.
10. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO SAFEGUARD ALL EXISTING STRUCTURES, UTILITIES, AND SURVEY MAKERS.
11. CONTRACTOR SHALL COORDINATE THIS PLAN WITH THE PLUMBING AND LANDSCAPE PLANS.

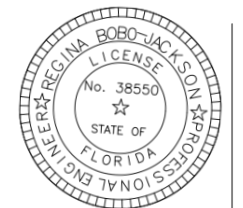
SEE PLAN BELOW FOR CONTINUATION

LEGEND

- EXISTING GRADE
- PROPERTY LINE
- EXIST. OVERHEAD POWER LINE
- PROPOSED DDCV & FDC
- PROPOSED 1" IRRIGATION METER
- PROPOSED 1" BACKFLOW PREVENTER
- PROPOSED SEWER CLEAN OUT
- PROPOSED ELEVATION
- PROPOSED DETECTABLE WARNING STRIP
- EXIST. 8" VCP SEWER MAIN
- EXIST. 2" WATER MAIN
- EXIST. GAS MAIN
- NEW 8" WATER MAIN & SERVICES (OTHER)



GATOR ENGINEERING ASSOCIATES, INC.
11380 TEMPLE STREET
COOPER CITY, FL 33330
TEL: (854) 434-5905 FAX: (854) 434-5904
CERTIFICATE OF AUTHORIZATION NUMBER 50230



POLK STREET APARTMENTS III
2742 & 2741 POLK STREET
HOLLYWOOD, FL

REVISIONS

GEA PROJECT NO.: 21046a
DATE: 06-07-2021
SCALE: AS SHOWN
DESIGNED BY: R.B.J.
DRAWN BY: L.B.
CHECKED BY: R.B.J.
APPROVED BY: R.B.J.

SHEET TITLE

CIVIL PLAN

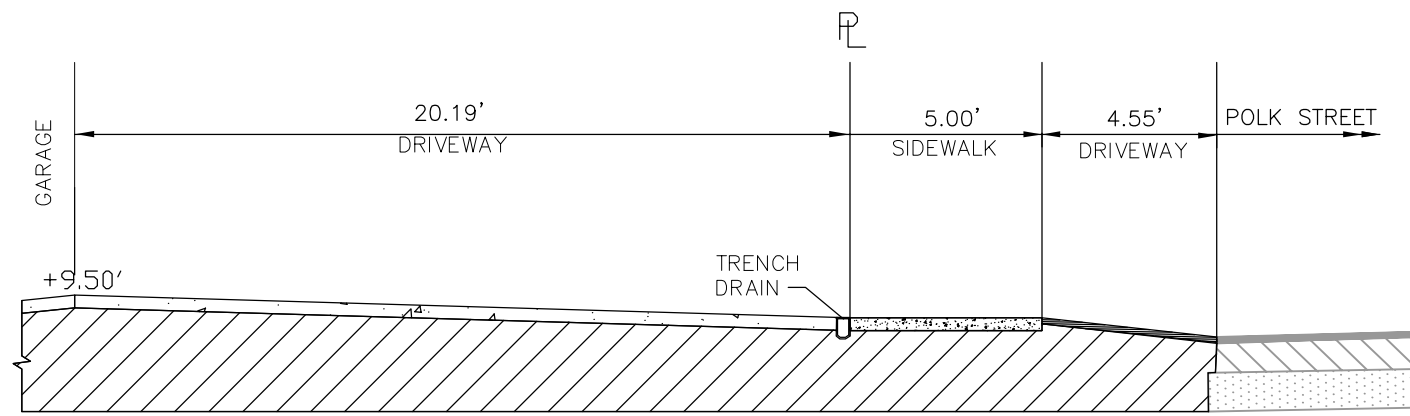
C2 OF 7

1. PROVIDE FILTER FABRIC OR OTHER METHOD OF SEDIMENT PROTECTION FOR ANY EXISTING CATCH BASIN/INLET WITHIN 100 FEET OF THE PROPERTY.
ANY SEDIMENT THAT IS TRACKED ONTO ROADS MUST BE SWEEPED UP IMMEDIATELY. SEDIMENT SHALL NOT BE REMOVED BY WASHING/FLUSHING WITH WATER AT THE RIGHT OF WAY. PRIOR TO THE START OF CONSTRUCTION, AN ENGINEERING ROUGH INSPECTION MUST BE SCHEDULED TO VERIFY EROSION AND SEDIMENTATION CONTROL IS SETUP PROPERLY. TO REQUEST AN INSPECTION, CALL (954) 828-5191.
2. AT ALL TIMES DURING CONSTRUCTION, ALL STORMWATER MUST REMAIN ONSITE. NO DISCHARGE INTO THE PUBLIC RIGHT OF WAY IS ALLOWED.
3. SIDEWALKS, PAVEMENT, SWALES AND DRAINAGE SYSTEMS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THESE PLANS AND DETAILS.
4. THESE PLANS SHALL BE COORDINATED WITH THE LANDSCAPE PLANS. NO TREE SHALL BE INSTALLED SUCH THAT THE BOTTOM SWALE ELEVATIONS AND VOLUMES ARE MINIMIZED.
5. ALL POTABLE WATER SERVICE CONNECTIONS REQUIRE BACKFLOW PREVENTERS.
6. CONTRACTOR SHALL VERIFY THE LOCATION OF THE EXISTING SEWER CLEAN-OUT. ALL NEW CLEAN-OUTS SHALL BE CONSTRUCTED PER CITY STANDARDS, DETAILS AND CRITERIA. THE USE OF EXISTING SEWER LATERALS ARE BEING PROPOSED FOR USE, THEREFORE THE INTEGRITY OF THE LATERAL IS TO BE VERIFIED THAT THERE ARE NO CRACKS OR LEAKS FROM THE MAIN TO CLEAN-OUT AT THE PROPERTY LINE. IF A LEAK OR CRACK IS IDENTIFIED, PROVIDE A NEW CLEAN-OUT AND LATERAL AT THE PROPERTY LINE AND INSERT A CURED-IN-PLACE PIPE INSIDE THE EXISTING SANITARY LATERAL PER ASTM F2361-11.
8. THE FINISH SURFACE OF BASE COURSE AND THAT OF THE WEARING SURFACE SHALL NOT VARY MORE THAN .04 FOOT FROM THE APPROVED GRADING PLAN (TEMPLATE) AND ALL AREA SHALL BE GRADED TO DRAIN. ANY IRREGULARITIES EXCEEDING THIS LIMIT SHALL BE CORRECTED.
9. THE ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL AN AS-BUILT DRAWING OF THE LIMEROCK BASE HAS BEEN SUBMITTED AND APPROVED BY THE EOR AND THE CITY ENGINEER OR DESIGNEE.
10. ALL APPLICABLE FEDERAL, STATE, COUNTY, DRAINAGE DISTRICT AND CITY PERMITS FOR CONSTRUCTION OF PAVING, GRADING, DRAINAGE, WATER, AND SANITARY SEWER SHALL BE OBTAINED PRIOR TO CONSTRUCTION.
11. ALL VEGETATION, MUCK AND ANY DELETERIOUS MATERIAL WITHIN THE ROW LIMITS OF ALL STREET AND ALLEYS AND REQUIRED OFF-STREET PARKING AREAS MUST BE REMOVED AND REPLACE WITH CLEAN FILL MATERIAL, FREE OF STUMPS, LARGE ROOTS OR OTHER MATTER NOT SUITABLE FOR INCLUSION IN ROADWAY FILL.
12. PAVEMENT MARKINGS SHALL BE FOOT REFLECTIVE PAINT EXCEPT AT ENTRANCE WHERE IT SHALL BE THERMOPLASTIC.

Type Demand	Type Unit	Demand /Unit	Unit	No. of Units	Demand
Prior Water Demand	Single Family Resident	300 gpd	ea.	2	600 gpd
Prior Wastewater Demand	Single Family Resident	300 gpd	ea.	2	600 gpd
Proposed Water Demand	Apartments	250 gpd	ea.	45	11,250 gpd
Proposed Wastewater Demand	Apartments	250 gpd	ea.	45	11,250 gpd

2742	EXISTING	PROPOSED
BUILDING FOOTPRINT	1,927.91 SQ FT	2,878.64 SQ FT
DRIVEWAY/WALKWAYS/PADS/POOL	4,038.41 SQ FT	10,245.01 SQ FT
LANDSCAPING	13,789.68 SQ FT	6,632.35 SQ FT
TOTAL AREA	19,756.00 SQ FT	19,756.00 SQ FT
TOTAL PERVIOUS AREA	13,789.68 SQ FT	6,632.35 SQ FT
TOTAL IMPERVIOUS AREA	5,966.32 SQ FT	13,123.65 SQ FT

2742	EXISTING	PROPOSED
BUILDING FOOTPRINT	1,387.82 SQ FT	0.00 SQ FT
DRIVEWAY/WALKWAYS/PADS	533.01 SQ FT	6,361.30 SQ FT
LANDSCAPING	8,335.17 SQ FT	3,894.70 SQ FT
TOTAL AREA	10,256.00 SQ FT	10,256.00 SQ FT
TOTAL PERVIOUS AREA	8,335.17 SQ FT	3,894.70 SQ FT
TOTAL IMPERVIOUS AREA	1,920.83 SQ FT	6,361.30 SQ FT



Plan view of the stormwater management facility. The layout includes a 7.00' SWALE, 16.00' PARKING STALLS, 22.00' ASPHALT DRIVEWAY, and a 5.00' SWALE. The total length is 50.00'. The elevation of the pipe is +7.38'.

The diagram shows a cross-section of a road profile. On the left, there is an 'ASPHALT DRIVEWAY' with a width of 20.91'. To the right of the driveway is a 'SWALE' with a width of 5.00'. The road surface is shown with a vertical curve of +7.50'. The ground level is indicated by a dashed line, and the road surface is shown as a solid line. A north arrow is located in the upper right corner, and a scale bar is in the lower right corner.



**GATOR ENGINEERING
ASSOCIATES, INC.**



DATE: REGINA BOBO-JACKSON, P.E.
FL P F NO: 38550

POLK STREET APARTMENTS II
2742 & 2741 POLK STREET
HOLLYWOOD, FL

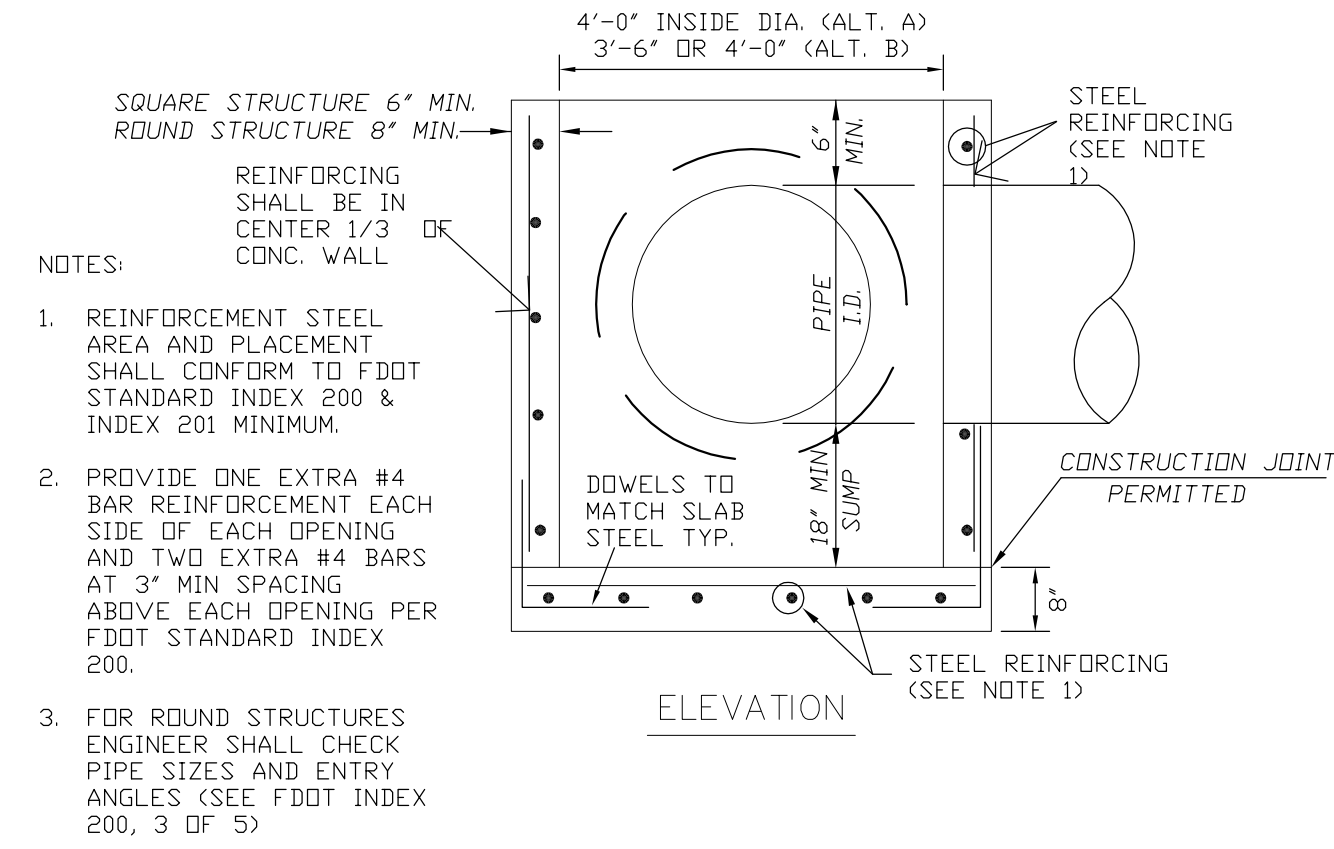
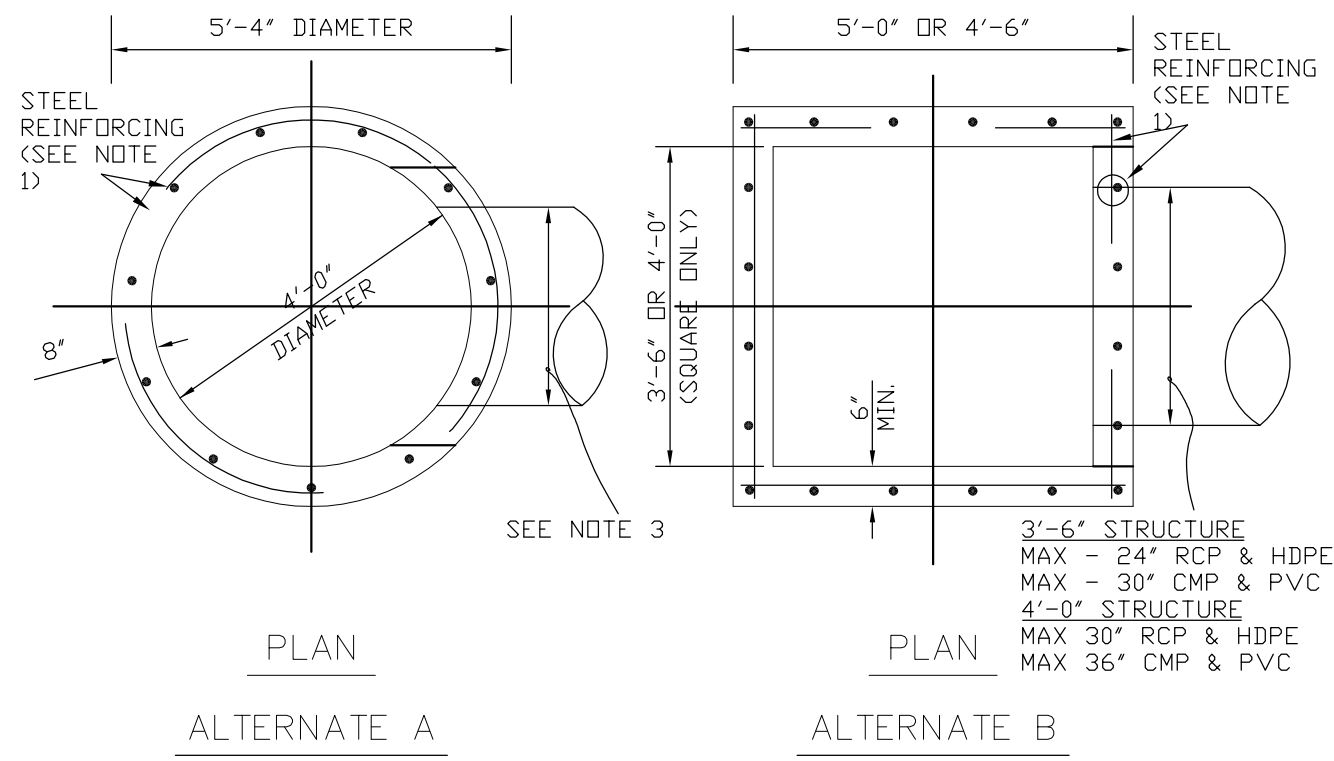
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GEA PROJECT NO.: 21046a
DATE: 06-07-2021
SCALE: AS SHOWN
DESIGNED BY: R.B.J.
DRAWN BY: L.B.
CHECKED BY: R.B.J
APPROVED BY: R.B.J

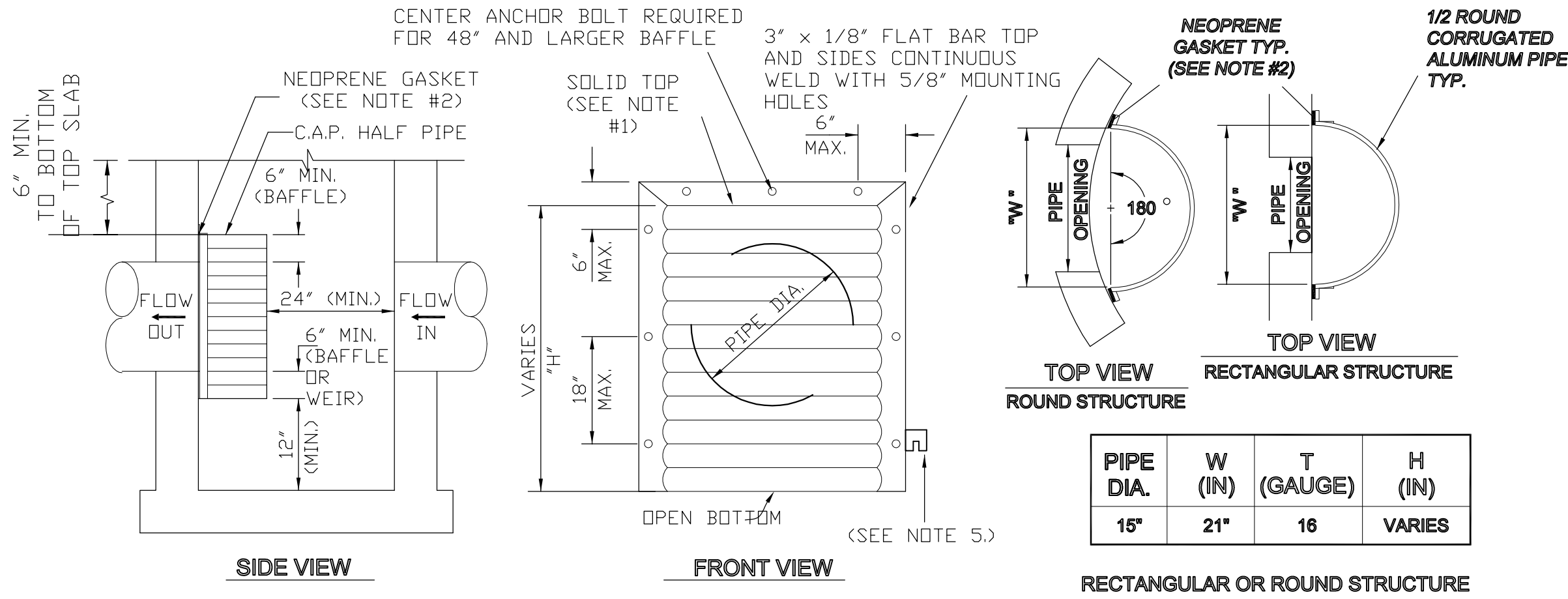
SHEET TITLE

NOTES &
SECTIONS

C3 OF 7

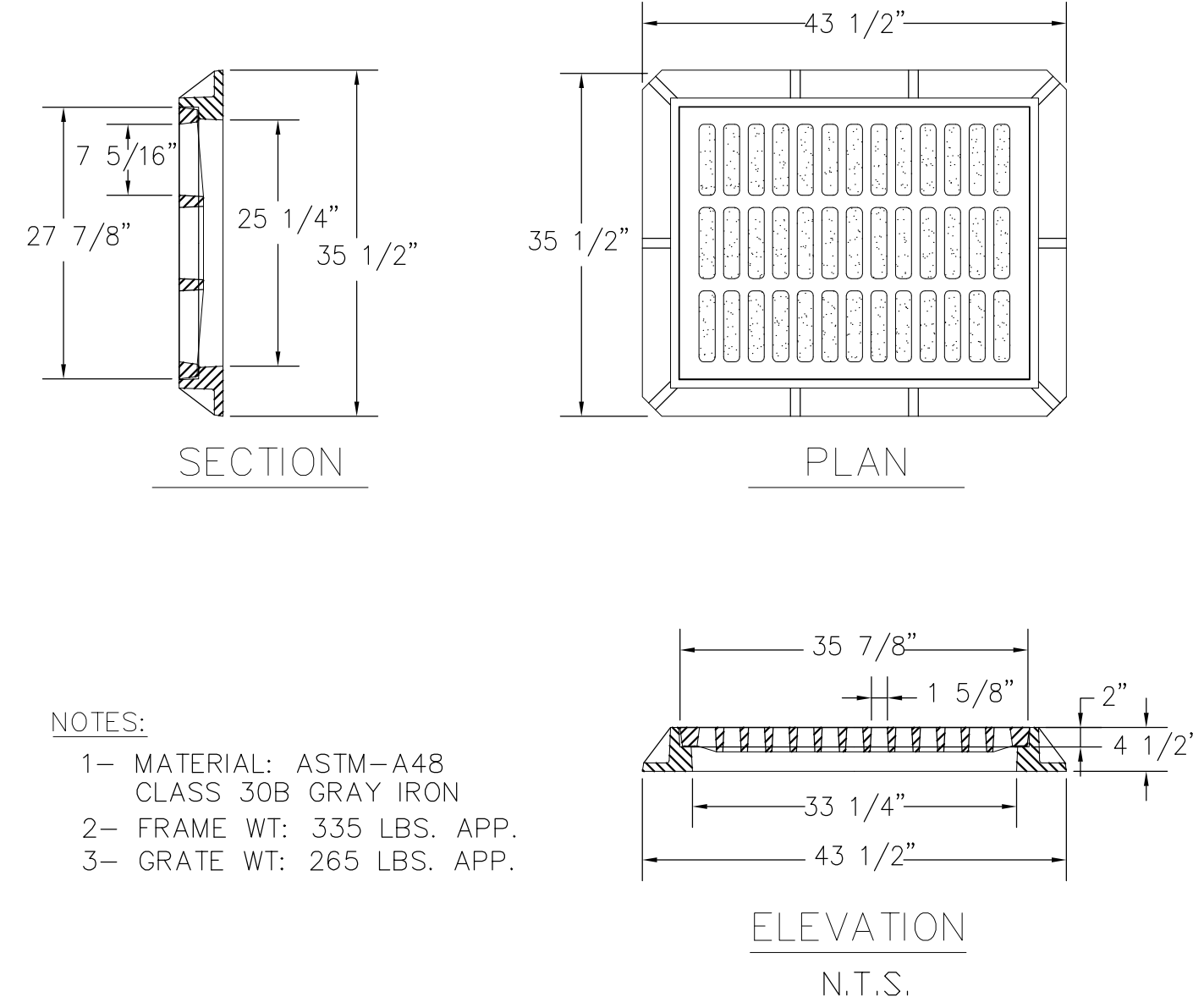


TYPE "P" - MANHOLE & INLET
STRUCTURE BOTTOM
NOT TO SCALE



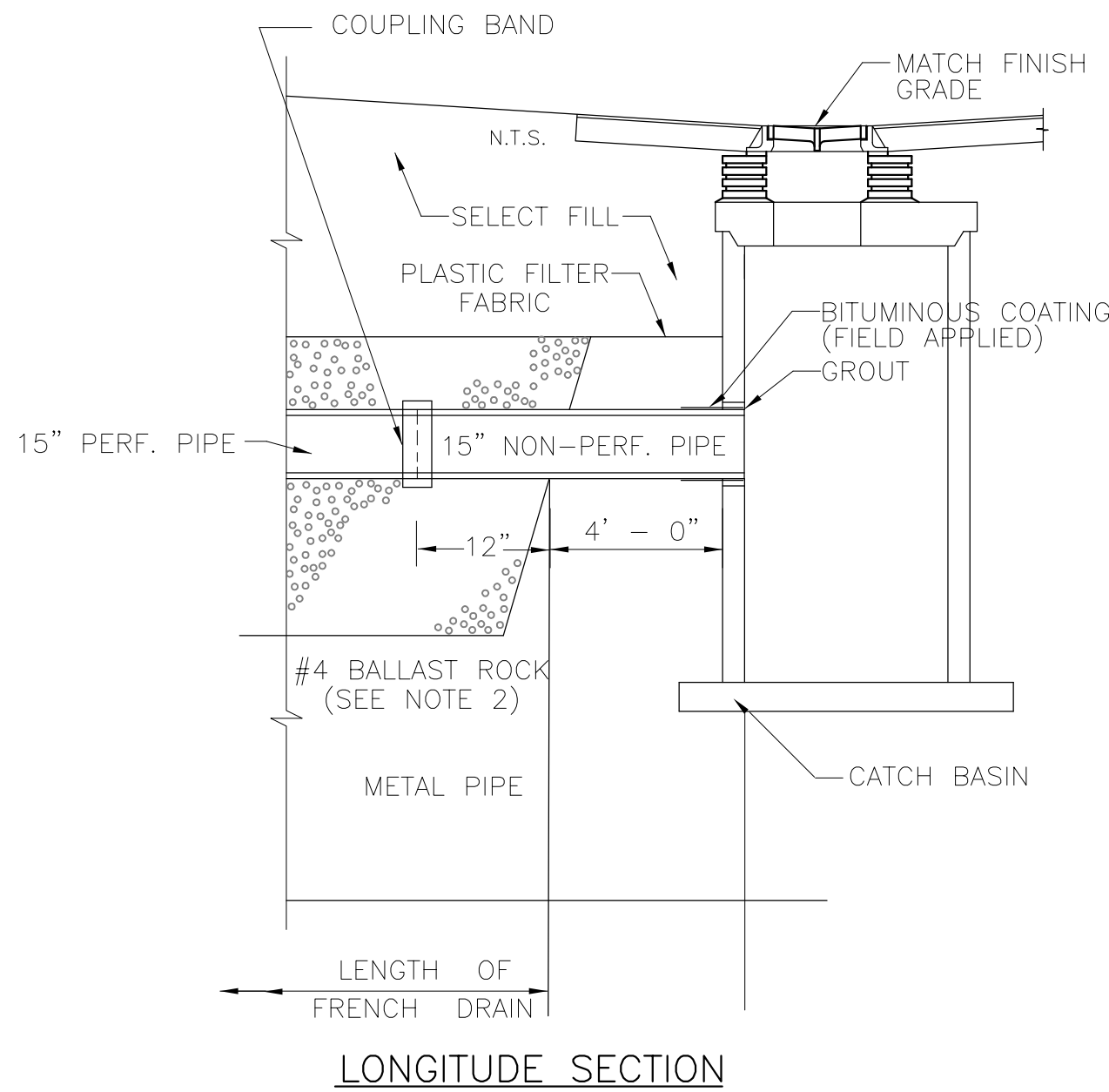
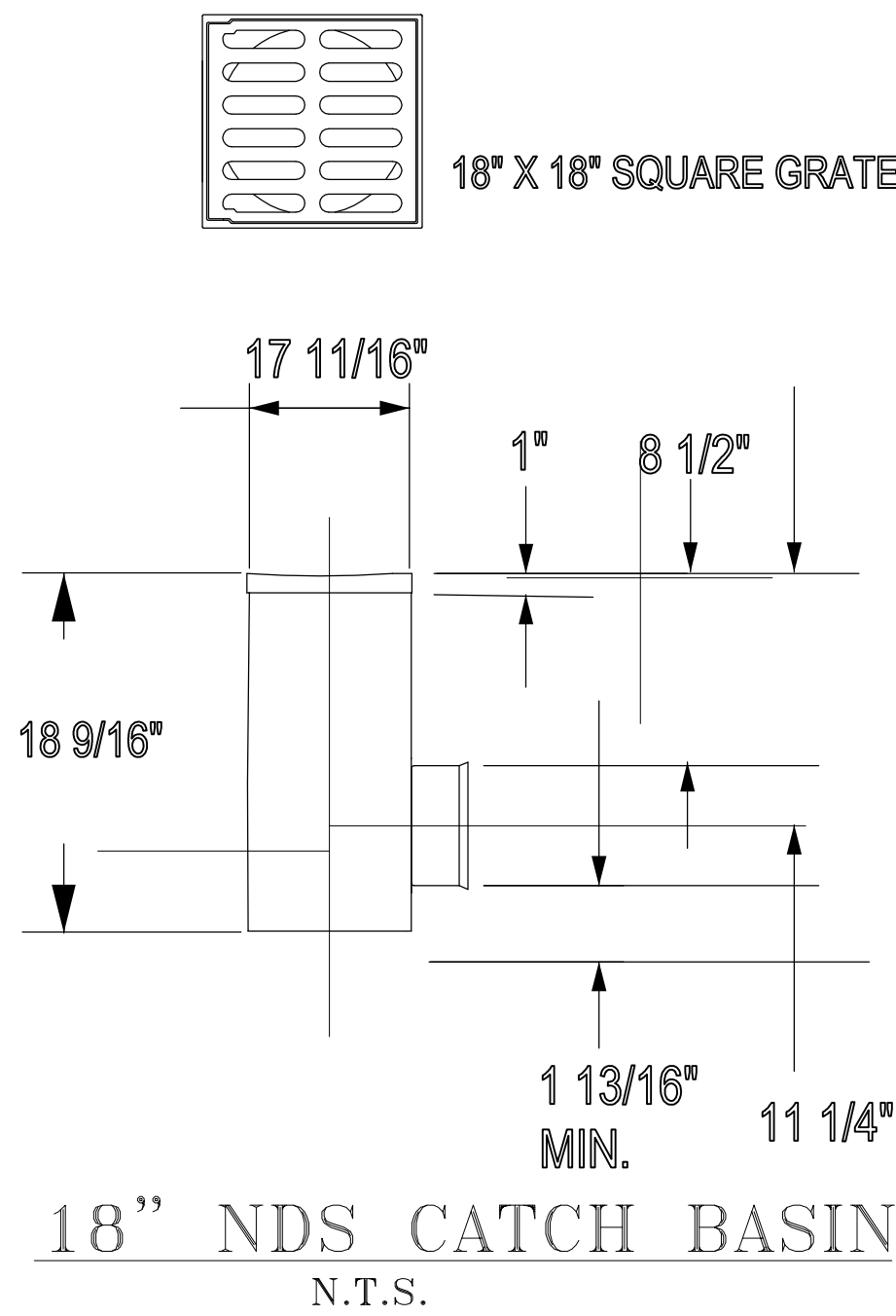
- NOTES:
- ALUMINUM SHEET OF SAME THICKNESS (GAUGE) AS PIPE SHALL BE WELDED TO CLOSE OPENING AT THE TOP OR BOTTOM.
 - NEOPRENE ADHESIVE BACKED GASKET, OR APPROVED EQUAL (1" x 2") SHALL BE INSTALLED ON THE SIDES AND TOP OF OF ALL BAFFLES AND BOTTOM OF ALL WEIRS.
 - POLLUTION RETARDANT BAFFLE TO BE FASTENED IN PLACE WITH 1/2" x 4" STAINLESS STEEL "RED HEAD" ANCHORS, OR APPROVED EQUAL. ALL MOUNTING HARDWARE TO BE STAINLESS STEEL.
 - FIBERGLASS BAFFLES ARE NOT PERMITTED.
 - BRACKETS MAY BE ADDED TO FLAT BARS TO EASE INSTALLATION IN ROUND STRUCTURES. SPACING TO MATCH HOLES IN FLAT BARS.
 - PIPE CORRUGATION SHALL BE ANNULAR.

POLLUTION RETARDANT BAFFLE DETAILS
NOT TO SCALE



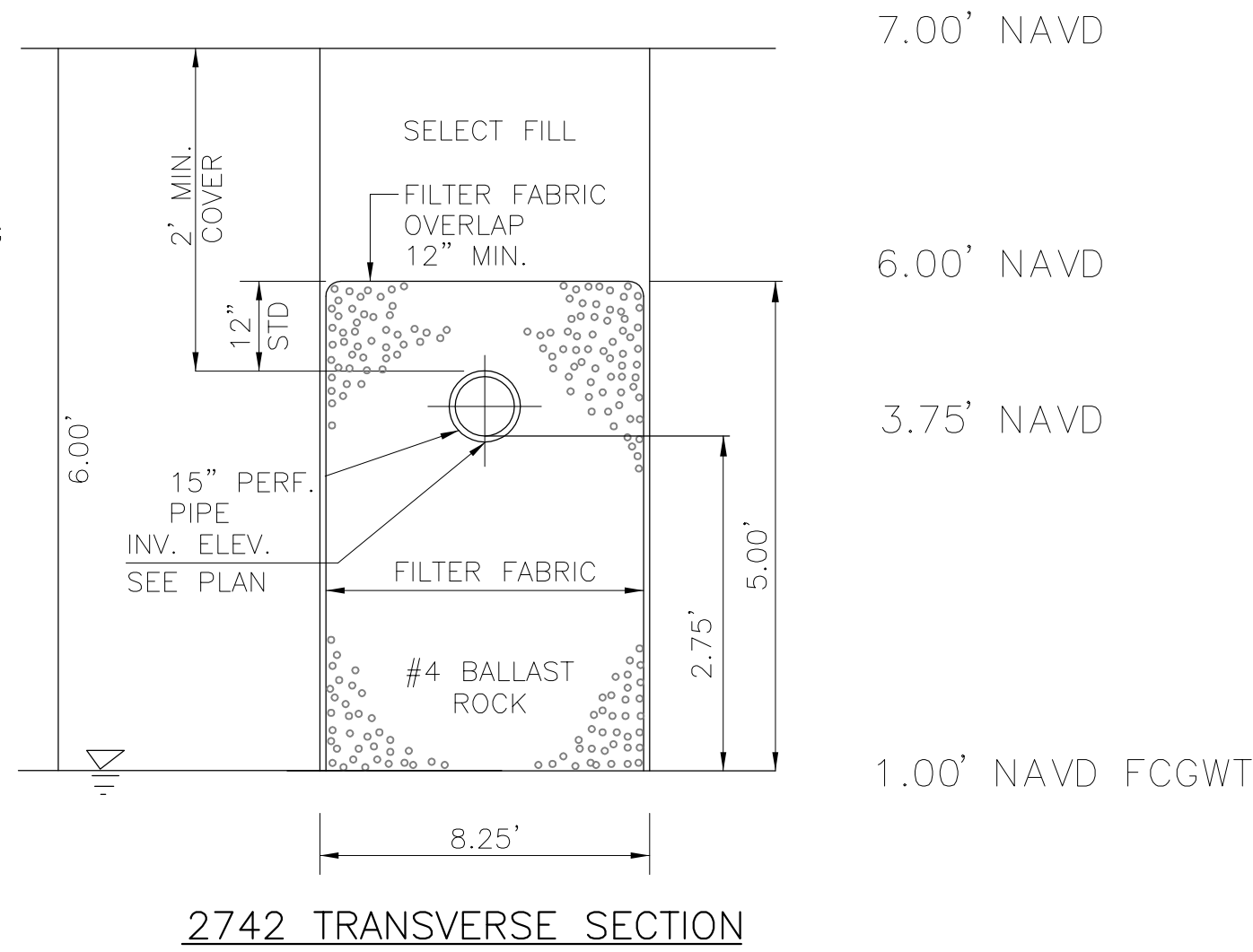
U.S. FOUNDRY #4155-6209
FRAME AND GRATE OR APPROVED
EQUAL PEDESTRIAN & BICYCLE
COMPATIBLE

INLET FRAME AND GRATE
NOT TO SCALE

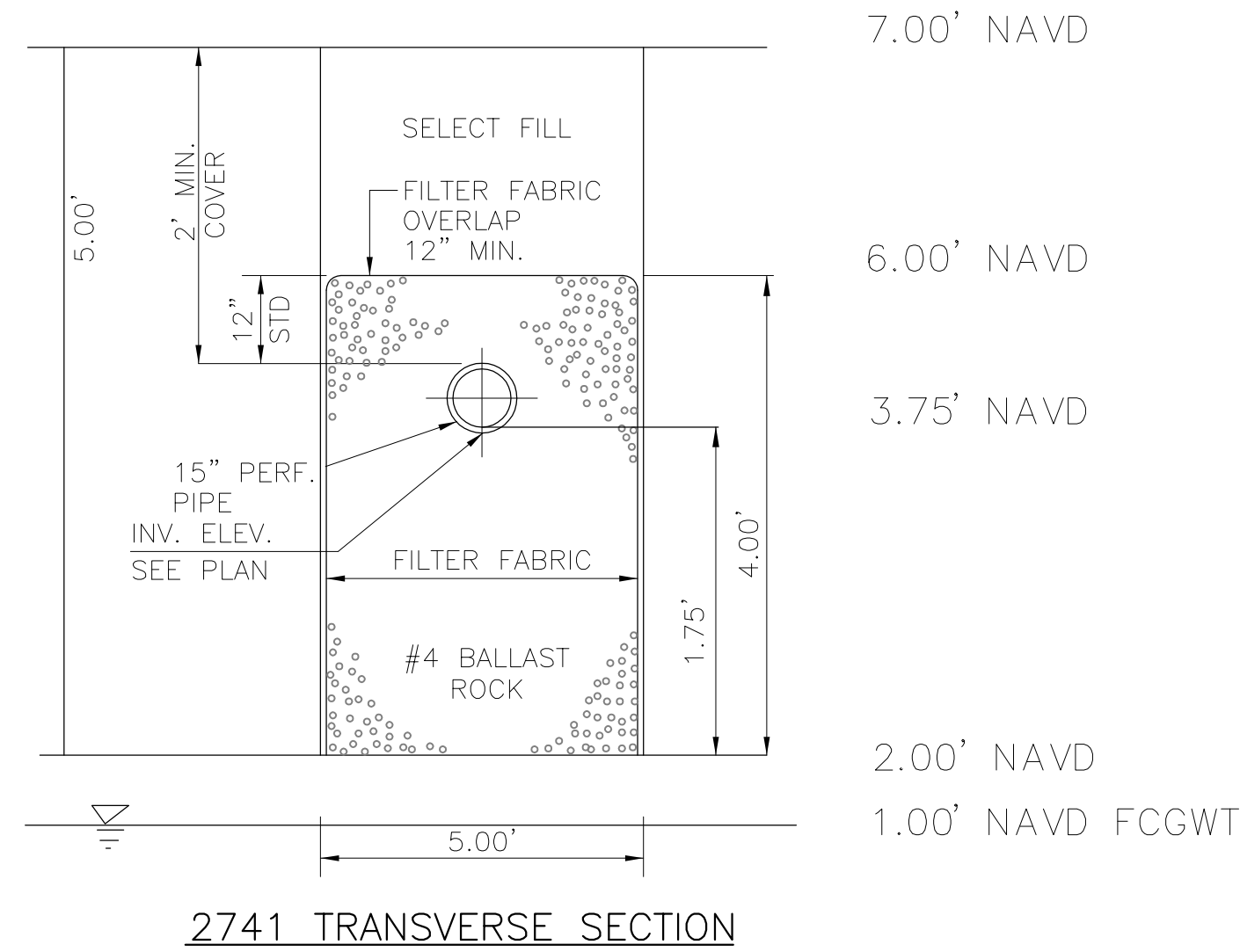


NOTES:

- FILTER FABRIC PER F.D.O.T. STD. INDEX # 285 SHALL BE USED AT EACH SIDE AND ON TOP, AND AT EACH END OF FRENCH DRAIN TRENCH.
- AFTER THE BALLAST ROCK HAS BEEN PLACED TO THE PROPER ELEVATION IT SHALL BE CAREFULLY VIBRATED OR COMPACTED IN ORDER TO ALLOW FOR INITIAL SETTLEMENT THAT MAY OCCUR. IF IT DOES TAKE PLACE, ADDITIONAL BALLAST ROCK WILL BE ADDED TO RESTORE THE BALLAST ROCK TO THE PROPER ELEVATION SO THAT THE EXFILTRATION TRENCH CAN BE COMPLETED IN ACCORDANCE WITH THE DETAIL.

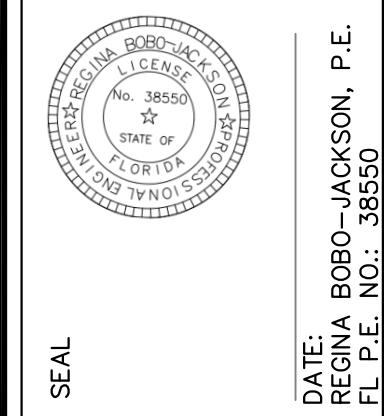


2742 TRANSVERSE SECTION



2741 TRANSVERSE SECTION

EXFILTRATION TRENCH DETAIL
N.T.S.



POLK STREET APARTMENTS III
2742 & 2741 POLK STREET
HOLLYWOOD, FL

REVISIONS	DATE	DESCRIPTION

GEA PROJECT NO.: 21046a
DATE: 06-07-2021
SCALE: AS SHOWN
DESIGNED BY: R.B.J.
DRAWN BY: L.B.
CHECKED BY: R.B.J.
APPROVED BY: R.B.J.

SHEET TITLE

CIVIL
DETAILS
C5 of 7

Polk Street Apartments

Hollywood, Florida

TRAFFIC STUDY

prepared for:
S&B Ent, LLC

KBP CONSULTING, INC.

December 2020
Updated August 2021

Polk Street Apartments

Hollywood, Florida

Traffic Study

December 2020

Updated August 2021

Prepared for:
S&B Ent, LLC

Prepared by:
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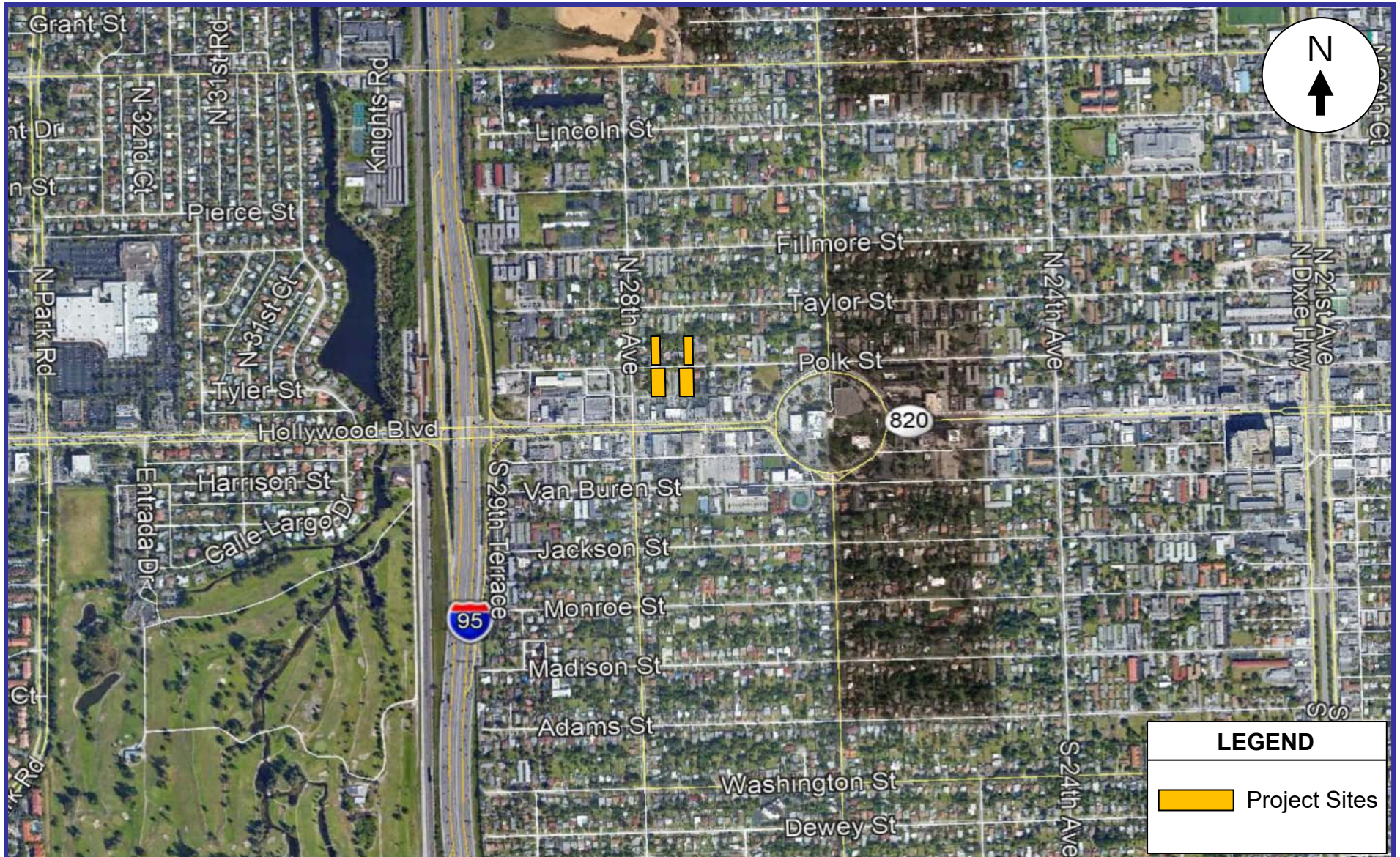
INTRODUCTION

A residential development to be known as Polk Street Apartments is proposed on several parcels of land along Polk Street between N. 26th Avenue and N. 28th Avenue in Hollywood, Broward County, Florida. The locations of the project sites are illustrated graphically in Figure 1 on the following page.

KBP Consulting, Inc. has been retained by S&B Ent, LLC to prepare a traffic study in connection with this proposed redevelopment project. This study addresses the trip generation characteristics associated with the proposed residential development, the projected peak period turning movement volumes at the project access driveways on Polk Street, traffic impacts at the nearby intersections, and the likely traffic circulation patterns to and from the site.

This traffic study is divided into nine (9) sections, as listed below:

1. Inventory
2. Existing Conditions
3. Traffic Counts
4. Trip Generation
5. Trip Distribution and Traffic Assignment
6. Traffic Circulation Patterns
7. Traffic Impact Analyses
8. Supplemental Analyses
9. Summary & Conclusions



INVENTORY

Existing Land Uses and Access

The subject project is comprised of two (2) separate sites and four (4) separate parcels of land with a total land area of approximately +/- 1.38 acres. The eastern site (2718-2720 and 2723 Polk Street) has a land area of 0.69 acre and consists three (3) low-rise multifamily residential dwelling units. The Folio ID Numbers for this portion of the development are as follows:

- 5142 16 02 3370
- 5142 16 02 3590

The western site (2741-2742 Polk Street) also has a land area of 0.69 acre and consists of two (2) single family residential dwelling units. The Folio ID Numbers for this portion of the development are as follows:

- 5142 16 02 3350
- 5142 16 02 3630

Vehicular access to these parcels is provided along Polk Street which is a two-lane local roadway with no on-street parking.

Proposed Land Uses and Access

Multifamily residential developments are proposed on both the eastern and the western sites. Both sites will be developed with 44 dwelling units in a four-story building (mid-rise format) to be located on the south side of Polk Street. There will be 33 parking spaces on the ground floor and an additional 16 parking spaces in a surface parking lot on the north side of Polk Street. Vehicular access to these sites will be provided by full access driveways on Polk Street. The parcels on the south side of Polk Street will also have emergency vehicle access only to the alleyway to the south. Appendix A contains the preliminary site plans for the project.

EXISTING CONDITIONS

This section of the report addresses the transportation system located in the immediate vicinity of the subject site.

Roadway System

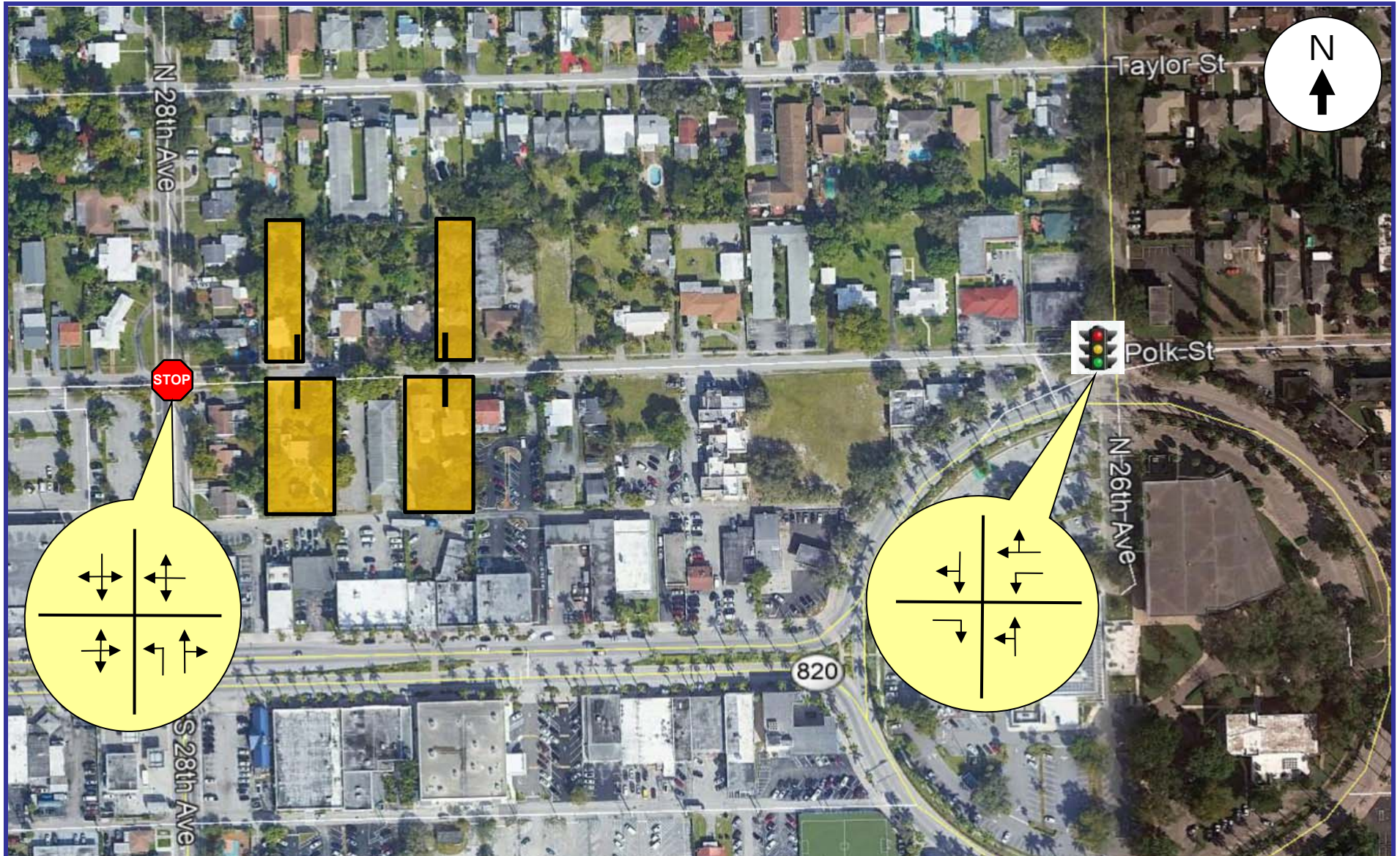
Within the limits of the project study area, Polk Street is a two-lane local roadway oriented in the east-west direction. This roadway connects N. 26th Avenue to the east and N. 28th Avenue to the west. Hollywood Boulevard (State Road 820) is located one block to the south of Polk Street. This roadway is a major east-west state-maintained arterial roadway. This facility provides regional access and has interchanges with Interstate 95 and the Florida's Turnpike. The immediately surrounding roadway network is characterized by an extensive grid system of two-lane / two-way local roadways. The project's utilization of this grid network is elaborated upon in subsequent sections of this report.

Study Intersections

Two (2) intersections were identified as the locations to be evaluated as part of this traffic impact analysis. These intersections are:

- Polk Street and N. 26th Avenue
- Polk Street and N. 28th Avenue

Figure 2 on the following page depicts the existing lane geometry of these two (2) intersections identified for analysis purposes.



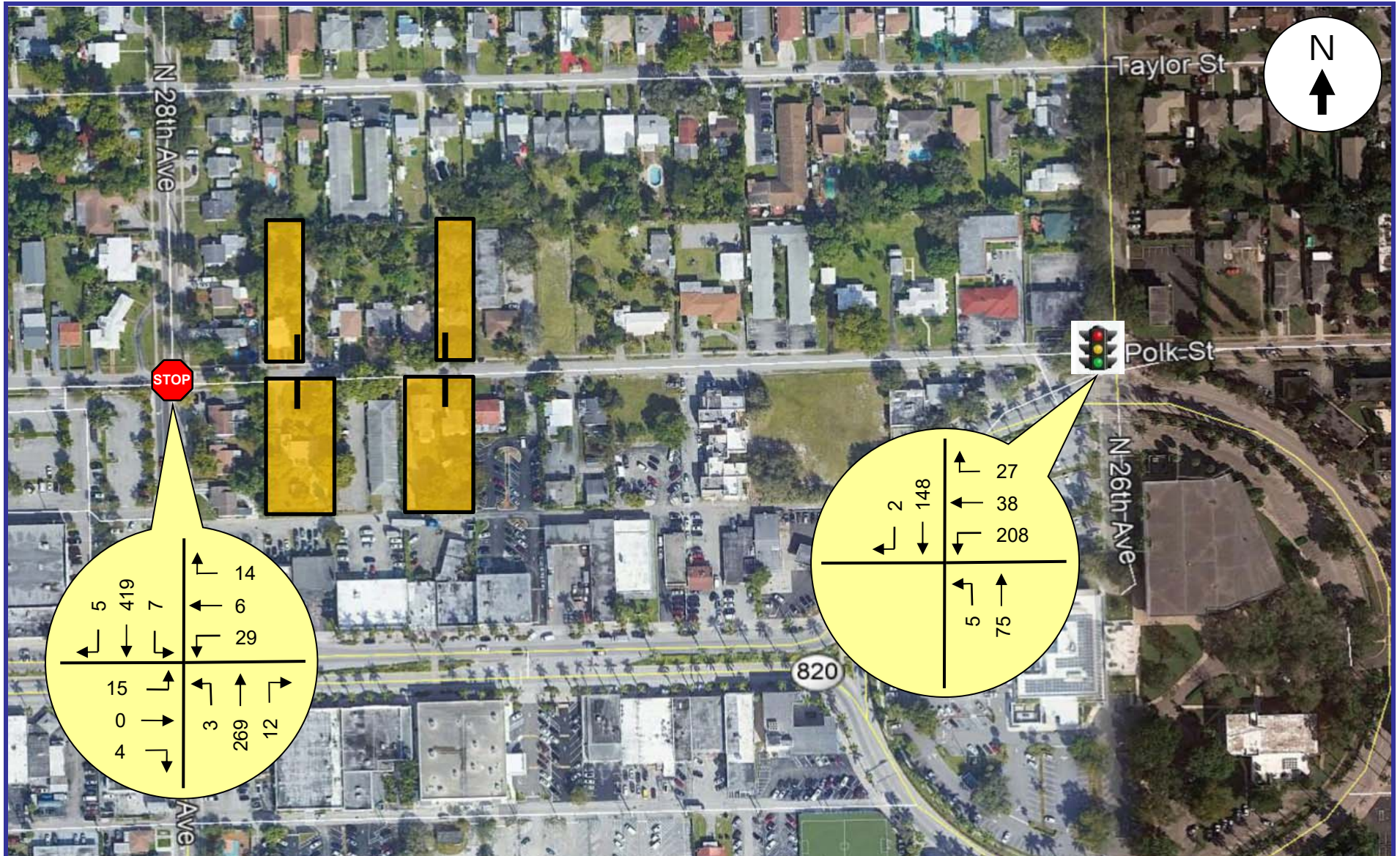
TRAFFIC COUNTS

KBP Consulting, Inc., in association with Traffic Survey Specialists, Inc., collected traffic data at the following intersections:

- Polk Street and N. 26th Avenue
- Polk Street and N. 28th Avenue

The intersection turning movement counts were collected on Tuesday, July 13, 2021, during the AM peak period (7:00 AM to 9:00 AM) and the PM peak period (4:00 PM to 6:00 PM). Appendix B contains the traffic data as collected in the field.

These counts have been adjusted by a peak season conversion factor of 1.06 to reflect average peak season conditions. (Please see Appendix C for the 2019 peak season factor category report published by the Florida Department of Transportation (FDOT) for Broward County. A review of the 2020 report reveals inconsistencies likely attributed to the COVID-19 conditions.) The resulting average peak season adjusted turning movement counts are presented in Figures 3 and 4 on the following pages.

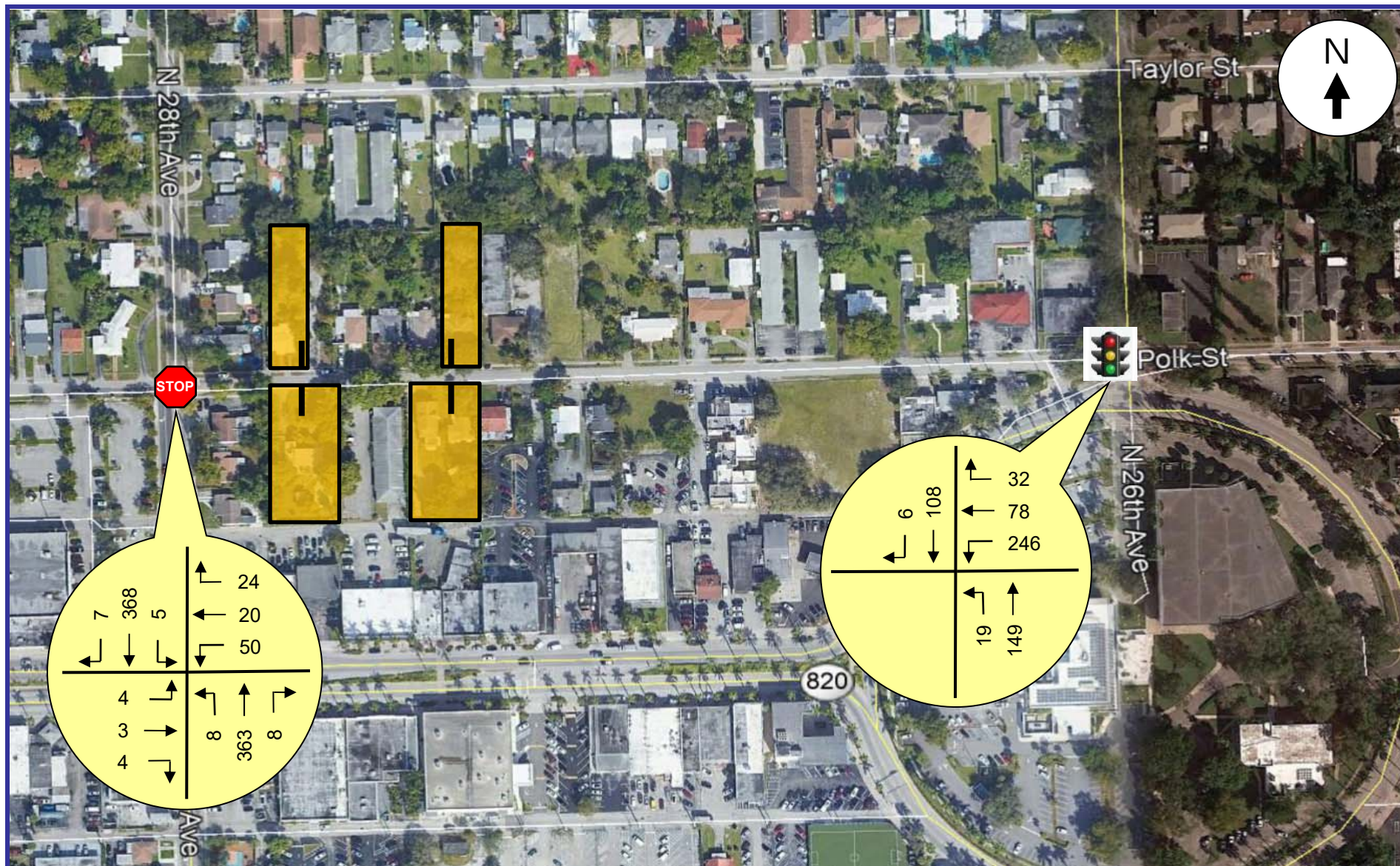


KBP
CONSULTING, INC.

Existing AM Peak Hour Traffic Counts

Source: Traffic Survey Specialists, Inc. (7/13/21)
Adjusted for Average Peak Season Conditions

FIGURE 3
Polk Street Apartments
Hollywood, Florida



KBP
CONSULTING, INC.

Existing PM Peak Hour Traffic Counts

Source: Traffic Survey Specialists, Inc. (7/13/21)
Adjusted for Average Peak Season Conditions

FIGURE 4
Polk Street Apartments
Hollywood, Florida

TRIP GENERATION

A trip generation analysis has been conducted for the proposed Polk Street Apartments project. The analysis was performed using the trip generation rates and equations published in the Institute of Transportation Engineer's (ITE) *Trip Generation Manual (10th Edition)*. The trip generation analysis was undertaken for daily, AM peak hour and PM peak hour conditions. According to the ITE report, the most appropriate "land use" categories and corresponding trip generation rates and equations for the existing and proposed development are as follows:

SINGLE-FAMILY DETACHED HOUSING – ITE LAND USE #210

- ❑ Weekday: $T = 9.44 (X)$
where T = number of trips and X = number of dwelling units
- ❑ AM Peak Hour: $T = 0.74 (X)$ (25% in / 75% out)
- ❑ PM Peak Hour: $T = 0.99 (X)$ (63% in / 37% out)

MULTIFAMILY HOUSING (LOW-RISE) – ITE LAND USE #220

- ❑ Weekday: $T = 7.32 (X)$
where T = number of trips and X = number of dwelling units
- ❑ AM Peak Hour: $T = 0.46 (X)$ (23% in / 77% out)
- ❑ PM Peak Hour: $T = 0.56 (X)$ (63% in / 37% out)

MULTIFAMILY HOUSING (MID-RISE) – ITE LAND USE #221

- ❑ Weekday: $T = 5.45 (X) - 1.75$
where T = number of trips and X = number of dwelling units
- ❑ AM Peak Hour: $\ln(T) = 0.98 \ln(X) - 0.98$ (26% in / 74% out)
- ❑ PM Peak Hour: $\ln(T) = 0.96 \ln(X) - 0.63$ (61% in / 39% out)

Utilizing the above-listed trip generation rates and equations from the referenced ITE manual, a trip generation analysis was undertaken for the existing and proposed development. The results of this effort are documented in Table 1 on the following page and applicable excerpts from the referenced ITE manual are presented in Appendix D.

Table 1								
Trip Generation Summary								
Polk Street Apartments - Hollywood, Florida								
Land Use	Size	Daily Trips	AM Peak Hour Trips			PM Peak Hour Trips		
			In	Out	Total	In	Out	Total
<i>Existing</i>								
Multifamily Housing (Low-Rise)	3 DU	22	0	1	1	1	1	2
Single Family Detached Housing	2 DU	19	0	1	1	1	1	2
Total		41	0	2	2	2	2	4
<i>Proposed</i>								
Multifamily Housing (Mid-Rise) - East	44 DU	238	4	11	15	12	8	20
Multifamily Housing (Mid-Rise) - West	44 DU	238	4	11	15	12	8	20
Total		476	8	22	30	24	16	40
Difference (Proposed - Existing)		435	8	20	28	22	14	36

Compiled by: KBP Consulting, Inc. (July 2021).

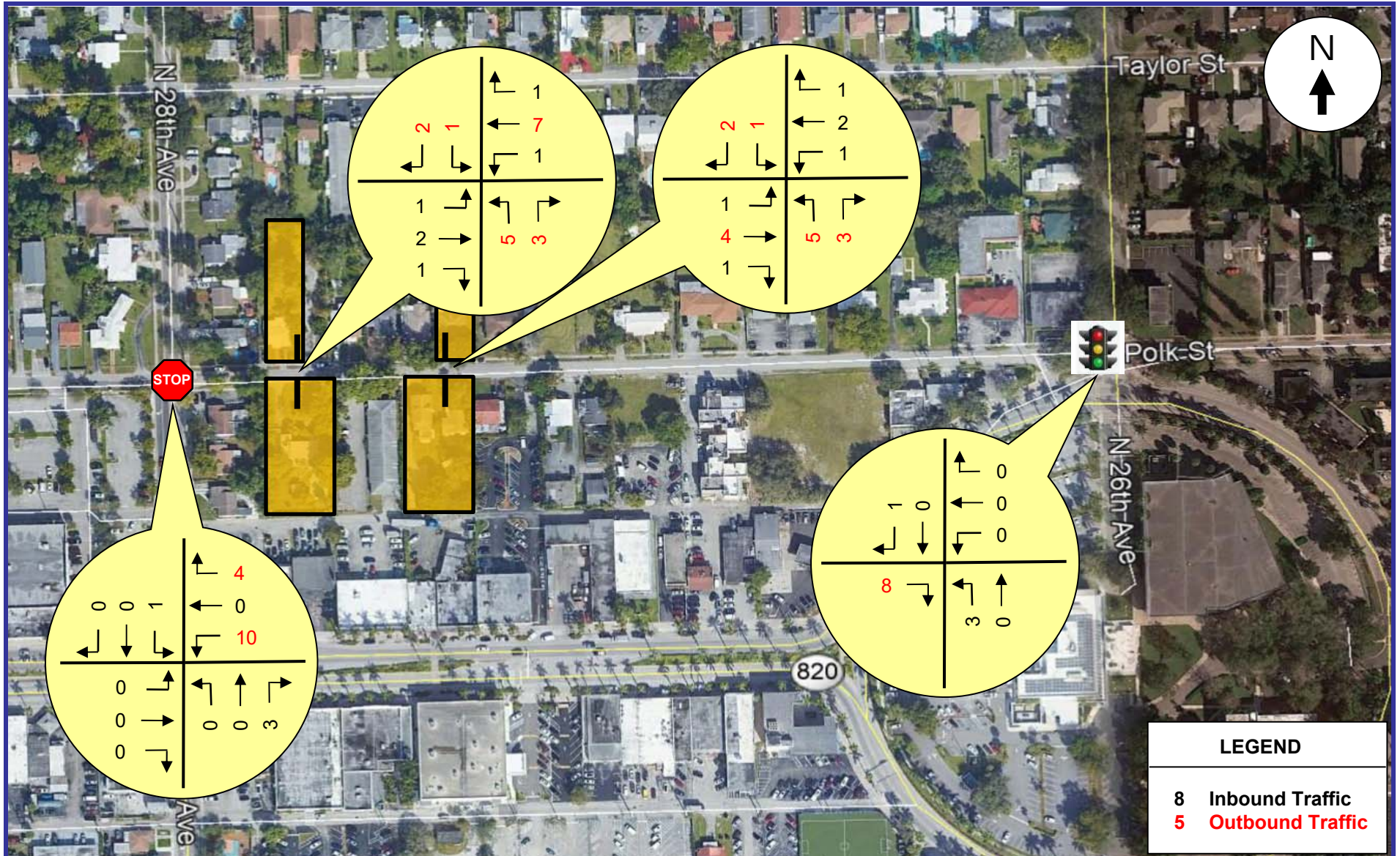
Source: Institute of Transportation Engineers (ITE) Trip Generation Manual (10th Edition).

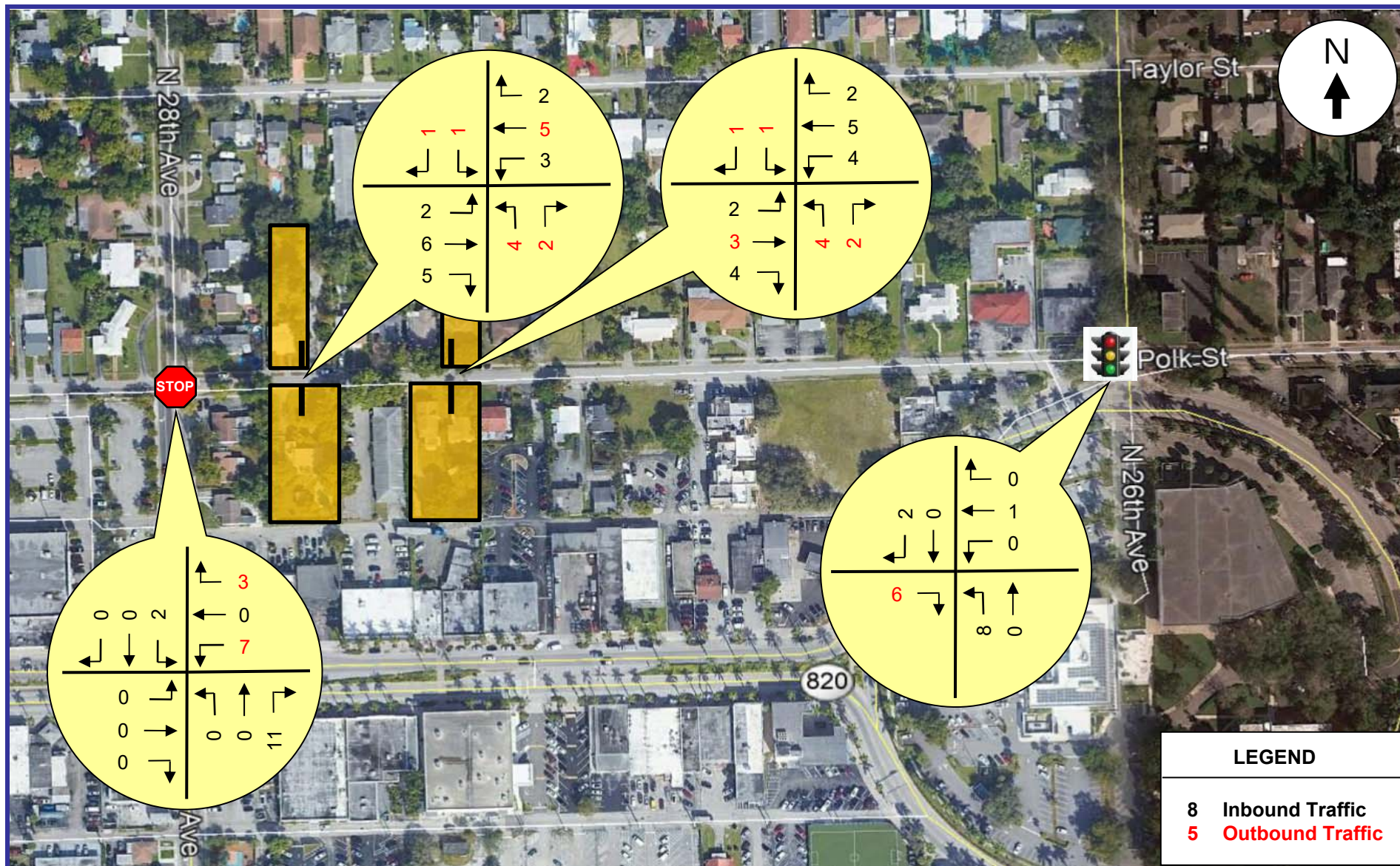
As indicated in Table 1 above, the proposed residential development is anticipated to generate approximately 476 daily vehicle trips, 30 AM peak hour vehicle trips (8 inbound and 22 outbound) and approximately 40 vehicle trips (24 inbound and 16 outbound) during the typical afternoon peak hour. When considering the existing single and multifamily residences on the subject sites this represents an increase of 435 daily vehicle trips, an increase of 28 AM peak hour vehicle trips, and an increase of 36 PM peak hour vehicle trips.

TRIP DISTRIBUTION AND TRAFFIC ASSIGNMENT

The trip distribution and traffic assignment for the proposed Polk Street Apartments residential development was developed based upon knowledge of the study area, examination of the surrounding roadway network characteristics, review of current traffic volumes, and existing land use patterns. The AM and PM peak hour traffic generated by the project was assigned to the project driveways and the intersections at Polk Street and N. 26th Avenue and at Polk Street and N. 28th Avenue. This information is summarized in Figures 5 and 6 on the following pages. The general directional distributions are as follows:

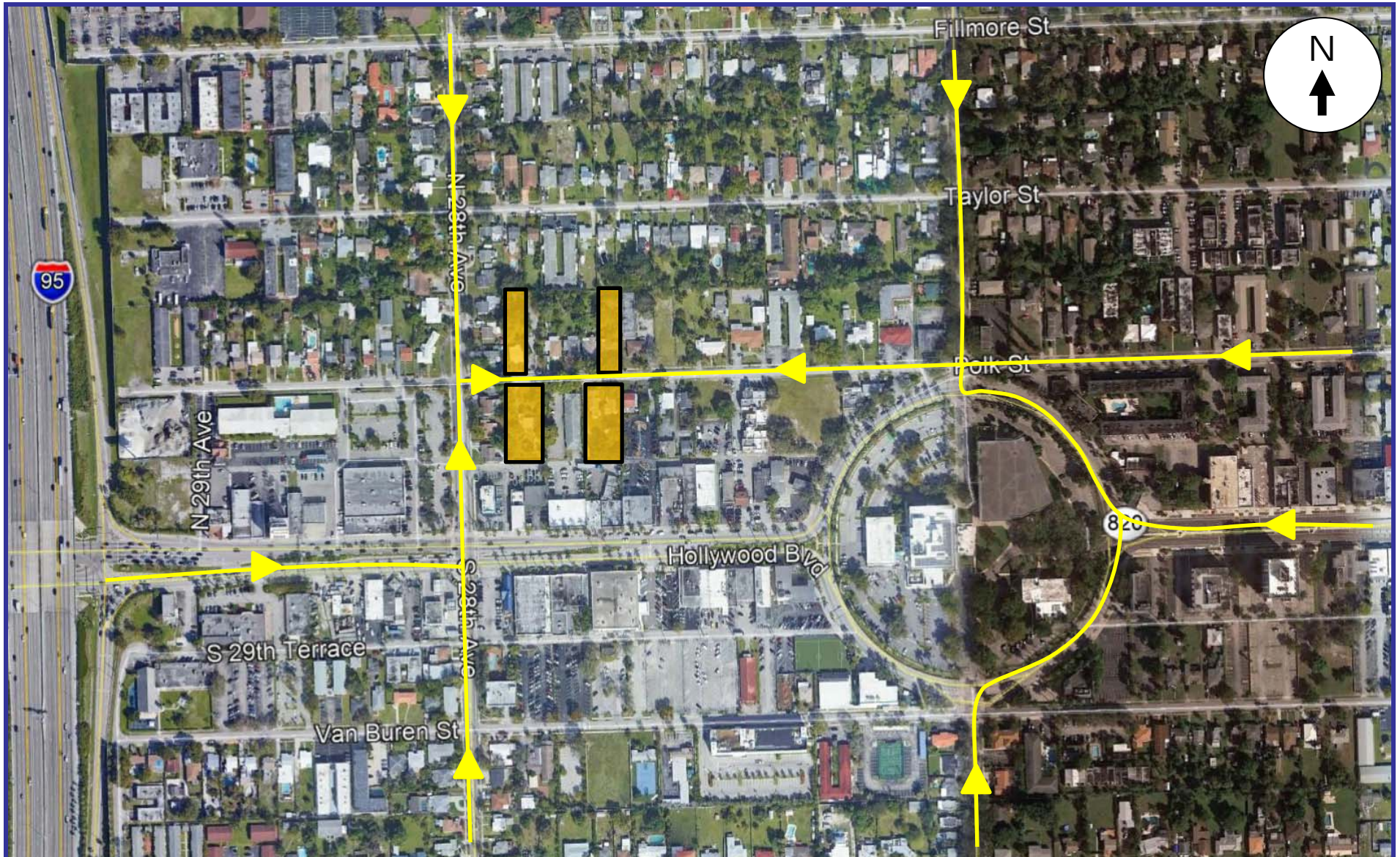
- 40% to and from the west via Hollywood Boulevard
- 30% to and from the east via Hollywood Boulevard
- 10% to and from the north via N. 26th Avenue
- 10% to and from the south via S. 26th Avenue
- 5% to and from the north via N. 28th Avenue
- 5% to and from the south via S. 28th Avenue





TRAFFIC CIRCULATION PATTERNS

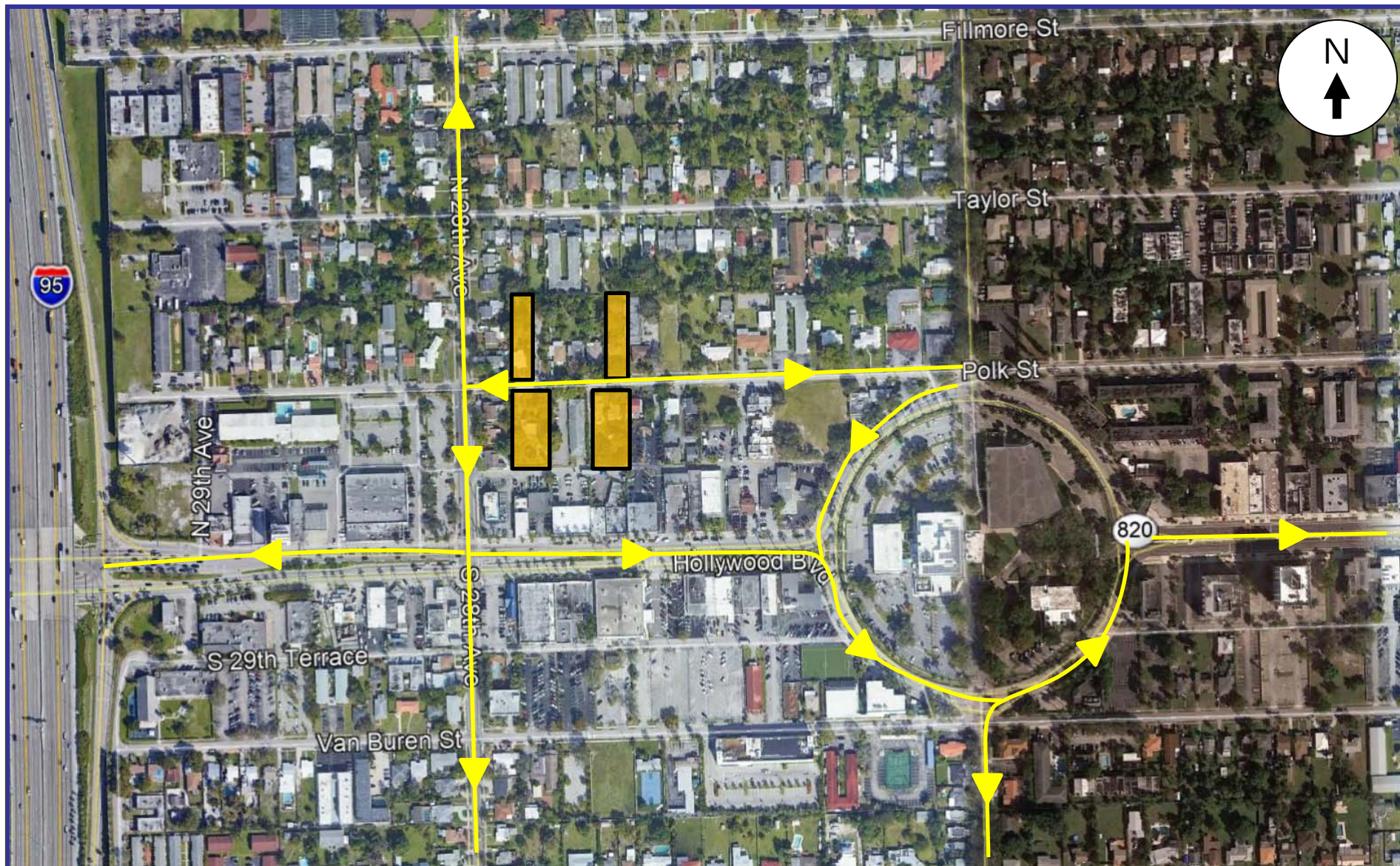
Since the intersection of Polk Street, N. 26th Avenue and Hollywood Boulevard has several turning movement restrictions, a more detailed evaluation and development of the likely traffic circulation patterns has been performed. Fortunately, the entire study area is characterized by an extensive grid roadway network that accommodates movements to and from all directions. The most likely inbound and outbound travel patterns are depicted graphically in Figures 7 and 8 on the following pages.



KBP
CONSULTING, INC.

Future Inbound Travel Patterns

FIGURE 7
Polk Street Apartments
Hollywood, Florida



KBP
CONSULTING, INC.

Future Outbound Travel Patterns

FIGURE 8
Polk Street Apartments
Hollywood, Florida

TRAFFIC IMPACT ANALYSES

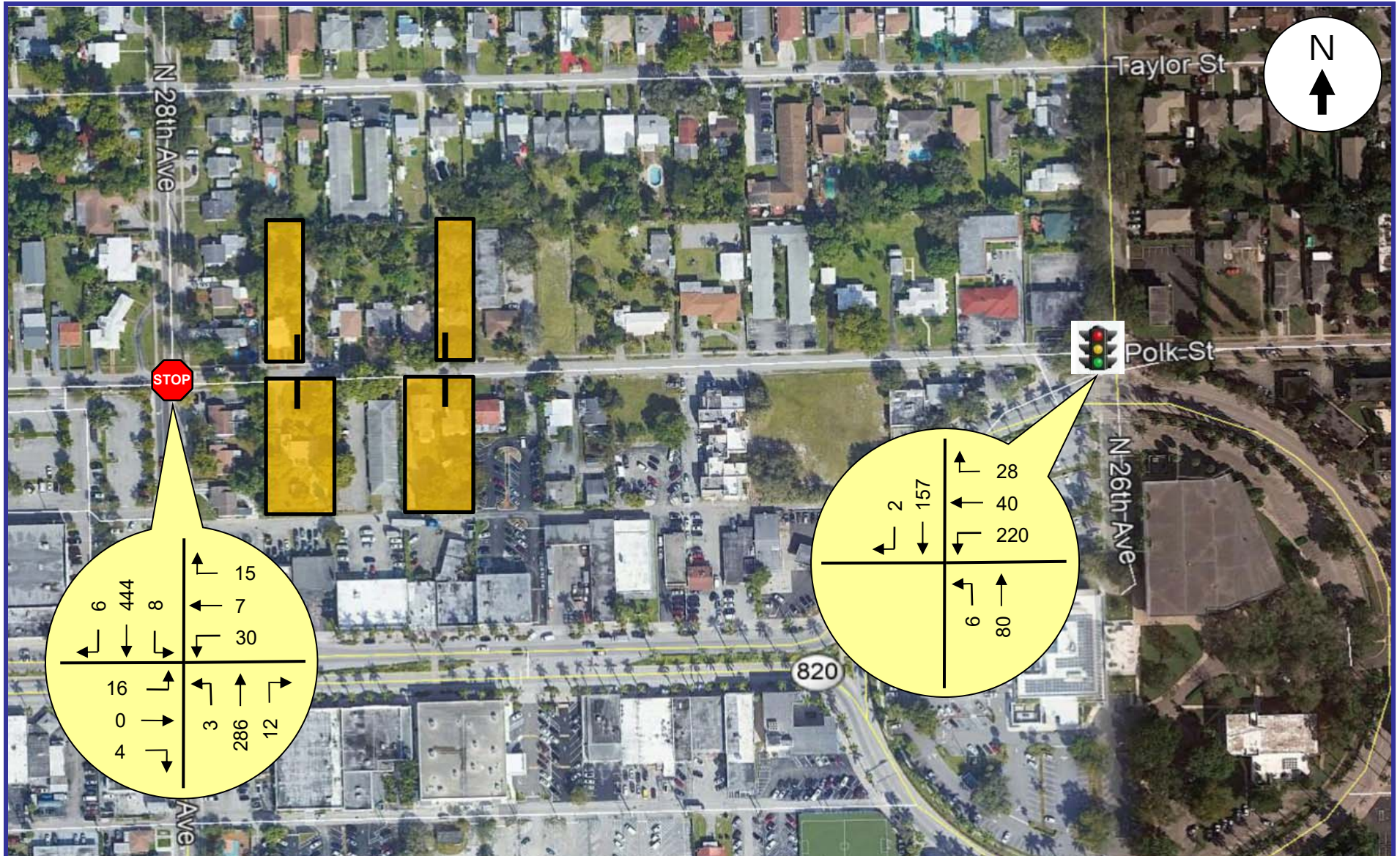
This section of the study is divided into two (2) primary parts. The first part of this section involves the development of the future build-out year (2023) traffic volumes for the study area. The second part of this section includes level-of-service analyses for existing and future conditions.

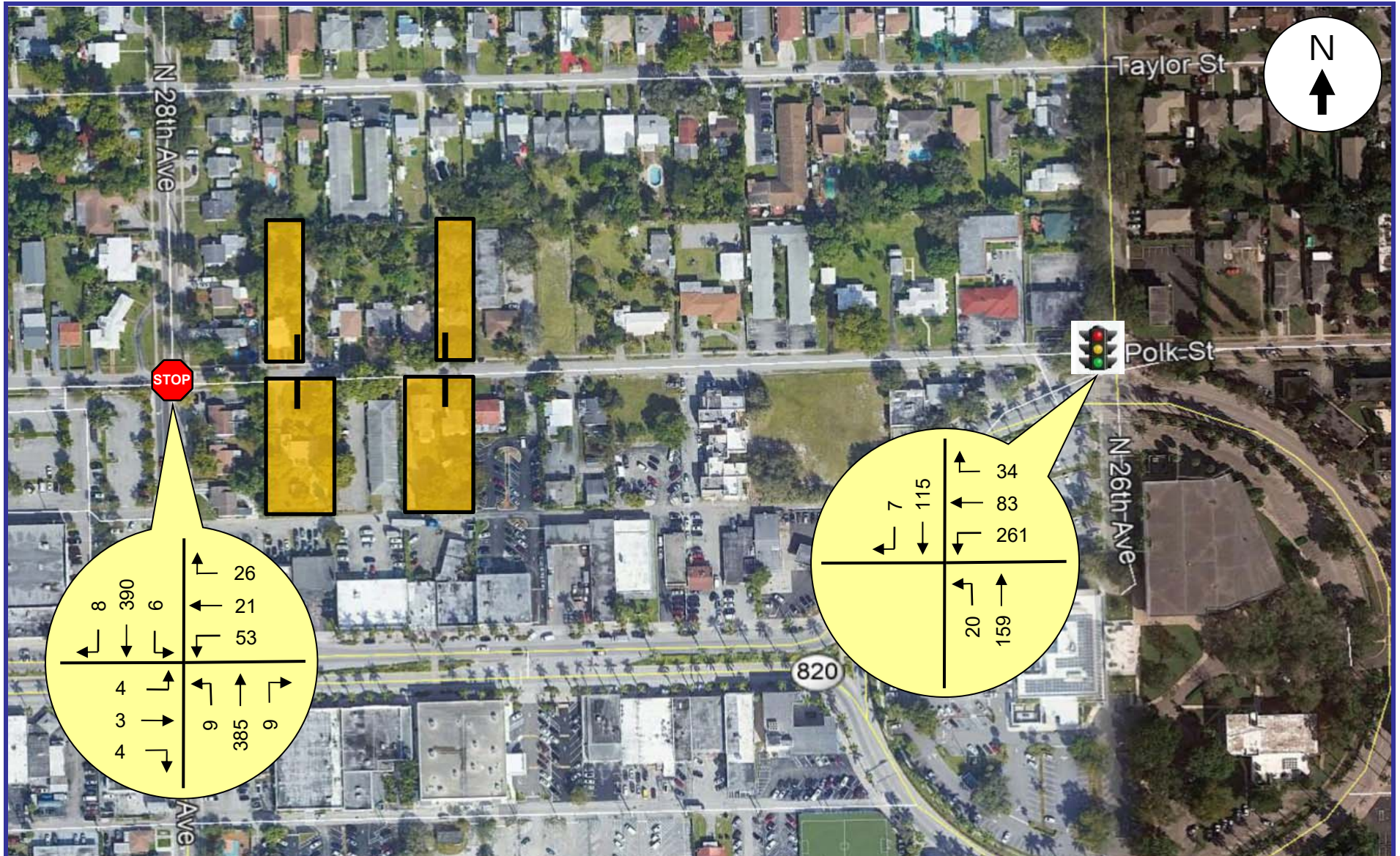
Future Conditions Traffic Volumes

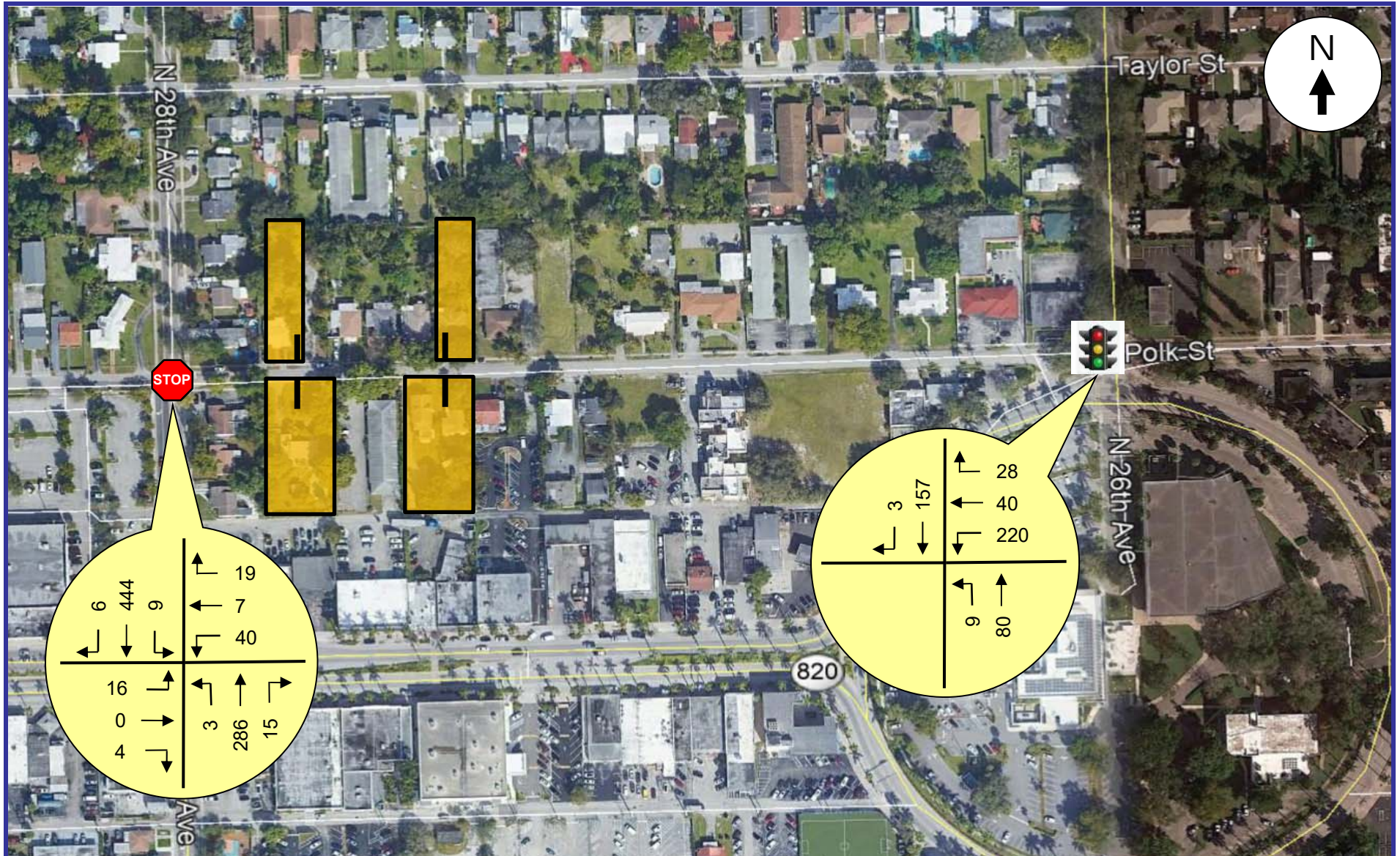
Future, build-out year (2023) traffic volumes were developed for the project study area in the following manner:

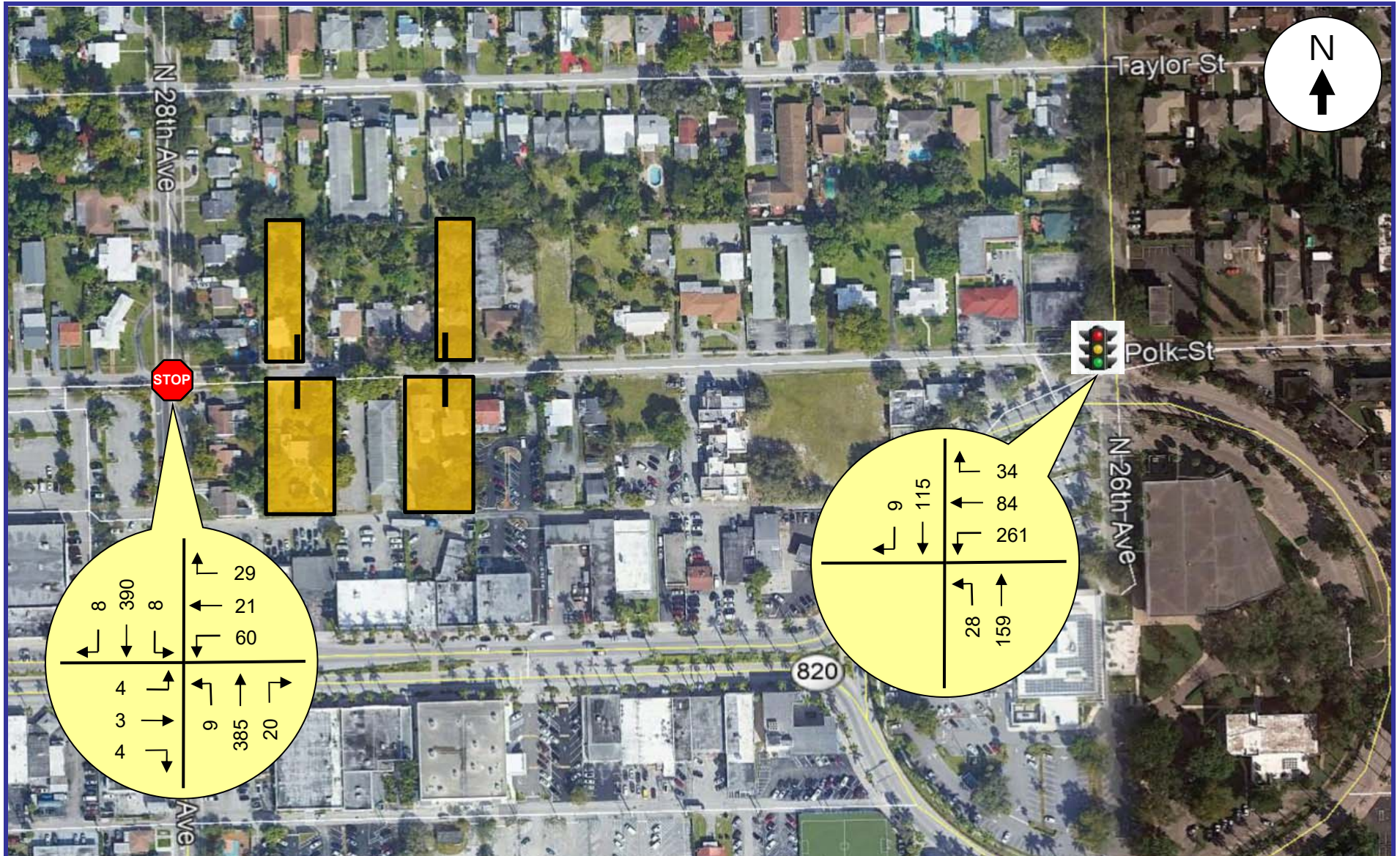
- **Average Peak Season Conversion Factor:** As referenced previously in this report, traffic data collected on Tuesday, July 13, 2021, was reviewed with respect to average peak season conditions. Based on FDOT's Peak Season Factor Category report (see Appendix C), the adjustment factor for data collected during this time period is 1.06.
- **Historic Traffic Growth:** Research relative to the background traffic growth in the area was conducted. Historic traffic count data (i.e. the past 5 years between 2014 and 2019) was obtained from the FDOT and is presented in Appendix E of this report. Traffic growth on Hollywood Boulevard east of I-95 was considered for this analysis. The referenced data indicates that the subject corridor has exhibited moderate growth of traffic volumes between 2014 and 2019. For the purposes of this analysis, a 3.0% annual growth rate has been applied.

The future traffic calculations (peak season adjustments, background traffic growth, and the traffic associated with the Polk Street Apartments project) for the study intersections are contained in Appendix F in tabular format. Figures 9 and 10 include future background traffic only (without the additional Polk Street Apartments traffic) and Figures 11 and 12 include the additional traffic anticipated to be generated by the proposed development.









Level of Service (LOS) Analyses – Intersections

Intersection capacity / level of service (LOS) analyses were conducted for both of the study intersections. These analyses were undertaken following the capacity / level of service procedures outlined in the 2010 Highway Capacity Manual (HCM) using the latest Synchro software. The results of these capacity analyses are summarized in Table 2 below.

Table 2 Intersection Levels of Service Polk Street Apartments - Hollywood, Florida						
Intersection / Movement	Existing (2021) Conditions		Future (2023) Conditions Without Project Traffic		Future (2023) Conditions With Project Traffic	
	AM Peak Hour	PM Peak Hour	AM Peak Hour	PM Peak Hour	AM Peak Hour	PM Peak Hour
Signalized Intersection						
Polk Street & N. 26th Avenue	B (14.0)	B (13.2)	B (14.3)	B (13.4)	B (14.4)	B (13.6)
- Northbound Approach	C (23.5)	C (24.9)	C (23.4)	C (25.0)	C (23.5)	C (25.3)
- Southbound Approach	C (27.6)	C (22.2)	C (28.0)	C (21.9)	C (28.1)	C (21.5)
- Westbound Approach	A (3.8)	A (4.8)	A (4.0)	A (5.0)	A (4.0)	A (5.3)
Unsignalized Intersection						
Polk Street & N. 28th Avenue						
- Northbound Left-Turn	A (8.2)	A (8.1)	A (8.3)	A (8.2)	A (8.3)	A (8.2)
- Southbound Left-Turn	A (7.9)	A (8.1)	A (7.9)	A (8.2)	A (7.9)	A (8.2)
- Eastbound Approach	C (16.0)	C (15.7)	C (17.1)	C (16.6)	C (17.3)	C (16.9)
- Westbound Approach	C (15.6)	C (19.9)	C (16.5)	C (21.9)	C (17.2)	C (23.3)

Source: Highway Capacity Manual and SYNCHRO.

Legend: D (37.7) = LOS (Average Delay - Seconds / Vehicle)

Signalized Intersection

- Polk Street and N. 26th Avenue** – This intersection currently operates at LOS “B” in the AM and PM peak hours. In the buildout year of 2023 both with and without the project traffic, these LOS conditions remain unchanged. In other words, the LOS does not degrade as a result of this project. Furthermore, it is noted that all approaches to this intersection operate at an acceptable LOS.

Unsignalized Intersection

- **Polk Street and N. 28th Avenue** – The northbound and southbound left-turn movements currently operate at LOS “A” in the AM and PM peak hours. And the stop-controlled approaches (eastbound and westbound) operate at LOS “C” in the AM and PM peak hours. In the buildout year of 2023 both with and without the project traffic, these LOS conditions remain unchanged.

The signal timing data for the Polk Street and N. 26th Avenue intersection has been obtained from Broward County Traffic Engineering and is presented in Appendix G. And the Synchro analysis printouts of the study intersections are contained in Appendix H.

SUPPLEMENTAL ANALYSES

Based upon conversations with City staff, several additional traffic operating parameters within the Polk Street corridor between N. 26th Avenue and N. 28th Avenue were observed and documented. These parameters are presented below.

Speed Data

The City of Hollywood has conducted a review and analysis of the traffic speeds and volumes within the Polk Street corridor. This has been done based upon observed speeding and cut-through traffic. A summary of the results of this analysis is presented in Appendix I. The posted speed limit of this corridor is 30 miles per hour (mph). According to the data collected by the City, the 85th percentile speed in this corridor is 38 mph and the daily volume is 1,116 vehicles with approximately 77% of this traffic traveling in a westbound direction. The results of this evaluation suggest that a flattop speed table should be installed 440 feet west of the brick crosswalk at N. 26th Avenue and a speed table should also be installed 440 feet to the east of the centerline of N. 28th Avenue.

Similar data was collected by the project team between Sunday, July 11, 2021, and Tuesday, July 13, 2021. The weekday volumes are generally consistent with that reported by the City. And the 85th percentile speed of approximately 39 mph is similar to that reported by the City. (This data is presented in Appendix J.) Based upon the similarity of the data, it is apparent that the proposed traffic calming measures are reasonable. As such, the Polk Street Apartments project team will coordinate with the City to ensure that the proposed traffic calming measures are compatible with the project and the proposed crosswalks.

Westbound Vehicle Queuing at N. 28th Avenue

Given that the Polk Street corridor is a known cut-through corridor, City staff requested that a queuing evaluation of the westbound approach at N. 28th Avenue be conducted during the peak hours. This data is presented in Appendix K. This analysis reveals a maximum observed queue in the AM peak period of two (2) vehicles. In the PM peak period, the maximum observed queue was four (4) vehicles which occurred between 5:00 and 5:30.

Based upon a vehicle length of 22 feet, the maximum queue would be equivalent to 88 feet. Given that these observations were conducted in the month of July, it is likely that the vehicle queues will be longer during the peak season. With a 50% increase to account for peak season conditions, a queue of six (6) vehicles would yield a queue length of approximately 132 feet. The proposed driveway and pedestrian crosswalk for the western site are located approximately 165 feet east of N. 28th Avenue. As such, it appears that these elements will be rarely blocked by westbound vehicles queued at the intersection of Polk Street and N. 28th Avenue.

Pedestrian Access & Safety

As noted on the previously referenced site plans presented in Appendix A, a portion of the required parking supply for both sites will be located on the opposite side (i.e. north side) of Polk Street. This condition will necessitate pedestrian crossings of Polk Street for some residents and their guests. In order to accommodate this activity raised pedestrian crosswalks with appropriate signing and marking are proposed for each site. For the eastern site (2718 / 2720 / 2723 Polk Street) the proposed crosswalk will be located approximately 365 feet east of the Polk Street and N. 28th Avenue intersection. And for the western site (2741 / 2742 Polk Street) the proposed crosswalk will be located approximately 160 feet east of the Polk Street and N. 28th Avenue intersection. The resulting spacing between crosswalks will be approximately 200 feet. In addition, it is noted that the placement of each of these crosswalks has considered existing driveways serving the area's residential properties and the entrances to the proposed buildings.

It is envisioned that these crosswalks will be dual purpose traffic calming elements. By implementing a raised speed table design, these crosswalks will slow the background traffic in the Polk Street corridor (between N. 26th and N. 28th Avenues). And, with the accompanying signs requiring vehicles to stop for pedestrians in the crosswalk, they will provide an enhanced safety area for pedestrians to cross Polk Street. Additionally, these elements are expected to be complementary to the planned traffic calming measures currently under consideration by the City within this corridor. Final design and implementation will require coordination with the City of Hollywood and the Broward County Traffic Engineering Division.

SUMMARY & CONCLUSIONS

A residential development to be known as Polk Street Apartments is proposed on several parcels of land along Polk Street between N. 26th Avenue and N. 28th Avenue in Hollywood, Broward County, Florida. Multifamily residential developments are proposed on an eastern site and a western site. Both sites will be developed with 44 dwelling units in a four-story building (mid-rise format) to be located on the south side of Polk Street. There will be 33 parking spaces on the ground floor and an additional 16 parking spaces in a surface parking lot on the north side of Polk Street. Vehicular access to these sites will be provided by full access driveways on Polk Street. The parcels on the south side of Polk Street will also have emergency vehicle access only to the alleyway to the south.

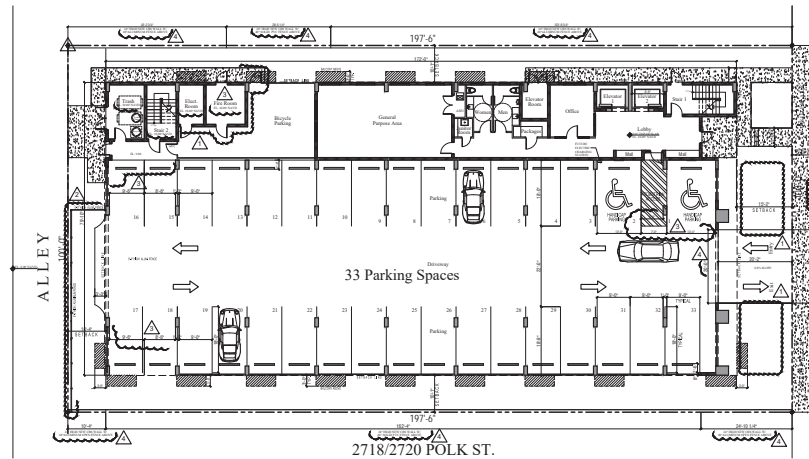
The Polk Street Apartments development is anticipated to generate approximately 476 daily vehicle trips, 30 AM peak hour vehicle trips (8 inbound and 22 outbound) and approximately 40 vehicle trips (24 inbound and 16 outbound) during the typical afternoon peak hour. When considering the existing single and multifamily residences on the subject sites this represents an increase of 435 daily vehicle trips, an increase of 28 AM peak hour vehicle trips, and an increase of 36 PM peak hour vehicle trips.

The intersection capacity analyses of the nearby study intersections indicate that both intersections will continue to operate at an acceptable Level of Service (LOS) and the project traffic will have a minimal (i.e. “de minimis”) impact that will not significantly degrade the overall LOS of the study intersections.

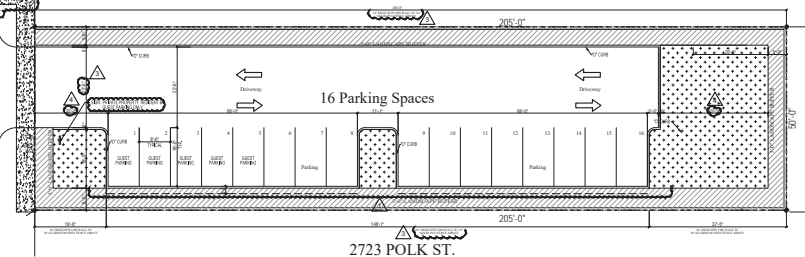
APPENDIX A

Polk Street Apartments

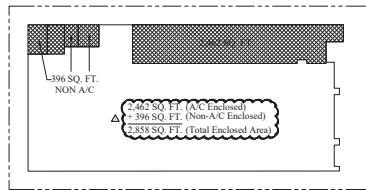
Site Plans



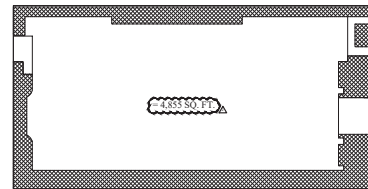
- NOTES
1. ALL CHANGES TO THE DESIGN WILL REQUIRE PLANNING REVIEW AND MAYBE SUBJECT TO BOARD APPROVAL.
 2. PAVEMENT MARKING SHALL COMPLY WITH THE MANUAL UNIFORM TRAFFIC CONTROL DEVICES.
 3. ALL SIGNAGE SHALL BE IN COMPLIANCE WITH THE ZONING AND LANDSCAPE DEVELOPMENT REGULATION.
 4. THIRD PARTY FLORIDA GREEN BUILDING COALITION CERTIFICATION TO BE PROVIDED.



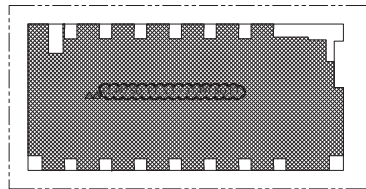
SITE PLAN
SCALE 1/16"=1'-0"



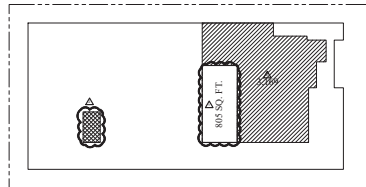
FIRST FLOOR ENCLOSED AREA DIAGRAM
SCALE 1/32"=1'-0"



PERVIOUS DIAGRAM
SCALE 1/32"=1'-0"



2ND THRU 4TH FLOOR ENCLOSED AREA DIAGRAM
SCALE 1/32"=1'-0"



ROOF POOL DECK COVERED ENCLOSED AREA DIAGRAM
SCALE 1/32"=1'-0"

CHALK STREET LANDSCAPE CALCULATION

VUA - SITE - BUFFER AREA x 15%	= 1,170 SQ. FT.
PERVIOUS AREA	= 4100 SQ. FT.
LANDSCAPE REQUIRED (40%)	= 1,665 SQ. FT.
BUFFER AREA	= 2,534 SQ. FT.
LANDSCAPE PROVIDED (40.2%)	= 4,119 SQ. FT.
IMPERVIOUS AREA	= 3742 SQ. FT.
DRIVEWAY AREA	= 2,389 SQ. FT.
HARDSCAPE PROVIDED (59.8%)	= 6,131 SQ. FT.

LEGAL DESCRIPTION

2718-2720 POLK ST.
LOT 13 LESS THE SOUTH 7-FEET THERE OF, BLOCK 21, HOLLYWOOD LITTLE RANCHES, ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 1, PAGE(S) 26 OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.
2723 POLK ST.

ZONING INFORMATION

ZONE	REMARKS	REQUIRED/ALLOWED	PROVIDED
ZONE	TO-1 (SOUTH OF POLK STREET)		
LAND USE	MC-1 (NORTH OF POLK STREET)		
LOT SIZE	BLDG. LOT - 197.5 x 100' - 19,750 SQ. FT. - 0.451 ACRES		
LOT AREA	PARKING LOT - 205' x 50' - 10,250 SQ. FT. - 0.235 ACRES		
TOTAL AREA OF LOTS	30,000 SQ. FT. - 0.689 ACRES		
F.A.R.	30,000 x 1.5 = 45,000 SQ. FT.	45,000 SQ. FT.	43,518 SQ. FT.
FRONT SETBACK		15 FT. MIN.	15'-2" & 30'-0"
SIDE SETBACK		10 FT. MIN.	10'-1"
REAR SETBACK		10 FT. MIN.	10'-4"
PERVIOUS AREA	2742/2741 POLK ST.		1,855 SQ. FT. - 0.11 ACRES
IMPERVIOUS AREA	2742/2741 POLK ST.		1,855 SQ. FT. - 0.11 ACRES
PARKING	47 REGULAR PARKING SPACES + 2 HANDICAP PARKING	49 SPACES	49 SPACES
PARKING SPACE SIZES		GARAGE PARKING - 10' x 15' x 15'	33 SPACES
		OPEN PARKING - 10' x 15' x 15'	16 SPACES
LIGHTING LEVEL	MAX. 0.5 FOOT-CANDLE AT ALL PROPERTY LINES		
NO. OF UNITS			44
UNIT SIZE	AIRCONDITIONED SPACE	650 SQ. FT. MIN. AVERAGE	650 SQ. FT. Inverse Unit
BALCONY SIZE	END UNITS - 105 SQ. FT. INTERIOR UNITS - 81 SQ. FT.		650 SQ. FT. End Unit
UNIT TYPE	1 BEDROOM, 1 1/2 BATH		44
AREA TABULATION			
FIRST FLOOR	GARAGE NON-A/C - 11,269 SQ. FT. A/C AREA - 2,462 SQ. FT.		13,731 SQ. FT.
TYPICAL FLOOR (2ND THRU 4TH)	A/C AREA - 12,356 SQ. FT. NON A/C AREA - 1,722 SQ. FT.		14,078 SQ. FT.
RECREATION LEVEL	A/C AREA - 3,990 SQ. FT. ROOF AREA - 13,677 SQ. FT.	30% OF ROOF - 4,103 SQ. FT.	3,964 SQ. FT. (29.0%)
TOTAL A/C AREA			42,555 SQ. FT.
TOTAL BUILDING LENGTH			172'-0"
STRUCTURE HEIGHT		50 Ft.	40'-0"
MAX. HEIGHT OF VERTICAL ENCROACHMENT			17'-0" OR TOTAL OF 57'-0"

PARKING CALCULATION:

44 UNITS x 1 U	= 44 SPACES
GUEST 1 SPACE/10U - 4.4 SPACES	= 48.4 SPACES
	= 49 SPACES REQUIRED

F.A.R. / ENCLOSE AREA CALCULATION

FIRST FLOOR	2,888 SQ. FT.
2ND THRU 4TH FLOOR	37,491 SQ. FT.
ROOF POOL DECK	1,169 SQ. FT.
TOTAL ENCLOSED AREA	41,518 SQ. FT.

The Benedict Bullock Group, PA

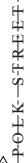
POLK STREET APARTMENTS I
2718-2720-2723 POLK STREET HOLLYWOOD, FLORIDA




ARCHITECT'S SEAL

BRIAN BULLOCK, ARCHITECT
AIR 98754

SCALE 1/16"=1'-0"
DATE 02-03-2021
DRAWN BY RST
PROJECT NO B82030

SITE PLAN
SP-1



- |  CITY OF SAN DIEGO
DEPARTMENT OF PUBLIC WORKS | |
|---|-----------------|
| VUA - SITE - BUFFER AREA x 15% | |
| VUA - 10,250 - 2,450 x 15% | = 1,170 SQ. FT. |
| PERVIOUS AREA | |
| LANDSCAPE REQUIRED (40%) | = 4100 SQ.FT. |
| LANDSCAPE AREA  | = 1,665 SQ.FT. |
| BUFFER AREA  | = 2,534 SQ.FT. |
| LANDSCAPE PROVIDED (40.2%) | = 4119 SQ.FT. |
| IMPERVIOUS AREA | |
| DRIVEWAY AREA | = 3742 SQ.FT. |
| PARKING AREA | = 2,389 SQ.FT. |
| HARDSCAPE PROVIDED (59.8%) | = 6,131 SQ.FT. |

<u>LEGAL DESCRIPTION</u>
<p>2741 POLK ST.</p> <p>THE WEST OF 1/2 LOT 12, IN BLOCK 32 OF HOLLYWOOD LITTLE RANCHES ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 1, PAGE(S) 26 OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.</p>
<p>2742 POLK ST.</p> <p>LOT 11 LESS THE SOUTH 7.5 FEET THERE OF, BLOCK 31, AN AMENDED PLAT OF THE HOLLYWOOD LITTLE RANCHES ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 1, PAGE(S) 26 OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.</p>



PERVIOUS DIAGRAM
SCALE 1/32"=1'-0"



<u>F.A.R. / ENCLOSE AREA CALCULATION</u>	
FIRST FLOOR	2,858 SQ. FT.
2ND THRU 4TH FLOOR	37,491 SQ. FT.
ROOF POOL DECK	3,169 SQ. FT.
TOTAL ENCLOSED AREA	43,518 SQ. FT.

PARKING CALCULATION:

44 UNITS x 1/U	- 44 SPACES	4
GUEST 1 SPACE/10U	- 4.4 SPACES	
	- 48.4 SPACES	
	- 49 SPACES REQUIRED	

ZONING INFORMATION			REQUIRED	ALLOWED	PROVIDED
ZONE	TC-1 (SOUTH OF POLK STREET)				
ZONE	MC-1 (NORTH OF POLK STREET)				
LAND USE	REGIONAL ACTIVITY CENTER, ETC.				
LOT SIZE	BLDG. LOT - 107.5 x 100; PARKING LOT 205' x 50'				
LOT AREA	BLDG. LOT - 107.5 x 100 = 10,750 SQ.FT. = 0.457 ACRES				
LOT AREA	PARKING LOT - 205' x 50' = 10,250 SQ.FT. = 0.235 ACRES				
TOTAL AREA OF LOTS	30,000 SQ.FT. = 0.689 ACRES				
F.A.R.	30,000 x 1.5 = 45,000 SQ.FT.		45,000 SQ.FT.		43,518 SQ.FT.
FRONT SETBACK			15 Ft. MIN.		15'-2" & 20'-0"
SIDE SETBACK			10 Ft. MIN.		10'-1"
REAR SETBACK			10 Ft. MIN.		10'-4"
PERVIOUS AREA	2742-2741 POLK ST.				14,855 SQ.FT. = 0.34 ACRES
IMPERVIOUS AREA	2742-2741 POLK ST.				14,095 SQ.FT. = 0.34 ACRES
PARKING	7 REGULAR PARKING SPACES + 2 HANDICAP PARKING		49 SPACES		49 SPACES
PARKING SPACE SIZES			GARAGE PARKING SPACES - 18' x 12' OPEN PARKING - 18'-6" x 18'-4"		33 SPACES 16 SPACES
LIGHTING LEVEL	MAX. 0.5 FOOT-CANDLE AT ALL PROPERTY LINES				
NO. OF UNITS					44
UNIT SIZE	AIR-CONDITIONED SPACE		650 SQ. FT. MIN. AVERAGE		650 SQ. FT. Minus Unit 852 SQ. FT. End Unit
BALCONY SIZE	END UNITS = 105 SQ. FT. INTERIOR UNITS = 81 SQ. FT.				
UNIT TYPE	1 BEDROOM, 1 1/2 BATH				44
AREA TABULATION					
FIRST FLOOR	GARAGE NON-A/C = 11,269 SQ.FT. A/C AREA = 2,462 SQ.FT.				13,731 SQ. FT.
FLOOR (2ND THRU 4TH)	A/C AREA = 12,356 SQ.FT. NON A/C AREA = 1,722 SQ.FT.				14,078 SQ. FT.
RECREATION LEVEL	A/C AREA = 2,990 SQ.FT. ROOF AREA = 13,677 SQ.FT.		30% OF ROOF = 4,083 SQ.FT.		3,964 SQ. FT. (29.0%)
TOTAL A/C AREA					42,555 SQ.FT.
TOTAL BUILDING LENGTH					172'-0"
STRUCTURE HEIGHT			50 Ft.		40'-0"
MAX. HEIGHT OF VERTICAL ENCROACHMENT					17'-0" OR TOTAL OF 57'-0"

REV	REVISIONS	BY	DATE
A	QTY PLAN CHECK COMMENTS	WIT	03-04-2027
A	QTY PLAN CHECK COMMENTS	WIT	03-04-2027
A	QTY PLAN CHECK COMMENTS	WIT	03-04-2027
A	QTY PLAN CHECK COMMENTS	WIT	07-02-2027

The
**Benedict
Bullock**
Group, PA

POLK STREET APARTMENTS II
2742-2741- POLK STREET HOLLYWOOD, FLORIDA

ARCHITECT'S SEAL

BRIAN BULLOCK, ARCHITECT
AR 95754

SITE PLAN

SP-1

ALL IDEAS, DESIGNS, ARRANGEMENTS, PLANS INDICATED OR REPRESENTED BY THIS DRAWING ARE OWNED BY, & THE PROPERTY OF THE ARCHITECT & WERE CREATED, EVOLVED, & DEVELOPED FOR USE ON, & IN CONNECTION WITH THE SPECIFIED PROJECT. NONE OF SUCH IDEAS, DESIGNS, ARRANGEMENTS, OR PLANS SHALL BE USED BY OR DISCLOSED TO ANY PERSON, FIRM, OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT THE WRITTEN PERMISSION OF BRIAN RA BALLOCK ARCHITECT. WRITTEN PERMISSION ON THESE DRAWINGS SHALL HAVE PRECEDENCE OVER SCALE DIMENSIONS. CONTRACTORS SHALL VERIFY & BE RESPONSIBLE FOR ALL DIMENSIONS & CONDITIONS ON THE JOB. THE ARCHITECT MUST BE NOTIFIED OF ANY DISCREPANCIES IMMEDIATELY UPON DISCOVERY.

APPENDIX B

Intersection Turning Movement Counts

Traffic Survey Specialists, Inc.

85 SE 4th Avenue, Unit 109, Delray Beach, Florida 33483
Phone (561) 272-3255

POLK STREET & N 26TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: JOHN FLOOD
SIGNALIZED

File Name : POLK26AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 1

Groups Printed- ALL VEHICLES

	N 26TH AVENUE From North				POLK STREET From East				N 26TH AVENUE From South				POLK STREET From West				
Start Time	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	Int. Total
07:00 AM	0	0	23	0	0	23	14	1	0	0	5	0	0	0	0	0	66
07:15 AM	0	0	19	3	0	34	3	0	0	0	6	0	0	0	0	0	65
07:30 AM	0	0	19	0	0	42	3	1	0	3	7	0	0	0	0	0	75
07:45 AM	0	0	25	0	0	52	7	4	0	2	15	0	0	0	0	0	105
Total	0	0	86	3	0	151	27	6	0	5	33	0	0	0	0	0	311
08:00 AM	0	0	43	0	0	54	12	7	0	0	14	0	0	0	0	0	130
08:15 AM	0	0	28	1	0	53	10	9	0	0	18	0	0	0	0	0	119
08:30 AM	0	0	34	1	0	46	8	5	0	2	20	0	0	0	0	0	116
08:45 AM	0	0	35	0	0	43	6	4	0	3	19	0	0	0	0	0	110
Total	0	0	140	2	0	196	36	25	0	5	71	0	0	0	0	0	475
04:00 PM	0	0	27	4	0	67	19	8	0	3	16	0	0	0	0	0	144
04:15 PM	0	0	33	0	0	59	15	6	0	1	29	0	0	0	0	0	143
04:30 PM	0	0	23	4	0	61	16	7	0	5	40	0	0	0	0	0	156
04:45 PM	0	0	29	0	0	51	12	3	0	1	29	0	0	0	0	0	125
Total	0	0	112	8	0	238	62	24	0	10	114	0	0	0	0	0	568
05:00 PM	0	0	28	1	0	65	22	8	0	6	34	0	0	0	0	0	164
05:15 PM	0	0	22	1	0	55	24	12	0	6	38	0	0	0	0	0	158
05:30 PM	0	0	28	1	0	46	21	8	0	4	48	0	0	0	0	0	156
05:45 PM	0	0	22	2	0	33	12	5	0	3	34	0	0	0	0	0	111
Total	0	0	100	5	0	199	79	33	0	19	154	0	0	0	0	0	589
Grand Total	0	0	438	18	0	784	204	88	0	39	372	0	0	0	0	0	1943
Apprch %	0	0	96.1	3.9	0	72.9	19	8.2	0	9.5	90.5	0	0	0	0	0	
Total %	0	0	22.5	0.9	0	40.3	10.5	4.5	0	2	19.1	0	0	0	0	0	

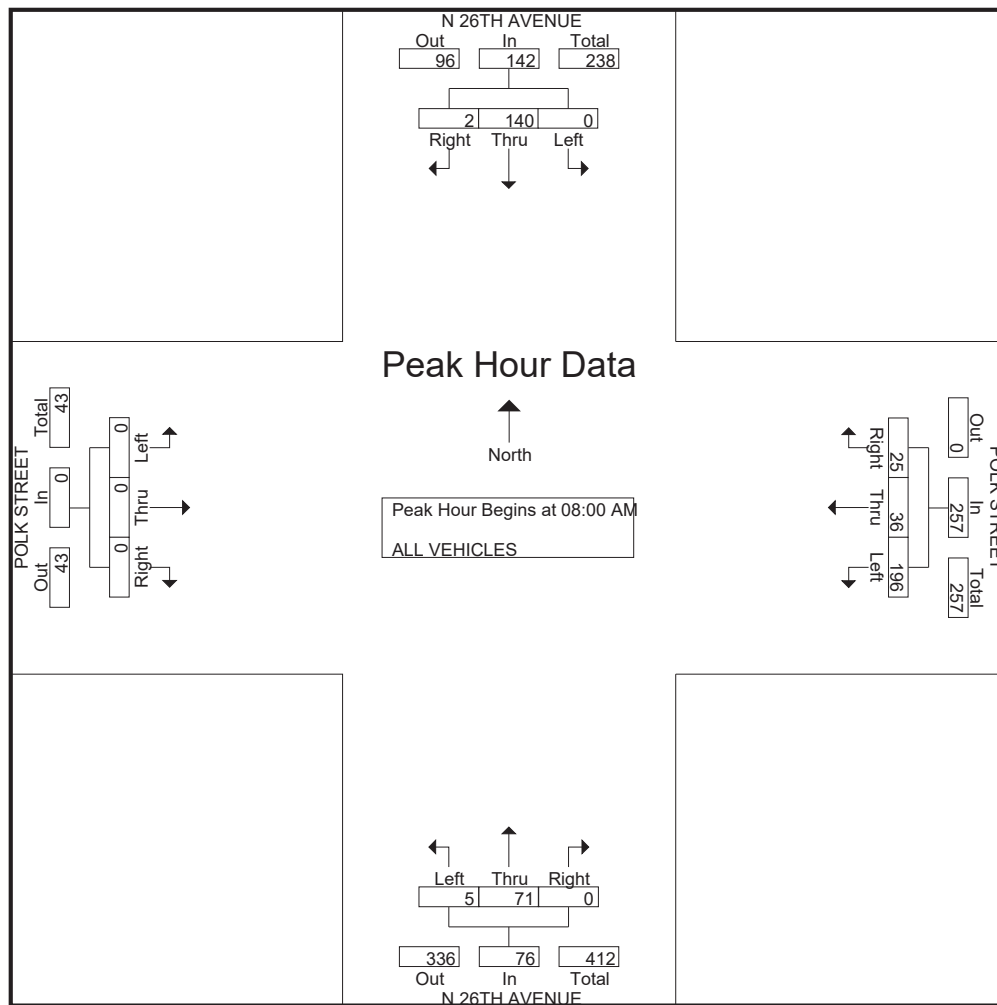
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Phone (561) 272-3255

POLK STREET & N 26TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: JOHN FLOOD
SIGNALIZED

File Name : POLK26AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 2

	N 26TH AVENUE From North					POLK STREET From East					N 26TH AVENUE From South					POLK STREET From West					
Start Time	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	Int. Total
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:00 AM																					
08:00 AM	0	0	43	0	43	0	54	12	7	73	0	0	14	0	14	0	0	0	0	0	130
08:15 AM	0	0	28	1	29	0	53	10	9	72	0	0	18	0	18	0	0	0	0	0	119
08:30 AM	0	0	34	1	35	0	46	8	5	59	0	2	20	0	22	0	0	0	0	0	116
08:45 AM	0	0	35	0	35	0	43	6	4	53	0	3	19	0	22	0	0	0	0	0	110
Total Volume	0	0	140	2	142	0	196	36	25	257	0	5	71	0	76	0	0	0	0	0	475
% App. Total	0	0	98.6	1.4		0	76.3	14	9.7		0	6.6	93.4	0		0	0	0	0		
PHF	.000	.000	.814	.500	.826	.000	.907	.750	.694	.880	.000	.417	.888	.000	.864	.000	.000	.000	.000	.000	.913



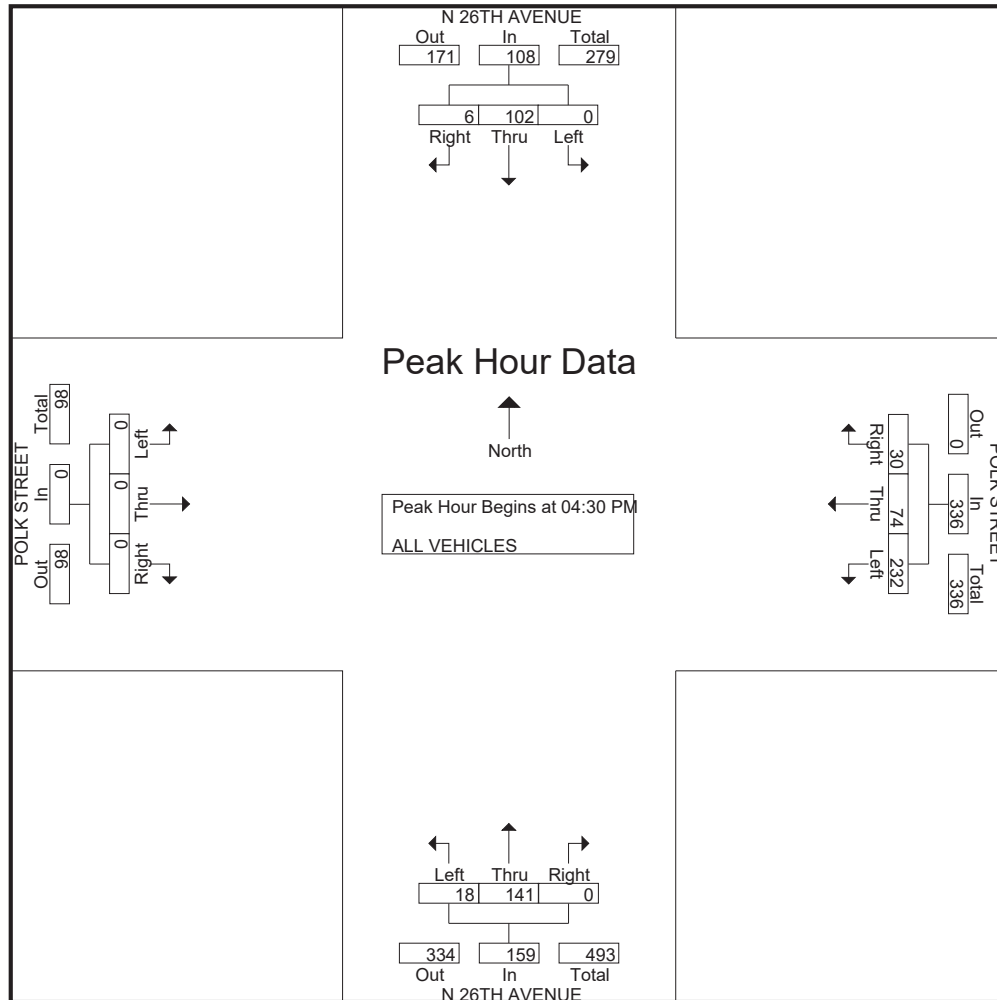
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POLK STREET & N 26TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: JOHN FLOOD
SIGNALIZED

File Name : POLK26AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 3

	N 26TH AVENUE From North					POLK STREET From East					N 26TH AVENUE From South					POLK STREET From West					
Start Time	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	Int. Total
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:30 PM																					
04:30 PM	0	0	23	4	27	0	61	16	7	84	0	5	40	0	45	0	0	0	0	0	156
04:45 PM	0	0	29	0	29	0	51	12	3	66	0	1	29	0	30	0	0	0	0	0	125
05:00 PM	0	0	28	1	29	0	65	22	8	95	0	6	34	0	40	0	0	0	0	0	164
05:15 PM	0	0	22	1	23	0	55	24	12	91	0	6	38	0	44	0	0	0	0	0	158
Total Volume	0	0	102	6	108	0	232	74	30	336	0	18	141	0	159	0	0	0	0	0	603
% App. Total	0	0	94.4	5.6		0	69	22	8.9		0	11.3	88.7	0		0	0	0	0		
PHF	.000	.000	.879	.375	.931	.000	.892	.771	.625	.884	.000	.750	.881	.000	.883	.000	.000	.000	.000	.000	.919



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POLK STREET & N 26TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: JOHN FLOOD
SIGNALIZED

File Name : POLK26AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 1

Groups Printed- PEDESTRIANS & BIKES

	N 26TH AVENUE From North				POLK STREET From East				N 26TH AVENUE From South				POLK STREET From West				Int. Total
Start Time	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	
07:00 AM	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	2
07:45 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	1	0	1	0	0	0	1	0	0	0	0	0	0	0	3
08:15 AM	3	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	4
08:30 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2
Total	3	0	2	0	0	0	0	0	0	0	0	0	1	0	0	0	6
04:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
04:15 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
04:45 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
Total	2	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	3
05:00 PM	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
05:30 PM	2	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	5
05:45 PM	1	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	2
Total	6	0	0	0	3	0	0	0	1	0	0	0	0	0	0	0	10
Grand Total	11	0	3	0	5	0	0	0	2	0	0	0	1	0	0	0	22
Apprch %	78.6	0	21.4	0	100	0	0	0	100	0	0	0	100	0	0	0	
Total %	50	0	13.6	0	22.7	0	0	0	9.1	0	0	0	4.5	0	0	0	

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POLK STREET & N 28TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: MARISA CRUZ
NOT SIGNALIZED

File Name : POLK28AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 1

Groups Printed- ALL VEHICLES

	N 28TH AVENUE From North				POLK STREET From East				N 28TH AVENUE From South				POLK STREET From West				
Start Time	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	Int. Total
07:00 AM	0	1	83	0	0	1	1	1	0	0	39	1	0	1	1	0	129
07:15 AM	0	0	112	0	0	4	1	1	0	1	47	0	0	3	0	1	170
07:30 AM	0	0	105	3	0	3	1	5	0	0	44	2	0	0	0	2	165
07:45 AM	0	0	109	1	0	4	4	3	0	2	61	0	0	1	0	1	186
Total	0	1	409	4	0	12	7	10	0	3	191	3	0	5	1	4	650
08:00 AM	0	0	100	0	0	8	1	3	0	2	58	2	0	1	0	1	176
08:15 AM	0	3	95	3	0	5	1	5	0	0	65	3	0	3	0	0	183
08:30 AM	0	3	107	0	0	10	2	1	0	0	61	2	0	2	0	3	191
08:45 AM	0	1	93	2	0	4	2	4	0	1	70	4	0	8	0	0	189
Total	0	7	395	5	0	27	6	13	0	3	254	11	0	14	0	4	739
04:00 PM	0	0	68	0	0	17	4	5	0	2	80	2	0	1	0	1	180
04:15 PM	0	0	90	2	0	11	1	6	0	4	75	3	0	0	0	0	192
04:30 PM	0	2	71	0	0	14	3	7	0	5	80	0	0	2	0	2	186
04:45 PM	0	1	83	0	0	9	2	2	0	0	85	1	0	2	0	0	185
Total	0	3	312	2	0	51	10	20	0	11	320	6	0	5	0	3	743
05:00 PM	0	2	88	1	0	12	8	9	0	3	87	4	0	1	2	0	217
05:15 PM	0	2	91	3	0	18	4	4	0	4	76	2	0	0	0	1	205
05:30 PM	0	0	85	3	0	8	5	8	0	1	94	1	0	1	1	3	210
05:45 PM	0	0	66	2	0	9	0	8	0	3	85	2	0	1	0	0	176
Total	0	4	330	9	0	47	17	29	0	11	342	9	0	3	3	4	808
Grand Total	0	15	1446	20	0	137	40	72	0	28	1107	29	0	27	4	15	2940
Apprch %	0	1	97.6	1.4	0	55	16.1	28.9	0	2.4	95.1	2.5	0	58.7	8.7	32.6	
Total %	0	0.5	49.2	0.7	0	4.7	1.4	2.4	0	1	37.7	1	0	0.9	0.1	0.5	

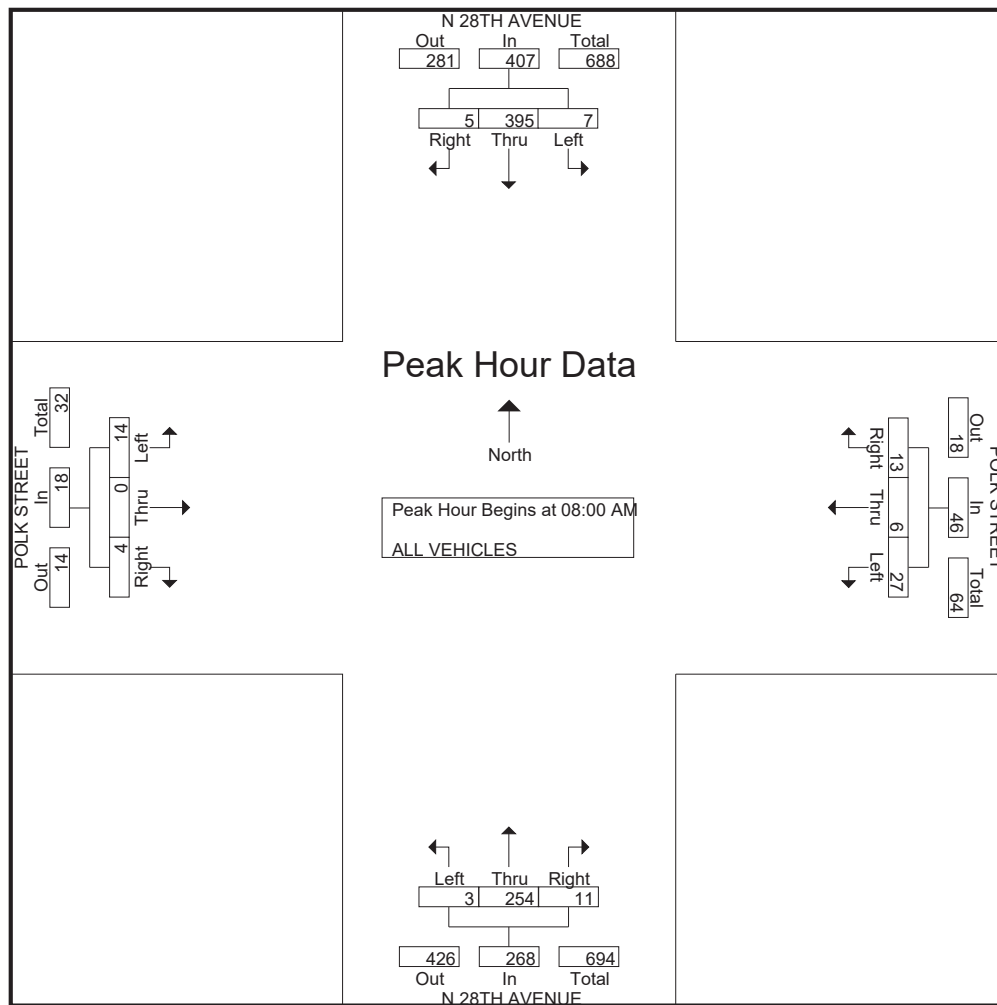
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POLK STREET & N 28TH AVENUE
HOLLYWOOD, FLORIDA
COUNTED BY: MARISA CRUZ
NOT SIGNALIZED

File Name : POLK28AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 2

	N 28TH AVENUE From North					POLK STREET From East					N 28TH AVENUE From South					POLK STREET From West					
Start Time	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	Int. Total
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:00 AM																					
08:00 AM	0	0	100	0	100	0	8	1	3	12	0	2	58	2	62	0	1	0	1	2	176
08:15 AM	0	3	95	3	101	0	5	1	5	11	0	0	65	3	68	0	3	0	0	3	183
08:30 AM	0	3	107	0	110	0	10	2	1	13	0	0	61	2	63	0	2	0	3	5	191
08:45 AM	0	1	93	2	96	0	4	2	4	10	0	1	70	4	75	0	8	0	0	8	189
Total Volume	0	7	395	5	407	0	27	6	13	46	0	3	254	11	268	0	14	0	4	18	739
% App. Total	0	1.7	97.1	1.2		0	58.7	13	28.3		0	1.1	94.8	4.1		0	77.8	0	22.2		
PHF	.000	.583	.923	.417	.925	.000	.675	.750	.650	.885	.000	.375	.907	.688	.893	.000	.438	.000	.333	.563	.967



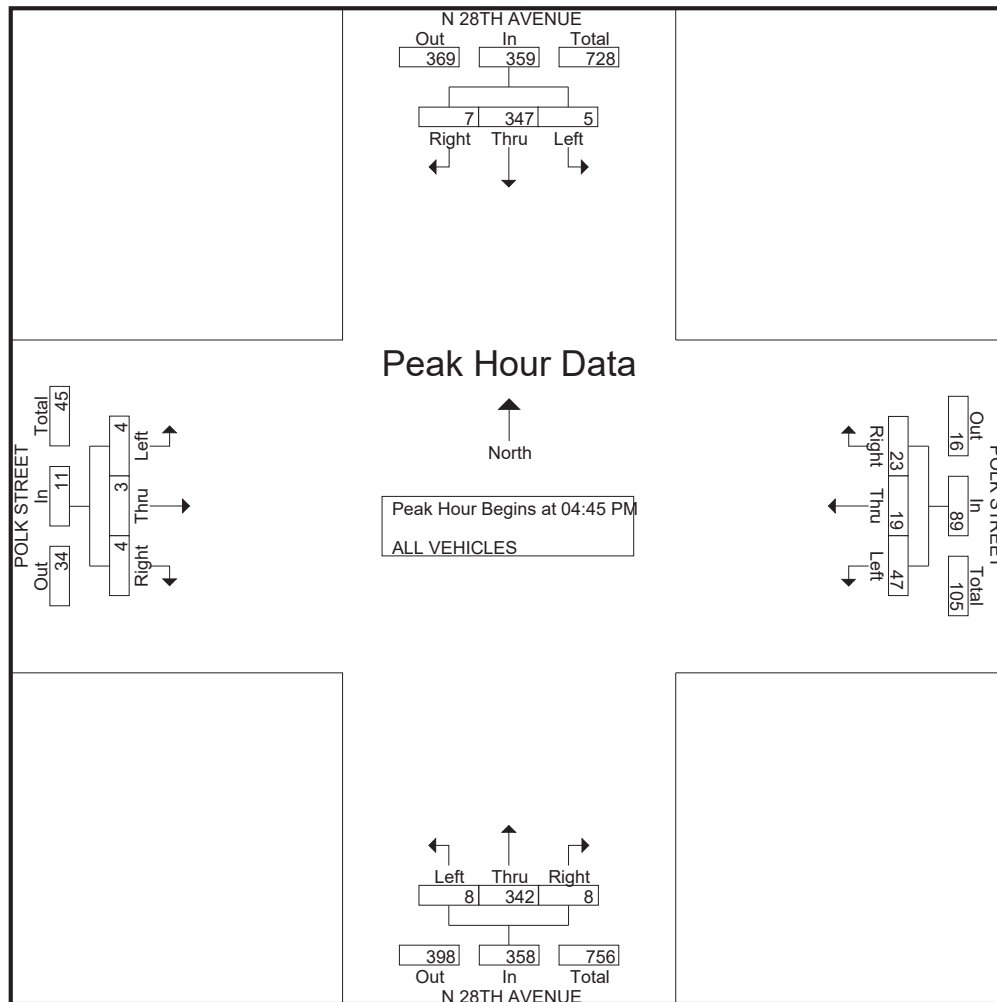
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HOLLYWOOD, FLORIDA
COUNTED BY: MARISA CRUZ
NOT SIGNALIZED

File Name : POLK28AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 3

	N 28TH AVENUE From North					POLK STREET From East					N 28TH AVENUE From South					POLK STREET From West					
Start Time	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	Int. Total
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:45 PM																					
04:45 PM	0	1	83	0	84	0	9	2	2	13	0	0	85	1	86	0	2	0	0	2	185
05:00 PM	0	2	88	1	91	0	12	8	9	29	0	3	87	4	94	0	1	2	0	3	217
05:15 PM	0	2	91	3	96	0	18	4	4	26	0	4	76	2	82	0	0	0	1	1	205
05:30 PM	0	0	85	3	88	0	8	5	8	21	0	1	94	1	96	0	1	1	3	5	210
Total Volume	0	5	347	7	359	0	47	19	23	89	0	8	342	8	358	0	4	3	4	11	817
% App. Total	0	1.4	96.7	1.9		0	52.8	21.3	25.8		0	2.2	95.5	2.2		0	36.4	27.3	36.4		
PHF	.000	.625	.953	.583	.935	.000	.653	.594	.639	.767	.000	.500	.910	.500	.932	.000	.500	.375	.333	.550	.941



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NOT SIGNALIZED

File Name : POLK28AV
Site Code : 00210085
Start Date : 7/13/2021
Page No : 1

Groups Printed- PEDESTRIANS & BIKES

	N 28TH AVENUE From North				POLK STREET From East				N 28TH AVENUE From South				POLK STREET From West				
Start Time	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Int. Total
07:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	2	0	0	0	3
07:15 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
07:30 AM	1	0	1	0	0	0	1	0	0	0	0	0	1	0	0	0	4
07:45 AM	0	0	0	0	1	0	0	0	0	0	0	0	1	0	0	0	2
Total	1	0	2	0	2	0	1	0	0	0	0	0	4	0	0	0	10
08:00 AM	2	0	0	0	1	0	0	0	0	0	1	0	1	0	1	0	6
08:30 AM	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	2
08:45 AM	0	0	0	0	1	0	0	0	0	0	0	0	3	0	0	0	4
Total	2	0	0	0	2	0	0	0	0	0	2	0	5	0	1	0	12
04:15 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
04:30 PM	0	0	2	0	2	0	0	0	1	0	0	0	0	0	1	0	6
04:45 PM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
Total	0	0	2	0	4	0	1	0	1	0	0	0	0	0	1	0	9
05:00 PM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
05:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1
05:30 PM	0	0	0	0	0	0	1	0	0	0	0	0	1	0	0	0	2
05:45 PM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	3	0	4
Total	0	0	0	0	0	0	2	0	0	0	1	0	2	0	3	0	8
Grand Total	3	0	4	0	8	0	4	0	1	0	3	0	11	0	5	0	39
Apprch %	42.9	0	57.1	0	66.7	0	33.3	0	25	0	75	0	68.8	0	31.2	0	
Total %	7.7	0	10.3	0	20.5	0	10.3	0	2.6	0	7.7	0	28.2	0	12.8	0	

APPENDIX C

FDOT

Peak Season Conversion Factor Report

2019 PEAK SEASON FACTOR CATEGORY REPORT - REPORT TYPE: ALL
 CATEGORY: 8601 CEN.-W OF US1 TO SR7

WEEK	DATES	SF	MOCF: 0.97 PSCF
1	01/01/2019 - 01/05/2019	1.00	1.03
2	01/06/2019 - 01/12/2019	1.00	1.03
3	01/13/2019 - 01/19/2019	1.01	1.04
4	01/20/2019 - 01/26/2019	1.00	1.03
5	01/27/2019 - 02/02/2019	0.99	1.02
* 6	02/03/2019 - 02/09/2019	0.98	1.01
* 7	02/10/2019 - 02/16/2019	0.97	1.00
* 8	02/17/2019 - 02/23/2019	0.97	1.00
* 9	02/24/2019 - 03/02/2019	0.97	1.00
*10	03/03/2019 - 03/09/2019	0.96	0.99
*11	03/10/2019 - 03/16/2019	0.96	0.99
*12	03/17/2019 - 03/23/2019	0.97	1.00
*13	03/24/2019 - 03/30/2019	0.97	1.00
*14	03/31/2019 - 04/06/2019	0.97	1.00
*15	04/07/2019 - 04/13/2019	0.98	1.01
*16	04/14/2019 - 04/20/2019	0.98	1.01
*17	04/21/2019 - 04/27/2019	0.99	1.02
*18	04/28/2019 - 05/04/2019	0.99	1.02
19	05/05/2019 - 05/11/2019	1.00	1.03
20	05/12/2019 - 05/18/2019	1.00	1.03
21	05/19/2019 - 05/25/2019	1.01	1.04
22	05/26/2019 - 06/01/2019	1.01	1.04
23	06/02/2019 - 06/08/2019	1.01	1.04
24	06/09/2019 - 06/15/2019	1.02	1.05
25	06/16/2019 - 06/22/2019	1.02	1.05
26	06/23/2019 - 06/29/2019	1.02	1.05
27	06/30/2019 - 07/06/2019	1.03	1.06
28	07/07/2019 - 07/13/2019	1.03	1.06
29	07/14/2019 - 07/20/2019	1.04	1.07
30	07/21/2019 - 07/27/2019	1.03	1.06
31	07/28/2019 - 08/03/2019	1.02	1.05
32	08/04/2019 - 08/10/2019	1.02	1.05
33	08/11/2019 - 08/17/2019	1.01	1.04
34	08/18/2019 - 08/24/2019	1.02	1.05
35	08/25/2019 - 08/31/2019	1.03	1.06
36	09/01/2019 - 09/07/2019	1.03	1.06
37	09/08/2019 - 09/14/2019	1.04	1.07
38	09/15/2019 - 09/21/2019	1.05	1.08
39	09/22/2019 - 09/28/2019	1.04	1.07
40	09/29/2019 - 10/05/2019	1.02	1.05
41	10/06/2019 - 10/12/2019	1.01	1.04
42	10/13/2019 - 10/19/2019	1.00	1.03
43	10/20/2019 - 10/26/2019	1.00	1.03
44	10/27/2019 - 11/02/2019	1.00	1.03
45	11/03/2019 - 11/09/2019	1.00	1.03
46	11/10/2019 - 11/16/2019	1.00	1.03
47	11/17/2019 - 11/23/2019	1.00	1.03
48	11/24/2019 - 11/30/2019	1.00	1.03
49	12/01/2019 - 12/07/2019	1.00	1.03
50	12/08/2019 - 12/14/2019	1.00	1.03
51	12/15/2019 - 12/21/2019	1.00	1.03
52	12/22/2019 - 12/28/2019	1.00	1.03
53	12/29/2019 - 12/31/2019	1.01	1.04

* PEAK SEASON

14-FEB-2020 15:39:26

830UPD

4_8601_PKSEASON.TXT

APPENDIX D

ITE Trip Generation Manual (10th Edition)

Relevant Excerpts

Land Use: 210

Single-Family Detached Housing

Description

Single-family detached housing includes all single-family detached homes on individual lots. A typical site surveyed is a suburban subdivision.

Additional Data

The number of vehicles and residents had a high correlation with average weekday vehicle trip ends. The use of these variables was limited, however, because the number of vehicles and residents was often difficult to obtain or predict. The number of dwelling units was generally used as the independent variable of choice because it was usually readily available, easy to project, and had a high correlation with average weekday vehicle trip ends.

This land use included data from a wide variety of units with different sizes, price ranges, locations, and ages. Consequently, there was a wide variation in trips generated within this category. Other factors, such as geographic location and type of adjacent and nearby development, may also have had an effect on the site trip generation.

Single-family detached units had the highest trip generation rate per dwelling unit of all residential uses because they were the largest units in size and had more residents and more vehicles per unit than other residential land uses; they were generally located farther away from shopping centers, employment areas, and other trip attractors than other residential land uses; and they generally had fewer alternative modes of transportation available because they were typically not as concentrated as other residential land uses.

Time-of-day distribution data for this land use are presented in Appendix A. For the six general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:00 and 5:00 p.m., respectively. For the two sites with Saturday data, the overall highest vehicle volume was counted between 3:00 and 4:00 p.m. For the one site with Sunday data, the overall highest vehicle volume was counted between 10:15 and 11:15 a.m.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in California, Connecticut, Delaware, Illinois, Indiana, Maryland, Minnesota, Montana, New Jersey, North Carolina, Ohio, Oregon, Pennsylvania, South Carolina, South Dakota, Tennessee, Vermont, and Virginia.

Source Numbers

100, 105, 114, 126, 157, 167, 177, 197, 207, 211, 217, 267, 275, 293, 300, 319, 320, 356, 357, 367, 384, 387, 407, 435, 522, 550, 552, 579, 598, 601, 603, 614, 637, 711, 716, 720, 728, 735, 868, 903, 925, 936

Single-Family Detached Housing (210)

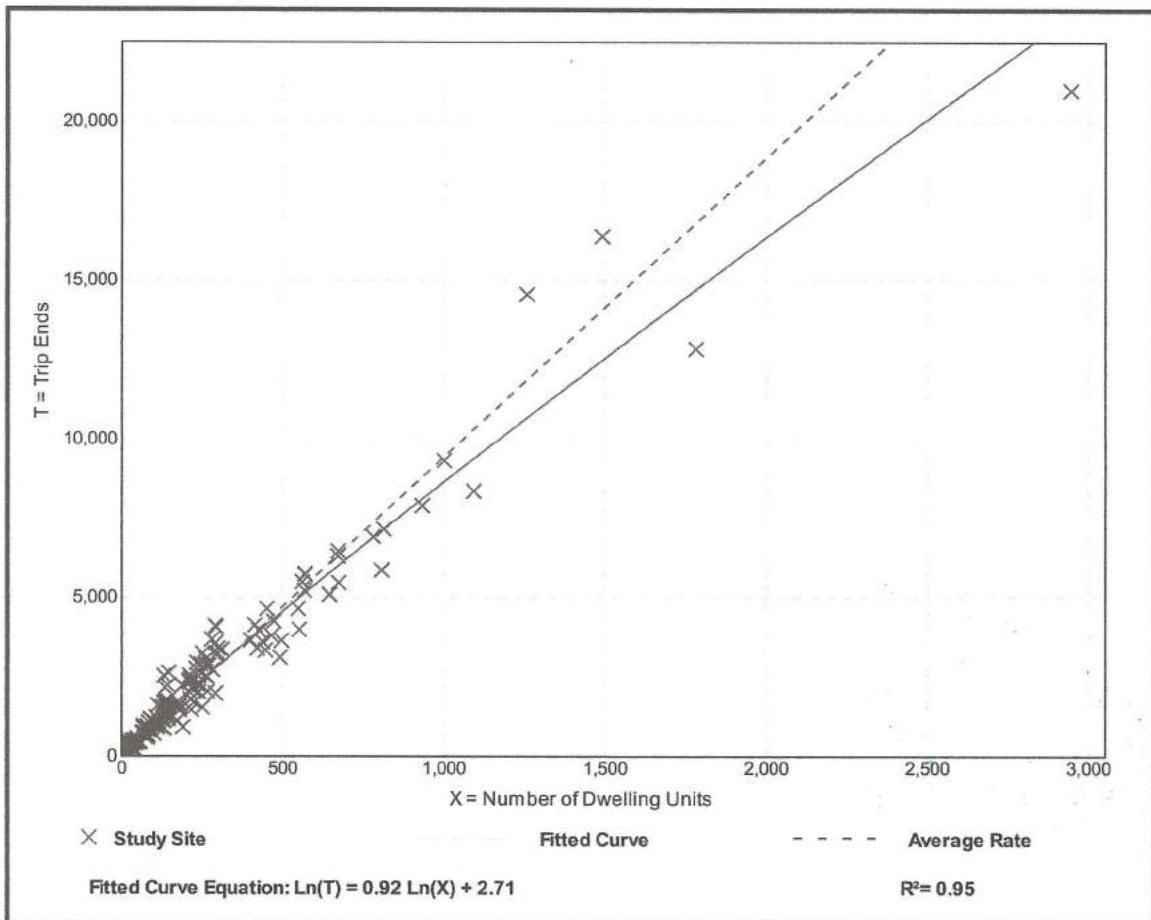
Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 159
Avg. Num. of Dwelling Units: 264
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
9.44	4.81 - 19.39	2.10

Data Plot and Equation



Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 173

Avg. Num. of Dwelling Units: 219

Directional Distribution: 25% entering, 75% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate

0.74

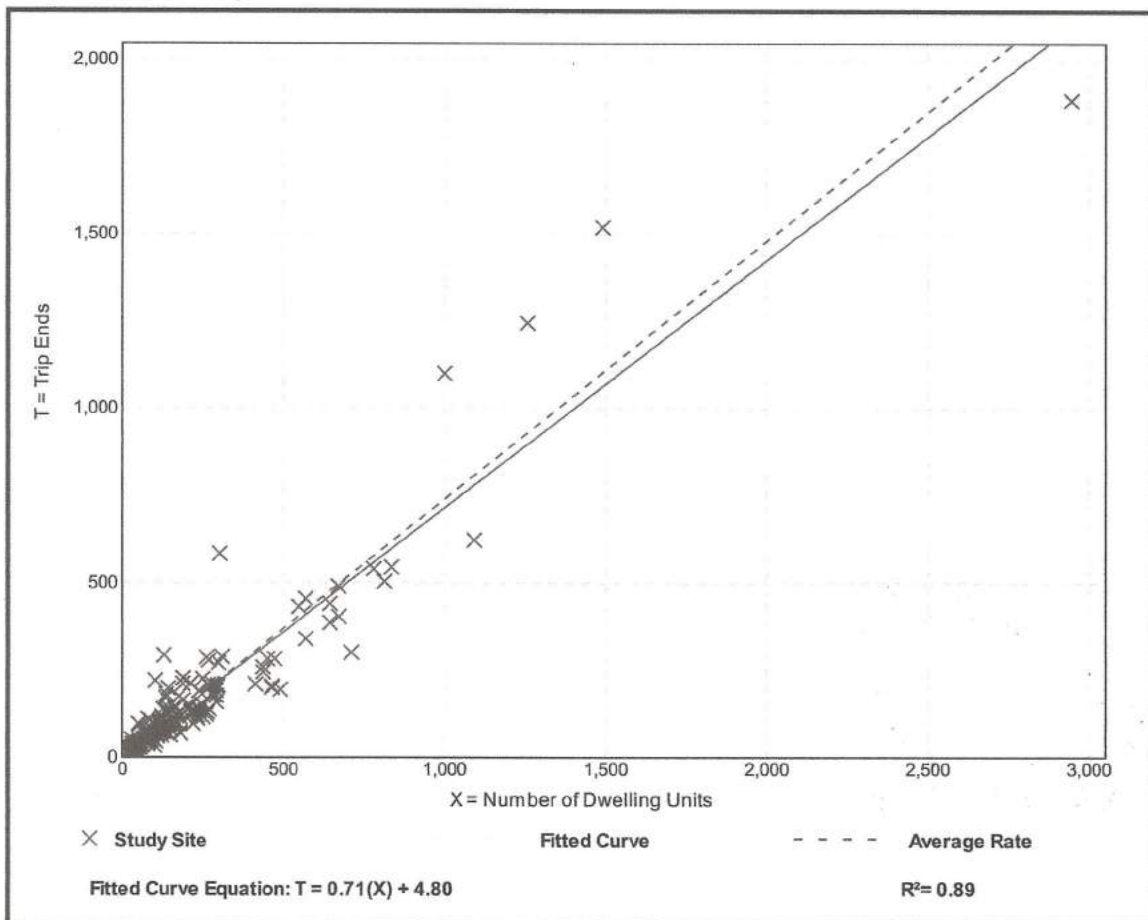
Range of Rates

0.33 - 2.27

Standard Deviation

0.27

Data Plot and Equation



Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 190

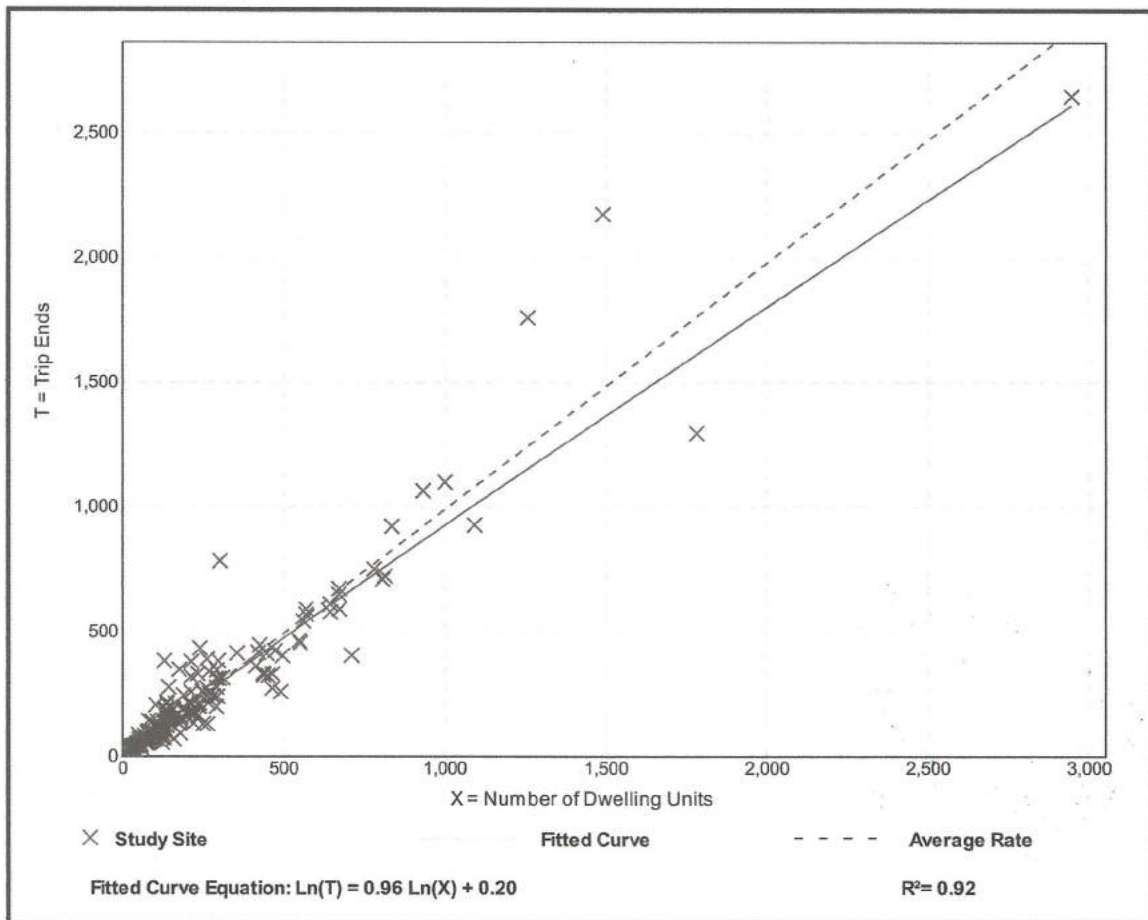
Avg. Num. of Dwelling Units: 242

Directional Distribution: 63% entering, 37% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.99	0.44 - 2.98	0.31

Data Plot and Equation



Land Use: 220

Multifamily Housing (Low-Rise)

Description

Low-rise multifamily housing includes apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have one or two levels (floors). Multifamily housing (mid-rise) (Land Use 221), multifamily housing (high-rise) (Land Use 222), and off-campus student apartment (Land Use 225) are related land uses.

Additional Data

In prior editions of *Trip Generation Manual*, the low-rise multifamily housing sites were further divided into rental and condominium categories. An investigation of vehicle trip data found no clear differences in trip making patterns between the rental and condominium sites within the ITE database. As more data are compiled for future editions, this land use classification can be reinvestigated.

For the three sites for which both the number of residents and the number of occupied dwelling units were available, there were an average of 2.72 residents per occupied dwelling unit.

For the two sites for which the numbers of both total dwelling units and occupied dwelling units were available, an average of 96.2 percent of the total dwelling units were occupied.

This land use included data from a wide variety of units with different sizes, price ranges, locations, and ages. Consequently, there was a wide variation in trips generated within this category. Other factors, such as geographic location and type of adjacent and nearby development, may also have had an effect on the site trip generation.

Time-of-day distribution data for this land use are presented in Appendix A. For the 10 general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:45 and 5:45 p.m., respectively. For the one site with Saturday data, the overall highest vehicle volume was counted between 9:45 and 10:45 a.m. For the one site with Sunday data, the overall highest vehicle volume was counted between 11:45 a.m. and 12:45 p.m.

For the one dense multi-use urban site with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:00 and 8:00 a.m. and 6:15 and 7:15 p.m., respectively.

For the three sites for which data were provided for both occupied dwelling units and residents, there was an average of 2.72 residents per occupied dwelling unit.

The average numbers of person trips per vehicle trip at the five general urban/suburban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.13 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.21 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in British Columbia (CAN), California, District of Columbia, Florida, Georgia, Illinois, Indiana, Maine, Maryland, Minnesota, New Jersey, New York, Ontario, Oregon, Pennsylvania, South Dakota, Tennessee, Texas, Utah, Virginia, and Washington.

It is expected that the number of bedrooms and number of residents are likely correlated to the number of trips generated by a residential site. Many of the studies included in this land use did not indicate the total number of bedrooms. To assist in the future analysis of this land use, it is important that this information be collected and included in trip generation data submissions.

Source Numbers

168, 187, 188, 204, 211, 300, 305, 306, 319, 320, 321, 357, 390, 412, 418, 525, 530, 571, 579, 583, 864, 868, 869, 870, 896, 903, 918, 946, 947, 948, 951

Multifamily Housing (Low-Rise) (220)

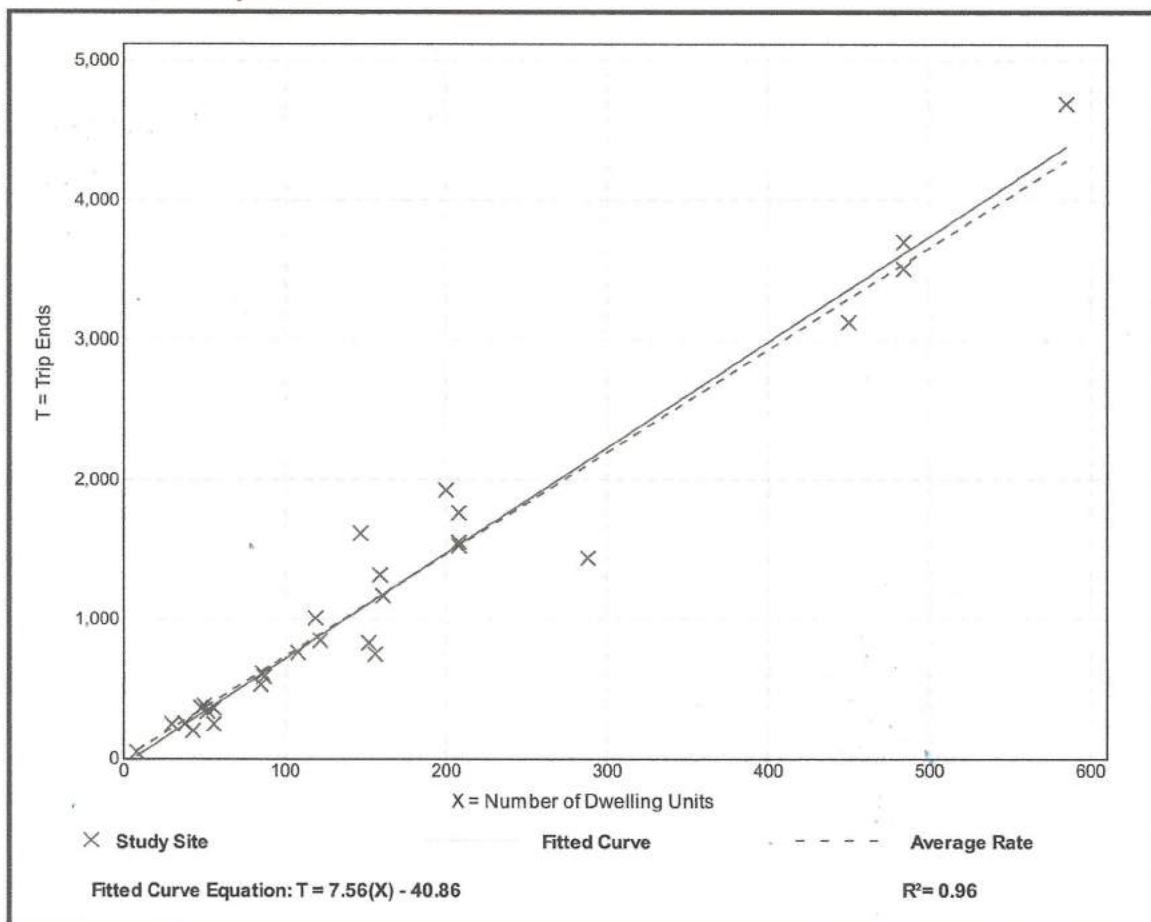
Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 29
Avg. Num. of Dwelling Units: 168
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
7.32	4.45 - 10.97	1.31

Data Plot and Equation



Multifamily Housing (Low-Rise) (220)

Vehicle Trip Ends vs: Dwelling Units

**On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.**

Setting/Location: General Urban/Suburban

Number of Studies: 42

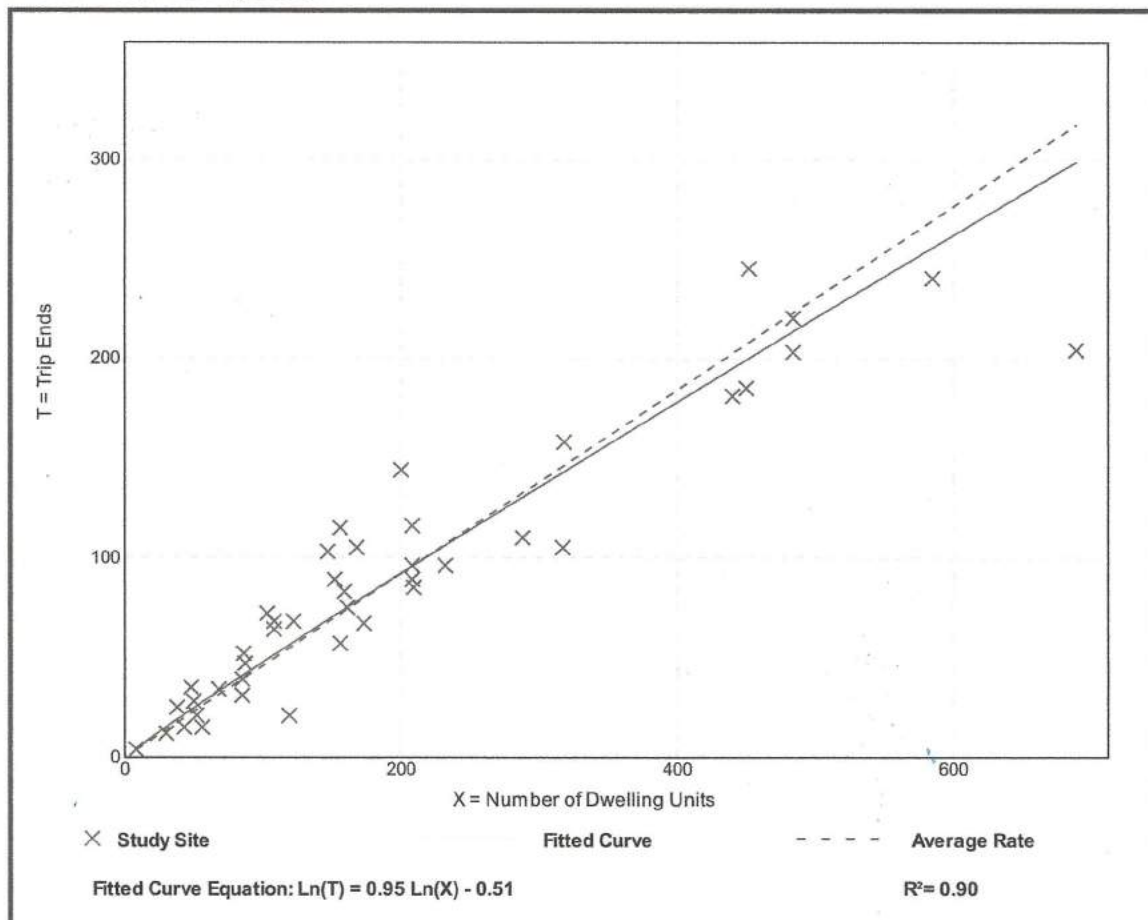
Avg. Num. of Dwelling Units: 199

Directional Distribution: 23% entering, 77% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.46	0.18 - 0.74	0.12

Data Plot and Equation



Multifamily Housing (Low-Rise) (220)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 50

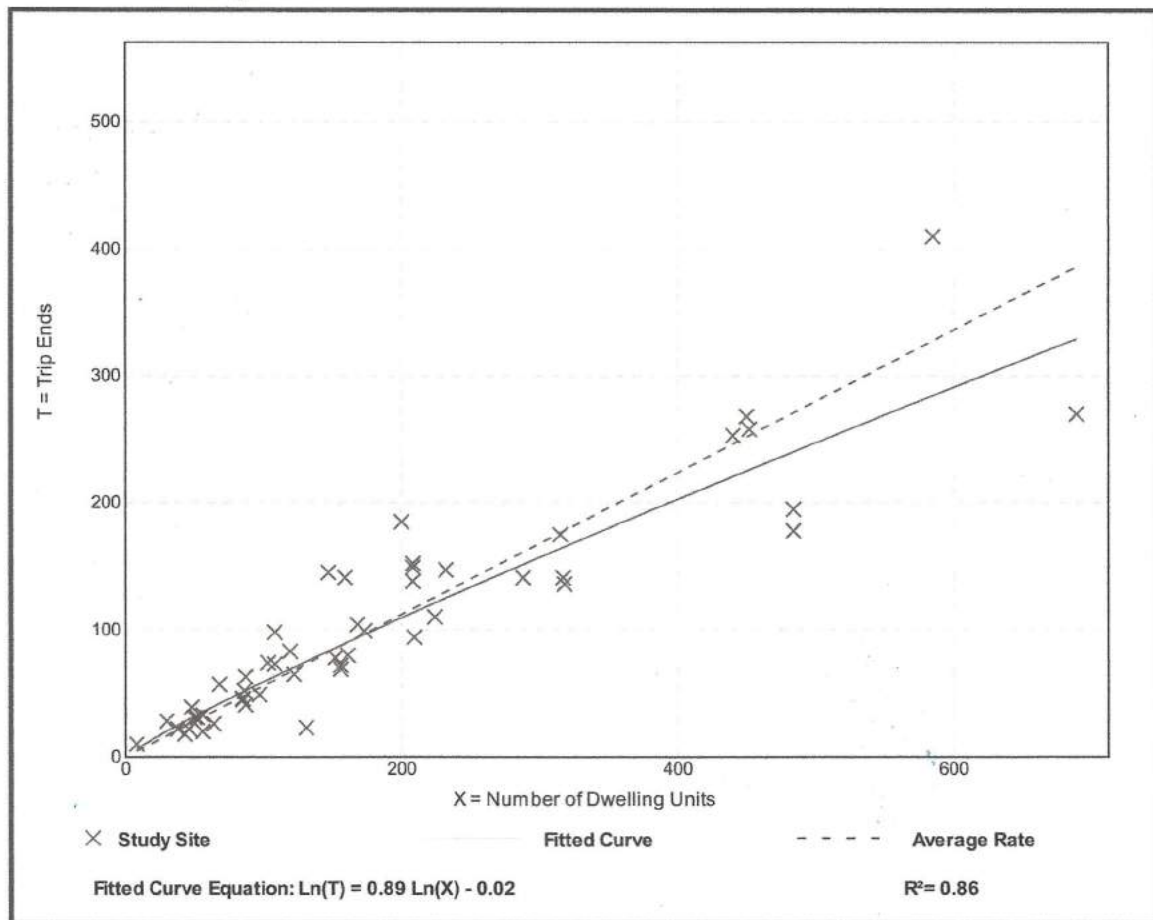
Avg. Num. of Dwelling Units: 187

Directional Distribution: 63% entering, 37% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.56	0.18 - 1.25	0.16

Data Plot and Equation



Land Use: 221

Multifamily Housing (Mid-Rise)

Description

Mid-rise multifamily housing includes apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have between three and 10 levels (floors). Multifamily housing (low-rise) (Land Use 220), multifamily housing (high-rise) (Land Use 222), off-campus student apartment (Land Use 225), and mid-rise residential with 1st-floor commercial (Land Use 231) are related land uses.

Additional Data

In prior editions of *Trip Generation Manual*, the mid-rise multifamily housing sites were further divided into rental and condominium categories. An investigation of vehicle trip data found no clear differences in trip making patterns between the rental and condominium sites within the ITE database. As more data are compiled for future editions, this land use classification can be reinvestigated.

For the six sites for which both the number of residents and the number of occupied dwelling units were available, there were an average of 2.46 residents per occupied dwelling unit.

For the five sites for which the numbers of both total dwelling units and occupied dwelling units were available, an average of 95.7 percent of the total dwelling units were occupied.

Time-of-day distribution data for this land use are presented in Appendix A. For the eight general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:00 and 8:00 a.m. and 4:45 and 5:45 p.m., respectively.

For the four dense multi-use urban sites with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:15 and 5:15 p.m., respectively. For the three center city core sites with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 6:45 and 7:45 a.m. and 5:00 and 6:00 p.m., respectively.

For the six sites for which data were provided for both occupied dwelling units and residents, there was an average of 2.46 residents per occupied dwelling unit.

For the five sites for which data were provided for both occupied dwelling units and total dwelling units, an average of 95.7 percent of the units were occupied.

The average numbers of person trips per vehicle trip at the five center city core sites at which both person trip and vehicle trip data were collected were as follows:

- 1.84 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.94 during Weekday, AM Peak Hour of Generator
- 2.07 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.59 during Weekday, PM Peak Hour of Generator

The average numbers of person trips per vehicle trip at the 32 dense multi-use urban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.90 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.90 during Weekday, AM Peak Hour of Generator
- 2.00 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.08 during Weekday, PM Peak Hour of Generator

The average numbers of person trips per vehicle trip at the 13 general urban/suburban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.56 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.88 during Weekday, AM Peak Hour of Generator
- 1.70 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.07 during Weekday, PM Peak Hour of Generator

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), British Columbia (CAN), California, Delaware, District of Columbia, Florida, Georgia, Illinois, Maryland, Massachusetts, Minnesota, New Hampshire, New Jersey, Ontario, Oregon, Pennsylvania, South Carolina, South Dakota, Tennessee, Utah, Virginia, and Wisconsin.

Source Numbers

168, 188, 204, 305, 306, 321, 357, 390, 436, 525, 530, 579, 638, 818, 857, 866, 901, 904, 910, 912, 918, 934, 936, 939, 944, 947, 948, 949, 959, 963, 964, 966, 967, 969, 970

Multifamily Housing (Mid-Rise) (221)

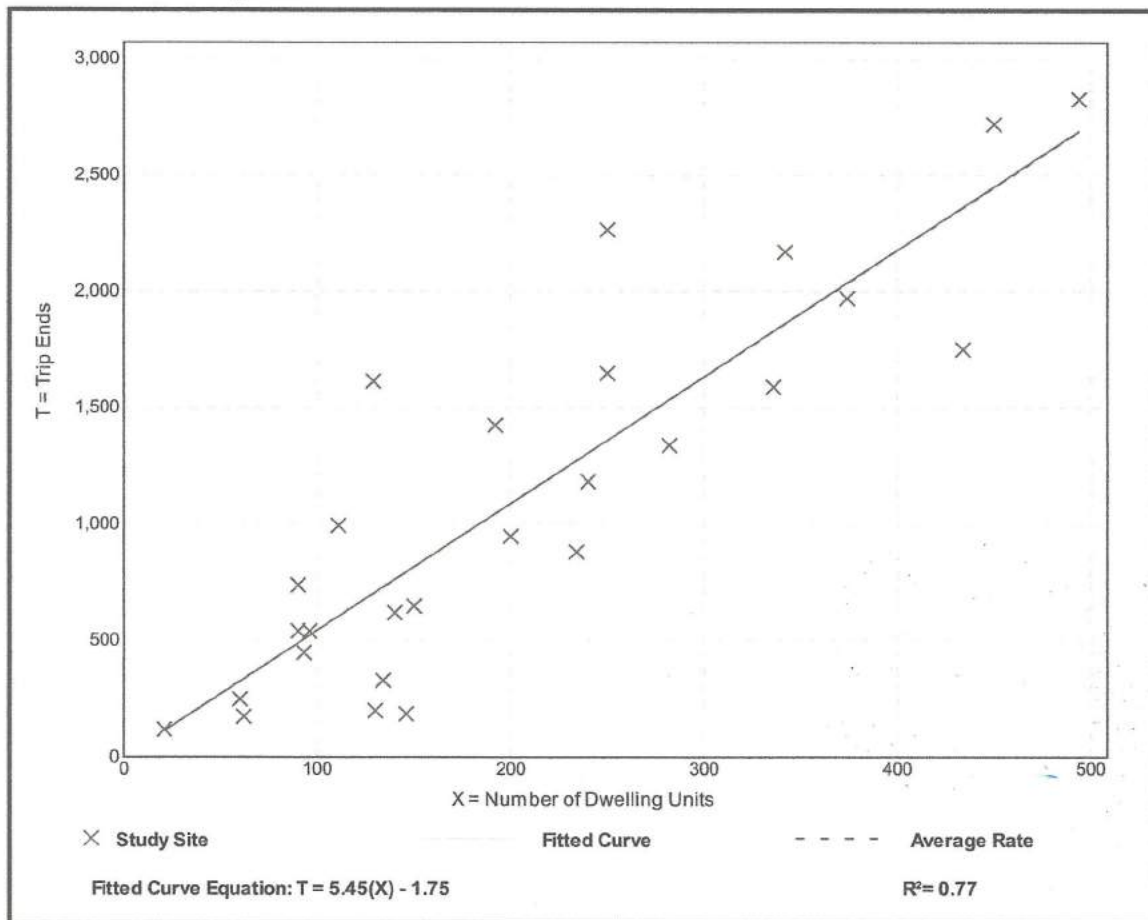
Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 27
Avg. Num. of Dwelling Units: 205
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
5.44	1.27 - 12.50	2.03

Data Plot and Equation



Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 53

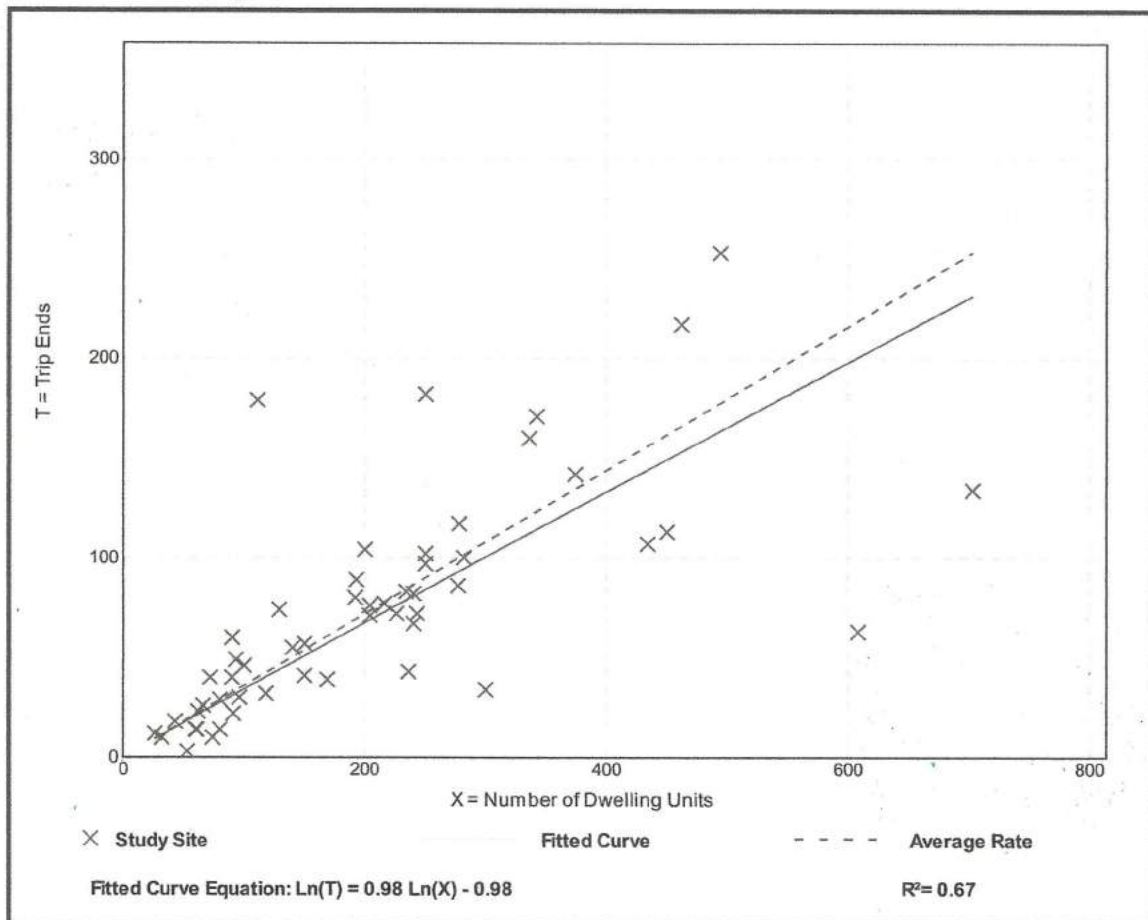
Avg. Num. of Dwelling Units: 207

Directional Distribution: 26% entering, 74% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.36	0.06 - 1.61	0.19

Data Plot and Equation



Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

**Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.**

Setting/Location: General Urban/Suburban

Number of Studies: 60

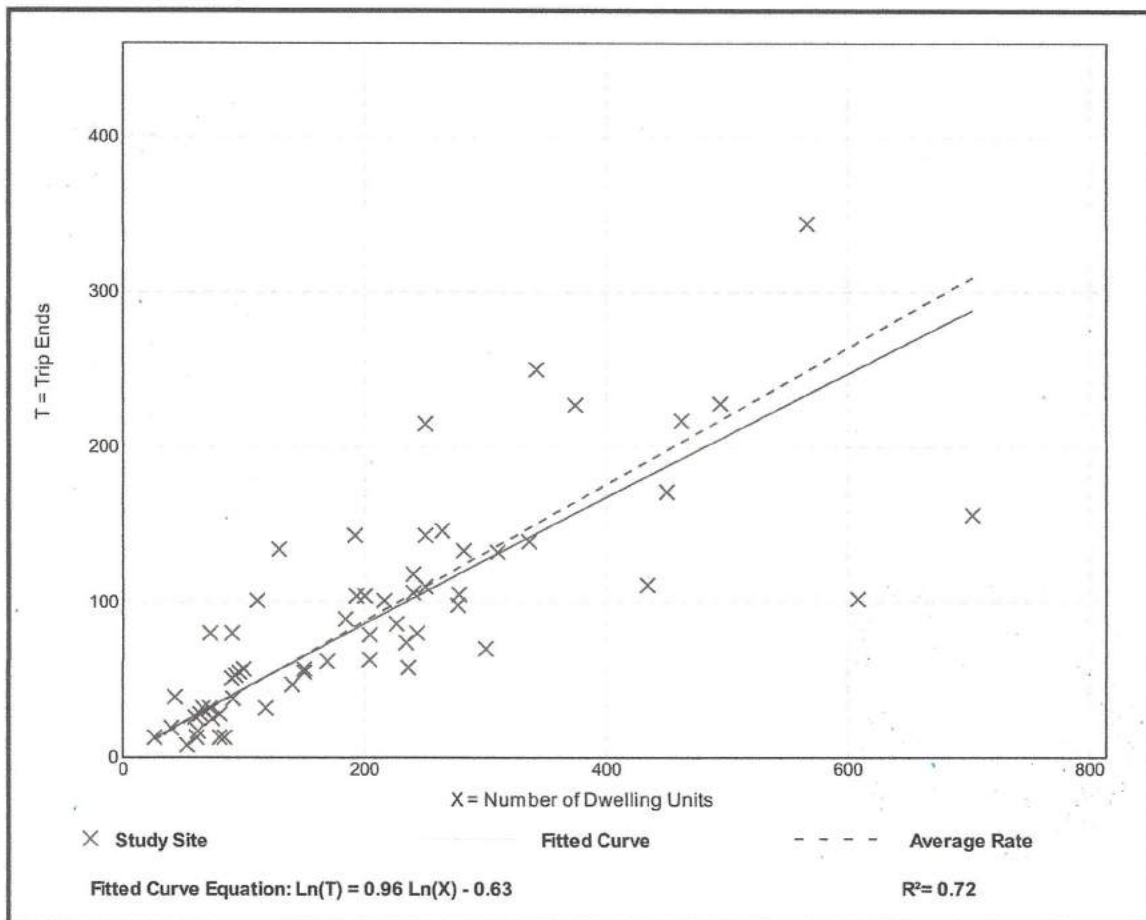
Avg. Num. of Dwelling Units: 208

Directional Distribution: 61% entering, 39% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.44	0.15 - 1.11	0.19

Data Plot and Equation



APPENDIX E

Growth Rate Analysis

FLORIDA DEPARTMENT OF TRANSPORTATION
TRANSPORTATION STATISTICS OFFICE
2020 HISTORICAL AADT REPORT

COUNTY: 86 - BROWARD

SITE: 0248 - SR 820/HOLLYWOOD BLVD - E OF SR 9/I-95

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
----	-----		-----		-----	-----	-----	-----
2020	47500	C	E 23000		W 24500	9.00	53.90	7.30
2019	55500	C	E 27500		W 28000	9.00	54.60	7.80
2018	53000	C	E 26500		W 26500	9.00	54.50	7.80
2017	42000	C	E 21000		W 21000	9.00	51.90	7.80
2016	49000	C	E 24000		W 25000	9.00	54.10	3.20
2015	43500	C	E 21000		W 22500	9.00	54.00	3.20
2014	48000	C	E 23500		W 24500	9.00	54.20	2.80
2013	46000	C	E 23000		W 23000	9.00	53.60	1.90
2012	54000	C	E 26000		W 28000	9.00	52.20	2.00
2011	49000	C	E 23500		W 25500	9.00	52.50	4.60
2010	50500	C	E 25500		W 25000	8.35	52.69	4.40
2009	51000	C	E 25000		W 26000	8.53	53.89	3.10
2008	49500	C	E 26000		W 23500	8.81	54.16	2.20
2007	50000	C	E 25500		W 24500	8.63	55.75	2.20
2006	49000	C	E 23500		W 25500	8.40	55.34	2.20
2005	46500	C	E 23000		W 23500	8.20	51.70	1.50

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE
S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE
V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN
*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

Polk Street Apartments

Hollywood, FL

Growth Rate Analysis

Site #860248 - SR 820 / Hollywood Boulevard - East of I-95

Year	Volume	Growth Rate
2014	48,000	
2019	55,500	2.95%

APPENDIX F

Future Traffic Volumes Spreadsheets

FUTURE TURNING MOVEMENT VOLUME ANALYSIS

Polk Street and N. 26th Avenue AM Peak Hour

Description	N. 26th Avenue Northbound			N. 26th Avenue Southbound			Polk Street Eastbound			Polk Street Westbound		
	Left	Through	Right	Left	Through	Right	Left	Through	Right	Left	Through	Right
Existing Traffic (7/13/2021)	5	71	0	0	140	2	0	0	0	196	36	25
Season Adjustment Factor	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06
2021 Peak Season Traffic	5	75	0	0	148	2	0	0	0	208	38	27
Annual Growth Rate	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%
2023 Background Traffic	6	80	0	0	157	2	0	0	0	220	40	28
Polk Street Apartments	3					1						
2023 Total Traffic	9	80	0	0	157	3	0	0	0	220	40	28

FUTURE TURNING MOVEMENT VOLUME ANALYSIS

Polk Street and N. 26th Avenue PM Peak Hour

Description	N. 26th Avenue Northbound			N. 26th Avenue Southbound			Polk Street Eastbound			Polk Street Westbound		
	Left	Through	Right	Left	Through	Right	Left	Through	Right	Left	Through	Right
Existing Traffic (7/13/2021)	18	141	0	0	102	6	0	0	0	232	74	30
Season Adjustment Factor	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06
2021 Peak Season Traffic	19	149	0	0	108	6	0	0	0	246	78	32
Annual Growth Rate	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%
2023 Background Traffic	20	159	0	0	115	7	0	0	0	261	83	34
Polk Street Apartments	8					2					1	
2023 Total Traffic	28	159	0	0	115	9	0	0	0	261	84	34

FUTURE TURNING MOVEMENT VOLUME ANALYSIS

Polk Street and N. 28th Avenue AM Peak Hour

Description	N. 28th Avenue Northbound			N. 28th Avenue Southbound			Polk Street Eastbound			Polk Street Westbound		
	Left	Through	Right	Left	Through	Right	Left	Through	Right	Left	Through	Right
Existing Traffic (7/13/2021)	3	254	11	7	395	5	14	0	4	27	6	13
Season Adjustment Factor	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06
2021 Peak Season Traffic	3	269	12	7	419	5	15	0	4	29	6	14
Annual Growth Rate	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%
2023 Background Traffic	3	286	12	8	444	6	16	0	4	30	7	15
Polk Street Apartments			3	1						10		4
2023 Total Traffic	3	286	15	9	444	6	16	0	4	40	7	19

FUTURE TURNING MOVEMENT VOLUME ANALYSIS

Polk Street and N. 28th Avenue PM Peak Hour

Description	N. 28th Avenue Northbound			N. 28th Avenue Southbound			Polk Street Eastbound			Polk Street Westbound		
	Left	Through	Right	Left	Through	Right	Left	Through	Right	Left	Through	Right
Existing Traffic (7/13/2021)	8	342	8	5	347	7	4	3	4	47	19	23
Season Adjustment Factor	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06
2021 Peak Season Traffic	8	363	8	5	368	7	4	3	4	50	20	24
Annual Growth Rate	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%	3.00%
2023 Background Traffic	9	385	9	6	390	8	4	3	4	53	21	26
Polk Street Apartments			11	2						7		3
2023 Total Traffic	9	385	20	8	390	8	4	3	4	60	21	29

APPENDIX G

Signal Timing Data

Station : 3165 - Hollywood Blvd & N 26 Ave/Polk St (Standard File)

Phase	1	2 (WR)	3 (SR)	4	5	6	7	8	9	10	11	12	13	14	15	16
Walk		7	7	7												
Ped Clearance		6	23	6												
Min Green		10	6	6												
Gap Ext		3	2	2												
Max1		45	25	20												
Max2																
Yellow Clr	4	4	4	4	4	4	4	4	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5
Red Clr		1	2	1					1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5
Red Revert																
Added Initial																
Max Initial																
Time Before Reduce																
Cars Before Reduce																
Time To Reduce																
Reduce By																
Min Gap																
Dynamic Max Limit																
Dynamic Max Step																
Enable		ON	ON	ON												
Auto Flash Entry				ON												
Auto Flash Exit		ON														
Non-Actuated 1						ON										
Non-Actuated 2																
Lock Call									ON	ON	ON	ON	ON	ON	ON	ON
Min Recall		ON														
Max Recall																
Ped Recall																
Soft Recall																
Dual Entry																
Sim Gap Enable	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON	ON
Guar Passage																
Rest In Walk		ON														
Cond Service																
Add Init Calc																

Preemption

Channel	1	2	3	4	5	6
Lock Input	ON	ON	ON	ON	ON	ON
Override Auto Flash					ON	ON
Override Higher Preempt					ON	ON
Flash in Dwell						
Link to Preempt						
Delay						
Min Duration						
Min Green	6	6	6	6		
Min Walk						
Ped Clear						
Track Green						
Min Dwell	8	8	8	8		
Max Presence	180	180	180	180		
Track Veh 1						
Track Veh 2						
Track Veh 3						
Track Veh 4						
Dwell Cyc Veh 1	3	2		4		
Dwell Cyc Veh 2						
Dwell Cyc Veh 3						
Dwell Cyc Veh 4						
Dwell Cyc Veh 5						
Dwell Cyc Veh 6						
Dwell Cyc Veh 7						
Dwell Cyc Veh 8						
Dwell Cyc Veh 9						
Dwell Cyc Veh 10						
Dwell Cyc Veh 11						
Dwell Cyc Veh 12						
Dwell Cyc Ped1						
Dwell Cyc Ped2						
Dwell Cyc Ped3						
Dwell Cyc Ped4						
Dwell Cyc Ped5						
Dwell Cyc Ped6						
Dwell vPed7						

Preempt LP

Channel	1	2	3	4
Min				
Max				
Enable				
Lock Mode	MAX	MAX	MAX	MAX
Coord in Preempt				
No Skip				
Priority P1				
Priority P2				
Priority P3				
Priority P4				
Lock				
Headway				
Group Lock				
Queue Jump				
Free Mode				
Alt Table				

Dwell Cyc Ped8						
Exit 1	4	3		2		
Exit 2						
Exit 3						
Exit 4						

Prepared By

Date Implemented

Reviewed By

Traffic Engineer

Coordination

[illegible]

[illegible][illegible]

User Comments:



BROWARD COUNTY TRAFFIC ENGINEERING
ACTUATED TRAFFIC SIGNAL TIMING SHEET

Intersection Number	3165	Initial Operation Date	UNKNOWN
Controller Type	2070 LN	System Number	3165
Modification Number	12	Modification Date	05/22/2012
Drawing/Project No	DES. GRP. 1	FPL Grid Number	87572211001
Intersection	HOLLYWOOD BLVD. (SR 820) and N 26 AVENUE/POLK STREET		
Municipality	HOLLYWOOD		

Controller Phase	1	2	3	4	5	6	7	8
Face Number		2,2R	4,8	2A				
Direction		WB	N/S	WB PED				
Initial Green(MIN)		10	6	6				
Vehicle Ext.(GAP)		3.0	2.0	2.0				
Maximum Green I		45	25	20				
Maximum Green II								
Yellow Clearance		4.0	4.0	4.0				
All Red Clearance		1.0	2.0	1.0				
Phase Recall		MIN	OFF	OFF				
Detector Delay								
Walk		7	7	7				
Pedestrian Clearance		6	23	6				
Permissive								
Flash Operation		YELLOW	RED	YELLOW				

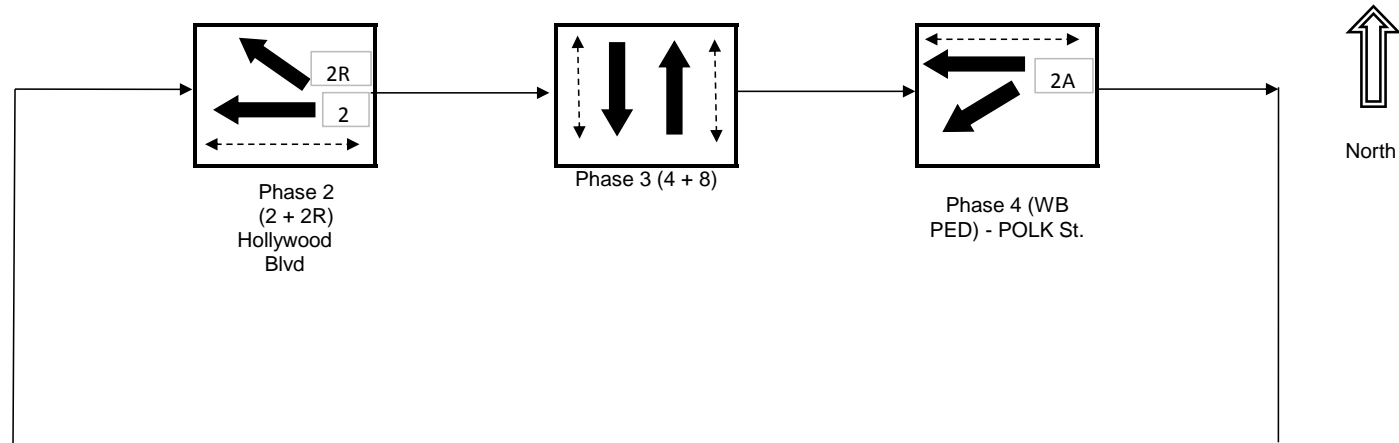
Attachment

NOTES:

1. DUAL ENTRY HARDWIRED NORTH/SOUTH.
2. MOD. 12 UPDATES ATMS.NOW PARAMETERS, NO CHANGE IN TIMING.

Submitted By _____ Approved By _____

Sequence Of Operation (3165) - Hollywood Blvd and N. 26 Ave / Polk St Hollywood








APPENDIX H

SYNCHRO Output

Existing (2021) SYNCHRO Output

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection												
Int Delay, s/veh	1.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	15	0	4	29	6	14	3	269	12	7	419	5
Future Vol, veh/h	15	0	4	29	6	14	3	269	12	7	419	5
Conflicting Peds, #/hr	2	0	0	0	0	2	5	0	2	2	0	5
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	97	97	97	97	97	97	97	97	97	97	97	97
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	15	0	4	30	6	14	3	277	12	7	432	5
Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	755	751	440	742	747	287	442	0	0	291	0	0
Stage 1	454	454	-	291	291	-	-	-	-	-	-	-
Stage 2	301	297	-	451	456	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	325	340	617	332	341	752	1118	-	-	1271	-	-
Stage 1	586	569	-	717	672	-	-	-	-	-	-	-
Stage 2	708	668	-	588	568	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	310	334	614	327	335	749	1113	-	-	1269	-	-
Mov Cap-2 Maneuver	310	334	-	327	335	-	-	-	-	-	-	-
Stage 1	582	562	-	713	669	-	-	-	-	-	-	-
Stage 2	685	665	-	580	561	-	-	-	-	-	-	-
Approach	EB		WB		NB			SB				
HCM Control Delay, s	16		15.6		0.1			0.1				
HCM LOS	C		C									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1113	-	-	346	391	1269	-	-				
HCM Lane V/C Ratio	0.003	-	-	0.057	0.129	0.006	-	-				
HCM Control Delay (s)	8.2	-	-	16	15.6	7.9	0	-				
HCM Lane LOS	A	-	-	C	C	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.2	0.4	0	-	-				

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations					
Traffic Volume (vph)	208	38	5	75	148
Future Volume (vph)	208	38	5	75	148
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases	2		4		
Detector Phase	2	2	4	4	8
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	20.0	20.0	20.0
Total Split (s)	40.0	40.0	20.0	20.0	20.0
Total Split (%)	66.7%	66.7%	33.3%	33.3%	33.3%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	42.7	42.7		9.7	9.7
Actuated g/C Ratio	0.71	0.71		0.16	0.16
v/c Ratio	0.18	0.06		0.30	0.55
Control Delay	5.0	3.3		23.7	29.6
Queue Delay	0.0	0.0		0.0	0.0
Total Delay	5.0	3.3		23.7	29.6
LOS	A	A		C	C
Approach Delay		4.6		23.7	29.6
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 0 (0%), Referenced to phase 2:WBTL, Start of Yellow

Natural Cycle: 45

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.55

Intersection Signal Delay: 15.0

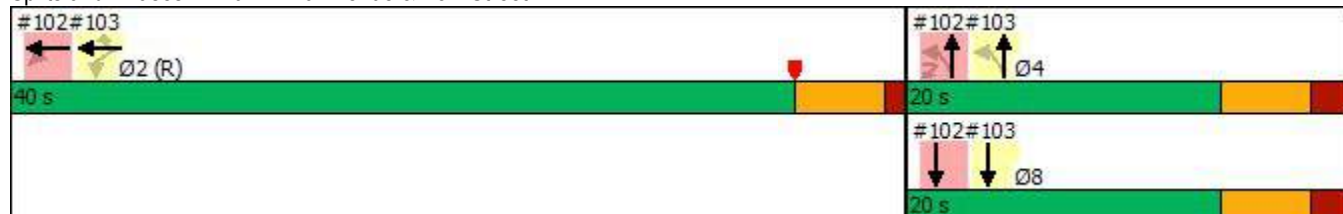
Intersection LOS: B

Intersection Capacity Utilization 29.2%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues














102: N 26 Avenue & Polk Street








Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	229	72	87	165
v/c Ratio	0.18	0.06	0.30	0.55
Control Delay	5.0	3.3	23.7	29.6
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	5.0	3.3	23.7	29.6
Queue Length 50th (ft)	27	4	28	56
Queue Length 95th (ft)	63	18	58	99
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1259	1250	421	433
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.18	0.06	0.21	0.38
Intersection Summary				

HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street

								
Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	208	38	27	5	75	148	2
Future Volume (vph)	0	208	38	27	5	75	148	2
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor		1.00	1.00			1.00	1.00	
Frpb, ped/bikes		1.00	1.00			1.00	1.00	
Flpb, ped/bikes		1.00	1.00			1.00	1.00	
Frt		1.00	0.94			1.00	1.00	
Flt Protected		0.95	1.00			1.00	1.00	
Satd. Flow (prot)		1770	1746			1857	1859	
Flt Permitted		0.95	1.00			0.97	1.00	
Satd. Flow (perm)		1770	1746			1805	1859	
Peak-hour factor, PHF	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Adj. Flow (vph)	0	229	42	30	5	82	163	2
RTOR Reduction (vph)	0	0	10	0	0	0	0	0
Lane Group Flow (vph)	0	229	62	0	0	87	165	0
Confl. Peds. (#/hr)								1
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2			4			
Actuated Green, G (s)		40.5	40.5			8.5	8.5	
Effective Green, g (s)		40.5	40.5			8.5	8.5	
Actuated g/C Ratio		0.68	0.68			0.14	0.14	
Clearance Time (s)		5.0	5.0			6.0	6.0	
Vehicle Extension (s)		2.0	2.0			2.0	2.0	
Lane Grp Cap (vph)		1194	1178			255	263	
v/s Ratio Prot			0.04				c0.09	
v/s Ratio Perm		c0.13				0.05		
v/c Ratio		0.19	0.05			0.34	0.63	
Uniform Delay, d1		3.6	3.3			23.2	24.3	
Progression Factor		1.00	1.00			1.00	1.00	
Incremental Delay, d2		0.4	0.1			0.3	3.3	
Delay (s)		4.0	3.4			23.5	27.6	
Level of Service		A	A			C	C	
Approach Delay (s)			3.8			23.5	27.6	
Approach LOS			A			C	C	
Intersection Summary								
HCM 2000 Control Delay			14.0			HCM 2000 Level of Service		B
HCM 2000 Volume to Capacity ratio			0.27					
Actuated Cycle Length (s)			60.0			Sum of lost time (s)		11.0
Intersection Capacity Utilization			29.2%			ICU Level of Service		A
Analysis Period (min)			15					
c Critical Lane Group								

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection												
Int Delay, s/veh	2.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	4	3	4	50	20	24	8	363	8	5	368	7
Future Vol, veh/h	4	3	4	50	20	24	8	363	8	5	368	7
Conflicting Peds, #/hr	0	0	0	0	0	0	2	0	0	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	4	3	4	53	21	26	9	386	9	5	391	7
Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	839	820	397	817	819	391	400	0	0	395	0	0
Stage 1	407	407	-	409	409	-	-	-	-	-	-	-
Stage 2	432	413	-	408	410	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	285	310	652	295	310	658	1159	-	-	1164	-	-
Stage 1	621	597	-	619	596	-	-	-	-	-	-	-
Stage 2	602	594	-	620	595	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	256	305	651	288	305	658	1157	-	-	1164	-	-
Mov Cap-2 Maneuver	256	305	-	288	305	-	-	-	-	-	-	-
Stage 1	615	592	-	614	591	-	-	-	-	-	-	-
Stage 2	553	589	-	609	590	-	-	-	-	-	-	-
Approach	EB		WB			NB			SB			
HCM Control Delay, s	15.7		19.9			0.2			0.1			
HCM LOS	C		C									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1157	-	-	348	341	1164	-	-				
HCM Lane V/C Ratio	0.007	-	-	0.034	0.293	0.005	-	-				
HCM Control Delay (s)	8.1	-	-	15.7	19.9	8.1	0	-				
HCM Lane LOS	A	-	-	C	C	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.1	1.2	0	-	-				

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations					
Traffic Volume (vph)	246	78	19	149	108
Future Volume (vph)	246	78	19	149	108
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases	2		4		
Detector Phase	2	2	4	4	8
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	25.0	25.0	25.0
Total Split (s)	35.0	35.0	25.0	25.0	25.0
Total Split (%)	58.3%	58.3%	41.7%	41.7%	41.7%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	38.5	38.5		10.5	10.5
Actuated g/C Ratio	0.64	0.64		0.18	0.18
v/c Ratio	0.24	0.10		0.59	0.38
Control Delay	5.9	4.1		30.2	24.2
Queue Delay	0.0	0.0		0.0	0.0
Total Delay	5.9	4.1		30.3	24.2
LOS	A	A		C	C
Approach Delay		5.4		30.3	24.2
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 42 (70%), Referenced to phase 2:WBTL, Start of Yellow

Natural Cycle: 50

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.59

Intersection Signal Delay: 15.3

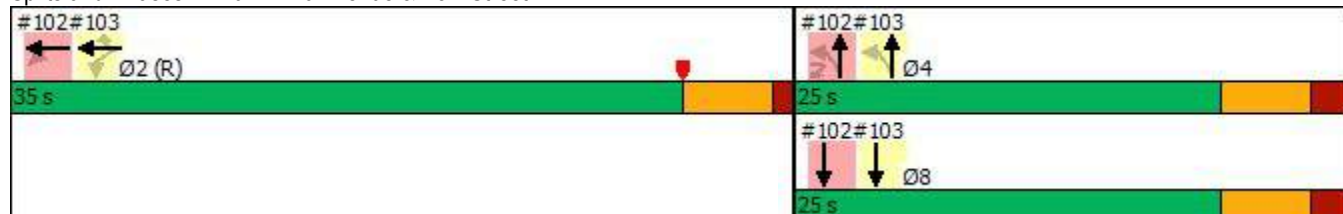
Intersection LOS: B

Intersection Capacity Utilization 38.4%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues














102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	267	120	183	124
v/c Ratio	0.24	0.10	0.59	0.38
Control Delay	5.9	4.1	30.2	24.2
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	5.9	4.1	30.3	24.2
Queue Length 50th (ft)	34	10	62	40
Queue Length 95th (ft)	78	31	106	74
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1135	1145	556	585
Starvation Cap Reductn	0	0	21	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.24	0.10	0.34	0.21
Intersection Summary				






HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street

								
Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	246	78	32	19	149	108	6
Future Volume (vph)	0	246	78	32	19	149	108	6
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor		1.00	1.00			1.00	1.00	
Frpb, ped/bikes		1.00	0.99			1.00	1.00	
Flpb, ped/bikes		1.00	1.00			1.00	1.00	
Frt		1.00	0.96			1.00	0.99	
Flt Protected		0.95	1.00			0.99	1.00	
Satd. Flow (prot)		1770	1767			1852	1849	
Flt Permitted		0.95	1.00			0.94	1.00	
Satd. Flow (perm)		1770	1767			1757	1849	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	0	267	85	35	21	162	117	7
RTOR Reduction (vph)	0	0	13	0	0	0	0	0
Lane Group Flow (vph)	0	267	107	0	0	183	124	0
Confl. Peds. (#/hr)				3				
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2			4			
Actuated Green, G (s)		38.5	38.5			10.5	10.5	
Effective Green, g (s)		38.5	38.5			10.5	10.5	
Actuated g/C Ratio		0.64	0.64			0.18	0.18	
Clearance Time (s)		5.0	5.0			6.0	6.0	
Vehicle Extension (s)		2.0	2.0			2.0	2.0	
Lane Grp Cap (vph)		1135	1133			307	323	
v/s Ratio Prot			0.06				0.07	
v/s Ratio Perm		c0.15				c0.10		
v/c Ratio		0.24	0.09			0.60	0.38	
Uniform Delay, d1		4.5	4.1			22.8	21.9	
Progression Factor		1.00	1.00			1.00	1.00	
Incremental Delay, d2		0.5	0.2			2.1	0.3	
Delay (s)		5.0	4.3			24.9	22.2	
Level of Service		A	A			C	C	
Approach Delay (s)			4.8			24.9	22.2	
Approach LOS			A			C	C	
Intersection Summary								
HCM 2000 Control Delay			13.2			HCM 2000 Level of Service		B
HCM 2000 Volume to Capacity ratio			0.31					
Actuated Cycle Length (s)			60.0			Sum of lost time (s)		11.0
Intersection Capacity Utilization			38.4%			ICU Level of Service		A
Analysis Period (min)			15					
c Critical Lane Group								

Future (2023) Background SYNCHRO Output

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection												
Int Delay, s/veh	1.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	16	0	4	30	7	15	3	286	12	8	444	6
Future Vol, veh/h	16	0	4	30	7	15	3	286	12	8	444	6
Conflicting Peds, #/hr	2	0	0	0	0	2	5	0	2	2	0	5
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	97	97	97	97	97	97	97	97	97	97	97	97
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	16	0	4	31	7	15	3	295	12	8	458	6
Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	802	797	466	788	794	305	469	0	0	309	0	0
Stage 1	482	482	-	309	309	-	-	-	-	-	-	-
Stage 2	320	315	-	479	485	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	302	319	597	309	321	735	1093	-	-	1252	-	-
Stage 1	565	553	-	701	660	-	-	-	-	-	-	-
Stage 2	692	656	-	568	552	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	286	313	594	303	315	732	1088	-	-	1250	-	-
Mov Cap-2 Maneuver	286	313	-	303	315	-	-	-	-	-	-	-
Stage 1	560	545	-	697	657	-	-	-	-	-	-	-
Stage 2	667	653	-	559	544	-	-	-	-	-	-	-
Approach	EB		WB		NB			SB				
HCM Control Delay, s	17.1		16.5		0.1			0.1				
HCM LOS	C		C									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1088	-	-	319	367	1250	-	-				
HCM Lane V/C Ratio	0.003	-	-	0.065	0.146	0.007	-	-				
HCM Control Delay (s)	8.3	-	-	17.1	16.5	7.9	0	-				
HCM Lane LOS	A	-	-	C	C	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.2	0.5	0	-	-				

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations					
Traffic Volume (vph)	220	40	6	80	157
Future Volume (vph)	220	40	6	80	157
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases	2		4		
Detector Phase	2	2	4	4	8
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	20.0	20.0	20.0
Total Split (s)	40.0	40.0	20.0	20.0	20.0
Total Split (%)	66.7%	66.7%	33.3%	33.3%	33.3%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	42.4	42.4		10.0	10.0
Actuated g/C Ratio	0.71	0.71		0.17	0.17
v/c Ratio	0.19	0.06		0.32	0.57
Control Delay	5.2	3.4		23.8	29.7
Queue Delay	0.0	0.0		0.0	0.0
Total Delay	5.2	3.4		23.8	29.7
LOS	A	A		C	C
Approach Delay		4.8		23.8	29.7
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 0 (0%), Referenced to phase 2:WBTL, Start of Yellow

Natural Cycle: 45

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.57

Intersection Signal Delay: 15.3

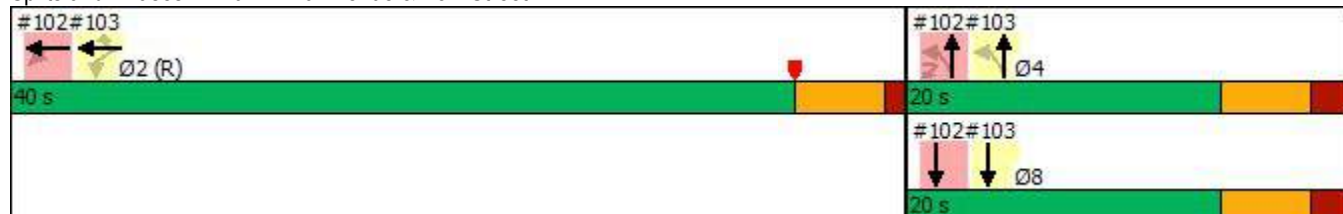
Intersection LOS: B

Intersection Capacity Utilization 30.5%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues














102: N 26 Avenue & Polk Street








Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	242	75	95	175
v/c Ratio	0.19	0.06	0.32	0.57
Control Delay	5.2	3.4	23.8	29.7
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	5.2	3.4	23.8	29.7
Queue Length 50th (ft)	29	5	31	59
Queue Length 95th (ft)	68	20	62	103
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1251	1244	416	433
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.19	0.06	0.23	0.40
Intersection Summary				

HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street

								
Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	220	40	28	6	80	157	2
Future Volume (vph)	0	220	40	28	6	80	157	2
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor		1.00	1.00			1.00	1.00	
Frpb, ped/bikes		1.00	1.00			1.00	1.00	
Flpb, ped/bikes		1.00	1.00			1.00	1.00	
Frt		1.00	0.94			1.00	1.00	
Flt Protected		0.95	1.00			1.00	1.00	
Satd. Flow (prot)		1770	1747			1856	1859	
Flt Permitted		0.95	1.00			0.96	1.00	
Satd. Flow (perm)		1770	1747			1787	1859	
Peak-hour factor, PHF	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Adj. Flow (vph)	0	242	44	31	7	88	173	2
RTOR Reduction (vph)	0	0	10	0	0	0	0	0
Lane Group Flow (vph)	0	242	65	0	0	95	175	0
Confl. Peds. (#/hr)								1
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2			4			
Actuated Green, G (s)		40.2	40.2			8.8	8.8	
Effective Green, g (s)		40.2	40.2			8.8	8.8	
Actuated g/C Ratio		0.67	0.67			0.15	0.15	
Clearance Time (s)		5.0	5.0			6.0	6.0	
Vehicle Extension (s)		2.0	2.0			2.0	2.0	
Lane Grp Cap (vph)		1185	1170			262	272	
v/s Ratio Prot			0.04				c0.09	
v/s Ratio Perm		c0.14				0.05		
v/c Ratio		0.20	0.06			0.36	0.64	
Uniform Delay, d1		3.8	3.4			23.1	24.1	
Progression Factor		1.00	1.00			1.00	1.00	
Incremental Delay, d2		0.4	0.1			0.3	3.9	
Delay (s)		4.2	3.5			23.4	28.0	
Level of Service		A	A			C	C	
Approach Delay (s)			4.0			23.4	28.0	
Approach LOS			A			C	C	
Intersection Summary								
HCM 2000 Control Delay			14.3			HCM 2000 Level of Service		B
HCM 2000 Volume to Capacity ratio			0.28					
Actuated Cycle Length (s)			60.0			Sum of lost time (s)		11.0
Intersection Capacity Utilization			30.5%			ICU Level of Service		A
Analysis Period (min)			15					
c Critical Lane Group								

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection												
Int Delay, s/veh	2.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	4	3	4	53	21	26	9	385	9	6	390	8
Future Vol, veh/h	4	3	4	53	21	26	9	385	9	6	390	8
Conflicting Peds, #/hr	0	0	0	0	0	0	2	0	0	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	4	3	4	56	22	28	10	410	10	6	415	9
Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	894	874	422	870	873	415	426	0	0	420	0	0
Stage 1	434	434	-	435	435	-	-	-	-	-	-	-
Stage 2	460	440	-	435	438	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	262	288	632	272	289	637	1133	-	-	1139	-	-
Stage 1	600	581	-	600	580	-	-	-	-	-	-	-
Stage 2	581	578	-	600	579	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	232	283	631	265	284	637	1131	-	-	1139	-	-
Mov Cap-2 Maneuver	232	283	-	265	284	-	-	-	-	-	-	-
Stage 1	593	576	-	595	575	-	-	-	-	-	-	-
Stage 2	529	573	-	589	574	-	-	-	-	-	-	-
Approach	EB		WB			NB			SB			
HCM Control Delay, s	16.6		21.9			0.2			0.1			
HCM LOS	C		C									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1131	-	-	322 318	1139	-	-					
HCM Lane V/C Ratio	0.008	-	-	0.036 0.335	0.006	-	-					
HCM Control Delay (s)	8.2	-	-	16.6 21.9	8.2	0	-					
HCM Lane LOS	A	-	-	C C	A A	-	-					
HCM 95th %tile Q(veh)	0	-	-	0.1 1.4	0	-	-					

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations					
Traffic Volume (vph)	261	83	20	159	115
Future Volume (vph)	261	83	20	159	115
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases	2		4		
Detector Phase	2	2	4	4	8
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	25.0	25.0	25.0
Total Split (s)	35.0	35.0	25.0	25.0	25.0
Total Split (%)	58.3%	58.3%	41.7%	41.7%	41.7%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	38.1	38.1		10.9	10.9
Actuated g/C Ratio	0.64	0.64		0.18	0.18
v/c Ratio	0.25	0.11		0.61	0.40
Control Delay	6.2	4.2		30.3	24.1
Queue Delay	0.0	0.0		0.1	0.0
Total Delay	6.2	4.2		30.4	24.1
LOS	A	A		C	C
Approach Delay		5.6		30.4	24.1
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 42 (70%), Referenced to phase 2:WBTL, Start of Yellow

Natural Cycle: 50

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.61

Intersection Signal Delay: 15.5

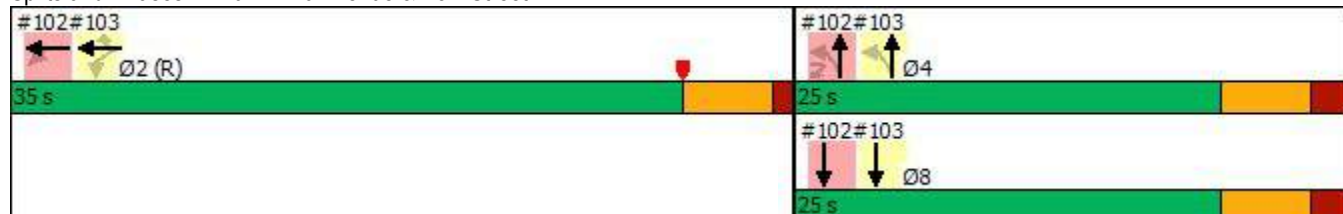
Intersection LOS: B

Intersection Capacity Utilization 44.6%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues














102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	284	127	195	133
v/c Ratio	0.25	0.11	0.61	0.40
Control Delay	6.2	4.2	30.3	24.1
Queue Delay	0.0	0.0	0.1	0.0
Total Delay	6.2	4.2	30.4	24.1
Queue Length 50th (ft)	37	11	66	43
Queue Length 95th (ft)	86	33	111	78
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1123	1135	556	585
Starvation Cap Reductn	0	0	25	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.25	0.11	0.37	0.23
Intersection Summary				






HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street

								
Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	261	83	34	20	159	115	7
Future Volume (vph)	0	261	83	34	20	159	115	7
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor		1.00	1.00			1.00	1.00	
Frpb, ped/bikes		1.00	0.99			1.00	1.00	
Flpb, ped/bikes		1.00	1.00			1.00	1.00	
Frt		1.00	0.96			1.00	0.99	
Flt Protected		0.95	1.00			0.99	1.00	
Satd. Flow (prot)		1770	1767			1852	1848	
Flt Permitted		0.95	1.00			0.94	1.00	
Satd. Flow (perm)		1770	1767			1757	1848	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	0	284	90	37	22	173	125	8
RTOR Reduction (vph)	0	0	14	0	0	0	0	0
Lane Group Flow (vph)	0	284	113	0	0	195	133	0
Confl. Peds. (#/hr)				3				
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2			4			
Actuated Green, G (s)		38.1	38.1			10.9	10.9	
Effective Green, g (s)		38.1	38.1			10.9	10.9	
Actuated g/C Ratio		0.64	0.64			0.18	0.18	
Clearance Time (s)		5.0	5.0			6.0	6.0	
Vehicle Extension (s)		2.0	2.0			2.0	2.0	
Lane Grp Cap (vph)		1123	1122			319	335	
v/s Ratio Prot			0.06				0.07	
v/s Ratio Perm		c0.16				c0.11		
v/c Ratio		0.25	0.10			0.61	0.40	
Uniform Delay, d1		4.8	4.3			22.6	21.7	
Progression Factor		1.00	1.00			1.00	1.00	
Incremental Delay, d2		0.5	0.2			2.4	0.3	
Delay (s)		5.3	4.5			25.0	21.9	
Level of Service		A	A			C	C	
Approach Delay (s)			5.0			25.0	21.9	
Approach LOS			A			C	C	
Intersection Summary								
HCM 2000 Control Delay			13.4			HCM 2000 Level of Service		B
HCM 2000 Volume to Capacity ratio			0.33					
Actuated Cycle Length (s)			60.0			Sum of lost time (s)		11.0
Intersection Capacity Utilization			44.6%			ICU Level of Service		A
Analysis Period (min)			15					
c Critical Lane Group								

Future (2023) Total SYNCHRO Output

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection												
Int Delay, s/veh	1.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	16	0	4	41	7	20	3	286	15	9	444	6
Future Vol, veh/h	16	0	4	41	7	20	3	286	15	9	444	6
Conflicting Peds, #/hr	2	0	0	0	0	2	5	0	2	2	0	5
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	97	97	97	97	97	97	97	97	97	97	97	97
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	16	0	4	42	7	21	3	295	15	9	458	6
Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	809	802	466	792	798	307	469	0	0	312	0	0
Stage 1	484	484	-	311	311	-	-	-	-	-	-	-
Stage 2	325	318	-	481	487	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	299	317	597	307	319	733	1093	-	-	1248	-	-
Stage 1	564	552	-	699	658	-	-	-	-	-	-	-
Stage 2	687	654	-	566	550	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	281	311	594	301	313	730	1088	-	-	1246	-	-
Mov Cap-2 Maneuver	281	311	-	301	313	-	-	-	-	-	-	-
Stage 1	559	544	-	696	655	-	-	-	-	-	-	-
Stage 2	657	651	-	556	542	-	-	-	-	-	-	-
Approach	EB		WB			NB			SB			
HCM Control Delay, s	17.3		17.2			0.1			0.2			
HCM LOS	C		C									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1088	-	-	314	366	1246	-	-				
HCM Lane V/C Ratio	0.003	-	-	0.066	0.192	0.007	-	-				
HCM Control Delay (s)	8.3	-	-	17.3	17.2	7.9	0	-				
HCM Lane LOS	A	-	-	C	C	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.2	0.7	0	-	-				

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations					
Traffic Volume (vph)	220	40	9	80	157
Future Volume (vph)	220	40	9	80	157
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases	2		4		
Detector Phase	2	2	4	4	8
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	20.0	20.0	20.0
Total Split (s)	40.0	40.0	20.0	20.0	20.0
Total Split (%)	66.7%	66.7%	33.3%	33.3%	33.3%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	42.4	42.4		10.0	10.0
Actuated g/C Ratio	0.71	0.71		0.17	0.17
v/c Ratio	0.19	0.06		0.34	0.57
Control Delay	5.2	3.4		24.1	29.7
Queue Delay	0.0	0.0		0.0	0.0
Total Delay	5.2	3.4		24.1	29.7
LOS	A	A		C	C
Approach Delay		4.8		24.1	29.7
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 0 (0%), Referenced to phase 2:WBTL, Start of Yellow

Natural Cycle: 45

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.57

Intersection Signal Delay: 15.4

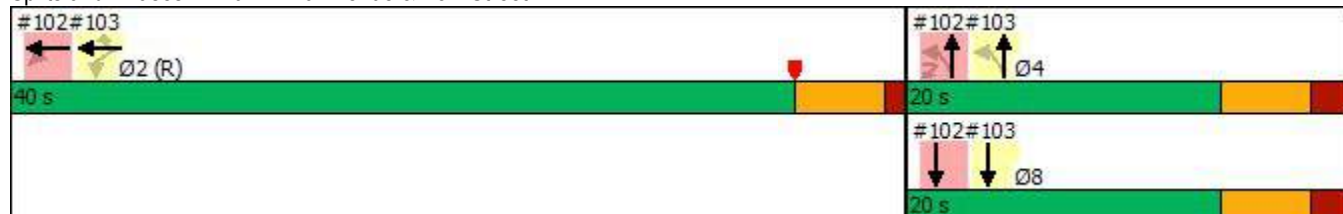
Intersection LOS: B

Intersection Capacity Utilization 33.1%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues














102: N 26 Avenue & Polk Street








Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	242	75	98	176
v/c Ratio	0.19	0.06	0.34	0.57
Control Delay	5.2	3.4	24.1	29.7
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	5.2	3.4	24.1	29.7
Queue Length 50th (ft)	29	5	32	59
Queue Length 95th (ft)	68	20	63	104
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1250	1243	409	433
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.19	0.06	0.24	0.41
Intersection Summary				

HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street

								
Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	220	40	28	9	80	157	3
Future Volume (vph)	0	220	40	28	9	80	157	3
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor		1.00	1.00			1.00	1.00	
Frpb, ped/bikes		1.00	1.00			1.00	1.00	
Flpb, ped/bikes		1.00	1.00			1.00	1.00	
Frt		1.00	0.94			1.00	1.00	
Flt Protected		0.95	1.00			0.99	1.00	
Satd. Flow (prot)		1770	1747			1853	1858	
Flt Permitted		0.95	1.00			0.94	1.00	
Satd. Flow (perm)		1770	1747			1755	1858	
Peak-hour factor, PHF	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Adj. Flow (vph)	0	242	44	31	10	88	173	3
RTOR Reduction (vph)	0	0	10	0	0	0	0	0
Lane Group Flow (vph)	0	242	65	0	0	98	176	0
Confl. Peds. (#/hr)								1
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2			4			
Actuated Green, G (s)		40.2	40.2			8.8	8.8	
Effective Green, g (s)		40.2	40.2			8.8	8.8	
Actuated g/C Ratio		0.67	0.67			0.15	0.15	
Clearance Time (s)		5.0	5.0			6.0	6.0	
Vehicle Extension (s)		2.0	2.0			2.0	2.0	
Lane Grp Cap (vph)		1185	1170			257	272	
v/s Ratio Prot			0.04				c0.09	
v/s Ratio Perm		c0.14				0.06		
v/c Ratio		0.20	0.06			0.38	0.65	
Uniform Delay, d1		3.8	3.4			23.1	24.1	
Progression Factor		1.00	1.00			1.00	1.00	
Incremental Delay, d2		0.4	0.1			0.3	3.9	
Delay (s)		4.2	3.5			23.5	28.1	
Level of Service		A	A			C	C	
Approach Delay (s)			4.0			23.5	28.1	
Approach LOS			A			C	C	
Intersection Summary								
HCM 2000 Control Delay			14.4			HCM 2000 Level of Service		B
HCM 2000 Volume to Capacity ratio			0.28					
Actuated Cycle Length (s)			60.0			Sum of lost time (s)		11.0
Intersection Capacity Utilization			33.1%			ICU Level of Service		A
Analysis Period (min)			15					
c Critical Lane Group								

HCM 2010 TWSC
101: N 28 Avenue & Polk Street

Intersection												
Int Delay, s/veh	3.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	4	3	4	60	21	29	9	385	21	8	390	8
Future Vol, veh/h	4	3	4	60	21	29	9	385	21	8	390	8
Conflicting Peds, #/hr	0	0	0	0	0	0	2	0	0	0	0	2
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	4	3	4	64	22	31	10	410	22	9	415	9
Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	908	892	422	882	885	421	426	0	0	432	0	0
Stage 1	440	440	-	441	441	-	-	-	-	-	-	-
Stage 2	468	452	-	441	444	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	256	281	632	267	284	632	1133	-	-	1128	-	-
Stage 1	596	578	-	595	577	-	-	-	-	-	-	-
Stage 2	575	570	-	595	575	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	225	275	631	259	278	632	1131	-	-	1128	-	-
Mov Cap-2 Maneuver	225	275	-	259	278	-	-	-	-	-	-	-
Stage 1	589	571	-	590	572	-	-	-	-	-	-	-
Stage 2	521	565	-	582	568	-	-	-	-	-	-	-
Approach	EB		WB			NB			SB			
HCM Control Delay, s	16.9		23.3			0.2			0.2			
HCM LOS	C		C									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1131	-	-	314 312	1128	-	-					
HCM Lane V/C Ratio	0.008	-	-	0.037 0.375	0.008	-	-					
HCM Control Delay (s)	8.2	-	-	16.9 23.3	8.2	0	-					
HCM Lane LOS	A	-	-	C C	A A	-	-					
HCM 95th %tile Q(veh)	0	-	-	0.1 1.7	0	-	-					

Timings

102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBL	NBT	SBT
Lane Configurations					
Traffic Volume (vph)	261	84	29	159	115
Future Volume (vph)	261	84	29	159	115
Turn Type	Perm	NA	Perm	NA	NA
Protected Phases		2		4	8
Permitted Phases	2		4		
Detector Phase	2	2	4	4	8
Switch Phase					
Minimum Initial (s)	10.0	10.0	6.0	6.0	6.0
Minimum Split (s)	24.0	24.0	25.0	25.0	25.0
Total Split (s)	35.0	35.0	25.0	25.0	25.0
Total Split (%)	58.3%	58.3%	41.7%	41.7%	41.7%
Yellow Time (s)	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	1.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0
Total Lost Time (s)	5.0	5.0		6.0	6.0
Lead/Lag					
Lead-Lag Optimize?					
Recall Mode	C-Max	C-Max	None	None	None
Act Effect Green (s)	37.6	37.6		11.4	11.4
Actuated g/C Ratio	0.63	0.63		0.19	0.19
v/c Ratio	0.26	0.11		0.63	0.39
Control Delay	6.5	4.4		30.6	23.3
Queue Delay	0.0	0.0		0.1	0.0
Total Delay	6.5	4.4		30.7	23.3
LOS	A	A		C	C
Approach Delay		5.9		30.7	23.3
Approach LOS		A		C	C

Intersection Summary

Cycle Length: 60

Actuated Cycle Length: 60

Offset: 42 (70%), Referenced to phase 2:WBTL, Start of Yellow

Natural Cycle: 50

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.63

Intersection Signal Delay: 15.8

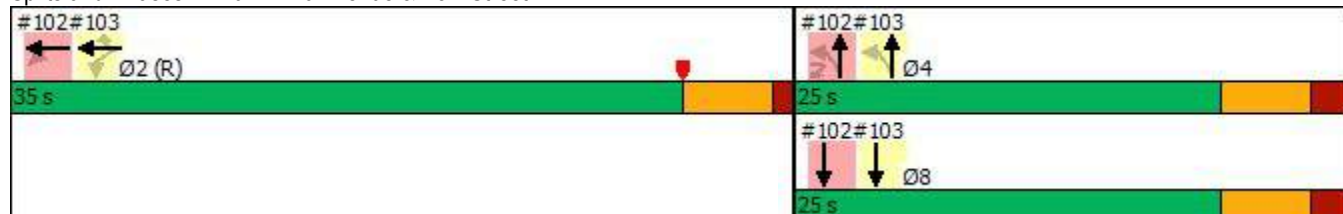
Intersection LOS: B

Intersection Capacity Utilization 45.2%

ICU Level of Service A

Analysis Period (min) 15

Splits and Phases: 102: N 26 Avenue & Polk Street



Queues














102: N 26 Avenue & Polk Street



Lane Group	WBL	WBT	NBT	SBT
Lane Group Flow (vph)	284	128	205	135
v/c Ratio	0.26	0.11	0.63	0.39
Control Delay	6.5	4.4	30.6	23.3
Queue Delay	0.0	0.0	0.1	0.0
Total Delay	6.5	4.4	30.7	23.3
Queue Length 50th (ft)	39	11	69	43
Queue Length 95th (ft)	89	35	115	78
Internal Link Dist (ft)		324	166	123
Turn Bay Length (ft)				
Base Capacity (vph)	1109	1122	542	583
Starvation Cap Reductn	0	0	27	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.26	0.11	0.40	0.23
Intersection Summary				

HCM Signalized Intersection Capacity Analysis

102: N 26 Avenue & Polk Street

								
Movement	EBR2	WBL	WBT	WBR	NBL	NBT	SBT	SBR
Lane Configurations								
Traffic Volume (vph)	0	261	84	34	29	159	115	9
Future Volume (vph)	0	261	84	34	29	159	115	9
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		5.0	5.0			6.0	6.0	
Lane Util. Factor		1.00	1.00			1.00	1.00	
Frpb, ped/bikes		1.00	0.99			1.00	1.00	
Flpb, ped/bikes		1.00	1.00			1.00	1.00	
Frt		1.00	0.96			1.00	0.99	
Flt Protected		0.95	1.00			0.99	1.00	
Satd. Flow (prot)		1770	1768			1848	1844	
Flt Permitted		0.95	1.00			0.92	1.00	
Satd. Flow (perm)		1770	1768			1712	1844	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	0	284	91	37	32	173	125	10
RTOR Reduction (vph)	0	0	14	0	0	0	0	0
Lane Group Flow (vph)	0	284	114	0	0	205	135	0
Confl. Peds. (#/hr)				3				
Turn Type	Perm	Perm	NA		Perm	NA	NA	
Protected Phases			2			4	8	
Permitted Phases	4	2			4			
Actuated Green, G (s)		37.6	37.6			11.4	11.4	
Effective Green, g (s)		37.6	37.6			11.4	11.4	
Actuated g/C Ratio		0.63	0.63			0.19	0.19	
Clearance Time (s)		5.0	5.0			6.0	6.0	
Vehicle Extension (s)		2.0	2.0			2.0	2.0	
Lane Grp Cap (vph)		1109	1107			325	350	
v/s Ratio Prot			0.06				0.07	
v/s Ratio Perm		c0.16				c0.12		
v/c Ratio		0.26	0.10			0.63	0.39	
Uniform Delay, d1		5.0	4.5			22.4	21.2	
Progression Factor		1.00	1.00			1.00	1.00	
Incremental Delay, d2		0.6	0.2			2.9	0.3	
Delay (s)		5.5	4.7			25.3	21.5	
Level of Service		A	A			C	C	
Approach Delay (s)			5.3			25.3	21.5	
Approach LOS			A			C	C	
Intersection Summary								
HCM 2000 Control Delay			13.6			HCM 2000 Level of Service		B
HCM 2000 Volume to Capacity ratio			0.34					
Actuated Cycle Length (s)			60.0			Sum of lost time (s)		11.0
Intersection Capacity Utilization			45.2%			ICU Level of Service		A
Analysis Period (min)			15					
c Critical Lane Group								

APPENDIX I

Polk Street Evaluation

SEGMENT DESCRIPTION

Date Prepared:	3/11/2021	Segment ID No.:	3.11	Speed Limit:	30 mph by State statute
STREET:	Polk St.	Traffic Count ID No.:	3.11.1 043	Posted:	No
From:	N. 26th Ave.	Jurisdiction:	City	Existing Daily Volume (Raw):	1,116
To:	N 28th Ave.	Functional Class:	Local	85th %ile Speed:	38
Length:	1,365 feet	Existing TC Devices:	No	Warrant Score:	15.9

Segment Map:

Segment lies just northwest of City Hall circle.
Traffic count was taken midblock.



Existing Intersection Controls:

West end: 2-way STOP on Polk St.
East end: Traffic signal at N. 28th Ave.



Legend

Intersection Control Type

- 2 way stop EW
- 2 way stop NS
- 3 way Stop
- 4 way stop
- Dead-End Treatment
- Other
- Stop Sign
- Traffic Circle Stop
- Traffic Circle Yield
- Traffic Signal
- Yield

Existing Traffic Calming Devices:

None on Polk St.
Speed humps on Taylor St. one block to the north are at 300 feet/500 feet/520 feet spacings.

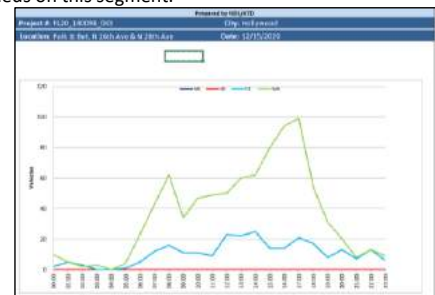
Typical Segment Image:

Midblock looking east



TRAFFIC CALMING ANALYSIS NOTES

1. Citizen comment: Need speed bumps 77% of traffic is westbound, as eastbound traffic is directed to westbound on City Hall circle at N. 26th Ave.
2. After review of options, speed tables are the practical approach to moderate the excessive travel speeds on this segment.
3. It is considered that 2 speed tables, as on Taylor St. to the north, would be sufficient.



RECOMMENDED ACTION

1. Install new standard flattop speed table 440 feet west of brick crosswalk at N. 26th Ave.
2. Install new speed table approx. 410 feet east of centerline of N. 28th Avenue.

These recommendations will place traffic calming devices at 410 feet/515 feet/440 feet spacings along this segment from west to east.



APPENDIX J

Polk Street – Speed and Volume Data

Traffic Survey Specialists, Inc. Daily Eastbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
00:00 - 00:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15 - 00:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:30 - 00:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:45 - 00:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:45 - 01:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 - 02:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45 - 03:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30 - 04:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30 - 05:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15 - 06:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:30 - 06:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:45 - 06:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 - 07:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:15 - 07:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30 - 07:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:45 - 07:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 - 08:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 - 08:29	0	2	0	0	0	0	0	0	0	0	0	0	0	2
08:30 - 08:44	0	0	0	2	1	0	0	0	0	0	0	0	0	3
08:45 - 08:59	0	0	0	0	2	0	0	0	0	0	0	0	0	2
09:00 - 09:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:15 - 09:29	0	0	1	1	1	0	0	0	0	0	0	0	0	3
09:30 - 09:44	0	0	2	0	1	0	0	0	0	0	0	0	0	3
09:45 - 09:59	0	0	1	2	1	0	0	0	0	0	0	0	0	4
10:00 - 10:14	0	0	0	0	0	1	0	0	0	0	0	0	0	1
10:15 - 10:29	0	1	1	0	2	0	0	0	0	0	0	0	0	4
10:30 - 10:44	0	1	0	1	1	0	0	0	0	0	0	0	0	3
10:45 - 10:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 - 11:14	0	0	2	0	0	0	0	0	1	0	0	0	0	3
11:15 - 11:29	0	0	1	2	0	0	0	0	0	0	0	0	0	3
11:30 - 11:44	0	1	0	0	1	0	0	0	1	0	0	0	0	3
11:45 - 11:59	0	1	0	0	0	1	0	0	0	0	0	0	0	2
12:00 - 12:14	0	0	1	1	0	0	0	0	0	0	0	0	0	2
12:15 - 12:29	0	2	2	1	2	1	0	0	0	0	0	0	0	8
12:30 - 12:44	0	0	1	0	2	0	0	0	0	0	0	0	0	3

Traffic Survey Specialists, Inc. Daily Eastbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
12:45 - 12:59	0	2	0	0	1	0	0	0	0	0	0	0	0	3
13:00 - 13:14	0	0	3	1	1	0	0	0	0	0	0	0	0	5
13:15 - 13:29	0	1	2	1	0	0	0	0	0	0	0	0	0	4
13:30 - 13:44	1	0	0	0	0	1	1	0	0	0	0	0	0	3
13:45 - 13:59	2	0	1	0	0	0	0	0	0	0	0	0	0	3
14:00 - 14:14	0	0	1	1	0	1	1	0	0	0	0	0	0	4
14:15 - 14:29	0	1	1	1	1	0	0	0	0	0	0	0	0	4
14:30 - 14:44	0	0	3	2	0	0	0	0	0	0	0	0	0	5
14:45 - 14:59	0	0	0	1	1	1	0	0	0	0	0	0	0	3
15:00 - 15:14	0	0	0	3	0	0	1	0	0	0	0	0	0	4
15:15 - 15:29	0	0	0	0	2	0	0	0	0	0	0	0	0	2
15:30 - 15:44	0	1	0	1	0	1	0	0	0	0	0	0	0	3
15:45 - 15:59	0	0	1	1	0	1	0	0	0	0	0	0	0	3
16:00 - 16:14	0	1	0	0	0	0	0	0	0	0	0	0	0	1
16:15 - 16:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:30 - 16:44	0	0	2	1	0	0	0	0	0	0	0	0	0	3
16:45 - 16:59	0	1	0	1	1	0	0	0	0	0	0	0	0	3
17:00 - 17:14	0	0	2	0	0	0	0	0	0	0	0	0	0	2
17:15 - 17:29	0	0	1	1	1	0	0	0	0	0	0	0	0	3
17:30 - 17:44	1	0	0	0	0	0	0	0	0	0	0	0	0	1
17:45 - 17:59	0	0	2	2	1	0	0	0	0	0	0	0	0	5
18:00 - 18:14	0	0	0	0	0	1	0	0	0	0	0	0	0	1
18:15 - 18:29	0	0	1	0	1	0	1	1	0	0	0	0	0	4
18:30 - 18:44	0	1	1	0	0	0	0	0	0	0	0	0	0	2
18:45 - 18:59	0	0	0	1	1	1	0	0	0	0	0	0	0	3
19:00 - 19:14	0	1	0	0	0	1	0	0	0	0	0	0	0	2
19:15 - 19:29	1	1	3	0	0	0	0	0	0	0	0	0	0	5
19:30 - 19:44	0	1	0	1	0	1	0	0	0	0	0	0	0	3
19:45 - 19:59	0	0	3	1	1	0	0	0	0	0	0	0	0	5
20:00 - 20:14	0	0	0	2	0	0	1	0	0	0	0	0	0	3
20:15 - 20:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
20:30 - 20:44	0	0	0	3	0	1	0	0	1	0	0	0	0	5
20:45 - 20:59	1	0	3	1	1	0	0	0	0	0	0	0	0	6
21:00 - 21:14	0	1	0	1	0	0	0	0	0	0	0	0	0	2
21:15 - 21:29	0	2	0	1	0	0	0	0	0	0	0	0	0	3
21:30 - 21:44	0	0	0	2	1	1	1	0	0	0	0	0	0	5
21:45 - 21:59	0	0	0	0	0	1	0	0	0	0	0	0	0	1
22:00 - 22:14	1	0	0	0	0	0	0	0	0	0	0	0	0	1
22:15 - 22:29	1	0	4	0	1	1	0	0	0	0	0	0	0	7
22:30 - 22:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:45 - 22:59	0	0	0	0	1	1	0	0	0	0	0	0	0	2
23:00 - 23:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:15 - 23:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
23:30 - 23:44	0	0	0	2	0	1	0	0	0	0	0	0	0	3
23:45 - 23:59	1	1	0	0	0	0	0	0	0	0	0	0	0	2
Totals	9	23	47	42	30	18	6	1	3	0	0	0	0	179
Percent of Total	5.0	12.8	26.3	23.5	16.8	10.1	3.4	0.6	1.7	0.0	0.0	0.0	0.0	100
Percent of AM	0.0	16.7	22.2	22.2	27.8	5.6	0.0	0.0	5.6	0.0	0.0	0.0	0.0	100
Percent of PM	6.3	11.9	27.3	23.8	14.0	11.2	4.2	0.7	0.7	0.0	0.0	0.0	0.0	100

Traffic Survey Specialists, Inc.

Daily Eastbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	8.7 MPH	Ten Mile Pace:	20 to 29 MPH	85th Percentile:	35.2 MPH
Mean Speed:	27.3 MPH	Percent in Ten Mile Pace:	49.7%	15th Percentile:	19.5 MPH
Median Speed:	26.4 MPH			90th Percentile:	38.2 MPH
Modal Speed:	23.0 MPH			95th Percentile:	40.5 MPH

Traffic Survey Specialists, Inc. Daily Eastbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
00:00 - 00:14	0	0	0	0	1	0	0	0	0	0	0	0	0	1
00:15 - 00:29	0	0	0	0	0	0	0	1	0	0	0	0	0	1
00:30 - 00:44	0	1	0	0	0	1	0	0	0	1	0	0	0	3
00:45 - 00:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
01:00 - 01:14	0	1	0	1	0	0	0	0	0	0	0	0	0	2
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	0	0	1	0	0	0	0	0	0	0	0	0	0	1
01:45 - 01:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 - 02:14	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:30 - 03:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45 - 03:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30 - 04:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30 - 05:44	1	0	0	0	0	0	1	0	0	0	0	0	0	2
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15 - 06:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:30 - 06:44	0	0	2	0	0	0	0	0	0	0	0	0	0	2
06:45 - 06:59	1	0	0	1	1	0	0	0	0	0	0	0	0	3
07:00 - 07:14	0	0	0	2	0	0	0	0	0	0	0	0	0	2
07:15 - 07:29	0	0	0	0	0	1	0	0	0	0	0	0	0	1
07:30 - 07:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:45 - 07:59	0	0	2	1	0	0	1	0	0	0	0	0	0	4
08:00 - 08:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 - 08:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
08:30 - 08:44	0	0	0	1	4	3	0	0	0	0	0	0	0	8
08:45 - 08:59	0	0	0	0	1	1	0	0	0	0	0	0	0	2
09:00 - 09:14	0	0	0	0	2	0	0	0	0	0	0	0	0	2
09:15 - 09:29	1	0	2	0	1	0	0	0	0	0	0	0	0	4
09:30 - 09:44	0	0	0	2	1	0	0	0	0	0	0	0	0	3
09:45 - 09:59	1	0	1	1	1	0	0	0	0	0	0	0	0	4
10:00 - 10:14	0	0	2	1	2	1	0	0	0	0	0	0	0	6
10:15 - 10:29	0	3	1	0	0	1	1	0	0	0	0	0	0	6
10:30 - 10:44	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:45 - 10:59	1	0	1	0	0	0	0	1	0	0	0	0	0	3
11:00 - 11:14	0	0	0	0	1	0	0	0	0	0	0	0	0	1
11:15 - 11:29	1	0	5	1	1	0	0	0	0	0	0	0	0	8
11:30 - 11:44	1	1	2	1	0	0	0	0	0	1	0	0	0	6
11:45 - 11:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
12:00 - 12:14	1	0	0	0	0	0	0	0	0	0	0	0	0	1
12:15 - 12:29	0	0	1	0	1	0	0	0	0	0	0	0	0	2
12:30 - 12:44	0	2	1	0	0	0	1	0	0	0	0	0	0	4

Traffic Survey Specialists, Inc. Daily Eastbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
12:45 - 12:59	0	1	1	0	0	0	0	0	0	0	0	0	0	2
13:00 - 13:14	0	0	1	2	0	0	0	0	0	0	0	0	0	3
13:15 - 13:29	0	0	1	1	1	0	0	0	0	0	0	0	0	3
13:30 - 13:44	0	1	1	0	0	0	1	0	0	0	0	0	0	3
13:45 - 13:59	1	0	2	3	1	0	0	0	0	0	0	0	0	7
14:00 - 14:14	0	1	3	2	0	0	0	0	0	0	0	0	0	6
14:15 - 14:29	0	2	0	1	1	1	0	0	0	0	0	0	0	5
14:30 - 14:44	0	0	2	0	1	0	0	0	0	0	0	0	0	3
14:45 - 14:59	1	1	1	1	0	1	1	0	0	0	0	0	0	6
15:00 - 15:14	0	0	1	1	0	0	0	0	0	0	0	0	0	2
15:15 - 15:29	0	2	1	0	0	1	0	0	0	0	0	0	0	4
15:30 - 15:44	0	2	0	0	0	1	1	0	0	0	0	0	0	4
15:45 - 15:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
16:00 - 16:14	0	2	1	0	1	0	0	0	0	0	0	0	0	4
16:15 - 16:29	1	1	1	1	0	1	0	0	0	0	0	0	0	5
16:30 - 16:44	0	0	0	2	0	0	0	0	0	0	0	0	0	2
16:45 - 16:59	1	1	0	1	1	0	0	0	0	0	0	0	0	4
17:00 - 17:14	1	1	0	1	0	1	0	0	0	0	0	0	0	4
17:15 - 17:29	2	1	1	1	0	1	1	0	0	0	0	0	0	7
17:30 - 17:44	0	0	2	0	1	0	0	0	0	0	0	0	0	3
17:45 - 17:59	1	0	1	0	3	1	0	0	0	0	0	0	0	6
18:00 - 18:14	0	1	0	0	1	0	0	0	0	0	0	0	0	2
18:15 - 18:29	0	0	0	2	0	0	0	0	0	0	0	0	0	2
18:30 - 18:44	0	0	0	2	1	2	0	0	0	0	0	0	0	5
18:45 - 18:59	0	1	1	1	2	0	0	0	0	0	0	0	0	5
19:00 - 19:14	0	2	2	0	0	1	0	0	0	0	0	0	0	5
19:15 - 19:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
19:30 - 19:44	1	0	1	0	0	0	0	1	0	0	0	0	0	3
19:45 - 19:59	0	0	2	2	1	1	0	0	0	0	0	0	0	6
20:00 - 20:14	0	0	1	1	0	0	0	0	0	0	0	0	0	2
20:15 - 20:29	0	0	1	1	2	0	0	0	0	0	0	0	0	4
20:30 - 20:44	0	1	1	0	0	0	0	0	0	0	0	0	0	2
20:45 - 20:59	0	0	0	1	0	1	0	0	0	0	0	0	0	2
21:00 - 21:14	0	0	1	1	0	0	0	0	0	1	0	0	0	3
21:15 - 21:29	0	1	0	0	1	0	0	0	0	0	0	0	0	2
21:30 - 21:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
21:45 - 21:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:00 - 22:14	0	0	0	0	0	1	0	0	0	0	0	0	0	1
22:15 - 22:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
22:30 - 22:44	0	1	3	1	0	0	0	0	0	0	0	0	0	5
22:45 - 22:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
23:00 - 23:14	0	0	1	1	1	0	0	0	0	0	0	0	0	3
23:15 - 23:29	0	1	0	0	0	0	0	0	0	0	0	0	0	1
23:30 - 23:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:45 - 23:59	0	0	0	0	2	0	0	0	0	0	0	0	0	2
Totals	19	34	57	45	39	22	8	3	0	3	0	0	0	230
Percent of Total	8.3	14.8	24.8	19.6	17.0	9.6	3.5	1.3	0.0	1.3	0.0	0.0	0.0	100
Percent of AM	11.0	9.8	24.4	17.1	19.5	9.8	3.7	2.4	0.0	2.4	0.0	0.0	0.0	100
Percent of PM	6.8	17.6	25.0	20.9	15.5	9.5	3.4	0.7	0.0	0.7	0.0	0.0	0.0	100

Traffic Survey Specialists, Inc.

Daily Eastbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.7 MPH	Ten Mile Pace:	20 to 29 MPH	85th Percentile:	35.3 MPH
Mean Speed:	26.6 MPH	Percent in Ten Mile Pace:	44.3%	15th Percentile:	17.6 MPH
Median Speed:	25.6 MPH			90th Percentile:	38.4 MPH
Modal Speed:	23.0 MPH			95th Percentile:	41.5 MPH

Traffic Survey Specialists, Inc. Daily Eastbound Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
00:00 - 00:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15 - 00:29	0	1	0	0	0	0	0	0	0	0	0	0	0	1
00:30 - 00:44	0	0	0	0	0	0	0	1	0	0	0	0	0	1
00:45 - 00:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	0	0	0	0	1	0	0	0	0	0	0	0	0	1
01:45 - 01:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
02:00 - 02:14	0	1	0	0	0	0	0	0	0	0	0	0	0	1
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:45 - 02:59	0	0	0	0	0	0	1	0	0	0	0	0	0	1
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45 - 03:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:30 - 04:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30 - 05:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15 - 06:29	0	0	1	1	0	0	0	0	0	0	0	0	0	2
06:30 - 06:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:45 - 06:59	0	0	0	2	0	0	0	0	0	0	0	0	0	2
07:00 - 07:14	0	0	1	0	1	1	0	0	0	0	0	0	0	3
07:15 - 07:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30 - 07:44	0	0	0	3	0	0	0	0	0	0	0	0	0	3
07:45 - 07:59	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 - 08:14	0	1	0	0	1	0	0	0	0	0	0	0	0	2
08:15 - 08:29	1	2	0	0	1	0	0	0	0	0	0	0	0	4
08:30 - 08:44	0	0	1	2	1	1	0	0	0	0	0	0	0	5
08:45 - 08:59	0	0	1	1	1	0	2	0	0	0	0	0	0	5
09:00 - 09:14	0	0	0	1	1	0	0	0	0	0	0	0	0	2
09:15 - 09:29	1	1	0	1	0	0	0	0	0	0	0	0	0	3
09:30 - 09:44	0	0	1	1	1	0	0	0	0	0	0	0	0	3
09:45 - 09:59	0	1	1	1	1	0	0	0	0	0	0	0	0	4
10:00 - 10:14	0	0	2	2	1	3	0	0	0	0	0	0	0	8
10:15 - 10:29	1	0	0	0	0	1	0	0	0	0	0	0	0	2
10:30 - 10:44	2	0	2	0	4	0	0	0	0	0	0	0	0	8
10:45 - 10:59	1	1	1	1	0	1	0	0	0	0	0	0	0	5
11:00 - 11:14	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:15 - 11:29	0	0	2	2	1	0	0	0	0	0	0	0	0	5
11:30 - 11:44	2	0	2	2	1	0	0	0	0	0	0	0	0	7
11:45 - 11:59	0	0	3	2	0	0	0	0	0	0	0	0	0	5
12:00 - 12:14	0	0	1	0	0	1	2	0	0	0	0	0	0	4
12:15 - 12:29	2	0	1	1	0	0	2	0	0	0	0	0	0	6
12:30 - 12:44	1	0	0	1	0	0	0	0	0	0	0	0	0	2

Traffic Survey Specialists, Inc. Daily Eastbound Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
12:45 - 12:59	0	0	0	1	1	0	0	0	0	0	0	0	0	2
13:00 - 13:14	0	0	1	3	0	0	0	0	0	0	0	0	0	4
13:15 - 13:29	1	0	1	0	2	0	0	0	0	0	0	0	0	4
13:30 - 13:44	0	0	1	2	0	1	1	0	0	0	0	0	0	5
13:45 - 13:59	1	0	0	1	0	1	0	0	0	0	0	0	0	3
14:00 - 14:14	0	0	3	0	3	0	0	0	0	0	0	0	0	6
14:15 - 14:29	1	1	0	2	1	1	0	0	0	0	0	0	0	6
14:30 - 14:44	0	2	3	0	1	0	1	0	0	0	0	0	0	7
14:45 - 14:59	0	0	1	2	0	0	0	0	0	0	0	0	0	3
15:00 - 15:14	0	0	2	0	0	1	0	0	0	0	0	0	0	3
15:15 - 15:29	0	0	1	0	0	2	0	0	0	0	0	0	0	3
15:30 - 15:44	0	0	1	2	0	0	0	0	0	0	0	0	0	3
15:45 - 15:59	1	0	1	1	0	0	0	0	0	0	0	0	0	3
16:00 - 16:14	0	0	1	0	0	0	0	0	0	0	0	0	0	1
16:15 - 16:29	0	1	0	1	0	0	0	0	0	0	0	0	0	2
16:30 - 16:44	0	0	0	2	1	1	0	0	0	0	0	0	0	4
16:45 - 16:59	0	0	2	0	0	0	0	0	0	0	0	0	0	2
17:00 - 17:14	0	1	1	0	0	0	1	1	0	0	0	0	0	4
17:15 - 17:29	1	1	1	1	0	0	0	0	0	0	0	0	0	4
17:30 - 17:44	0	0	1	0	0	0	0	0	0	0	0	0	0	1
17:45 - 17:59	0	0	0	2	0	0	0	0	0	0	0	0	0	2
18:00 - 18:14	0	1	1	2	0	0	0	0	0	0	0	0	0	4
18:15 - 18:29	0	0	0	1	1	0	0	0	0	0	0	0	0	2
18:30 - 18:44	0	1	2	0	0	2	0	0	0	0	0	0	0	5
18:45 - 18:59	1	1	0	0	0	0	2	0	0	0	0	0	0	4
19:00 - 19:14	0	0	0	1	1	0	0	0	0	0	0	0	0	2
19:15 - 19:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
19:30 - 19:44	0	1	0	2	0	2	0	0	0	0	0	0	0	5
19:45 - 19:59	0	0	0	2	1	1	0	0	0	0	0	0	0	4
20:00 - 20:14	0	0	0	0	1	0	0	0	0	0	0	0	0	1
20:15 - 20:29	0	0	1	0	0	0	1	0	0	0	0	0	0	2
20:30 - 20:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
20:45 - 20:59	0	1	0	1	1	1	0	0	0	0	0	0	0	4
21:00 - 21:14	1	0	3	0	0	0	0	0	0	0	0	0	0	4
21:15 - 21:29	0	0	0	1	1	0	0	0	0	0	0	0	0	2
21:30 - 21:44	0	0	3	1	1	0	0	0	0	0	0	0	0	5
21:45 - 21:59	1	1	0	0	2	0	0	0	0	0	0	0	0	4
22:00 - 22:14	1	1	0	1	0	0	0	0	0	0	0	0	0	3
22:15 - 22:29	0	1	0	0	1	0	0	0	0	0	0	0	0	2
22:30 - 22:44	0	0	0	1	1	0	1	0	0	0	0	0	0	3
22:45 - 22:59	0	0	3	1	1	0	0	0	0	0	0	0	0	5
23:00 - 23:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:15 - 23:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
23:30 - 23:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:45 - 23:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
Totals	23	24	55	59	38	21	14	2	0	0	0	0	0	236
Percent of Total	9.7	10.2	23.3	25.0	16.1	8.9	5.9	0.8	0.0	0.0	0.0	0.0	0.0	100
Percent of AM	12.4	11.2	20.2	25.8	18.0	7.9	3.4	1.1	0.0	0.0	0.0	0.0	0.0	100
Percent of PM	8.2	9.5	25.2	24.5	15.0	9.5	7.5	0.7	0.0	0.0	0.0	0.0	0.0	100

Traffic Survey Specialists, Inc. **Daily Eastbound Speeds (MPH)**

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.2 MPH	Ten Mile Pace:	20 to 29 MPH	85th Percentile:	35.3 MPH
Mean Speed:	26.7 MPH	Percent in Ten Mile Pace:	48.3%	15th Percentile:	18.0 MPH
Median Speed:	26.6 MPH			90th Percentile:	38.7 MPH
Modal Speed:	28.0 MPH			95th Percentile:	41.6 MPH

Traffic Survey Specialists, Inc. Daily Westbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
00:00 - 00:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15 - 00:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:30 - 00:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:45 - 00:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:45 - 01:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 - 02:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45 - 03:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30 - 04:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30 - 05:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15 - 06:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:30 - 06:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:45 - 06:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 - 07:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:15 - 07:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30 - 07:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:45 - 07:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 - 08:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 - 08:29	0	1	1	0	0	0	2	0	0	0	0	0	0	4
08:30 - 08:44	0	0	1	0	4	1	0	0	0	0	0	0	0	6
08:45 - 08:59	0	0	1	0	0	1	0	0	0	0	0	0	0	2
09:00 - 09:14	0	1	1	0	1	1	1	0	0	0	0	0	0	5
09:15 - 09:29	0	1	0	0	3	3	1	0	0	0	0	0	0	8
09:30 - 09:44	0	1	0	1	1	3	2	1	0	0	0	0	0	9
09:45 - 09:59	0	1	0	0	2	2	0	0	0	0	0	0	0	5
10:00 - 10:14	0	1	2	2	3	2	0	0	0	0	0	0	0	10
10:15 - 10:29	0	0	1	0	6	2	0	1	0	0	0	0	0	10
10:30 - 10:44	2	0	0	1	2	1	0	1	0	0	0	0	0	7
10:45 - 10:59	1	1	1	0	2	2	3	0	0	0	0	0	0	10
11:00 - 11:14	0	0	1	1	4	0	1	0	0	0	0	0	0	7
11:15 - 11:29	0	0	2	4	2	0	0	1	0	0	0	0	0	9
11:30 - 11:44	0	1	0	1	6	3	0	1	0	0	0	0	0	12
11:45 - 11:59	0	0	1	1	3	0	0	0	0	0	0	0	0	5
12:00 - 12:14	0	0	0	1	4	1	1	0	0	0	0	0	0	7
12:15 - 12:29	0	0	1	2	3	2	1	0	0	0	0	0	0	9
12:30 - 12:44	0	1	0	1	4	3	1	0	0	0	0	0	0	10

Traffic Survey Specialists, Inc. Daily Westbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
12:45 - 12:59	0	0	2	0	0	0	1	0	0	0	0	0	0	3
13:00 - 13:14	0	0	2	4	3	1	2	0	0	0	0	0	0	12
13:15 - 13:29	0	1	0	1	1	3	0	1	0	0	0	0	0	7
13:30 - 13:44	0	0	0	0	1	0	0	0	0	0	0	0	0	1
13:45 - 13:59	0	1	0	1	1	4	0	0	0	0	0	0	0	7
14:00 - 14:14	0	2	0	0	2	3	1	1	3	0	0	0	0	12
14:15 - 14:29	0	1	0	3	1	5	0	0	0	0	0	0	0	10
14:30 - 14:44	0	2	2	4	2	2	1	0	0	0	0	0	0	13
14:45 - 14:59	0	0	0	1	1	2	1	0	0	0	0	0	0	5
15:00 - 15:14	0	1	1	2	2	5	0	0	0	0	0	0	0	11
15:15 - 15:29	0	0	0	2	1	1	1	1	0	0	0	0	0	6
15:30 - 15:44	0	0	1	0	3	1	1	0	1	0	0	0	0	7
15:45 - 15:59	0	0	0	4	0	2	1	1	0	0	0	0	0	8
16:00 - 16:14	1	0	0	0	4	1	0	0	0	0	0	0	0	6
16:15 - 16:29	0	0	3	4	4	3	2	0	0	0	0	0	0	16
16:30 - 16:44	0	0	1	2	3	2	1	0	0	0	0	0	0	9
16:45 - 16:59	0	0	4	4	3	4	0	0	0	0	0	0	0	15
17:00 - 17:14	0	1	0	4	3	1	0	0	0	0	0	0	0	9
17:15 - 17:29	0	0	3	0	4	2	0	0	0	0	0	0	0	9
17:30 - 17:44	1	2	0	2	2	1	0	0	0	1	0	0	0	9
17:45 - 17:59	0	0	1	1	5	2	1	0	0	0	0	0	0	10
18:00 - 18:14	0	0	0	1	4	3	3	1	0	0	0	0	0	12
18:15 - 18:29	0	0	1	5	1	1	1	1	0	1	0	0	0	11
18:30 - 18:44	0	1	1	3	3	0	0	0	0	0	0	0	0	8
18:45 - 18:59	0	1	2	4	2	4	1	1	1	0	0	0	0	16
19:00 - 19:14	0	0	3	2	2	3	1	0	0	0	0	0	0	11
19:15 - 19:29	2	1	3	1	3	1	1	0	0	0	0	0	0	12
19:30 - 19:44	0	1	0	1	4	1	1	0	0	0	0	0	0	8
19:45 - 19:59	0	1	1	1	4	2	1	0	0	0	0	0	0	10
20:00 - 20:14	1	0	1	2	2	2	1	0	0	0	0	0	0	9
20:15 - 20:29	0	0	1	1	2	5	1	0	0	0	0	0	0	10
20:30 - 20:44	0	0	1	2	1	1	0	1	0	0	0	0	0	6
20:45 - 20:59	0	0	2	1	3	0	0	0	0	0	0	0	0	6
21:00 - 21:14	2	1	3	2	0	0	0	1	0	0	0	0	0	9
21:15 - 21:29	1	0	1	1	3	0	0	0	0	0	0	0	0	6
21:30 - 21:44	0	0	1	0	1	0	2	0	0	0	1	0	0	5
21:45 - 21:59	0	1	1	1	2	1	0	1	0	0	0	0	0	7
22:00 - 22:14	1	0	0	2	2	0	1	0	0	0	0	0	0	6
22:15 - 22:29	1	0	1	2	0	0	0	0	0	0	0	0	0	4
22:30 - 22:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
22:45 - 22:59	0	1	0	2	4	0	0	0	0	0	0	0	0	7
23:00 - 23:14	0	0	1	1	0	0	0	0	1	0	0	0	0	3
23:15 - 23:29	0	1	0	2	0	1	0	0	0	0	0	0	0	4
23:30 - 23:44	0	0	1	0	1	1	0	0	0	0	0	0	0	3
23:45 - 23:59	0	0	1	2	0	1	0	0	0	0	0	0	0	4
Totals	13	30	59	93	140	99	40	15	6	2	1	0	0	498
Percent of Total	2.6	6.0	11.8	18.7	28.1	19.9	8.0	3.0	1.2	0.4	0.2	0.0	0.0	100
Percent of AM	2.8	7.3	11.0	10.1	35.8	19.3	9.2	4.6	0.0	0.0	0.0	0.0	0.0	100
Percent of PM	2.6	5.7	12.1	21.1	26.0	20.1	7.7	2.6	1.5	0.5	0.3	0.0	0.0	100

Traffic Survey Specialists, Inc.

Daily Westbound Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	8.8 MPH	Ten Mile Pace:	30 to 39 MPH	85th Percentile:	40.3 MPH
Mean Speed:	32.0 MPH	Percent in Ten Mile Pace:	48.0%	15th Percentile:	23.2 MPH
Median Speed:	32.3 MPH			90th Percentile:	42.1 MPH
Modal Speed:	33.0 MPH			95th Percentile:	45.8 MPH

Traffic Survey Specialists, Inc. Daily Westbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
00:00 - 00:14	0	1	0	1	0	0	1	0	0	0	0	0	0	3
00:15 - 00:29	0	0	0	1	0	1	1	0	0	0	0	0	0	3
00:30 - 00:44	1	0	0	0	0	0	0	0	0	0	0	0	0	1
00:45 - 00:59	0	0	0	0	0	1	2	0	0	0	0	0	0	3
01:00 - 01:14	0	1	0	0	0	0	0	0	0	0	0	0	0	1
01:15 - 01:29	0	0	0	0	0	1	1	0	0	0	0	0	0	2
01:30 - 01:44	0	0	0	0	0	0	1	0	0	1	0	0	0	2
01:45 - 01:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
02:00 - 02:14	1	0	1	0	0	0	0	0	0	0	0	0	0	2
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	1	0	1	0	0	0	0	0	0	0	0	0	0	2
03:30 - 03:44	0	0	0	1	0	1	0	0	0	0	0	0	0	2
03:45 - 03:59	1	0	0	0	2	0	1	0	0	0	0	0	0	4
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	1	1	0	0	0	0	0	2
04:30 - 04:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:45 - 04:59	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 - 05:14	0	0	0	0	0	0	0	0	1	0	0	0	0	1
05:15 - 05:29	0	0	0	0	0	1	0	0	0	0	0	0	0	1
05:30 - 05:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	1	0	1	1	0	1	0	0	0	0	0	0	4
06:15 - 06:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
06:30 - 06:44	0	0	0	1	2	0	1	0	0	0	0	0	0	4
06:45 - 06:59	0	1	1	3	1	2	0	0	0	0	0	0	0	8
07:00 - 07:14	0	0	1	0	0	1	1	0	0	0	0	0	0	3
07:15 - 07:29	1	0	3	1	0	0	0	0	0	1	0	0	0	6
07:30 - 07:44	0	0	2	1	3	0	1	0	0	0	0	0	0	7
07:45 - 07:59	1	0	2	1	1	1	0	0	0	0	0	0	0	6
08:00 - 08:14	0	0	3	2	2	2	0	0	0	0	0	0	0	9
08:15 - 08:29	0	2	1	2	4	0	1	1	0	0	0	0	0	11
08:30 - 08:44	0	0	1	3	3	2	1	0	0	0	0	0	0	10
08:45 - 08:59	0	0	0	2	3	4	0	0	0	0	0	0	0	9
09:00 - 09:14	0	0	1	3	4	0	0	0	0	0	0	0	0	8
09:15 - 09:29	0	0	1	1	1	4	0	0	0	0	0	0	0	7
09:30 - 09:44	0	0	5	3	4	0	0	1	0	0	0	0	0	13
09:45 - 09:59	0	1	2	3	1	2	1	0	0	0	0	0	0	10
10:00 - 10:14	0	0	1	3	2	1	0	0	0	0	0	0	0	7
10:15 - 10:29	0	2	1	5	0	1	0	0	0	0	1	0	0	10
10:30 - 10:44	0	0	0	0	3	1	1	0	1	0	0	0	0	6
10:45 - 10:59	0	1	2	2	1	1	1	1	0	0	0	0	0	9
11:00 - 11:14	0	0	1	3	3	1	0	1	0	0	0	0	0	9
11:15 - 11:29	0	0	3	1	5	0	2	0	0	0	0	0	0	11
11:30 - 11:44	0	0	2	3	5	2	0	0	0	0	0	0	0	12
11:45 - 11:59	1	0	5	1	1	0	0	0	0	0	0	0	0	8
12:00 - 12:14	0	0	1	5	7	2	3	0	0	0	0	0	0	18
12:15 - 12:29	0	1	1	2	4	3	1	0	0	0	0	0	0	12
12:30 - 12:44	0	1	2	2	1	2	0	0	0	0	0	0	0	8

Traffic Survey Specialists, Inc. Daily Westbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
12:45 - 12:59	0	1	2	6	1	0	0	0	0	0	0	0	0	10
13:00 - 13:14	0	0	2	0	0	0	0	0	0	0	0	0	0	2
13:15 - 13:29	1	0	0	1	0	0	1	0	0	0	0	0	0	3
13:30 - 13:44	0	0	4	3	0	2	0	0	0	0	0	0	0	9
13:45 - 13:59	0	1	2	2	2	1	1	0	0	0	0	0	0	9
14:00 - 14:14	1	1	2	3	1	1	0	0	0	0	0	0	0	9
14:15 - 14:29	0	0	5	2	3	0	1	0	0	0	0	0	0	11
14:30 - 14:44	0	1	2	4	1	1	0	0	0	0	0	0	0	9
14:45 - 14:59	2	0	2	0	0	0	0	1	0	0	0	0	0	5
15:00 - 15:14	0	1	3	1	5	0	1	0	0	0	0	0	0	11
15:15 - 15:29	1	0	0	3	3	2	4	0	0	0	0	0	0	13
15:30 - 15:44	0	1	1	5	6	3	2	0	0	0	0	0	0	18
15:45 - 15:59	0	0	0	0	4	2	0	0	0	0	0	0	0	6
16:00 - 16:14	0	0	1	3	5	3	3	0	0	0	0	0	0	15
16:15 - 16:29	1	1	1	2	4	3	1	1	0	0	0	0	0	14
16:30 - 16:44	0	1	2	1	3	4	1	0	0	0	0	0	0	12
16:45 - 16:59	2	0	1	3	3	4	2	0	0	0	0	0	0	15
17:00 - 17:14	0	0	1	8	14	4	0	0	0	0	0	0	0	27
17:15 - 17:29	1	0	4	10	5	6	4	0	1	0	0	0	0	31
17:30 - 17:44	0	2	0	5	11	6	3	0	0	0	0	0	0	27
17:45 - 17:59	0	0	2	1	1	2	2	0	0	0	0	0	0	8
18:00 - 18:14	0	0	0	2	6	5	2	0	0	1	0	0	0	16
18:15 - 18:29	0	0	1	3	4	2	2	1	0	0	0	0	0	13
18:30 - 18:44	0	0	2	5	4	0	0	1	0	0	0	0	0	12
18:45 - 18:59	0	0	2	2	4	2	1	0	0	0	0	0	0	11
19:00 - 19:14	0	0	0	2	4	3	1	1	0	0	0	0	0	11
19:15 - 19:29	0	0	1	2	1	1	0	0	0	0	0	0	0	5
19:30 - 19:44	0	1	2	1	0	4	0	1	1	0	0	0	0	10
19:45 - 19:59	0	0	2	0	1	1	0	0	0	0	0	0	0	4
20:00 - 20:14	0	0	1	2	1	0	1	0	0	0	0	0	0	5
20:15 - 20:29	0	0	1	1	2	1	0	0	0	0	0	0	0	5
20:30 - 20:44	0	0	1	2	1	1	0	0	0	0	0	0	0	5
20:45 - 20:59	0	1	0	1	4	0	0	0	0	0	0	0	0	6
21:00 - 21:14	0	0	2	2	0	1	0	0	0	0	0	0	0	5
21:15 - 21:29	0	0	0	2	1	2	1	0	0	0	0	0	0	6
21:30 - 21:44	0	1	0	0	1	1	0	0	0	0	0	0	0	3
21:45 - 21:59	0	0	0	0	1	0	0	1	0	0	0	0	0	2
22:00 - 22:14	0	0	1	0	1	1	1	0	0	1	0	0	0	5
22:15 - 22:29	1	0	1	1	1	0	0	0	0	0	0	0	0	4
22:30 - 22:44	0	0	1	1	2	0	0	0	0	0	0	0	0	4
22:45 - 22:59	0	1	0	0	1	1	0	0	0	0	0	0	0	3
23:00 - 23:14	0	3	0	0	0	0	0	1	0	0	0	0	0	4
23:15 - 23:29	1	1	2	0	1	0	1	0	0	0	0	0	0	6
23:30 - 23:44	0	0	0	0	1	0	0	0	0	0	0	0	0	1
23:45 - 23:59	0	1	0	1	0	1	1	0	0	0	0	0	0	4
Totals	19	32	102	151	179	108	60	13	4	4	1	0	0	673
Percent of Total	2.8	4.8	15.2	22.4	26.6	16.0	8.9	1.9	0.6	0.6	0.1	0.0	0.0	100
Percent of AM	3.6	5.0	18.6	22.2	24.0	13.6	8.6	2.3	0.9	0.9	0.5	0.0	0.0	100
Percent of PM	2.4	4.6	13.5	22.6	27.9	17.3	9.1	1.8	0.4	0.4	0.0	0.0	0.0	100

Traffic Survey Specialists, Inc.

Daily Westbound Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	8.6 MPH	Ten Mile Pace:	25 to 34 MPH	85th Percentile:	39.9 MPH
Mean Speed:	31.3 MPH	Percent in Ten Mile Pace:	49.0%	15th Percentile:	22.9 MPH
Median Speed:	31.1 MPH			90th Percentile:	41.4 MPH
Modal Speed:	33.0 MPH			95th Percentile:	44.8 MPH

Traffic Survey Specialists, Inc. Daily Westbound Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
00:00 - 00:14	0	0	0	0	1	0	0	0	0	0	1	0	0	2
00:15 - 00:29	0	0	1	0	1	0	0	0	1	0	0	0	0	3
00:30 - 00:44	0	0	0	0	0	1	1	1	0	0	0	0	0	3
00:45 - 00:59	0	1	0	0	1	0	0	0	0	0	0	0	0	2
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:45 - 01:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 - 02:14	0	0	1	0	0	0	0	0	0	0	0	0	0	1
02:15 - 02:29	0	0	0	0	1	0	0	0	0	0	0	0	0	1
02:30 - 02:44	0	0	1	0	0	1	0	0	0	0	0	0	0	2
02:45 - 02:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 - 03:14	0	0	0	0	0	1	0	0	0	0	0	0	0	1
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:45 - 03:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
04:00 - 04:14	1	0	0	1	1	0	0	0	0	0	0	0	0	3
04:15 - 04:29	1	0	0	0	0	0	0	0	0	0	1	0	0	2
04:30 - 04:44	0	1	0	1	0	0	0	0	0	0	0	0	0	2
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	1	0	0	0	0	0	0	0	0	1
05:30 - 05:44	0	0	0	0	1	1	0	0	0	0	0	0	0	2
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	2	1	0	0	0	0	0	0	0	0	0	3
06:15 - 06:29	1	1	2	2	1	3	0	0	0	0	0	0	0	10
06:30 - 06:44	2	0	0	2	1	1	0	1	0	0	0	0	0	7
06:45 - 06:59	0	0	1	2	1	2	0	0	0	0	0	0	0	6
07:00 - 07:14	0	1	1	0	1	0	0	0	0	0	0	0	0	3
07:15 - 07:29	1	0	2	1	4	0	1	0	0	0	0	0	0	9
07:30 - 07:44	1	2	1	0	1	2	1	0	0	0	0	0	0	8
07:45 - 07:59	0	0	1	0	3	2	1	0	0	0	0	0	0	7
08:00 - 08:14	0	0	1	2	6	1	0	1	2	0	0	0	0	13
08:15 - 08:29	0	4	1	2	1	2	0	1	0	0	0	0	0	11
08:30 - 08:44	0	0	0	0	3	5	2	1	0	0	0	0	0	11
08:45 - 08:59	0	0	1	0	5	3	0	0	0	0	0	0	0	9
09:00 - 09:14	0	1	0	4	3	2	0	1	0	0	0	0	0	11
09:15 - 09:29	0	0	0	1	1	1	1	0	1	1	0	0	0	6
09:30 - 09:44	0	0	4	2	4	3	1	1	0	0	0	0	0	15
09:45 - 09:59	2	2	3	2	2	0	0	0	0	0	0	0	0	11
10:00 - 10:14	0	0	2	2	3	2	0	0	0	0	0	0	0	9
10:15 - 10:29	0	1	0	3	4	0	0	0	0	0	0	0	0	8
10:30 - 10:44	0	1	0	1	1	0	1	1	0	0	0	0	0	5
10:45 - 10:59	1	2	1	1	2	1	2	0	0	0	0	0	0	10
11:00 - 11:14	0	1	3	1	1	0	1	0	0	0	0	0	0	7
11:15 - 11:29	1	0	0	0	8	0	0	0	1	0	0	0	0	10
11:30 - 11:44	2	0	1	4	0	1	0	0	0	0	0	0	0	8
11:45 - 11:59	1	0	0	6	0	3	0	0	0	0	0	0	0	10
12:00 - 12:14	0	0	0	6	1	0	1	0	1	0	0	0	0	9
12:15 - 12:29	1	0	0	4	1	4	2	0	0	0	0	0	0	12
12:30 - 12:44	1	1	1	1	1	3	0	0	0	0	0	0	0	8

Traffic Survey Specialists, Inc. Daily Westbound Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
12:45 - 12:59	0	0	4	7	8	2	1	0	0	0	0	0	0	22
13:00 - 13:14	0	1	1	1	0	3	1	2	0	0	0	0	0	9
13:15 - 13:29	0	0	0	3	6	2	0	1	0	0	0	0	0	12
13:30 - 13:44	2	0	3	1	4	3	1	0	0	0	0	0	0	14
13:45 - 13:59	0	1	0	2	2	5	1	1	0	0	0	0	0	12
14:00 - 14:14	0	2	1	4	3	3	1	0	0	0	0	0	0	14
14:15 - 14:29	1	0	1	3	9	3	0	2	0	0	0	0	0	19
14:30 - 14:44	0	1	1	2	2	1	3	0	0	0	0	0	0	10
14:45 - 14:59	0	0	4	3	2	4	0	1	0	0	0	0	0	14
15:00 - 15:14	0	0	2	1	0	3	3	2	0	0	0	0	0	11
15:15 - 15:29	0	0	2	4	5	3	2	1	0	0	0	0	0	17
15:30 - 15:44	0	1	2	4	10	5	2	0	0	0	0	0	0	24
15:45 - 15:59	0	1	3	2	6	3	2	0	0	0	0	0	0	17
16:00 - 16:14	0	1	3	3	5	6	4	1	0	0	0	0	0	23
16:15 - 16:29	1	0	0	6	4	2	3	0	0	0	0	0	0	16
16:30 - 16:44	0	3	3	6	9	4	1	0	0	0	0	0	0	26
16:45 - 16:59	0	0	0	1	5	3	4	0	0	0	0	0	0	13
17:00 - 17:14	0	0	0	10	6	7	1	0	0	0	0	0	1	25
17:15 - 17:29	0	1	2	0	14	7	1	0	0	0	0	0	0	25
17:30 - 17:44	0	0	1	5	7	4	1	0	0	0	0	0	0	18
17:45 - 17:59	0	0	2	4	7	4	1	0	0	0	0	0	0	18
18:00 - 18:14	0	1	1	2	6	3	2	0	0	0	0	0	0	15
18:15 - 18:29	0	1	3	3	7	6	0	0	0	0	0	0	0	20
18:30 - 18:44	1	1	0	3	4	3	1	1	0	0	0	0	0	14
18:45 - 18:59	1	1	1	2	1	1	1	0	0	0	0	0	0	8
19:00 - 19:14	2	1	0	2	1	1	0	0	0	0	0	0	0	7
19:15 - 19:29	0	0	2	9	2	2	0	0	0	0	0	0	0	15
19:30 - 19:44	0	1	1	2	2	0	0	0	0	0	0	0	0	6
19:45 - 19:59	1	0	0	1	3	1	0	1	1	0	0	0	0	8
20:00 - 20:14	0	1	1	1	1	1	0	0	0	0	0	0	0	5
20:15 - 20:29	0	0	0	1	4	0	0	0	0	0	0	0	0	5
20:30 - 20:44	0	1	0	3	0	1	0	0	0	0	0	0	0	5
20:45 - 20:59	0	0	0	2	1	1	0	0	0	0	0	0	0	4
21:00 - 21:14	0	1	0	1	2	0	1	0	0	0	0	0	0	5
21:15 - 21:29	1	0	0	1	0	0	0	0	0	0	0	0	0	2
21:30 - 21:44	1	0	0	1	0	1	0	0	0	0	0	0	0	3
21:45 - 21:59	2	0	0	1	1	1	0	0	0	0	0	0	0	5
22:00 - 22:14	0	1	1	0	0	1	0	0	0	0	0	0	0	3
22:15 - 22:29	1	1	0	1	0	0	1	0	0	0	0	0	0	4
22:30 - 22:44	0	0	0	1	1	0	0	0	0	0	0	0	0	2
22:45 - 22:59	0	1	1	0	1	0	1	0	0	0	0	0	0	4
23:00 - 23:14	0	0	0	1	0	3	0	0	0	0	0	0	0	4
23:15 - 23:29	0	1	1	0	0	0	1	0	0	1	0	0	0	4
23:30 - 23:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:45 - 23:59	0	0	1	1	1	0	0	0	0	0	0	0	0	3
Totals	31	45	79	163	219	148	56	21	7	2	2	0	1	774
Percent of Total	4.0	5.8	10.2	21.1	28.3	19.1	7.2	2.7	0.9	0.3	0.3	0.0	0.1	100
Percent of AM	6.4	8.1	12.8	17.4	27.2	16.2	5.1	3.4	2.1	0.4	0.9	0.0	0.0	100
Percent of PM	3.0	4.8	9.1	22.6	28.8	20.4	8.2	2.4	0.4	0.2	0.0	0.0	0.2	100

Traffic Survey Specialists, Inc.

Daily Westbound Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.1 MPH	Ten Mile Pace:	25 to 34 MPH	85th Percentile:	39.9 MPH
Mean Speed:	31.5 MPH	Percent in Ten Mile Pace:	49.4%	15th Percentile:	23.0 MPH
Median Speed:	31.9 MPH			90th Percentile:	41.2 MPH
Modal Speed:	33.0 MPH			95th Percentile:	45.3 MPH

Traffic Survey Specialists, Inc. Daily Total Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
00:00 - 00:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:15 - 00:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:30 - 00:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
00:45 - 00:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:45 - 01:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 - 02:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:45 - 03:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30 - 04:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30 - 05:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:15 - 06:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:30 - 06:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:45 - 06:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 - 07:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:15 - 07:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30 - 07:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:45 - 07:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 - 08:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 - 08:29	0	3	1	0	0	0	2	0	0	0	0	0	0	6
08:30 - 08:44	0	0	1	2	5	1	0	0	0	0	0	0	0	9
08:45 - 08:59	0	0	1	0	2	1	0	0	0	0	0	0	0	4
09:00 - 09:14	0	1	1	0	1	1	1	0	0	0	0	0	0	5
09:15 - 09:29	0	1	1	1	4	3	1	0	0	0	0	0	0	11
09:30 - 09:44	0	1	2	1	2	3	2	1	0	0	0	0	0	12
09:45 - 09:59	0	1	1	2	3	2	0	0	0	0	0	0	0	9
10:00 - 10:14	0	1	2	2	3	3	0	0	0	0	0	0	0	11
10:15 - 10:29	0	1	2	0	8	2	0	1	0	0	0	0	0	14
10:30 - 10:44	2	1	0	2	3	1	0	1	0	0	0	0	0	10
10:45 - 10:59	1	1	1	0	2	2	3	0	0	0	0	0	0	10
11:00 - 11:14	0	0	3	1	4	0	1	0	1	0	0	0	0	10
11:15 - 11:29	0	0	3	6	2	0	0	1	0	0	0	0	0	12
11:30 - 11:44	0	2	0	1	7	3	0	1	1	0	0	0	0	15
11:45 - 11:59	0	1	1	1	3	1	0	0	0	0	0	0	0	7
12:00 - 12:14	0	0	1	2	4	1	1	0	0	0	0	0	0	9
12:15 - 12:29	0	2	3	3	5	3	1	0	0	0	0	0	0	17
12:30 - 12:44	0	1	1	1	6	3	1	0	0	0	0	0	0	13

Traffic Survey Specialists, Inc. Daily Total Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
12:45 - 12:59	0	2	2	0	1	0	1	0	0	0	0	0	0	6
13:00 - 13:14	0	0	5	5	4	1	2	0	0	0	0	0	0	17
13:15 - 13:29	0	2	2	2	1	3	0	1	0	0	0	0	0	11
13:30 - 13:44	1	0	0	0	1	1	1	0	0	0	0	0	0	4
13:45 - 13:59	2	1	1	1	1	4	0	0	0	0	0	0	0	10
14:00 - 14:14	0	2	1	1	2	4	2	1	3	0	0	0	0	16
14:15 - 14:29	0	2	1	4	2	5	0	0	0	0	0	0	0	14
14:30 - 14:44	0	2	5	6	2	2	1	0	0	0	0	0	0	18
14:45 - 14:59	0	0	0	2	2	3	1	0	0	0	0	0	0	8
15:00 - 15:14	0	1	1	5	2	5	1	0	0	0	0	0	0	15
15:15 - 15:29	0	0	0	2	3	1	1	1	0	0	0	0	0	8
15:30 - 15:44	0	1	1	1	3	2	1	0	1	0	0	0	0	10
15:45 - 15:59	0	0	1	5	0	3	1	1	0	0	0	0	0	11
16:00 - 16:14	1	1	0	0	4	1	0	0	0	0	0	0	0	7
16:15 - 16:29	0	0	3	4	4	3	2	0	0	0	0	0	0	16
16:30 - 16:44	0	0	3	3	3	2	1	0	0	0	0	0	0	12
16:45 - 16:59	0	1	4	5	4	4	0	0	0	0	0	0	0	18
17:00 - 17:14	0	1	2	4	3	1	0	0	0	0	0	0	0	11
17:15 - 17:29	0	0	4	1	5	2	0	0	0	0	0	0	0	12
17:30 - 17:44	2	2	0	2	2	1	0	0	0	1	0	0	0	10
17:45 - 17:59	0	0	3	3	6	2	1	0	0	0	0	0	0	15
18:00 - 18:14	0	0	0	1	4	4	3	1	0	0	0	0	0	13
18:15 - 18:29	0	0	2	5	2	1	2	2	0	1	0	0	0	15
18:30 - 18:44	0	2	2	3	3	0	0	0	0	0	0	0	0	10
18:45 - 18:59	0	1	2	5	3	5	1	1	1	0	0	0	0	19
19:00 - 19:14	0	1	3	2	2	4	1	0	0	0	0	0	0	13
19:15 - 19:29	3	2	6	1	3	1	1	0	0	0	0	0	0	17
19:30 - 19:44	0	2	0	2	4	2	1	0	0	0	0	0	0	11
19:45 - 19:59	0	1	4	2	5	2	1	0	0	0	0	0	0	15
20:00 - 20:14	1	0	1	4	2	2	2	0	0	0	0	0	0	12
20:15 - 20:29	0	0	1	1	2	5	1	0	0	0	0	0	0	10
20:30 - 20:44	0	0	1	5	1	2	0	1	1	0	0	0	0	11
20:45 - 20:59	1	0	5	2	4	0	0	0	0	0	0	0	0	12
21:00 - 21:14	2	2	3	3	0	0	0	1	0	0	0	0	0	11
21:15 - 21:29	1	2	1	2	3	0	0	0	0	0	0	0	0	9
21:30 - 21:44	0	0	1	2	2	1	3	0	0	0	1	0	0	10
21:45 - 21:59	0	1	1	1	2	2	0	1	0	0	0	0	0	8
22:00 - 22:14	2	0	0	2	2	0	1	0	0	0	0	0	0	7
22:15 - 22:29	2	0	5	2	1	1	0	0	0	0	0	0	0	11
22:30 - 22:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
22:45 - 22:59	0	1	0	2	5	1	0	0	0	0	0	0	0	9
23:00 - 23:14	0	0	1	1	0	0	0	0	1	0	0	0	0	3
23:15 - 23:29	0	1	1	2	0	1	0	0	0	0	0	0	0	5
23:30 - 23:44	0	0	1	2	1	2	0	0	0	0	0	0	0	6
23:45 - 23:59	1	1	1	2	0	1	0	0	0	0	0	0	0	6
Totals	22	53	106	135	170	117	46	16	9	2	1	0	0	677
Percent of Total	3.2	7.8	15.7	19.9	25.1	17.3	6.8	2.4	1.3	0.3	0.1	0.0	0.0	100
Percent of AM	2.1	9.7	13.8	13.1	33.8	15.9	6.9	3.4	1.4	0.0	0.0	0.0	0.0	100
Percent of PM	3.6	7.3	16.2	21.8	22.7	17.7	6.8	2.1	1.3	0.4	0.2	0.0	0.0	100

Traffic Survey Specialists, Inc. Daily Total Speeds (MPH)

Study Date: Sunday, 07/11/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.0 MPH	Ten Mile Pace:	25 to 34 MPH	85th Percentile:	39.6 MPH
Mean Speed:	30.7 MPH	Percent in Ten Mile Pace:	45.1%	15th Percentile:	21.5 MPH
Median Speed:	30.8 MPH			90th Percentile:	40.8 MPH
Modal Speed:	33.0 MPH			95th Percentile:	45.2 MPH

Traffic Survey Specialists, Inc. Daily Total Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
00:00 - 00:14	0	1	0	1	1	0	1	0	0	0	0	0	0	4
00:15 - 00:29	0	0	0	1	0	1	1	1	0	0	0	0	0	4
00:30 - 00:44	1	1	0	0	0	1	0	0	0	1	0	0	0	4
00:45 - 00:59	0	0	0	1	0	1	2	0	0	0	0	0	0	4
01:00 - 01:14	0	2	0	1	0	0	0	0	0	0	0	0	0	3
01:15 - 01:29	0	0	0	0	0	1	1	0	0	0	0	0	0	2
01:30 - 01:44	0	0	1	0	0	0	1	0	0	1	0	0	0	3
01:45 - 01:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
02:00 - 02:14	2	0	1	0	0	0	0	0	0	0	0	0	0	3
02:15 - 02:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:30 - 02:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:45 - 02:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
03:00 - 03:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:15 - 03:29	1	1	1	0	0	0	0	0	0	0	0	0	0	3
03:30 - 03:44	0	0	0	1	0	1	0	0	0	0	0	0	0	2
03:45 - 03:59	1	0	0	0	2	0	1	0	0	0	0	0	0	4
04:00 - 04:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:15 - 04:29	0	0	0	0	0	0	1	1	0	0	0	0	0	2
04:30 - 04:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:45 - 04:59	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 - 05:14	0	0	0	0	0	0	0	0	1	0	0	0	0	1
05:15 - 05:29	0	0	0	0	0	1	0	0	0	0	0	0	0	1
05:30 - 05:44	1	0	0	0	0	0	1	0	0	0	0	0	0	2
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	1	0	1	1	0	1	0	0	0	0	0	0	4
06:15 - 06:29	0	0	1	0	0	0	0	0	0	0	0	0	0	1
06:30 - 06:44	0	0	2	1	2	0	1	0	0	0	0	0	0	6
06:45 - 06:59	1	1	1	4	2	2	0	0	0	0	0	0	0	11
07:00 - 07:14	0	0	1	2	0	1	1	0	0	0	0	0	0	5
07:15 - 07:29	1	0	3	1	0	1	0	0	0	1	0	0	0	7
07:30 - 07:44	0	1	2	1	3	0	1	0	0	0	0	0	0	8
07:45 - 07:59	1	0	4	2	1	1	1	0	0	0	0	0	0	10
08:00 - 08:14	0	0	3	2	2	2	0	0	0	0	0	0	0	9
08:15 - 08:29	0	2	2	2	4	0	1	1	0	0	0	0	0	12
08:30 - 08:44	0	0	1	4	7	5	1	0	0	0	0	0	0	18
08:45 - 08:59	0	0	0	2	4	5	0	0	0	0	0	0	0	11
09:00 - 09:14	0	0	1	3	6	0	0	0	0	0	0	0	0	10
09:15 - 09:29	1	0	3	1	2	4	0	0	0	0	0	0	0	11
09:30 - 09:44	0	0	5	5	5	0	0	1	0	0	0	0	0	16
09:45 - 09:59	1	1	3	4	2	2	1	0	0	0	0	0	0	14
10:00 - 10:14	0	0	3	4	4	2	0	0	0	0	0	0	0	13
10:15 - 10:29	0	5	2	5	0	2	1	0	0	0	1	0	0	16
10:30 - 10:44	1	0	0	0	3	1	1	0	1	0	0	0	0	7
10:45 - 10:59	1	1	3	2	1	1	1	2	0	0	0	0	0	12
11:00 - 11:14	0	0	1	3	4	1	0	1	0	0	0	0	0	10
11:15 - 11:29	1	0	8	2	6	0	2	0	0	0	0	0	0	19
11:30 - 11:44	1	1	4	4	5	2	0	0	0	1	0	0	0	18
11:45 - 11:59	1	0	5	2	1	0	0	0	0	0	0	0	0	9
12:00 - 12:14	1	0	1	5	7	2	3	0	0	0	0	0	0	19
12:15 - 12:29	0	1	2	2	5	3	1	0	0	0	0	0	0	14
12:30 - 12:44	0	3	3	2	1	2	1	0	0	0	0	0	0	12

Traffic Survey Specialists, Inc. Daily Total Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
12:45 - 12:59	0	2	3	6	1	0	0	0	0	0	0	0	0	12
13:00 - 13:14	0	0	3	2	0	0	0	0	0	0	0	0	0	5
13:15 - 13:29	1	0	1	2	1	0	1	0	0	0	0	0	0	6
13:30 - 13:44	0	1	5	3	0	2	1	0	0	0	0	0	0	12
13:45 - 13:59	1	1	4	5	3	1	1	0	0	0	0	0	0	16
14:00 - 14:14	1	2	5	5	1	1	0	0	0	0	0	0	0	15
14:15 - 14:29	0	2	5	3	4	1	1	0	0	0	0	0	0	16
14:30 - 14:44	0	1	4	4	2	1	0	0	0	0	0	0	0	12
14:45 - 14:59	3	1	3	1	0	1	1	1	0	0	0	0	0	11
15:00 - 15:14	0	1	4	2	5	0	1	0	0	0	0	0	0	13
15:15 - 15:29	1	2	1	3	3	3	4	0	0	0	0	0	0	17
15:30 - 15:44	0	3	1	5	6	4	3	0	0	0	0	0	0	22
15:45 - 15:59	0	0	0	0	5	2	0	0	0	0	0	0	0	7
16:00 - 16:14	0	2	2	3	6	3	3	0	0	0	0	0	0	19
16:15 - 16:29	2	2	2	3	4	4	1	1	0	0	0	0	0	19
16:30 - 16:44	0	1	2	3	3	4	1	0	0	0	0	0	0	14
16:45 - 16:59	3	1	1	4	4	4	2	0	0	0	0	0	0	19
17:00 - 17:14	1	1	1	9	14	5	0	0	0	0	0	0	0	31
17:15 - 17:29	3	1	5	11	5	7	5	0	1	0	0	0	0	38
17:30 - 17:44	0	2	2	5	12	6	3	0	0	0	0	0	0	30
17:45 - 17:59	1	0	3	1	4	3	2	0	0	0	0	0	0	14
18:00 - 18:14	0	1	0	2	7	5	2	0	0	1	0	0	0	18
18:15 - 18:29	0	0	1	5	4	2	2	1	0	0	0	0	0	15
18:30 - 18:44	0	0	2	7	5	2	0	1	0	0	0	0	0	17
18:45 - 18:59	0	1	3	3	6	2	1	0	0	0	0	0	0	16
19:00 - 19:14	0	2	2	2	4	4	1	1	0	0	0	0	0	16
19:15 - 19:29	0	0	1	2	1	1	0	0	0	0	0	0	0	5
19:30 - 19:44	1	1	3	1	0	4	0	2	1	0	0	0	0	13
19:45 - 19:59	0	0	4	2	2	2	0	0	0	0	0	0	0	10
20:00 - 20:14	0	0	2	3	1	0	1	0	0	0	0	0	0	7
20:15 - 20:29	0	0	2	2	4	1	0	0	0	0	0	0	0	9
20:30 - 20:44	0	1	2	2	1	1	0	0	0	0	0	0	0	7
20:45 - 20:59	0	1	0	2	4	1	0	0	0	0	0	0	0	8
21:00 - 21:14	0	0	3	3	0	1	0	0	0	1	0	0	0	8
21:15 - 21:29	0	1	0	2	2	2	1	0	0	0	0	0	0	8
21:30 - 21:44	0	1	0	0	1	1	0	0	0	0	0	0	0	3
21:45 - 21:59	0	0	0	0	1	0	0	1	0	0	0	0	0	2
22:00 - 22:14	0	0	1	0	1	2	1	0	0	1	0	0	0	6
22:15 - 22:29	1	0	2	1	1	0	0	0	0	0	0	0	0	5
22:30 - 22:44	0	1	4	2	2	0	0	0	0	0	0	0	0	9
22:45 - 22:59	0	1	0	1	1	1	0	0	0	0	0	0	0	4
23:00 - 23:14	0	3	1	1	1	0	0	1	0	0	0	0	0	7
23:15 - 23:29	1	2	2	0	1	0	1	0	0	0	0	0	0	7
23:30 - 23:44	0	0	0	0	1	0	0	0	0	0	0	0	0	1
23:45 - 23:59	0	1	0	1	2	1	1	0	0	0	0	0	0	6
Totals	38	66	159	196	218	130	68	16	4	7	1	0	0	903
Percent of Total	4.2	7.3	17.6	21.7	24.1	14.4	7.5	1.8	0.4	0.8	0.1	0.0	0.0	100
Percent of AM	5.6	6.3	20.1	20.8	22.8	12.5	7.3	2.3	0.7	1.3	0.3	0.0	0.0	100
Percent of PM	3.5	7.8	16.3	22.2	24.8	15.3	7.7	1.5	0.3	0.5	0.0	0.0	0.0	100

Traffic Survey Specialists, Inc. Daily Total Speeds (MPH)

Study Date: Monday, 07/12/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.1 MPH	Ten Mile Pace:	25 to 34 MPH	85th Percentile:	39.2 MPH
Mean Speed:	30.1 MPH	Percent in Ten Mile Pace:	45.8%	15th Percentile:	21.2 MPH
Median Speed:	30.8 MPH			90th Percentile:	40.5 MPH
Modal Speed:	33.0 MPH			95th Percentile:	44.4 MPH

Traffic Survey Specialists, Inc.

Daily Total Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
00:00 - 00:14	0	0	0	0	1	0	0	0	0	0	1	0	0	2
00:15 - 00:29	0	1	1	0	1	0	0	0	1	0	0	0	0	4
00:30 - 00:44	0	0	0	0	0	1	1	2	0	0	0	0	0	4
00:45 - 00:59	0	1	0	0	1	0	0	0	0	0	0	0	0	2
01:00 - 01:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:15 - 01:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:30 - 01:44	1	0	0	0	1	0	0	0	0	0	0	0	0	2
01:45 - 01:59	0	0	0	1	0	0	0	0	0	0	0	0	0	1
02:00 - 02:14	0	1	1	0	0	0	0	0	0	0	0	0	0	2
02:15 - 02:29	0	0	0	0	1	0	0	0	0	0	0	0	0	1
02:30 - 02:44	1	0	1	0	0	1	0	0	0	0	0	0	0	3
02:45 - 02:59	0	0	0	0	0	0	1	0	0	0	0	0	0	1
03:00 - 03:14	0	0	0	0	0	1	0	0	0	0	0	0	0	1
03:15 - 03:29	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:30 - 03:44	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:45 - 03:59	0	0	0	0	1	0	0	0	0	0	0	0	0	1
04:00 - 04:14	1	0	0	1	1	0	0	0	0	0	0	0	0	3
04:15 - 04:29	1	1	0	0	0	0	0	0	0	0	1	0	0	3
04:30 - 04:44	0	1	0	1	0	0	0	0	0	0	0	0	0	2
04:45 - 04:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 - 05:14	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:15 - 05:29	0	0	0	0	1	0	0	0	0	0	0	0	0	1
05:30 - 05:44	0	0	0	0	1	1	0	0	0	0	0	0	0	2
05:45 - 05:59	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 - 06:14	0	0	2	1	0	0	0	0	0	0	0	0	0	3
06:15 - 06:29	1	1	3	3	1	3	0	0	0	0	0	0	0	12
06:30 - 06:44	2	0	0	2	1	1	0	1	0	0	0	0	0	7
06:45 - 06:59	0	0	1	4	1	2	0	0	0	0	0	0	0	8
07:00 - 07:14	0	1	2	0	2	1	0	0	0	0	0	0	0	6
07:15 - 07:29	1	0	2	1	4	0	1	0	0	0	0	0	0	9
07:30 - 07:44	1	2	1	3	1	2	1	0	0	0	0	0	0	11
07:45 - 07:59	1	1	1	0	3	2	1	0	0	0	0	0	0	9
08:00 - 08:14	0	1	1	2	7	1	0	1	2	0	0	0	0	15
08:15 - 08:29	1	6	1	2	2	2	0	1	0	0	0	0	0	15
08:30 - 08:44	0	0	1	2	4	6	2	1	0	0	0	0	0	16
08:45 - 08:59	0	0	2	1	6	3	2	0	0	0	0	0	0	14
09:00 - 09:14	0	1	0	5	4	2	0	1	0	0	0	0	0	13
09:15 - 09:29	1	1	0	2	1	1	1	0	1	1	0	0	0	9
09:30 - 09:44	0	0	5	3	5	3	1	1	0	0	0	0	0	18
09:45 - 09:59	2	3	4	3	3	0	0	0	0	0	0	0	0	15
10:00 - 10:14	0	0	4	4	4	5	0	0	0	0	0	0	0	17
10:15 - 10:29	1	1	0	3	4	1	0	0	0	0	0	0	0	10
10:30 - 10:44	2	1	2	1	5	0	1	1	0	0	0	0	0	13
10:45 - 10:59	2	3	2	2	2	2	2	0	0	0	0	0	0	15
11:00 - 11:14	1	1	3	1	1	0	1	0	0	0	0	0	0	8
11:15 - 11:29	1	0	2	2	9	0	0	0	1	0	0	0	0	15
11:30 - 11:44	4	0	3	6	1	1	0	0	0	0	0	0	0	15
11:45 - 11:59	1	0	3	8	0	3	0	0	0	0	0	0	0	15
12:00 - 12:14	0	0	1	6	1	1	3	0	1	0	0	0	0	13
12:15 - 12:29	3	0	1	5	1	4	4	0	0	0	0	0	0	18
12:30 - 12:44	2	1	1	2	1	3	0	0	0	0	0	0	0	10

Traffic Survey Specialists, Inc. Daily Total Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

	1-15	15-20	20-25	25-30	30-35	35-40	40-45	45-50	50-55	55-60	60-65	65-70	70-100	Total
12:45 - 12:59	0	0	4	8	9	2	1	0	0	0	0	0	0	24
13:00 - 13:14	0	1	2	4	0	3	1	2	0	0	0	0	0	13
13:15 - 13:29	1	0	1	3	8	2	0	1	0	0	0	0	0	16
13:30 - 13:44	2	0	4	3	4	4	2	0	0	0	0	0	0	19
13:45 - 13:59	1	1	0	3	2	6	1	1	0	0	0	0	0	15
14:00 - 14:14	0	2	4	4	6	3	1	0	0	0	0	0	0	20
14:15 - 14:29	2	1	1	5	10	4	0	2	0	0	0	0	0	25
14:30 - 14:44	0	3	4	2	3	1	4	0	0	0	0	0	0	17
14:45 - 14:59	0	0	5	5	2	4	0	1	0	0	0	0	0	17
15:00 - 15:14	0	0	4	1	0	4	3	2	0	0	0	0	0	14
15:15 - 15:29	0	0	3	4	5	5	2	1	0	0	0	0	0	20
15:30 - 15:44	0	1	3	6	10	5	2	0	0	0	0	0	0	27
15:45 - 15:59	1	1	4	3	6	3	2	0	0	0	0	0	0	20
16:00 - 16:14	0	1	4	3	5	6	4	1	0	0	0	0	0	24
16:15 - 16:29	1	1	0	7	4	2	3	0	0	0	0	0	0	18
16:30 - 16:44	0	3	3	8	10	5	1	0	0	0	0	0	0	30
16:45 - 16:59	0	0	2	1	5	3	4	0	0	0	0	0	0	15
17:00 - 17:14	0	1	1	10	6	7	2	1	0	0	0	0	1	29
17:15 - 17:29	1	2	3	1	14	7	1	0	0	0	0	0	0	29
17:30 - 17:44	0	0	2	5	7	4	1	0	0	0	0	0	0	19
17:45 - 17:59	0	0	2	6	7	4	1	0	0	0	0	0	0	20
18:00 - 18:14	0	2	2	4	6	3	2	0	0	0	0	0	0	19
18:15 - 18:29	0	1	3	4	8	6	0	0	0	0	0	0	0	22
18:30 - 18:44	1	2	2	3	4	5	1	1	0	0	0	0	0	19
18:45 - 18:59	2	2	1	2	1	1	3	0	0	0	0	0	0	12
19:00 - 19:14	2	1	0	3	2	1	0	0	0	0	0	0	0	9
19:15 - 19:29	0	0	2	9	2	2	0	0	0	0	0	0	0	15
19:30 - 19:44	0	2	1	4	2	2	0	0	0	0	0	0	0	11
19:45 - 19:59	1	0	0	3	4	2	0	1	1	0	0	0	0	12
20:00 - 20:14	0	1	1	1	2	1	0	0	0	0	0	0	0	6
20:15 - 20:29	0	0	1	1	4	0	1	0	0	0	0	0	0	7
20:30 - 20:44	0	1	0	3	0	1	0	0	0	0	0	0	0	5
20:45 - 20:59	0	1	0	3	2	2	0	0	0	0	0	0	0	8
21:00 - 21:14	1	1	3	1	2	0	1	0	0	0	0	0	0	9
21:15 - 21:29	1	0	0	2	1	0	0	0	0	0	0	0	0	4
21:30 - 21:44	1	0	3	2	1	1	0	0	0	0	0	0	0	8
21:45 - 21:59	3	1	0	1	3	1	0	0	0	0	0	0	0	9
22:00 - 22:14	1	2	1	1	0	1	0	0	0	0	0	0	0	6
22:15 - 22:29	1	2	0	1	1	0	1	0	0	0	0	0	0	6
22:30 - 22:44	0	0	0	2	2	0	1	0	0	0	0	0	0	5
22:45 - 22:59	0	1	4	1	2	0	1	0	0	0	0	0	0	9
23:00 - 23:14	0	0	0	1	0	3	0	0	0	0	0	0	0	4
23:15 - 23:29	0	1	2	0	0	0	1	0	0	1	0	0	0	5
23:30 - 23:44	0	0	0	0	0	0	0	0	0	0	0	0	0	0
23:45 - 23:59	0	0	1	1	2	0	0	0	0	0	0	0	0	4
Totals	54	69	134	222	257	169	70	23	7	2	2	0	1	1010
Percent of Total	5.3	6.8	13.3	22.0	25.4	16.7	6.9	2.3	0.7	0.2	0.2	0.0	0.1	100
Percent of AM	8.0	9.0	14.8	19.8	24.7	13.9	4.6	2.8	1.5	0.3	0.6	0.0	0.0	100
Percent of PM	4.1	5.8	12.5	23.0	25.8	18.1	8.0	2.0	0.3	0.1	0.0	0.0	0.1	100

Traffic Survey Specialists, Inc. Daily Total Speeds (MPH)

Study Date: Tuesday, 07/13/2021

Unit ID:

Location: Polk Street Between N 28th Ave & N 26th Ave.

Comments: Hollywood, Florida

Standard Deviation:	9.4 MPH	Ten Mile Pace:	25 to 34 MPH	85th Percentile:	39.3 MPH
Mean Speed:	30.4 MPH	Percent in Ten Mile Pace:	47.4%	15th Percentile:	21.3 MPH
Median Speed:	30.6 MPH			90th Percentile:	40.3 MPH
Modal Speed:	33.0 MPH			95th Percentile:	44.6 MPH

APPENDIX K

Polk Street Queueing Data

Traffic Survey Specialists, Inc. 85 SE 4th Avenue, Unit 109, Delray Beach, Florida 33483
Maximum Observed Queue of Westbound Traffic at Polk Street & N 28th Avenue
Hollywood, Florida July 13th, 2021
Observed By: Luis Palomino Westbound Polk Street at N 28th Avenue

7:00 AM	0
7:15 AM	0
7:30 AM	0
7:45 AM	0
8:00 AM	2
8:15 AM	0
8:30 AM	2
8:45 AM	0

4:00 PM	3
4:15 PM	2
4:30 PM	3
4:45 PM	2
5:00 PM	4
5:15 PM	4
5:30 PM	2
5:45 PM	3