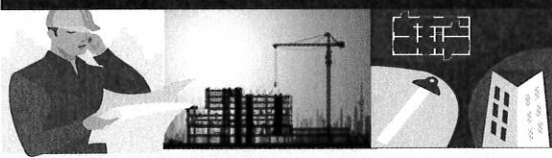


ATTACHMENT I

Application Package

PLANNING & DEVELOPMENT SERVICES



File No. (internal use only): _____

2600 Hollywood Boulevard Room 315
Hollywood, FL 33022

GENERAL APPLICATION



Tel: (954) 921-3471
Fax: (954) 921-3347

This application must be completed in full and submitted with all documents to be placed on a Board or Committee's agenda.

The applicant is responsible for obtaining the appropriate checklist for each type of application.

Applicant(s) or their authorized legal agent must be present at all Board or Committee meetings.

At least one set of the submitted plans for each application must be signed and sealed (i.e. Architect or Engineer).

Documents and forms can be accessed on the City's website at
http://www.hollywoodfl.org/comm_planning/appforms.htm



APPLICATION TYPE (CHECK ONE):

- ☒ Technical Advisory Committee ☐ Historic Preservation Board
☒ City Commission * ☒ Planning and Development Board

Date of Application: 9-15-2014*

Location Address: 3001 Taft Street, 2301 N 29 Ave (Okomo); 2900 Sheridan St (FDOT)

Lot(s): See exhibit A for legal description Block(s): _____ Subdivision: _____

Folio Number(s): 514208010070, 5142090030, 0031 (okomo); 5042090010, 0021 (FDOT)

Zoning Classification: Planned Development (PD) Land Use Classification: TOD

Existing Property Use: Mobile Homes, Park and Ride lot Sq Ft/Number of Units: 200 +/-

Is the request the result of a violation notice? () Yes (X) No If yes, attach a copy of violation.

Has this property been presented to the City before? If yes, check all that apply and provide File Number(s) and Resolution(s): O-2007-08, O-2007-35 (am.2008-35), R-2008-401, R-2008-252

- ☒ Economic Roundtable ☒ Technical Advisory Committee ☐ Historic Preservation Board
☒ City Commission ☒ Planning and Development

Explanation of Request: Site plan approval for 336 residential units in 3-story building with 1-story clubhouse. Approval for site plan for public park.* Rezoning approval PDD Submittal 9-15-2014 *

Number of units/rooms: 336 units Sq Ft: _____

Value of Improvement: \$34 million Estimated Date of Completion: August 2016

Will Project be Phased? (X) Yes () No If Phased, Estimated Completion of Each Phase
Phase I Completion - August 2016
Phase II - TBD

Name of Current Property Owner: Okomo Associates, LLC. attn: David Deutch

Address of Property Owner: 9400 South Dadeland Blvd., Suite 100, Miami, FL 33156

Telephone: 305-854-7100 Fax: 305-859-9858 Email Address: david@pinnaclehousing.com

Name of Consultant/Representative/Tenant (circle one): Calvin, Giordano & Associates, Inc.

Address: 1800 Eller Drive, Suite 600, Fort Lauderdale, FL 33316 Telephone: 954-921-7781

Fax: 954-921-8807 Email Address: JDolan@cgasolutions.com

Date of Purchase: _____ Is there an option to purchase the Property? Yes () No (X)

If Yes, Attach Copy of the Contract.

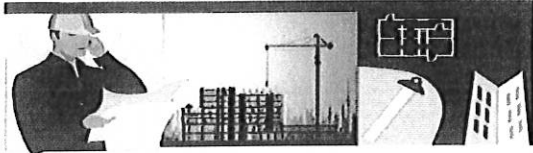
List Anyone Else Who Should Receive Notice of the Hearing: _____

Address: _____

Email Address: _____

Debbie Orshefsky, Greenberg Traurig (Okomo counsel)
401 E. Las Olas Blvd. Suite 2000, Ft. Lauderdale, FL 33301
954-768-8234; fax 954-759-5534; OrshefskyD@gtlaw.com

PLANNING & DEVELOPMENT SERVICES



2600 Hollywood Boulevard Room 315
Hollywood, FL 33022

GENERAL APPLICATION

CERTIFICATION OF COMPLIANCE WITH APPLICABLE REGULATIONS

The applicant/owner(s) signature certifies that he/she has been made aware of the criteria, regulations and guidelines applicable to the request. This information can be obtained in Room 315 of City Hall or on our website at www.hollywoodfl.org. The owner(s) further certifies that when required by applicable law, including but not limited to the City's Zoning and Land Development Regulations, they will post the site with a sign provided by the Office of Planning and Development Services. The owner(s) will photograph the sign the day of posting and submit photographs to the Office of Planning and Development Services as required by applicable law. Failure to post the sign will result in violation of State and Municipal Notification Requirements and Laws.

(I)(We) certify that (I) (we) understand and will comply with the provisions and regulations of the City's Zoning and Land Development Regulations, Design Guidelines, Design Guidelines for Historic Properties and City's Comprehensive Plan as they apply to this project. (I)(We) further certify that the above statements and drawings made on any paper or plans submitted herewith are true to the best of (my)(our) knowledge. (I)(We) understand that the application and attachments become part of the official public records of the City and are not returnable.

Signature of Current Owner: David O. Deutch Date: 9/11/14

PRINT NAME: David O. Deutch, Okomo Associates, LLC Date: _____

Signature of Consultant/Representative: Hoyt Holden Date: 9/12/14

PRINT NAME: Jean Jordan, AICP, Calvin, Giordano & Associates Inc. Date: _____

Signature of Tenant: _____ Date: _____

PRINT NAME: _____ Date: _____

CURRENT OWNER POWER OF ATTORNEY

I am the current owner of the described real property and that I am aware of the nature and effect the request for (project description) _____ to my property, which is hereby made by me or I am hereby authorizing (name of the representative) _____ to be my legal representative before the _____ (Board and/or Committee) relative to all matters concerning this application.

Sworn to and subscribed before me
this _____ day of _____

SIGNATURE OF CURRENT OWNER

Notary Public State of Florida

PRINT NAME

My Commission Expires: _____ (Check One) _____ Personally known to me; OR _____

PLANNING & DEVELOPMENT SERVICES



File No. (internal use only): _____

2600 Hollywood Boulevard Room 315
Hollywood, FL 33022

GENERAL APPLICATION



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Value of Improvement: \$34 million Estimated Date of Completion: August 2016

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Phase I Completion - August 2016

Phase II - TBD

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Address: 1800 Eller Drive, Suite 600, Fort Lauderdale, FL 33316 Telephone: 954-921-7781

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Date of Purchase: _____ Is there an option to purchase the Property? Yes () No (X)

If Yes, Attach Copy of the Contract.

List Anyone Else Who Should Receive Notice of the Hearing: _____

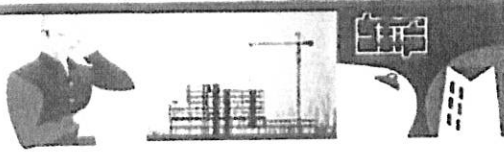
Address: _____

Email Address: _____

Debbie Orshefsky, Greenberg Traurig (Okomo counsel)

401 E. Las Olas Blvd. Suite 2000, Ft. Lauderdale, FL 33301
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PLANNING & DEVELOPMENT SERVICES



2600 Hollywood Boulevard Room 315
Hollywood, FL 33022

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Signature of Current Owner John Portera Date 09/12/2014

PRINT NAME John Portera, FDOT Date _____

Signature of Consultant/Representative Lynn Zolezzi Date 9/12/14

PRINT NAME Lynn Zolezzi, AICP, Land Design South, Inc. Date _____

Signature of Tenant N/A Date _____

PRINT NAME _____ Date _____

CURRENT OWNER POWER OF ATTORNEY

I am the current owner of the described real property and that I am aware of the nature and effect the request for (project description) Master Plan Modifications to my property, which is hereby made by me or I am hereby authorizing (name of the representative) Lynn Zolezzi, AICP to be my legal representative before the FLC, FDOT & City Commission (Board and/or Committee) relative to all matters concerning this application.

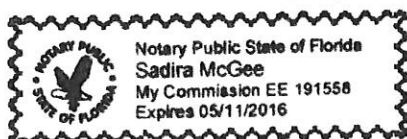
Sworn to and subscribed before me
this 15th day of SEPTEMBER, 2014

Sadira McGee
Notary Public State of Florida

John Portera
SIGNATURE OF CURRENT OWNER

JOHN PORTERA, FDOT
PRINT NAME

My Commission Expires 5/11/2016 (Check One) ☒ Personally known to me/ OR _____



LAND DESCRIPTION
SHERIDAN STATION PLAT
CITY OF HOLLYWOOD, BROWARD COUNTY, FLORIDA

All of Parcels A, B, C and D of SHERIDAN STATION PLAT according to the plat thereof as recorded in Plat Book 181, Pages 51 through 55 of the Public Records of Broward County, Florida. Said lands lying in the City of Hollywood, Broward County, Florida.

SHERIDAN STATION ZONING MODIFICATION JUSTIFICATION STATEMENT

The project, previously known as Sheridan Stationside Village, is the 40± acre property bounded by Sheridan and Taft Streets on the north and south and I-95 and the CSX (TriRail) railroad tracks on the east and west. The City of Hollywood approved land use, zoning and plat approvals for the project in 2007.

The following development approvals were obtained for the combined 40-acre property:

- (1) City of Hollywood Ordinance No. 2007-28 changed the City's land use plan designation to Transit Oriented Development ("TOD");
- (2) Broward County Ordinance No. 2007-28 changed the County's the land use plan designation to TOD;
- (3) City of Hollywood Ordinance No. 2007-35, as amended by Ordinance No. 2008-35, rezoned the property to Planned Development ("PD");
- (4) City of Hollywood Resolution No. 2008-401 approved the Site Plan for the project;
- (5) City of Hollywood Resolution No. R-2008-252 was for plat approval. The Broward County Board of County Commissioners approved the amended plat on October 9, 2007.

FDOT is the fee simple owner of the approximately 18 acres of the property which includes the Sheridan TriRail station. Okomo Associates, LLC is the fee simple owner of the remaining 22 acres which is adjacent to the Tri-Rail station between Sheridan and Taft and bounded on the east by I-95.

During the original approval process, Okomo was leasing the 18 acre FDOT property, however, that lease was terminated on May 30, 2012. The entitlements approved for Sheridan Stationside Village are now being allocated between the two owners and Okomo is amending and moving forward with the first 500 unit residential phase of the project and calling it Sheridan Station.

The original PD Master Plan and Design Guidelines had no internal separations between the parcels owned by FDOT and Okomo due to the lease that was in place at the time. Now that the parcels have been separated, the Master Plan and Design Guidelines must also be revised to reflect the fact that the residential (Okomo) and mixed use (FDOT) parcels will be developed at different times.

The conditions presented in this scenario meet the criteria established for the approval of zoning modifications. Those criteria are as follows:

- (a) That the petition for a change of zoning district will not result in spot zoning or contract zoning.

Response: The Applicant is not requesting a change in zoning district. The property is currently designated PD and will remain as such after the modification to the PD Master Plan and PD Design Guidelines are approved. The request, therefore, meets this condition of approval.

- (b) That the proposed change is consistent with, and in furtherance of, the Goals, Objectives and Policies of the City's Comprehensive Plan.

Response: The Applicant is currently in the process of amending the phasing requirements for the project in the City and County Comprehensive Plans. The Land Use Plan Application addresses consistency with the goals, objectives and policies in the both the city and county plans and that consistency has not been an issue during the review process to date.

- (c) That conditions have substantially changed from the date the present zoning district classification was placed on the property, which makes the passage of the proposed change necessary.

Response: As noted in the introduction, the termination of the lease between Okomo and FDOT which occurred after the present PD zoning was approved is a change in conditions that require the amendments to the PD Master Plan and Design Guidelines for the project.

- (d) That the proposed change will not adversely influence living conditions in the neighborhood.

Response: The proposed modifications to the PD Master Plan and Design Guidelines are minor and the impact analysis included with the application show that there are no significant impacts from the proposed change.

- (e) That the proposed change is compatible with the development(s) within the same district/neighborhood.

Response: The proposed modifications to the PD Master Plan and Design Guidelines are minor and are compatible with the original intent of the PD adopted to implement the objectives of the TOD land use designation for the property.

ATTACHMENT II

Previous Ordinances

ORDINANCE NO. O-2007-35

(05-ZJ-72c)

AN ORDINANCE OF THE CITY OF HOLLYWOOD, FLORIDA, CHANGING THE ZONING DESIGNATION OF THE PROPERTY GENERALLY LOCATED NORTH OF TAFT STREET, SOUTH OF SHERIDAN STREET, EAST OF THE CSX RAILWAY AND WEST OF I-95 AS MORE SPECIFICALLY DESCRIBED IN EXHIBIT "A", FROM C-4 (MEDIUM HIGH INTENSITY COMMERCIAL), TD (TRAILER DISTRICT) AND IM-1 (LOW INTENSITY INDUSTRIAL AND MANUFACTURING DISTRICT) TO PD (PLANNED DEVELOPMENT DISTRICT); APPROVING THE PLANNED DEVELOPMENT (PD) MASTER DEVELOPMENT PLAN FOR THE SUBJECT PROPERTY (HEREINAFTER KNOWN AS "SHERIDAN STATIONSIDE VILLAGE MASTER PLAN"); AND AMENDING THE CITY'S ZONING MAP TO REFLECT THE CHANGE IN ZONING DESIGNATION.

WHEREAS, the Zoning and Land Development Regulations provide that an application for a change of zone may be filed; and

WHEREAS, an application (05-ZJ-72c) was filed with the Office of Planning requesting a change of zone from C-4 (Medium High Intensity Commercial), TD (Trailer District), and IM-1 (Low Intensity Industrial and Manufacturing District) to PD (Planned Development District), for property generally located north of Taft Street, south of Sheridan Street, east of the CSX railway and west of I-95, with approximately 38.32 net acres/40.37 gross acres as more particularly described in Exhibit "A" attached hereto and incorporated herein by reference ('subject property'); and

WHEREAS, the purpose of this request for change in zoning designation is to allow the development of the site which currently consists of a Tri-Rail Station, Park & Ride lot and a residential trailer park (mostly vacant) with approximately 38.32 net acres in size to proceed in accordance with the PD (Planned Development District) Ordinance and the City's Comprehensive Plan Transit Oriented Development requirements, in order to construct a mixed use planned development consisting of both residential and non-residential uses as more specifically set forth in Exhibit "B" attached hereto and incorporated herein by reference; and

WHEREAS, the existing property has a current city zoning designation of C-4, TD and IM-1 and a Future Land Use Designation of Low/Medium Residential (6-10 units/acre), Transportation and Industrial; and

WHEREAS, the applicant is in the process of obtaining a Future Land Use Designation of TOD (Transit Oriented Development) for the existing property; and

WHEREAS, the subject property is adjacent to IM-1 and GU to the north and south, GU and I-95 on the east, and Transportation, IM-1 and C-5 on the west; and

WHEREAS, the PD ordinance requires that a Master Development Plan be submitted by the applicant for review by City staff and the Planning and Zoning Board; and

WHEREAS, such Master Development Plan shall include (1) a boundary survey; (2) schematic representation of the land uses; (3) delineation of internal circulation; (4) points of connection of the local streets to the trafficways; (5) general location and size of any community facility to be included in the PD; (6) an indication of existing vegetation and other natural features with plans for conservation and mitigation; (7) schematic depiction of existing and proposed surface water management elements; and (8) schematic depiction of the water distribution and wastewater collection facilities and drainage system; and

WHEREAS, as this property will have a land use designation of Transit Oriented Development ("TOD"), the TOD is required to meet development thresholds set forth in the City's Comprehensive Plan Land Use Element for TOD's; and

WHEREAS, the Director of the Office of Planning and City staff, following analysis of the proposed Master Development Plan and its associated documents, have determined that the proposed Plan is consistent with the Zoning and Land Development Regulations, is consistent with the City of Hollywood Comprehensive Plan, and has therefore recommended that the Master Development Plan be approved; and

WHEREAS, the Director of the Office of Planning, following analysis of the application and its associated documents, has determined that the proposed change of zoning is consistent with the Zoning and Land Development Regulations, is consistent with the City of Hollywood Comprehensive Plan and the City-Wide Master Plan, and has therefore recommended that it be approved; and

WHEREAS, on October 23, 2007, the Planning and Zoning Board met and reviewed the above noted request for a change of zone to PD (Planned Development District) and approval of the proposed Master Development Plan and have forwarded a recommendation of approval to the City Commission; and

WHEREAS, subsequent to the Planning and Zoning Board meeting, the Developer and Office of Planning staff agreed to place conditions on the proposed Master Plan as set forth in Exhibit "C" attached hereto and incorporated herein by reference; and

WHEREAS, the City Commission finds that the rezoning request to PD (Planned Development District), along with approval of the Master Development Plan, are consistent with the Zoning and Land Development Regulations, are consistent with the City of Hollywood's Comprehensive Plan, and is in the best interest of the citizens of the City of Hollywood;

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COMMISSION OF THE CITY OF HOLLYWOOD, FLORIDA:

Section 1: That the applicant has presented competent substantial evidence that the rezoning request to PD is consistent with the Zoning and Land Development Regulations, is consistent with the City of Hollywood's Comprehensive Plan, and there is no legitimate public purpose in maintaining the existing zoning.

Section 2: That the subject property as more particularly described in Exhibit "A" is hereby rezoned from the zoning designations of C-4 (Medium High Intensity Commercial), TD (Trailer District) and IM-1 (Low Intensity Industrial and Manufacturing District) to PD (Planned Development District).

Section 3: That the Sheridan Stationside Village Master Development Plan as more specifically described in Exhibit "B" and Exhibit "C" attached hereto and incorporated herein by reference, is hereby approved.

Section 4: That the Official Zoning Map of the City of Hollywood is hereby amended to incorporate the above described change in zoning designation for the subject parcel.

Section 5: That all sections or parts of sections of the Zoning and Land Development Regulations, Code of Ordinances, and all ordinances or parts thereof and all resolutions or parts thereof in conflict herewith are hereby repealed to the extent of such conflict.

Section 6: That if any word, phrase, clause, subsection or section of this ordinance is for any reason held unconstitutional or invalid, the invalidity thereof shall not affect the validity of any remaining portions of this ordinance.

(05-ZJ-72c) ORDINANCE REZONING PROPERTY NORTH OF TAFT STREET, SOUTH OF SHERIDAN STREET, EAST OF THE CSX RAILWAY AND WEST OF I-95 (SHERIDAN STATIONSIDE VILLAGE) TO PD AND APPROVING THE MASTER DEVELOPMENT PLAN

Section 7: That this Ordinance shall be in full force and effect immediately upon its passage and adoption.

Advertised Dec 7, 2007.

PASSED on first reading this 5 day of Dec, 2007.

PASSED AND ADOPTED on second reading this 19 day of Dec, 2007.

RENDERED this 28 day of Dec, 2007.


MARA GIULIANI, MAYOR

ATTEST:

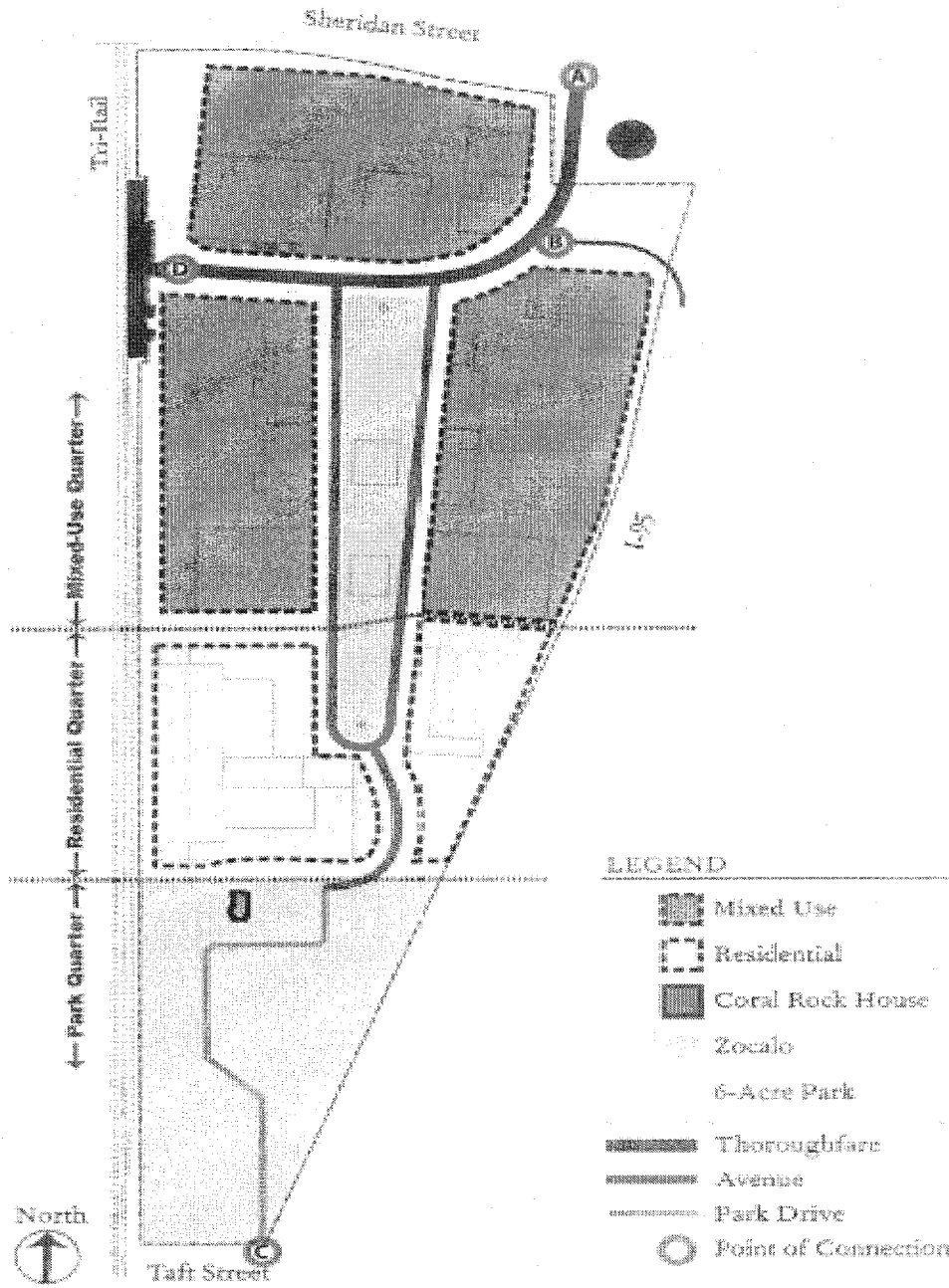

PATRICIA A. CERNY, MMC, CITY CLERK

APPROVED AS TO FORM & LEGALITY
for the use and reliance of the
City of Hollywood, Florida, only.


ALAN FALLIK, INTERIM CITY ATTORNEY

12/26/07pac

Exhibit "B"



LEGEND

- Mixed Use
- Residential
- Coral Rock House
- Zocalo
- 6-Acre Park
- Thoroughfare
- Avenue
- Park Drive
- Point of Connection

GENERAL NOTES

1. The site is located within the boundaries of the City of Hollywood, Florida, and is subject to the City's Comprehensive Zoning Ordinance.
2. The site is currently zoned C-1 (Community Center) and is proposed to be rezoned to a higher density residential zone.
3. The proposed development consists of a 10-story mixed-use building with ground-floor retail and residential units above.
4. The development is designed to be pedestrian-friendly and include a public plaza (Zocalo) and a park area.
5. The project is consistent with the City's Comprehensive Zoning Ordinance and the Hollywood Station Side Village Master Plan.

PROPOSED DEVELOPMENT

| Item | Description | Quantity |
|------|-----------------------|----------|
| 1 | Residential Units | 100 |
| 2 | Commercial Units | 50 |
| 3 | Public Plaza (Zocalo) | 1 |
| 4 | Park Area | 6 Acres |

Table 1: Land Use Data

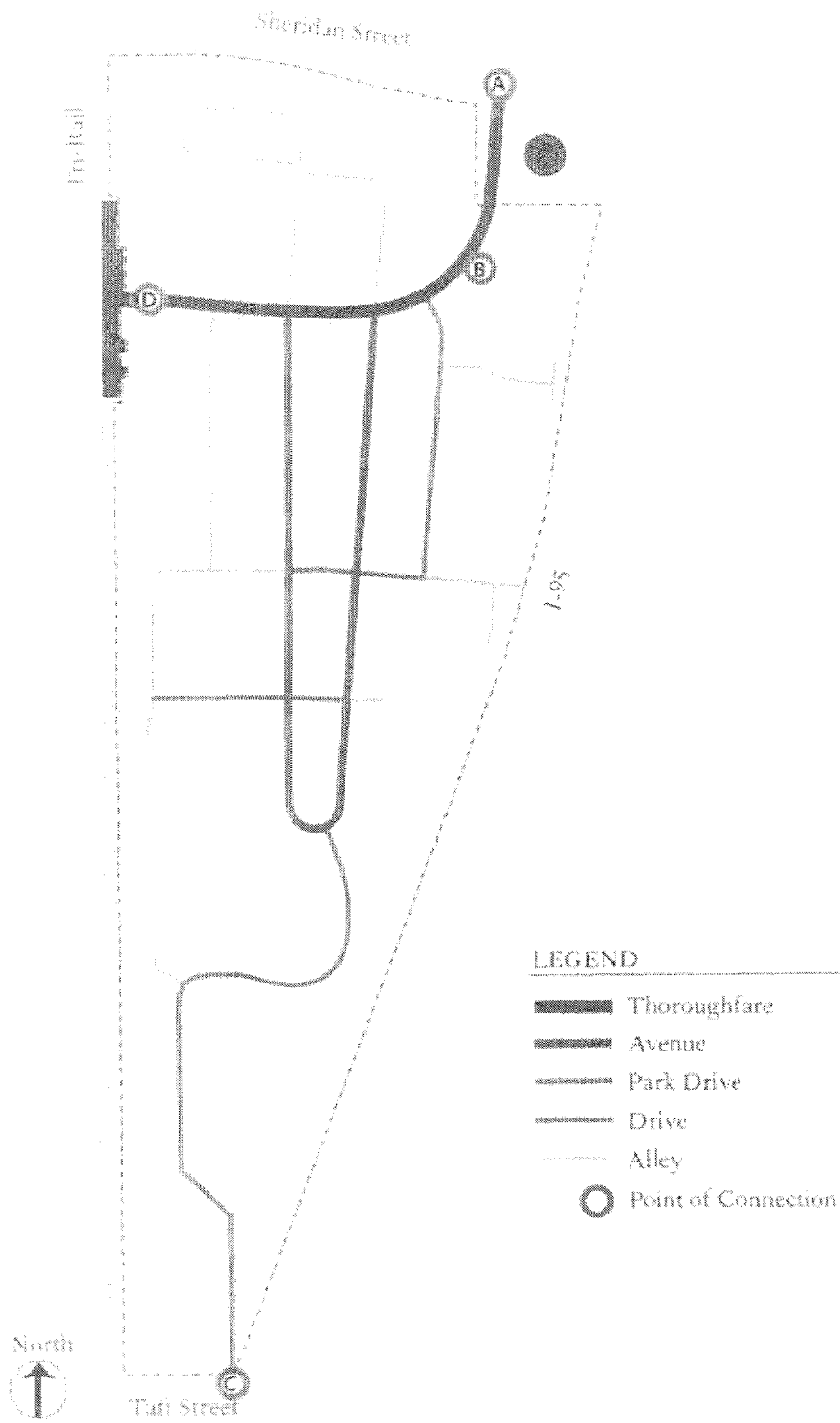
| Land Use | Area (Acres) | Population (Est.) |
|--------------|--------------|-------------------|
| Residential | 100 | 1,000 |
| Commercial | 50 | 500 |
| Public Plaza | 1 | 100 |
| Park | 6 | 600 |

Table 2: Mixed-Use Building Data

| Building | Floor | Area (Sq Ft) | Use |
|------------|-------|--------------|-------------|
| Building A | 1 | 10,000 | Commercial |
| | 2-10 | 90,000 | Residential |
| Building B | 1 | 10,000 | Commercial |
| | 2-10 | 90,000 | Residential |

Table 3: Mixed-Use Building Data (Continued)

| Building | Floor | Area (Sq Ft) | Use |
|------------|-------|--------------|-------------|
| Building C | 1 | 10,000 | Commercial |
| | 2-10 | 90,000 | Residential |
| Building D | 1 | 10,000 | Commercial |
| | 2-10 | 90,000 | Residential |



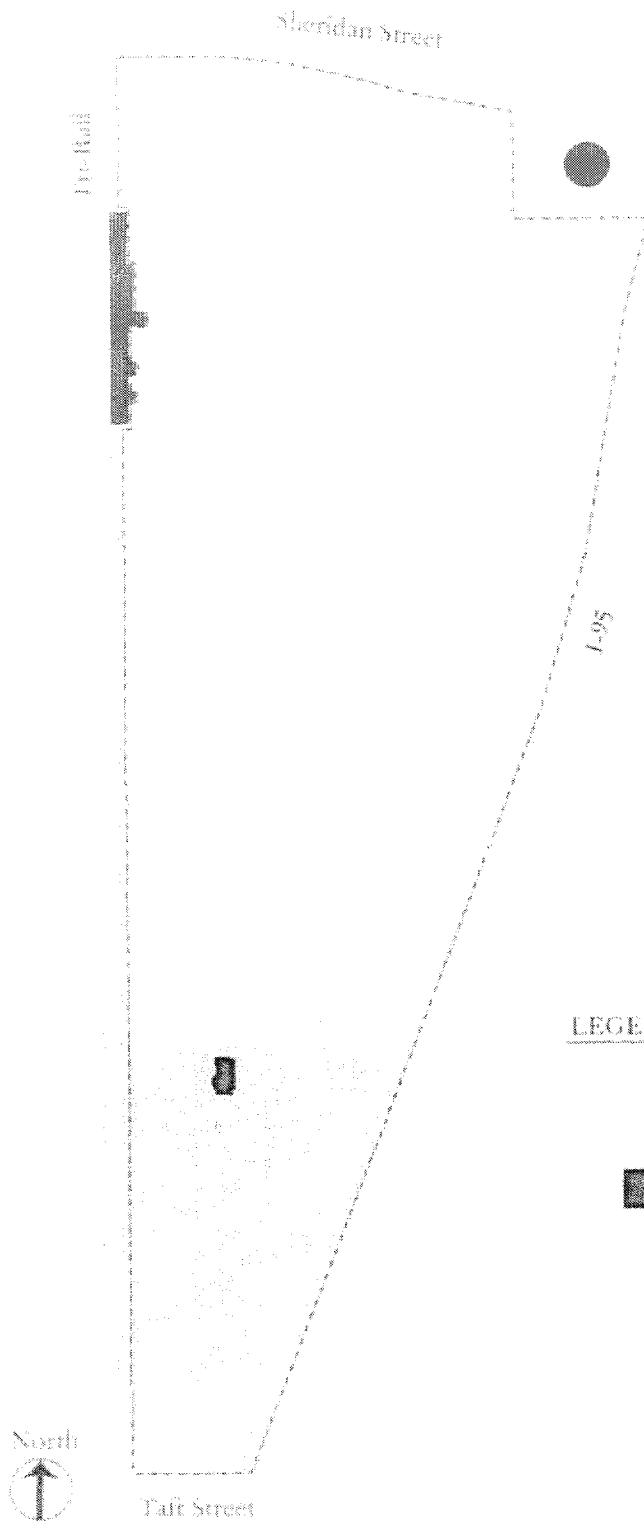
STREET NETWORK
SHERIDAN STATION SIDE VILLAGE
Hollywood, Florida

ram

PINNACLE
DRYING GROUP

Greg M





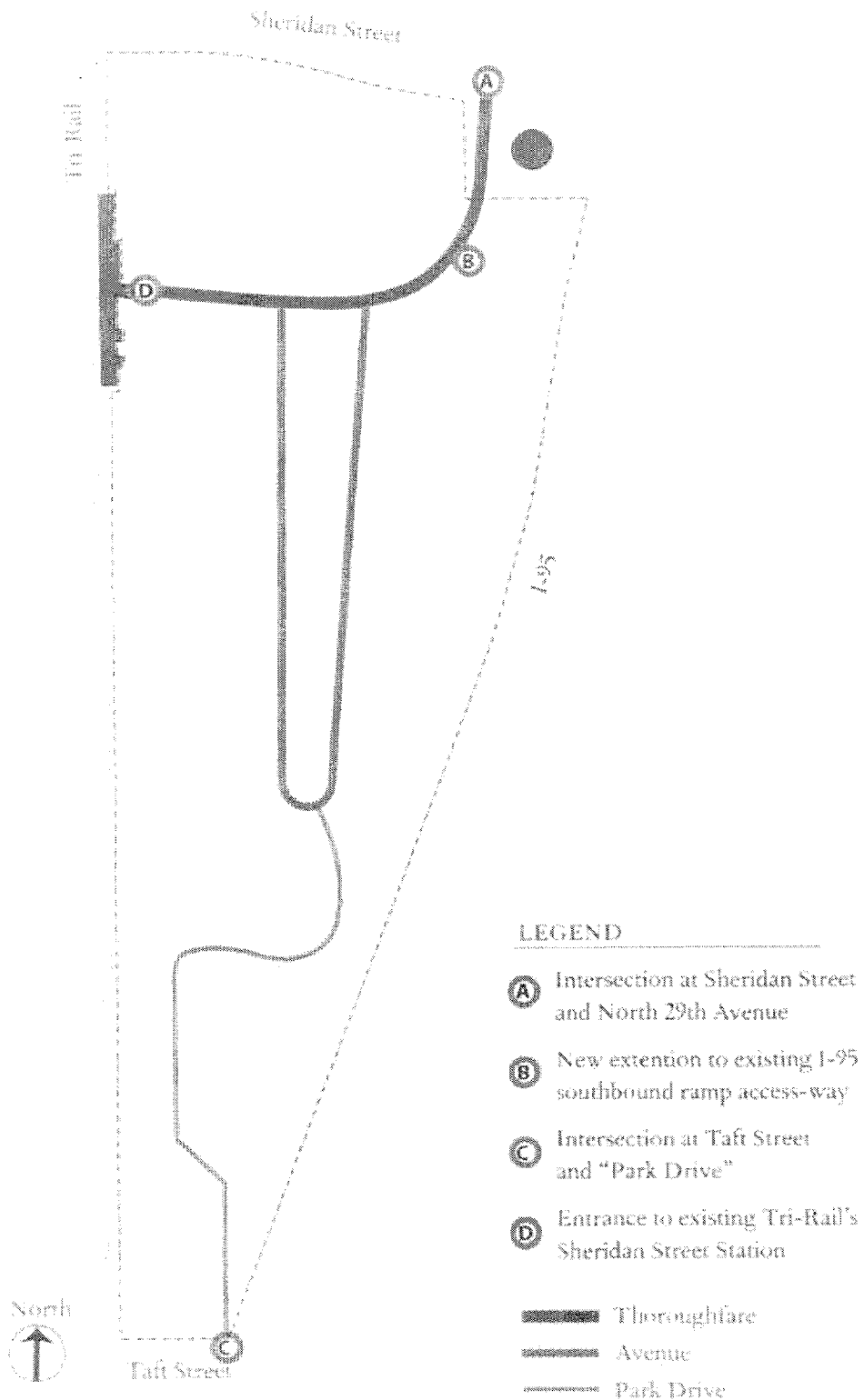
LEGEND

Community Center
1,700 Square Feet

Sheridan Park
6 Acres



| | |
|-------------------------------------|-----------------|
| <p>COMMUNITY FACILITIES</p> | |
| <p>SHERIDAN STATIONSIDE VILLAGE</p> | |
| <p>Hollywood, Florida</p> | |
| <p>ram</p> | <p>PINNACLE</p> |
| <p>6000</p> | |



LEGEND

- (A) Intersection at Sheridan Street and North 29th Avenue
- (B) New extension to existing I-95 southbound ramp access-way
- (C) Intersection at Taft Street and "Park Drive"
- (D) Entrance to existing Tri-Rail's Sheridan Street Station

- Thoroughfare
- Avenue
- Park Drive



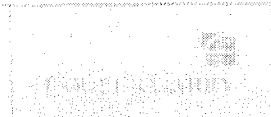
POINTS OF CONNECTION

SHERIDAN STATION SIDE VILLAGE
Hollywood, Florida

ram

PINNACLE
HORSE GROUP

Greg M



Improvements at Point of Connection A & B:

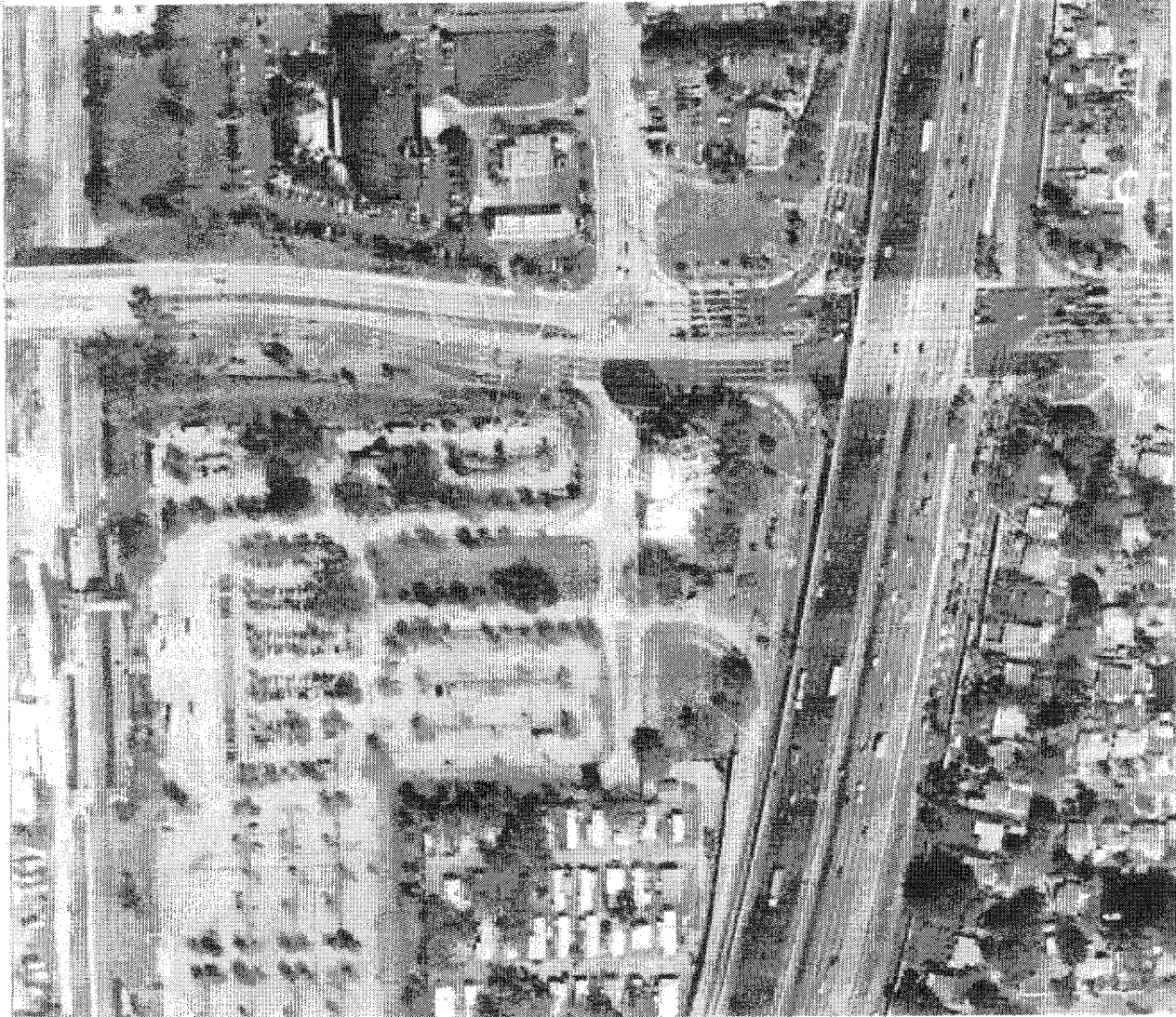
IMPROVEMENTS:

1. REDESIGN ROAD TO HAVE TWO THROUGH LANE, TWO RIGHT TURN LANES TO BEHIND LEFT TURN WESTBOUND LANE ON CENTER ST
2. ADD A THIRD LEFT TURN LANE ON THE SOUTH BOUND OFF RAMP
3. ADD RIGHT TURN LANE AND CHANGE EXISTING TO A THIRD LEFT TURN ON THE NORTH BOUND OFF RAMP
4. WIDEN THE LENGTH OF BOTH LANES BEFORE MERGING TO ONE LANE FOR THE SOUTH BOUND OFF RAMP
5. ADD AN OFF-RAMP ON THE SOUTH BOUND OFF RAMP
6. ADD AND REDESIGN EASTBOUND TURN LANES & THROUGH LANE AND TWO LEFT TURN LANES
7. WIDEN EASTBOUND LANE THROUGH THE 1/4 MILE TO INTERSECTION AND N 25TH INTERSECTION

8. ADD ADDITIONAL DESIGNATED NORTHBOUND LEFT TURN LANE
9. ADD ADDITIONAL EASTBOUND THIRD LANE
10. ADD DEDICATED THE SOUTHBOUND RIGHT TURN LANE
11. ADD AND REDESIGN EASTBOUND RIGHT TURN LANE AND ADD ON N 25TH AVE
12. ADD AND REDESIGN EASTBOUND LEFT TURN LANE ON N 25TH AVE
13. ADD AND REDESIGN TWO RIGHT TURN LANES, A THROUGH LANE AND TWO LEFT TURN LANES
14. ADD A SECOND WESTBOUND LEFT TURN LANE

LEGEND

- NEW CONSTRUCTION
- REALIGNED FOR NORTHBOUND TURN LANES
- REALIGNED THROUGH LANES
- CHANGE IN LANE DESIGNATION

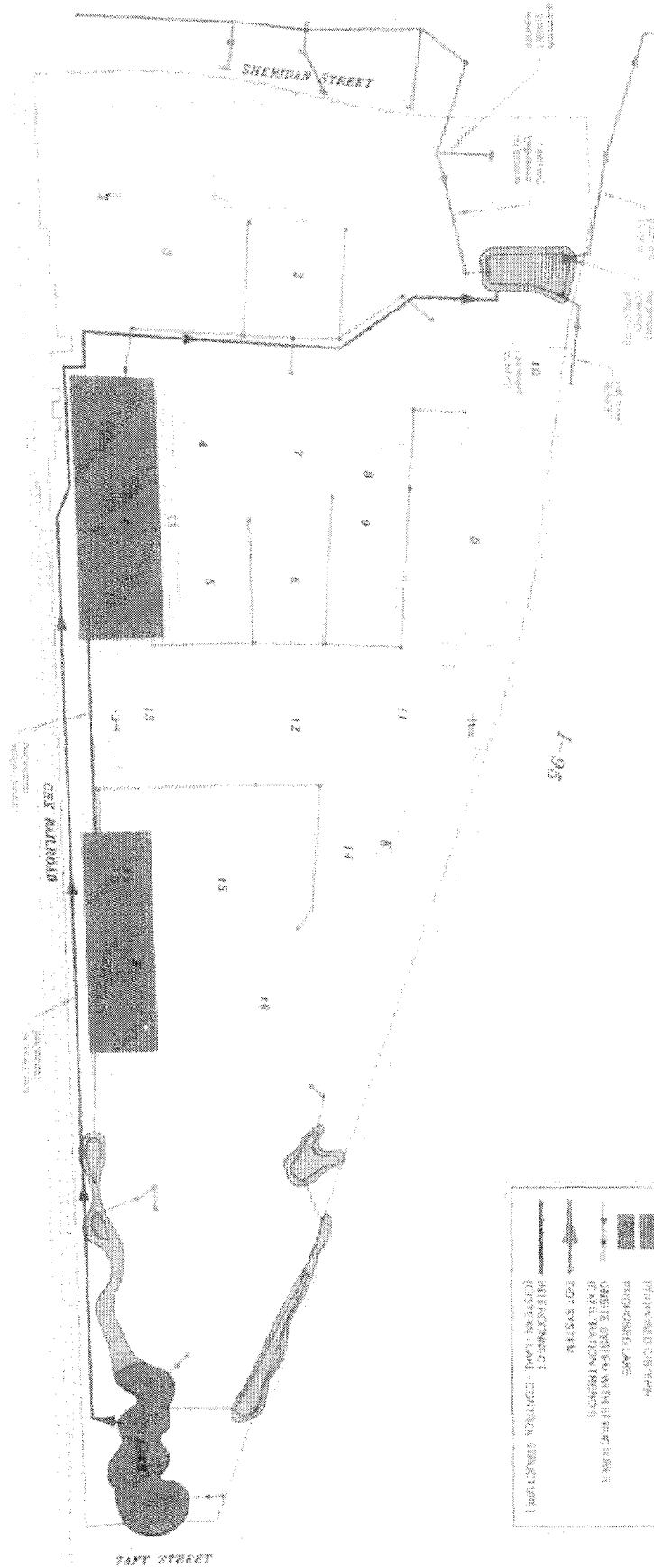


Improvements along Taft Street:

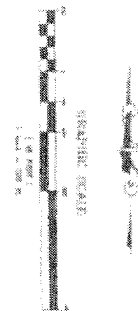
1. CONSTRUCT A WESTBOUND RIGHT TURN LANE AND A THIRD NORTH
2. ADD A RE-DESIGNED RIGHT TURN LANE ON TAFT
3. RE-DESIGN EASTBOUND AND ADD WESTBOUND RIGHT TURN LANE ON N 25TH AVE
4. CONSTRUCT EASTBOUND AND WESTBOUND LEFT TURN LANE ON N 25TH AVE
5. ADD A SECOND NORTH AND SOUTH

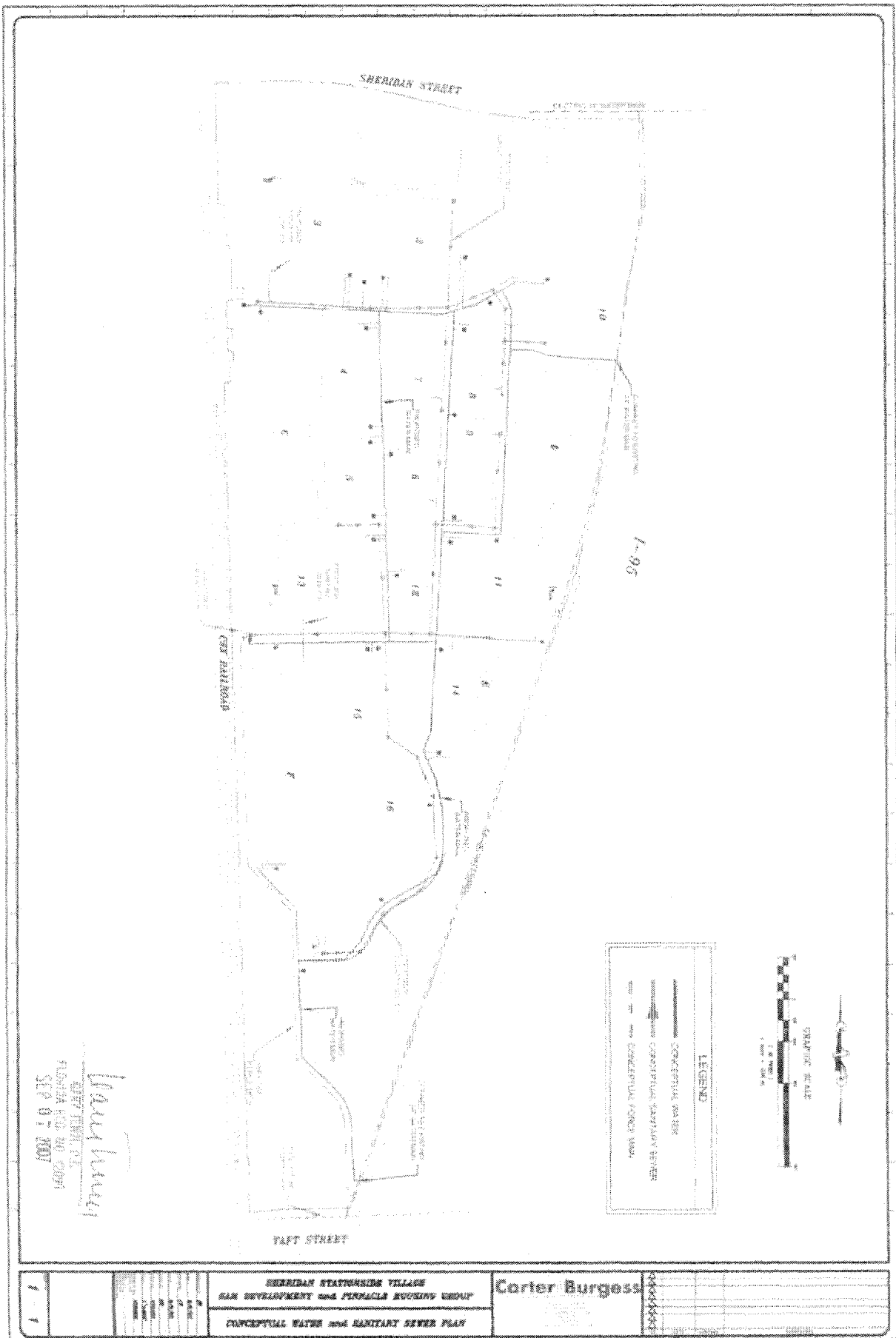
| DATE: 01/11/2017 | | | PROJECT: 1701 | | | TRAFFIC IMPROVEMENTS | | |
|---|--|--|---------------|--|--|----------------------|--|--|
| SHERIDAN STATIONSIDE VILLAGE | | | | | | | | |
| Hollywood, Florida | | | | | | | | |
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Gregg



Law

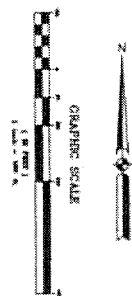
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Barth
 CIVIL ENGINEER
 FLORIDA REG. NO. 0001
 SEP 07 2007

SHERIDAN STATIONSIDE VILLAGE
PLAN DEVELOPMENT AND PRELIMINARY DESIGN GROUP
CONCEPTUAL WATER AND SANITARY SEWER PLAN

Carter Burgess

[illegible]

DEVELOPMENT GUIDELINES FOR SHERIDAN STATIONSIDE VILLAGE

WILLIAMSON COUNTY, TENNESSEE
2007

The existing multi-modal Tri-rail Station area is envisioned by FDOT and Tri-Rail to become a more *intensely developed* station site area with potential *mixed-uses* that will be directly connected to the rail system, the interstate roadway system and the City of Hollywood.

In consonance with the concept of creating discrete development nodes, and taking advantage of the existing and future synergies to be created at Sheridan Station, the City-Wide Master Plan envisions the long-term potential creation of a mixed-use *transit village*. . . . Linked to the Tri-Rail Station and its associated development, this concept would create an activity node that will offer a *unique lifestyle* with direct *access to all major regional centers* and the City of Hollywood.

— *City of Hollywood - City-Wide Master Plan Document*

The point of cities is multiplicity of choice.

— *Jane Jacobs*

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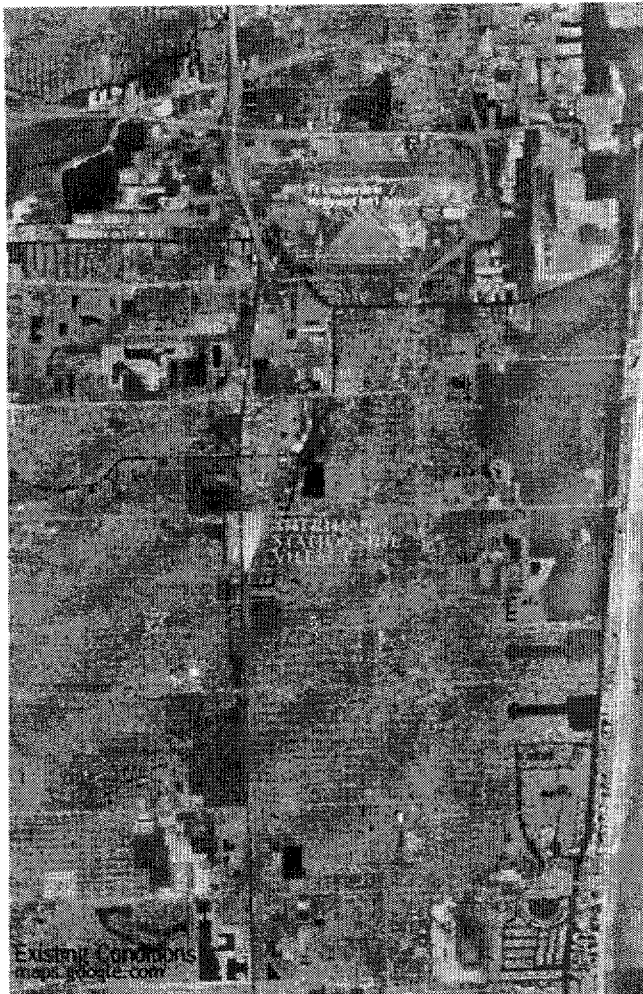
Kimley-Horn & Associates, Inc., Ft. Lauderdale, FL

Calvin Giordano & Associates, Ft. Lauderdale, FL

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INTRODUCTION



Site Location

SHERIDAN STATIONSIDE VILLAGE

A NEW MODEL FOR DEVELOPMENT

Bounded by the Tri-Rail System on the West and I-95 on the East, Sheridan Stationside Village is anticipated to become a hub for multi-modal transportation in Broward County. It will be the first in Broward County to realize the potential of Transit Oriented Development (TOD), and, by incorporating urban design concepts into the TOD model, it will provide a response to the local need for residential and commercial development in a manner which is both environmentally sensitive and civically aware.

Since further greenfield expansion in Hollywood is constrained, Sheridan Stationside Village will also serve as a new model for urban in-fill development. Through efficient site design, higher urban residential density, and careful design of the street environment, the development will reduce vehicular traffic and encourage walking, bicycling and the use of transit as significant means of transportation.

For its residents, Sheridan Stationside Village will provide a dynamic neighborhood that promotes interaction in a public environment through its extensive parks, community amenities, and vibrant streets and plazas.

The design guidelines presented here elaborate Sheridan Stationside Village's concept and provide direction, in order to ensure that the development will:

- Provide diversity in building envelopes to evoke and excite the development's varied public realm;
- Establish a compatible mixed-use development where daily needs are within walking distance;
- Utilize compact building forms and efficient circulation systems to maximize the urban in-fill opportunity;
- Conserve a part of the Live Oak tree canopy area as a park and the community's recreational destination;
- Creatively capture the TOD's pedestrian energy through engaging streets and plazas, and thereby effect a memorable public 'Place'.
- Be environmentally responsible and ensure long-term neighborhood sustainability by using LEED established guidelines in conjunction with Chapter 52 of South Florida Building Code and other innovative concepts;
- Insure a pedestrian-friendly environment by integrating ground floor building uses with the streets and utilizing traffic calming concepts;
- Provide the region with a destination for living, working, shopping, and recreating that is less reliant on the personal automobile;
- Create venues for civic and cultural events.

1 SCHEMATIC REPRESENTATION

SCHEMATIC OVERVIEW

In order to maximize site potentials and make for a better neighborhood transition, the Sheridan Stationside Village district is divided into the following three quarters:

- The Mixed-Use Quarter,
- The Residential Quarter, and
- The Park Quarter

Additionally, in order to provide for a pedestrian-friendly street-life while accommodating the needs of the various uses, five street types are classified, and in order of their

traffic volume/prominence, they are:

- Thoroughfare,
- Avenue,
- Park Drive,
- Drive, and
- Alley

As a focal point of the district, the center of the site is an urban plaza called the Zocalo. It will provide a public gathering space incorporating the civic and cultural uses that are critical to providing the Village a unique identity.

Complementing the Zocalo is the 6-acre park at the south end of the site. It draws its character from the dense canopy of mature Live Oaks.

THE MIXED-USE QUARTER

Retail, office, hotel, and residential uses are intended for this Quarter, and the goal is to create a vibrant street-life.

That part of the Zocalo which falls into this Quarter must offer a place for members of the urban community to relax in a visually stimulating environment.

THE RESIDENTIAL QUARTER

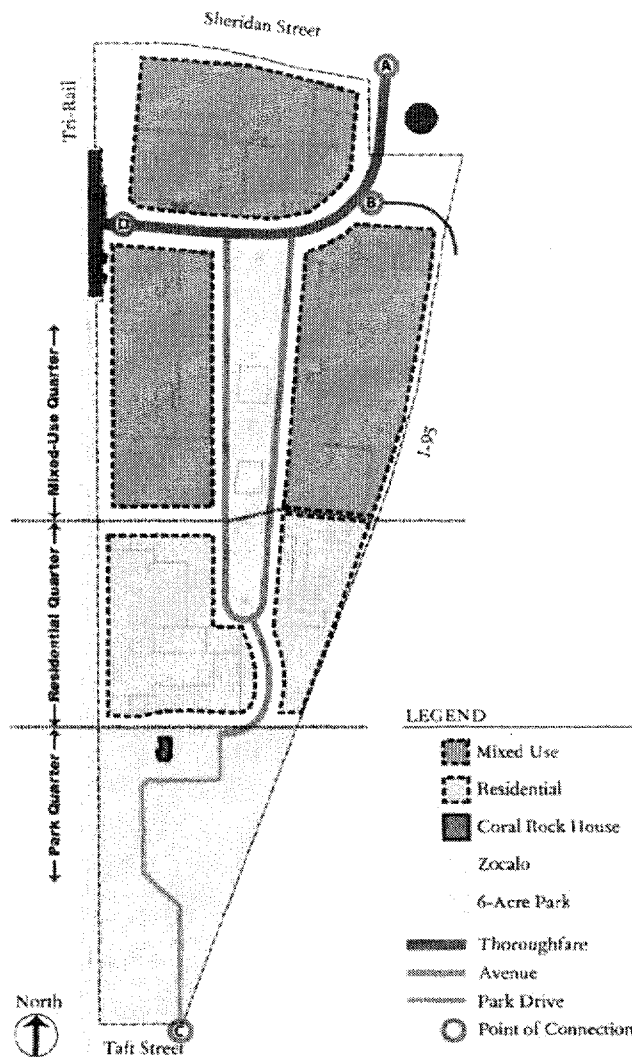
The Residential Quarter offers a living experience which is urban yet in close proximity to the natural landscape (the 6-acre park).

Retail and office establishments are permitted in this Quarter but they are discouraged.

THE PARK QUARTER

The Park Quarter contains the historic coral rock house and the 6-acre park.

It is the intention of this quarter to provide for passive recreational activities and a place for community gatherings.



The Master Layout

(with ghosted schematic plan that is subject to change)

PERMITTED STREET TYPES FOR THE QUARTERS

Mixed-Use Quarter:

- "Thoroughfare"
- "Avenue"
- "Drive"
- "Alley"

Residential Quarter:

- "Avenue"
- "Drive"
- "Alley"

Park Quarter:

- "Park Drive"
- "Drive"

LAND USES

Permitted Land Uses for Sheridan Stationside Village include:

- Retail
- Office
- Hotel
- Residential
- Coral rock house, and
- Parking Garage

The permitted uses for office and retail shall include the following uses:

- Commercial Uses, which may include any activity where there is an exchange of goods or services for monetary gain. Such activities include but are not limited to retail sales, offices, eating and drinking facilities, theaters, transient lodging facilities and similar uses.
- Office Uses, which may include administrative, professional, research, and financial offices.
- Pedestrian Oriented Commercial Uses, which may include a use that provides retail, service, eating/drinking or entertainment services which primarily rely upon walk-in customers or clients and are open to the public for a minimum of six hours between the hours of 9:00 a.m. and 6:00 p.m., Monday through Friday.
- Personal Services, which may include establishments which are primarily engaged in providing services involving the care of a person, his or her apparel, or pets. These

uses include but are not limited to hair salons, spas, laundry and dry cleaning (drop off only), pet grooming salon, shoe repair, alterations, tailoring and jewelry repair, photographic studios, reducing salons, health clubs, clothing retail.

- Restaurant, Outdoor Cafe
- Retail (indoor and outdoor), which may include the sale of commodities or goods which are sold to the ultimate consumer.

The following uses are prohibited within the Mixed-Use and Residential Quarters:

- Assembly of pre-manufactured parts for sale on the premises
- Automotive Paint Body
- Automotive sales, new
- Automotive sales, old
- Automotive repair and storage
- Carwash or detailing
- Consignment Shops
- Funeral Homes
- Outdoor Storage
- Pawnshops
- Psychic Help Uses
- Service Stations
- Thrift Shops
- Wholesaling and Warehousing

The average dwelling unit size must be a minimum of 750 s.f.

Twenty percent of all residential units must be designated as workforce housing, and should be affordable to households making between 80 and 140% of the local average income.

MAXIMUM DEVELOPMENT PROGRAM

| | |
|---------------------------|--------------|
| Retail | 300,000 s.f. |
| Office* | 245,000 s.f. |
| Hotel | 150 keys |
| Residential | 1,050 units |
| Existing coral rock house | 1,700 s.f. |

DENSITY & INTENSITY TABLE

Site Area:

| | |
|-------------------|-----------------------|
| Developable Sites | 1,497,020 s.f. |
| 6-acre park | 261,360 s.f. |
| Total | 1,758,380 s.f. |
| | 40.4 acres |

Residential Density:

| | |
|--------------------------|---------------|
| 1,050 units/40.4 acres = | 26 units/acre |
|--------------------------|---------------|

Hotel Keys (Rooms):

| | |
|-----------------------|-------------|
| 150 keys/40.4 acres = | 4 keys/acre |
|-----------------------|-------------|

Commercial Area:

| | |
|--------------|---------------------|
| Retail | 300,000 s.f. |
| Office* | 245,000 s.f. |
| Total | 545,000 s.f. |

Commercial Intensity:

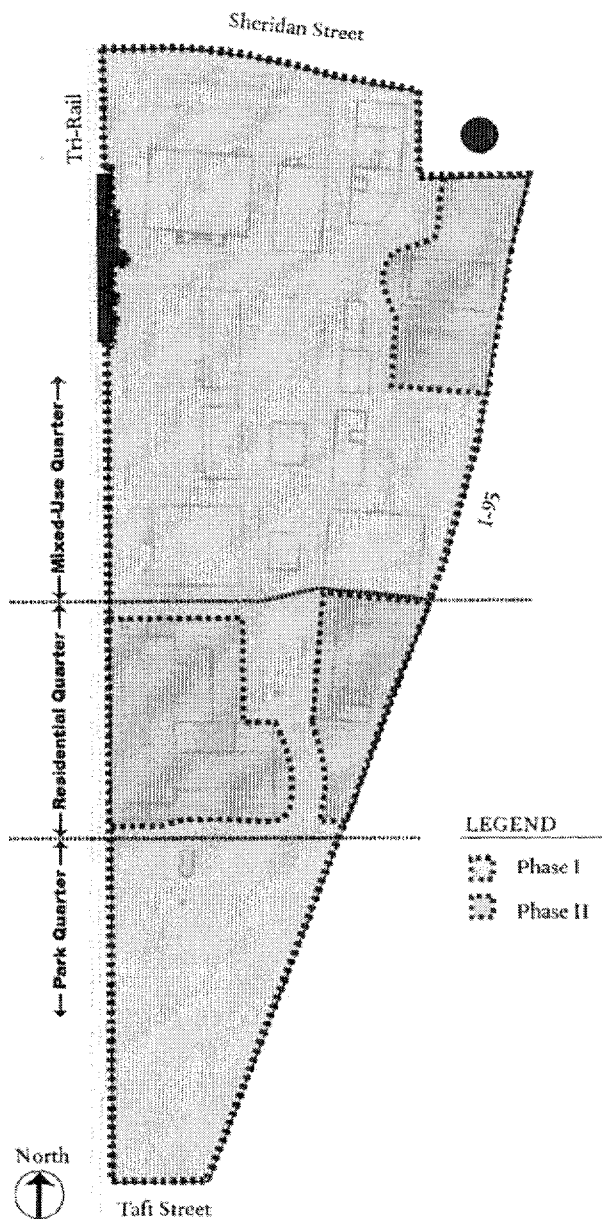
| | |
|-------------------------------|--------------------------|
| 545,000 s.f./1,758,380 s.f. = | .31 commercial intensity |
|-------------------------------|--------------------------|

*The maximum amount of office use may increase to the TOD land use plan amendment approved level of 299,00 s.f. provided that the revised development program is evaluated with respect to any requirement for DRI review.

MAXIMUM BUILDING HEIGHTS

| | |
|---------------------|----------|
| Mixed-Use Quarter | 120 feet |
| Residential Quarter | 135 feet |
| Park Quarter | 30 feet |

A Building's height is measured from a point of average elevation along the street(s) it faces to the top of the flat roof deck at the top habitable floor. Decorative pitched roof elements, decorative architectural features, elevator rooms, roof stairs, and rooftop mechanical equipment rooms are not included in determining roof height. In no case shall any of these elements exceed 150 feet.



Phasing Diagram

PHASING

Sheridan Stationside Village is to be developed in two major phases:

- * Phase I
- * Phase II

These phases can be further subdivided into smaller phases, but prior to the issuance of building permits for more than 500 residential units, a minimum of 100,000 square feet of commercial retail and/or office square footage must have been completed or be under construction.

The building program of Phase I is to be developed in the Mixed-Use Quarter. Furthermore, phasing might change based on market conditions.

The 6-acre park area will be established in Phase I.

AUTOMOBILE PARKING & LOADING PARKING DIMENSIONS

All angled parking stalls including 90 degree stalls shall be 9' wide by 18' deep in both structured parking decks and surface parking lots.

The drive aisles for accessing 90 degree parking stalls must be a minimum of 24' wide.

In a structured parking deck, the slope of a parkable ramp cannot exceed 6%, and the slope of a non-parkable speed ramp cannot exceed 12%.

MINIMUM PARKING REQUIREMENTS

The parking requirements for the Sheridan Stationside Village transit-oriented development (TOD) are uniquely calculated based on the interconnectivity of the site, the mix of land uses, and the orientation to and emphasis on non-vehicular traffic. Parking requirements have been based on a review of the City of Hollywood Zoning and Land Development Regulations, as well as material published by the Institute of Transportation Engineers (ITE), the Urban Land Institute (ULI), and other sources.

Because the parking characteristics of Sheridan Station Village are unique to a transit-oriented development and this mix of land uses, the requirements have been written to be flexible in case the development is not built as planned. This ensures adequate parking regardless of the final intensities and mix of land uses.

Based on the mix of office and retail uses anticipated for this development (which assumes that a portion of the office workers will also be retail users, creating "internalization") and the availability of mass transit, Sheridan Stationside Village proposes reduced minimum parking rates for each use which are shown in adjacent Table 1.

Additional parking count credits based on time of day variation (Shared Parking) might also be achieved if the parking structure is serving uses which utilize parking at different times of day.

TABLE 1 - BASE PARKING RATE

| <u>Use/Rate</u> | |
|---|---|
| Hotel - | 0.75 spaces/ room |
| Retail - | 4 spaces/ 1,000 s.f. |
| Office - | 2.5 spaces/ 1,000 s.f. |
| Restaurant - | 10 spaces/ 1,000 s.f. |
| Residential * - | 1.3 spaces/ unit + 1 space/ 5 units guest |
| * Guest parking is not required with use of shared parking. | |

SHARED PARKING MATRIX

The shared parking matrices may be used if certain uses which utilize parking at different times throughout the day are in proximity to each other. As an example, restaurant uses see peak parking demand in the evening hours, whereas office uses have a low demand in the day hours. The same parking area can accommodate office uses during the day, and restaurant uses in the evening. The time of day variation rate was based on methodologies outlined in the Urban Land Institute's publication Shared Parking. In order for the shared parking rate to be used, the parking areas must be demonstrated to accommodate parking for at least two of the following land uses:

- * Hotel
- * Retail
- * Restaurant
- * Office
- * Residential
- * Tri-Rail/Transit

If at least two of these uses do not utilize the parking area, the shared parking rate cannot be applied. The parking requirement, determined by either of the rates outlined above, as applicable, is multiplied by the percentages in Tables 2 and 3 for each use. The maximum parking demand must be calculated for the weekday time period and for the Saturday time period separately. This maximum demand determines the maximum parking requirement for the combined uses.

Tri-Rail/Transit parking can be included in the shared parking calculations according to Tables 2 and 3 if approved by the appropriate governing agencies.

The shared parking matrices are illustrated in Tables 2 and 3.

RESIDENTIAL PARKING

Residential parking must be provided at the rate of 1.3 spaces per unit, plus guest parking of one space per 5 units. The guest parking requirement is not required if the parking

Table 2 – Shared Parking Matrix (Weekday)

| Time | Residential | Retail | Office | Transit | Restaurant | Hotel |
|-------|-------------|--------|--------|---------|------------|-------|
| 6 AM | 100% | 0% | 3% | 3% | 0% | 100% |
| 7 AM | 87% | 8% | 20% | 20% | 2% | 85% |
| 8 AM | 79% | 18% | 63% | 63% | 5% | 65% |
| 9 AM | 73% | 42% | 93% | 93% | 10% | 55% |
| 10 AM | 68% | 68% | 100% | 100% | 20% | 45% |
| 11 AM | 59% | 87% | 100% | 100% | 30% | 35% |
| 12 PM | 60% | 97% | 90% | 90% | 50% | 30% |
| 1 PM | 59% | 100% | 90% | 90% | 70% | 30% |
| 2 PM | 60% | 97% | 97% | 97% | 60% | 35% |
| 3 PM | 61% | 95% | 93% | 93% | 60% | 35% |
| 4 PM | 66% | 87% | 77% | 77% | 50% | 45% |
| 5 PM | 77% | 79% | 47% | 47% | 70% | 60% |
| 6 PM | 85% | 82% | 23% | 23% | 90% | 70% |
| 7 PM | 94% | 89% | 7% | 7% | 100% | 75% |
| 8 PM | 96% | 87% | 7% | 7% | 100% | 90% |
| 9 PM | 98% | 61% | 3% | 3% | 100% | 95% |
| 10 PM | 99% | 32% | 3% | 3% | 90% | 100% |
| 11 PM | 100% | 13% | 0% | 0% | 70% | 100% |
| 12 AM | 100% | 0% | 0% | 0% | 50% | 100% |

Multiplying the gross total parking demand of any use by the corresponding distribution percentage at a given time of the day will yield the actual parking demand at that time.

Table 3 – Shared Parking Matrix (Saturday)

| Time | Residential | Retail | Office | Transit | Restaurant | Hotel |
|-------|-------------|--------|--------|---------|------------|-------|
| 6 AM | 100% | 0% | 0% | 0% | 0% | 90% |
| 7 AM | 95% | 3% | 3% | 3% | 2% | 70% |
| 8 AM | 88% | 10% | 10% | 10% | 3% | 60% |
| 9 AM | 81% | 30% | 13% | 13% | 6% | 50% |
| 10 AM | 71% | 45% | 13% | 13% | 8% | 40% |
| 11 AM | 71% | 73% | 17% | 17% | 10% | 35% |
| 12 PM | 71% | 85% | 17% | 17% | 30% | 30% |
| 1 PM | 70% | 95% | 13% | 13% | 45% | 30% |
| 2 PM | 71% | 100% | 10% | 10% | 45% | 35% |
| 3 PM | 73% | 100% | 7% | 7% | 45% | 40% |
| 4 PM | 75% | 90% | 7% | 7% | 45% | 50% |
| 5 PM | 81% | 75% | 3% | 3% | 60% | 60% |
| 6 PM | 85% | 65% | 3% | 3% | 90% | 70% |
| 7 PM | 87% | 60% | 3% | 3% | 95% | 80% |
| 8 PM | 92% | 55% | 3% | 3% | 100% | 90% |
| 9 PM | 95% | 40% | 0% | 0% | 100% | 95% |
| 10 PM | 96% | 38% | 0% | 0% | 95% | 100% |
| 11 PM | 98% | 13% | 0% | 0% | 85% | 100% |
| 12 AM | 100% | 0% | 0% | 0% | 70% | 100% |

Multiplying the gross total parking demand of any use by the corresponding distribution percentage at a given time of the day will yield the actual parking demand at that time.

structure serving the residential uses is shared with retail, or office uses or if on-street parking is available front of the building.

An entire garage or individual floors of a garage may be gated to restrict entry to residents only. All the spaces included in this gated section must be considered reserved and cannot be included in any shared parking calculations.

OFFICE PARKING

Office parking may be reserved during normal business hours on weekdays, but not past 6:00 P.M.

TRI-RAIL/ TRANSIT PARKING

These parking spaces can be included in the shared parking calculations only with an explicit written permission of the appropriate governing agencies.

UPDATED PARKING ANALYSIS

Upon occupancy of 75 percent of the total square footage in each development phase, the developer may opt to perform a parking occupancy study, signed and sealed by a Registered Professional Engineer in the State of Florida, to determine the actual parking characteristics of the development and to aid in predicting parking requirements for future phases.

LOADING

Off-street loading spaces shall be provided as follows:

For each retail store or similar use which has an aggregate gross floor area of:

- * Less than 24,999 sq. ft. will require no off-street loading berths,
- * 25,000 sq. ft. - 49,999 sq. ft. will require one off-street loading berth,
- * 50,000 sq. ft. - 99,999 sq. ft. will require two off-street loading berths,
- * Each 80,000 sq. ft. above 100,000 sq. ft. will require 1 additional off-street loading berth.

Loading Requirements for all uses not included here shall conform to Article 7 of the City of Hollywood Zoning & Land Development Regulations.

Note: If two or more uses are part of a vertically integrated structure, off-street loading facilities supplied to meet the needs of one use can be considered as meeting off-street loading needs of any other use in the structure.

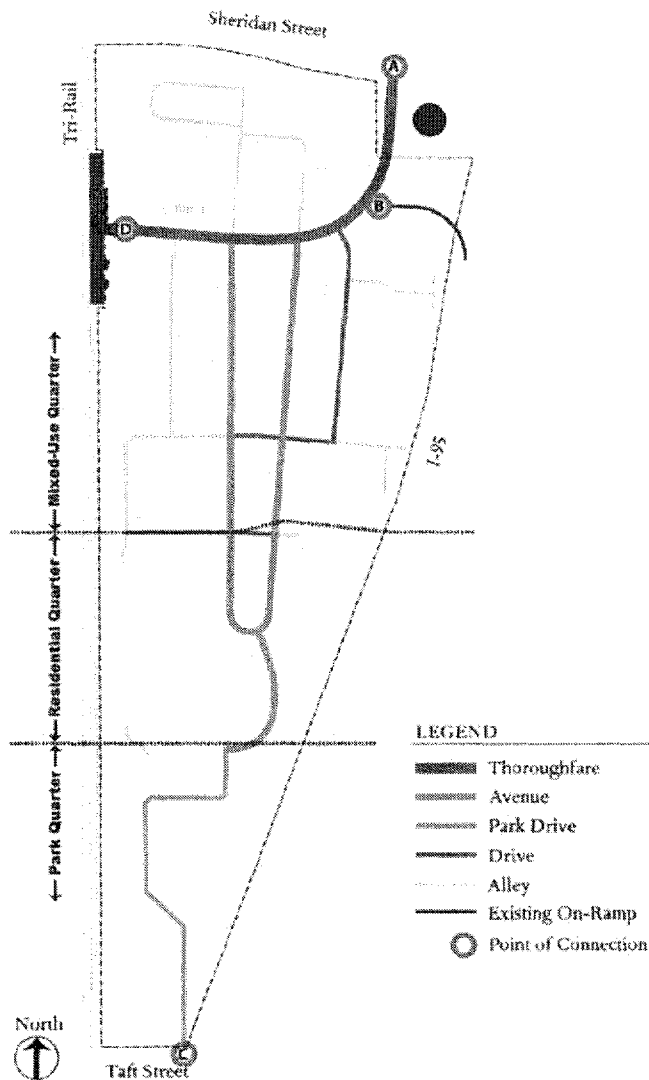
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INTERNAL CIRCULATION

STREETS

Traffic calming, pedestrian safety, and the efficient flow of vehicular traffic must be the goal of the street network.

Although PDs typically require building setbacks of 25 feet, in order to lend the district a pedestrian-friendly urban environment, these guidelines propose the following:

- Setbacks for all buildings along internal streets must be as called out in the Dimensional Data Chart (following section).
- Setbacks for all buildings bordering the rail-road right-of-way and Sheridan Street must be a minimum of 20 feet.
- Setbacks are not required for all buildings bordering the I-95 right-of-way.



Street Network

Schematic Representation (subject to change)

On-street parking is encouraged along the "Avenue", the "Drive", and the "Alley".

Raised table intersections are encouraged at crosswalk intersections, and they are required within any roadways that traverse the Zocalo.

Crosswalks must be of a contrasting surface treatment to the roadway and will be raised 6" when they occur in areas other than at street intersections.

Sidewalk amenity zones must include adequate street lights, seating, trash receptacles, and bicycle parking facilities.

On any street, the required minimum clear pedestrian walking zone must be separated from the vehicular drive lane by at least one of the following:

- Tree/amenity zone
- Parallel automobile parking stalls
- Bollards

Separated vehicular drop-off areas along a street are permitted only on the "Avenue", the "Drive", and the "Alley", and only when they measure 140 feet or less between the outer two curb-cuts. Where they are provided, the following applies:

- Where a Porte Cochere structure less than 35 feet in height is provided, the minimum building setback for the Porte Cochere structure may be modified to 5 feet from the back of street-side curb provided that: 1) there are no overhead projections into the modified minimum setback requirement, 2) the continuity of the required minimum clear pedestrian walking zone is maintained, and 3) the required separation between the pedestrian zone and the street side vehicular drive lane is maintained. In such a scenario, a tree/amenity zone is not required in the center island created by the vehicular separation.
- Buildings do not have to conform to the maximum setback requirements of a street for that portion of entire building that fronts the drop-off area. The portion of a building that fronts the drop-off area may be defined as one which occurs anywhere within a distance extending 20' away from the outer two curb-cuts of the drop-off area along the street.

Whenever a public plaza, garden, or other open space amenity borders a street, the following applies:

- Tree/amenity zone is not required.
- A building that occurs within such a space and is surrounded on all sides by it does not have to conform to the maximum and minimum building setback requirements provided that: 1) there are no overhead projections within 5 feet of the back of curb, 2) the continuity of the required minimum clear pedestrian walking zone is maintained, and

3) the required separation between the pedestrian zone and the vehicular drive lane is maintained.

Transit buses may travel only on the "Thoroughfare".

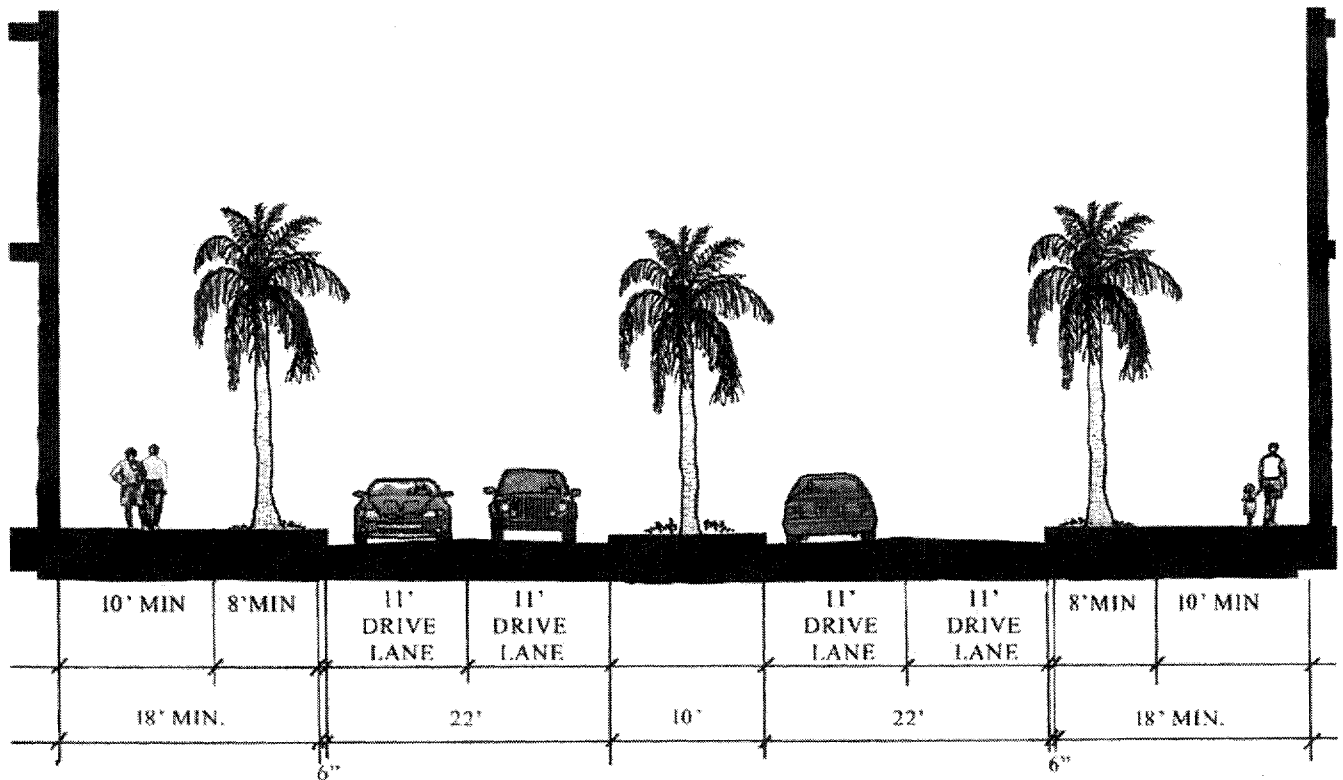
DIMENSIONAL DATA

| | Thoroughfare | Avenue | Park Drive | Drive | Alley |
|--|----------------------------|--|---------------------------------|---|----------------------------|
| Street Dimensional Requirements: | | | | | |
| Minimum Lane width | 11'; 15' if 1-lane roadway | 11'; 15' if 1-lane roadway | 11'; 15' if 1-lane roadway | 11'; 15' if 1-lane roadway | 11'; 15' if 1-lane roadway |
| Minimum Dimensions of a parallel parking stall | Not Permitted | 8.5' x 24' | 8.5' x 24' | 8.5' x 24' | 8.5' x 24' |
| Minimum angled parking unit stall size (including 90 deg.) | Not Permitted | 9' x 18'; 90 deg. Not Permitted | 9' x 18' | 9' x 18' | 9' x 18' |
| Minimum Dimensions of parallel truck loading berth | Not Permitted | 10' W x 25' L x 14' H | Not Permitted | 10' W x 25' L x 14' H | 10' W x 25' L x 14' H |
| Minimum Width of tree/ amenity zone adjoining parallel parking | 5' from back of curb | 5' from back of curb | 5' from back of curb* | 5' from back of curb | N/A |
| Minimum Width of tree/ amenity zone adjoining a drive lane | 8' from back of curb | 8' from back of curb | 8' from back of curb* | 8' from back of curb | N/A |
| Maximum spacing between trees | 30' | 30' | N/A | 30' | None |
| Minimum size of pervious tree planting area | 4' x 4' | 4' x 4' | N/A | 4' x 4' | 4' x 4' |
| Minimum pedestrian walking zone width | 10' | 10'; 7' if at a ground floor residential use | 8'; required on any 1 side only | 8'; 5' if at a ground floor residential use | N/A |
| Minimum headroom clearance at sidewalk | 12' | 12' | N/A | 12' | N/A |
| Front Set-Back Requirements: | | | | | |
| Minimum building setback from back of curb | 18' | 15' | N/A | 13' | 0' |
| Maximum building setback from back of curb | 30' | 25' | N/A | 25' | No Limit |
| Maximum overhead projections into required minimum setback | 8' | 8' | N/A | 8' | 0' |
| Usage Requirements: | | | | | |
| Residential uses at ground floor permitted | No | Yes | N/A | Yes | Yes |
| Structured Parking Garages at ground floor permitted | No | No | No | Yes | Yes |
| Maximum length of façade without intervening fenestration | 25' | 25' | N/A | 25' | 50' |
| Fences permitted | Only for outdoor seating | Only for outdoor seating | Yes | Only for outdoor seating | Yes |
| Maximum height of top rail of fence | 3' | 3' | 6' | 3' | 10' |
| Arcades & colonnades (non-residential use) permitted | Yes | Yes | N/A | Yes | Yes |
| Minimum interior clear width of arcade (if provided) | 8' | 8' | N/A | 8' | 6' |
| Minimum interior clear height of arcade (if provided) | 12' | 12' | N/A | 12' | 9' |
| Parking & Loading Requirements: | | | | | |
| Parallel parking permitted | No | Yes | Yes | Yes | Yes |
| Maximum number of continuous parallel parking spaces | N/A | 10 | 3 | 10 | No Limit |
| Angled parking permitted | No | Only if a 2-lane roadway | Yes | No | Only if a 2-lane roadway |
| Maximum number of continuous angled parking spaces | N/A | 10 | 6 | 10 | No Limit |
| Stopping permitted | No | Yes | Yes | Yes | Yes |
| Loading permitted | No | Yes | No | Yes | Yes |
| Enclosed service area access permitted | No | Yes | N/A | Yes | Yes |

* May be reduced for tree preservation

THOROUGHFARE

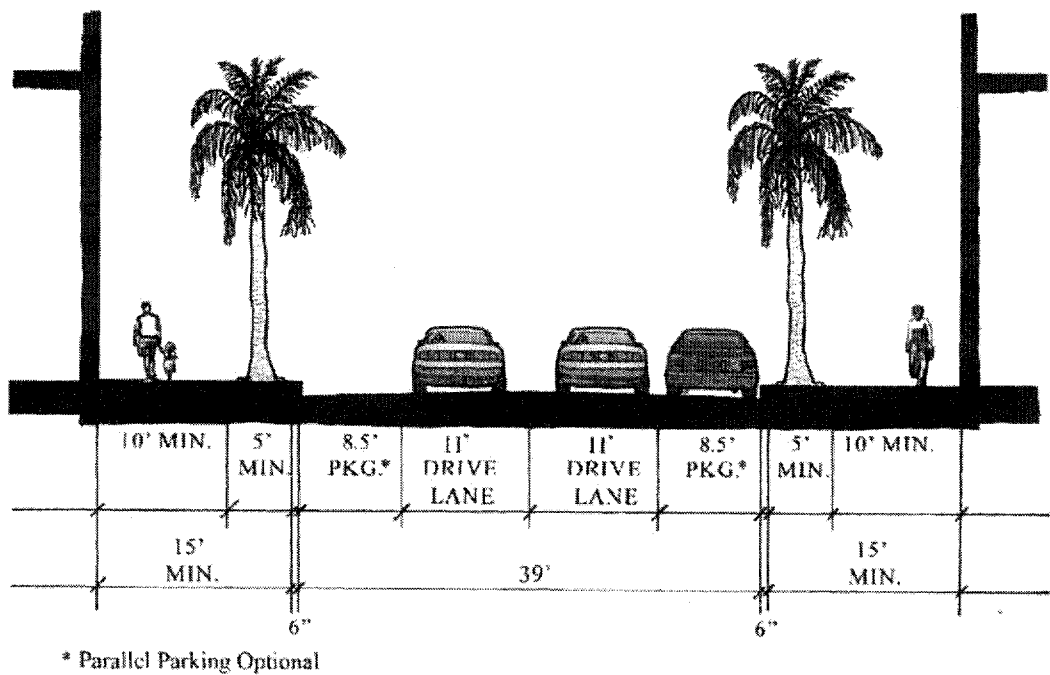
"Thoroughfare" represent the primary vehicular routes into and out of the site. Transit buses and tractor trailers may travel on "Thoroughfare" to reach the Transit Station or service areas.



Typical Thoroughfare Section

AVENUE

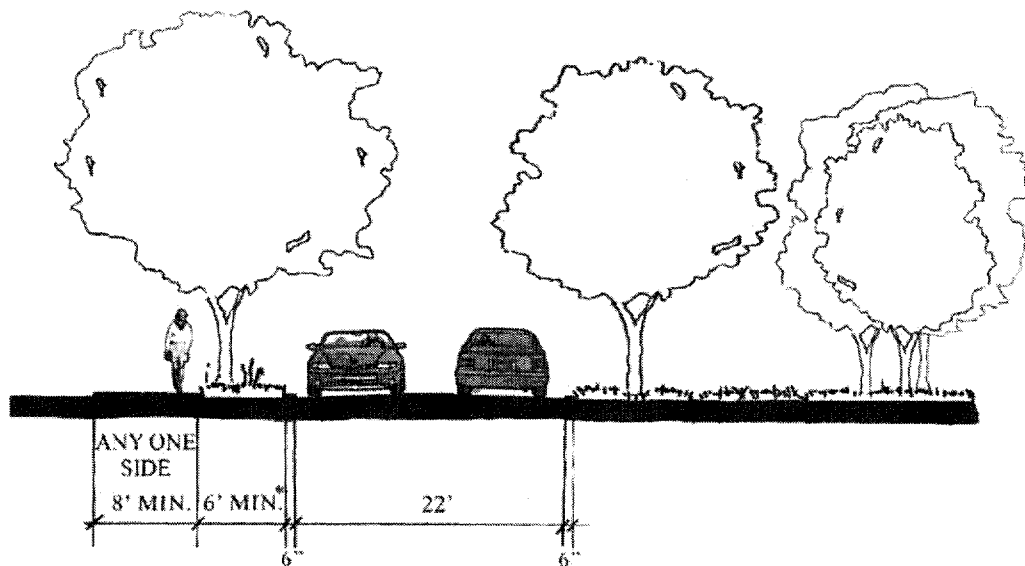
"Avenue" represent the primary road-form that connects the three Quarters internally. The "Avenue" is intended to provide a high level of interaction between vehicles and pedestrians to the storefronts and the Zocalo.



Typical Avenue Section

PARK DRIVE

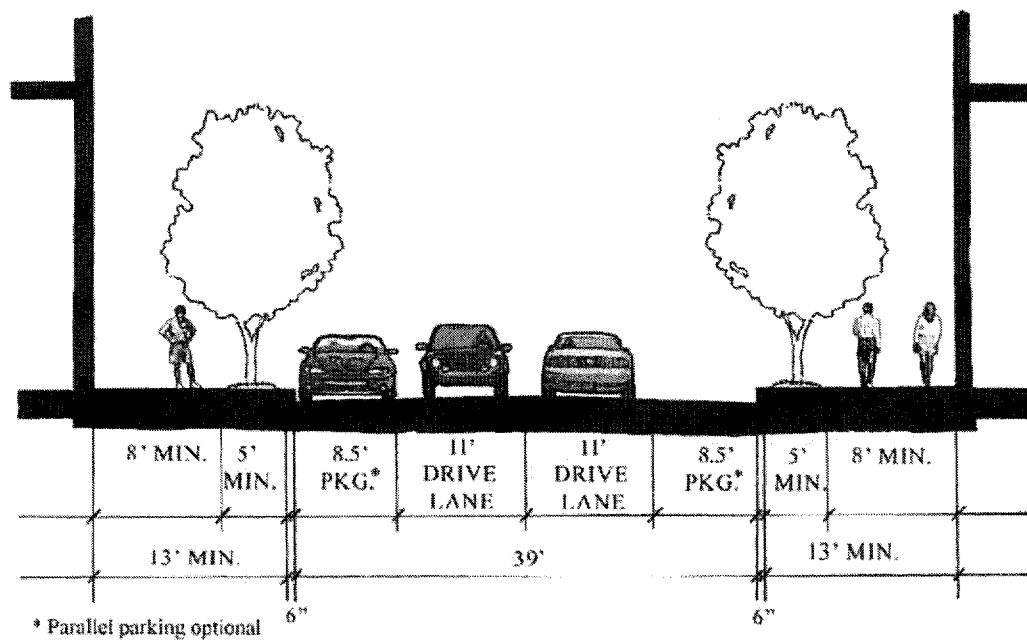
"Park Drive" is a special roadway that traverses the 6-acre park. It is sensitive to the preservation of the existing Live Oak tree canopy and it has a passive level of activity.



Typical Park Drive Section

DRIVE

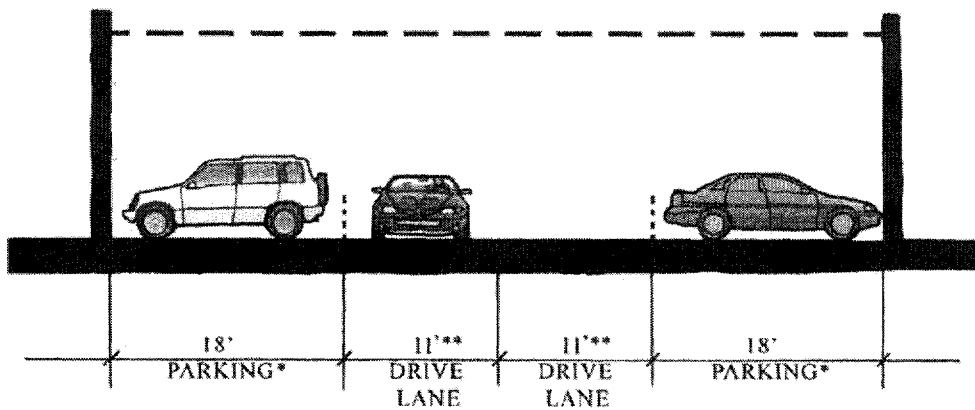
"Drive" is a roadway that supplements the "Thoroughfare" and "Avenue" in traffic flow by providing access to structured parking decks and building service areas.



Typical Drive Section

ALLEY

"Alley" is utilitarian in nature and its primary purpose is to provide access to structured parking decks and building service areas. They also help connect other road-forms so as to form multiple routes to traverse the site.



*Perpendicular/ angled parking optional

**12' required if perpendicular parking is provided

Typical Alley Section

OUTDOOR SEATING REGULATIONS

APPLICABILITY

Outdoor seating shall be permitted as an accessory use to a restaurant, business, or institution serving food or beverages in an enclosed area, subject to the standards listed below:

(1) Entitlement.

A. Non-waiter service. Outdoor seating shall be permitted for any restaurant, business or institution that does not provide waiter service (i.e. coffee shops, fast casual and limited service restaurants etc.) not to exceed 30% of the enclosed Gross Leaseable Area (GLA) of that restaurant, business or institution. In no case shall additional parking or traffic equivalency be required where outdoor seating is permitted for non-waiter service facilities.

B. Waiter service. Outdoor seating shall be permitted for any restaurant, business or institution that does provide full waiter service (i.e. full service restaurants) not to exceed 20% of the enclosed GLA of that restaurant, business or institution. In no case shall additional parking or traffic equivalency be required where outdoor seating is permitted for waiter service facilities.

(2) Access. The outdoor seating area is adjacent to, and has direct access through a doorway to that portion of the business or institution which is enclosed.

(3) Location. The outdoor seating is located directly adjacent to the restaurant or food service establishment and is owned or leased for this purpose.

(4) General circulation. The outdoor seating can be accommodated without impeding the access of the general public to one or more of the following:

A. The enclosed portion of the restaurant or food service establishment;

B. Any other use located within the same building or structure; or

C. Any common elements shared by the restaurant or food service establishment and any other users of the same building or structure.

(5) Safety. Outdoor seating shall comply with all building, fire, and safety code requirements.

(6) Benches. Businesses, institutions, or uses that do not serve food or beverages, excluding vending machines, may be approved for outdoor bench seating.

REVIEW

Outdoor seating may be included as an element of an overall application for development order approval, or as an amendment to an existing development order. In addition to any other requirements contained herein, each application for approval of outdoor seating shall include the following information:

(1) Site plan. A site plan, at a scale acceptable to the city, which indicates the following:

A. The building for which the outdoor seating is proposed;

B. The location of the restaurant or food service establishment;

C. The proposed location of the outdoor seating, including any fencing,

D. Screening or materials to separate the seating area; and

E. The location of any sidewalks or other pedestrian walkways or passageways adjacent to or affected by the proposed outdoor seating; and the location of all existing or additional parking to be provided for the seating.

(2) Consent. A copy of the written consent of the individual, corporation, or other entity that owns the property upon which the outdoor seating will be located.

MINIMUM STANDARDS

Outdoor seating shall, at a minimum, comply with the standard listed below:

(1) Walkways. Outdoor seating shall be arranged, when in use, in a manner that provides a pedestrian walkway of not less than four feet in width adjacent to each table.

(2) Multiple tenants. Outdoor seating located on a pedestrian walkway which provides access to more than one occupant of a building shall provide an unobstructed passageway of at least six feet in width. The unobstructed passageway shall be located adjacent to, but not through, the outdoor seating area.

(3) Location. Outdoor seating can be located only along the perimeter of the affected restaurant or food service establishment, but shall not be located in front of any other user or tenant.

(4) Prohibited location. Outdoor seating shall not be located within any area designated for parking.

(5) Fencing or screening. Unless located within an inner court, outdoor seating shall provide fencing or screening as a means to physically and visually separate such use from any adjacent public passageway or walkway. Fencing and screening shall be a maximum of three feet in height, and may include planter boxes or other dividers. Fencing and

screening shall not be provided through the use of tables, chairs, or other seating.

(6) Compatibility. Outdoor seating, including fencing and screening materials, shall be compatible in color and style with the exterior of the building. Signs, lettering, or advertising, excluding permitted menu board, shall not be attached to outdoor seating. Small labels may be permanently attached to the furnishing to identify ownership for security purposes.

(7) Storage. Outdoor seating and furnishings shall be stored in a secure manner when not in use.

(8) Hours of operation. Excluding outdoor seating located in inner courtyards, outdoor seating service shall comply with the hours of operation noted below:

A. Sunday through Thursday. All sales and service of food and beverages are prohibited between the hours of 11:00 p.m. and 7:00 a.m.

B. Friday through Saturday. All sales and service of food and beverages are prohibited between the hours of 12:00 p.m. and 7:00 a.m.

BICYCLE CIRCULATION & PARKING

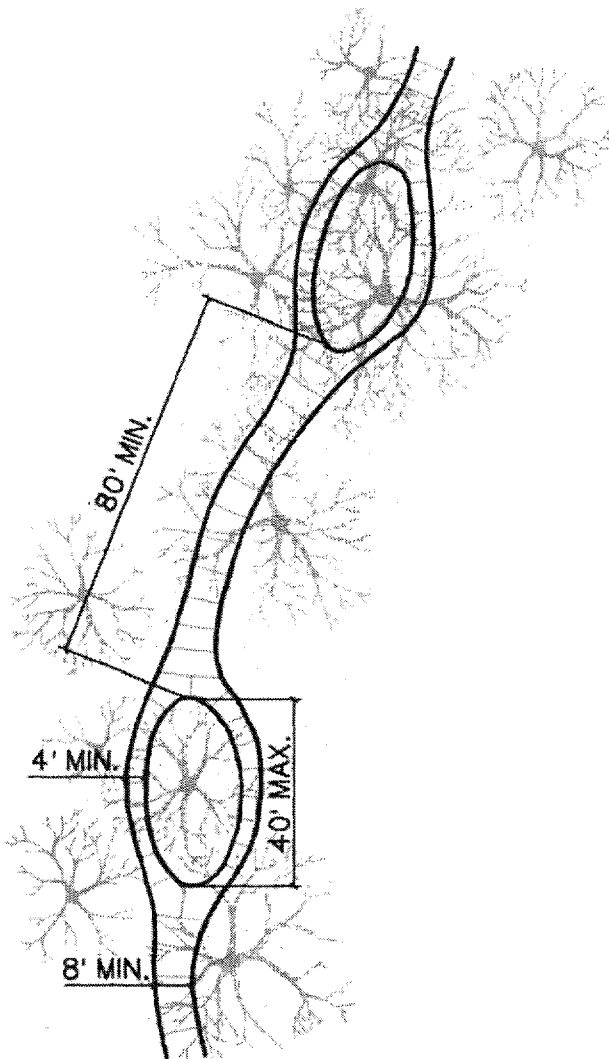
Bicycle parking shall be provided at the rate of 1 space per every 80 car parking spaces and they shall be conveniently located in the tree/ amenity zones of the streets next to major building entrances, recreational areas, and transit facilities.

REST AREAS (BIKE NODES)

A minimum of 1 covered rest station called a Bike Node must be provided for cyclists and pedestrians at strategic locations such as the Zocalo, the historic coral rock house, or the transit station.

CIRCULATION

The Park Quarter must include a combined sidewalk/bike trail that must be linked to Taft Street and the Residential Quarter. The minimum width of this sidewalk/bike-trail is 8'. Any portion may be divided into two separate paths of 4'.



Bike-trail dimensions through the 6-acre park

width each in order to 1) avoid low lying branches or trees, 2) better preserve soil aeration capacity, and 3) add variety.

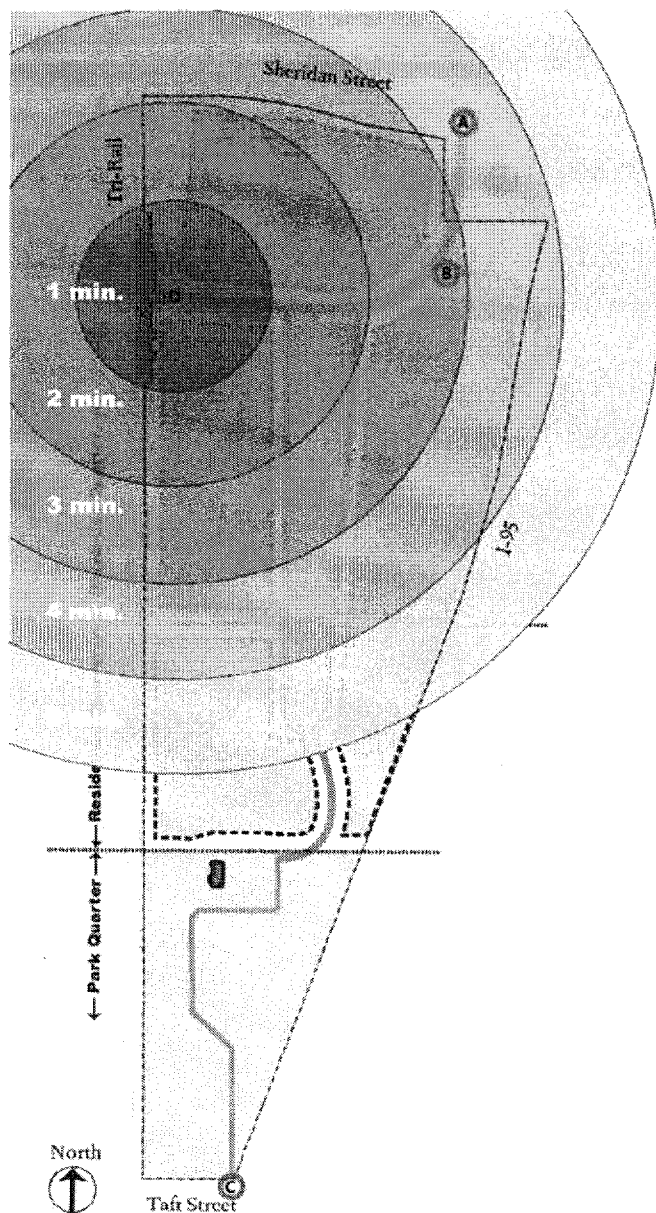
BIKE NODE. REQUIRED AMENITIES & STANDARDS

- Permanent, structured roof cover
- Shaded bench seating
- Trash receptacle
- Bicycle parking facility
- Adequate lighting
- Vegetation in close proximity
- Connection to at least one pedestrian walkway, street, or bike trail

BIKE NODE. OPTIONAL AMENITIES

- Beverage vending machine
- Emergency Call Box
- A piece of public art
- Drinking water fountain
- Bike trail/ area map display

3 POINTS OF CONNECTION



Points of Connection and 5 minute walking radii

POINTS OF CONNECTION

The following are designated as points of connection between the Sheridan Stationside Village district and its regional context:

- A. Intersection at Sheridan Street and North 29th Avenue
- B. Existing I-95 southbound ramp access-way and the new extension to it
- C. Intersection at Taft Street and "Park Drive".
- D. Entrance to the existing Tri-Rail "Sheridan Street Station"

It is required that the Intersection at Taft Street and "Park Drive" be located a minimum of 150' away from the railroad right-of-way along Taft Street.

Vehicular connection between Sheridan Street and the transit station is required to be as direct as possible with an emphasis given to establishing an early visual connection between the two.

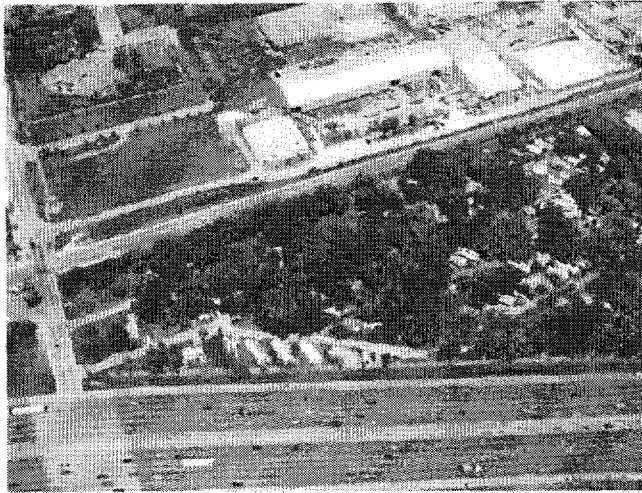
The Transit Station is required to provide immediate connection to the Bus Station, at least one structured parking deck that provides transit parking, and direct connection to a pedestrian sidewalk and bike trail.

4 COMMUNITY FACILITIES

THE 6-ACRE PARK

The 6-acre park must be a minimum of 6 acres, and it must include biking and walking trails as well as dedicated picnic areas.

Pedestrian trails and bike-paths may coincide in the 6-acre park area without segregation.



Live-Oak Canopy in the 6-Acre Park

THE ZOCALO & EVENTS SPACE

Only the "Thoroughfare", "Avenue", and "Drive" can border on the Zocalo, and the "Alley" is prohibited.

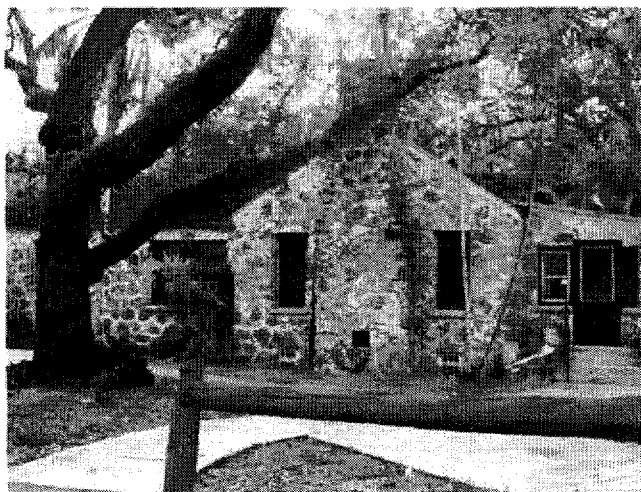
Allowed uses and activities of the Zocalo include:

- Retail sales & services
- Restaurant & Dining uses
- Temporary street vendors
- Water fountains
- Street fairs
- Community gatherings

The Mixed-Use Quarter must provide for an on-site storage location to store a limited amount of equipment and furnishings that may be utilized to hold an event.

THE HISTORIC CORAL ROCK HOUSE

The historic coral rock house is a rare example of an oolitic rock ("coral rock") construction in the Mediterranean Revival style. All efforts must be made in order to preserve this structure.



Historic Coral Rock House

SECTION 5
LANDSCAPE DESIGN GUIDELINES

LANDSCAPE AND IRRIGATION DESIGN

Landscape and irrigation requirements shall comply with the latest version of the City of Hollywood Landscape Manual, Hollywood, Florida, unless specifically noted in these design guidelines.

EXEMPTIONS: Parking areas located under or within the building envelope shall be exempt from all landscape requirements as described herein.

HARDSCAPE MATERIALS

The use of "hardscape materials" such as brick pavers, bollards, planters and pots and urns, are encouraged within the Sheridan Stationside Village project. In addition, high albedo materials such as porous concrete and more efficient percolating materials such as turf block may be considered.

CONCRETE AND BRICK PAVERS

Brick pavers shall be used on roads and pedestrian walkways to provide a visual relief from the asphalt and concrete surfaces, and as visual enhancements to denote key intersections, nodes and other 'areas of interest'. The use of brick pavers shall also serve to demark pedestrian zones from vehicular areas, including parking areas and crosswalks. The colors, styles and materials should be consistent and complimentary to the building architecture and color scheme and must follow current ADA guidelines for texture warnings and contrasting colors at crosswalks and curb cuts. Manufacturer specifications for installation, at a minimum, must be followed.

BOLLARDS

Bollards may be used for several functions: Spatial definition, separation of pedestrian and vehicular areas, security

and as lighting features.

When used for spatial definition, bollards should be spaced so as not to impede pedestrian circulation and shall adhere, at a minimum, to ADA guidelines for minimum accessible widths. These bollards should be complimentary to the building architecture in both material finish and color.

When used as traffic separators adjacent to parallel parking spaces, bollards shall be placed at a sufficient distance from the edge of curb so as not to interfere with car doors and accessibility.

When used as a security feature, bollards should be selected specifically for that purpose. Reinforced anchoring and heavier, stronger materials should be used, with respect to the building architecture still in mind, but not necessarily the top priority. Incorporating bollards of this type around drop off areas and the train station should be considered.

When used as a lighting feature, bollards should be placed along the main pedestrian walk ways and intersections. Low wattage fixtures should be given high consideration, as well as ambient light spill, sky pollution and vandal resistance.

HIGH ALBEDO MATERIALS

High Albedo Materials (from the National Ready Mix Concrete Association) - Concrete surfaces have a much higher albedo (measure of reflectance) than competitive paving materials. The inherently light color of concrete naturally reflects heat and light and are therefore encouraged. Studies have shown that concrete's reflectivity can lower lighting costs by as much as 30% over other pavements.

ON GRADE PLANTERS

On grade planters are planting areas surrounded by and set flush with impervious areas. If these planters are within a brick paver area, they are to be bordered by brick pavers in a single soldier row with mitered corners, with the same paver used for the edge treatment, or outside border, of the paver field. Soil is to be maintained at 3" below the edge of the brick paver to accommodate mulch or other similar treatment in order to prevent washout. These cutouts are not to be used if they occur within a pedestrian area and sufficient ADA accessibility is not achieved around the entire cut out, unless large trees or palms are to be planted and an ADA approved tree grate is used. Manufacturer's specifications for tree grate installation shall be adhered to. Landscape Plans shall provide an approved detail for this installation.

RAISED PLANTERS

Raised planters are enclosed above-ground planting areas that are used for vertical relief from the ground plane, for spatial definition, or to add height to other landscape elements. Raised planters, if utilized, shall have a minimum

top of wall height of 18" and a maximum wall height of 42". Raised planters shall not impede vehicular sight lines and shall be filled with drainage gravel, filter fabric and suitable soil as described within the landscape specifications. If large trees or palms are to be planted within raised planters, the minimum inside width of the planter shall be at least 2 times the width of the rootball of the tree or palm. Wall caps, material finishes and colors should compliment building architecture.

POTS AND URNS

Pots and urns may be utilized where raised planters and/or planter cut outs are not feasible or safe. These elements must not block access ways or cause pedestrian walkways to be non-conforming to ADA standards for accessible sidewalk widths. Material and color selections should be complementary to the building architecture and of a material that is both durable and vandal resistant. Plant material specified for these elements should be of a type not expected to out grow the restricted space and should be xeric in nature to allow for infrequent hand watering or drip irrigation.

LANDSCAPE MATERIALS

The intention of this section is not to restate the City of Hollywood's Landscape Ordinances or Landscape manual, but to supplement it. The City's codes are to be considered the minimum standards by which these guidelines adhere. Unless otherwise noted, the City's minimums shall apply.

TREES

The use of native canopy trees are greatly encouraged as a means of providing shade and adhering to Xeriscape, NatureScape, and LEED principles. Canopy Trees are encouraged in and around parking areas, to provide shade for parked cars, and along pedestrian walk ways and outdoor seating areas. Any existing on-site trees or palms intended to remain in place, shall be protected as per the City of Hollywood's Landscape Manual. Any on-site existing trees or palms scheduled to be relocated on site shall also adhere to all applicable local and state guidelines for pruning, excavating, storing and transplanting these materials.

Tree sizes should be in proportion to adjacent building heights and shall not interfere with any potential overhead utilities or required lighting. However, if trees are to be planted along the major vehicular and pedestrian streets through out the project, they shall be a minimum of 16-18' overall height, 8'

minimum spread, with a clear trunk of at least 6-7' for pedestrian clearance. These trees shall be planted a minimum of 4' from the edge of curb to avoid vehicular damage from overhanging limbs.

Each main ingress/egress point shall have at least one "Signature Tree" (or palm) on each side of the ingress/egress area as an identifying characteristic for the project. This tree should be of a species not commonly utilized and should not be repeated elsewhere on the project. Examples of such trees may include, but not be limited to: Southern Magnolia Tree, Specimen Oak Trees, Satin Leaf, Pandanus, Weeping Podocarpus, or African Tulip. These trees shall be at a minimum 18' height, 10' spread, with 7-8' of clear trunk and be Florida Fancy or better.

Flowering Trees may also be used, but should be limited to accents and shall not be considered shade or canopy trees.

Trees planted to satisfy the City's perimeter buffer requirements shall adhere to minimum code requirements for size and spacing.

PALMS

In South Florida, palms have become part of the vernacular. They are an easily recognizable symbol of the area and therefore should have a presence at Sheridan Stationside Village. The use of native palms, however, is greatly encouraged, due to their higher drought tolerance when compared to other more exotic species. Phoenix species, however, are a viable and widely acceptable exception to this rule. The City of Hollywood's Landscape Codes regarding minimum and maximum allowable percentages of palms in lieu of trees shall apply.

Palms are encouraged where there is not sufficient area to support a canopy shade tree. Their overall heights should be proportionate to the surrounding buildings and if palms are to be used as a main vertical element along the primary vehicular and pedestrian circulation routes, they shall be of a species with "vertical" growth characteristics (as opposed to "curved trunk"), with a single trunk, a minimum of 8' of "grey wood", "clear trunk" or "wood to nut" depending on the selected species. Examples of these types of palms include (but are not limited to) Royal Palms, Date Palms and Washington Palms. However, Washington Palms should be used sparingly and only if there is adequate wind protection, due to their susceptibility to severe damage in high winds.

If a "Signature Palm" is used as the identifying characteristic in the ingress/egress area instead of a tree, then this palm should be of a species not commonly utilized and should not be repeated elsewhere on the project. Examples of such palms may include, but not be limited to: Phoenix Reclinata, Canary Island Date Palm, or Bismarkia, Specimen Oak Trees,

Satin Leaf, Pandanus, Weeping Podocarpus, or African Tulip. These palms, if of a multi-trunk species, shall have a minimum of 7 canes and shall be a minimum of 18' overall height. Single trunk palm species shall have a minimum of 10' of "grey wood", "clear trunk" or "wood to nut". All Signature Palms shall be Florida Fancy or better.

HEDGES/SHRUBS

Hedges and shrubs are to be used for foundation plantings, screening and spatial definition. As mentioned throughout this section, the use of native and drought tolerant plant materials is greatly encouraged. Special attention should be given to the plant's light requirements, as this is an urban setting with many buildings blocking sunlight from penetrating, as well as their potential for environmental or human hazards, especially when planted adjacent to pedestrian walk ways.

When hedges are used for the screening of mechanical equipment or other unsightly above ground services (i.e., dumpsters), they shall be a minimum of 36" in height and planted leaf tip to leaf tip, to form an almost instantaneous screen. When used for foundation plantings, or as a perimeter property hedge, the City of Hollywood's minimum standards shall apply. However, the use of flowering hedges for these applications is strongly discouraged, due to their less opaque nature.

Flowering shrubs used as under planting for street trees or palms, is encouraged, as long as the plant's light and nutritional needs are met. Flowering shrubs as potted plants would also be considered an excellent application of this material. All shrubs shall adhere to the City of Hollywood's minimum standards for sizes and native requirements.

SOD

St. Augustine sod (*Stenotaphrum* spp.) shall be used on all areas of the site not covered by concrete, asphalt, brick pavers, building, or any other "hard surface", or shrub, hedge and groundcover beds. The sod line around planting beds, where applicable, should be a minimum of 4" from the edge of the shrubs, hedges and groundcovers, and a minimum of 18" from the trunks of trees and palms. Retention areas may be "seeded" if applicable and if these areas are not in direct public view.

LANDSCAPE CONSTRUCTION MATERIALS

PLANTING SOILS

TOPSOIL - Topsoil shall be a 80-20 mix, 80% fresh water sand (medium to coarse grade) and 20% inland glades muck thoroughly mixed with a commercial shredder/blender or equivalent. It shall be suitable for ornamental plant growth and free from hard clods, stiff clay, hardpan, gravel, subsoil, brush, large roots, refuse or other deleterious material, and of reasonably uniform quality. No site mixing will be acceptable. 6 inches of topsoil depth shall be required throughout.

PLANTING SOILS - Planting soil shall be an evenly blended mixture of 50% "muck", 25% coarse sand, 15% sphagnum peat moss and 10% sterilized cow manure. Add 2 pounds of fertilizer, specified to each cubic yard of soil and thoroughly mix. Mix shall be suitable for plant growth and free from hard clods, stiff clay, hardpan, gravel, brush, large roots, nematodes, weeds, refuse, or other deleterious material, and of reasonably uniform quality.

PALM SAND - Planting soil mixture to be placed as backfill around the root balls of all Palms shall consist of a mixture as specified above. Bottom ¼ of planting pit shall be backfilled with clean, coarse-grained builder's sand.

Final Specifications for soil conditioners and soil backfill mixtures shall be called out by the Landscape Architect and shall be appropriate for the plant species and micro-environmental conditions indigenous to the job site.

MULCH

Mulch shall be used in all planting beds and tree and palm rings throughout the site. Mulch helps to retain moisture and helps to prevent or minimize weeds. No Cypress Mulch, Melaleuca Mulch, or colored/dyed mulch should be used as each presents an unnecessary environmental impact. Generally, mulch is applied at 2-3" thick. The City of Hollywood's Landscape Code requirements shall apply.

ROOT BARRIERS

Root barriers shall be used where potential damage caused by certain species of trees known for spreading root systems is a concern. Refer to the City of Hollywood's Landscape Manual for specific root barrier requirements.

TREE BRACING

At a minimum, trees and palms shall be braced per Section 8 of the City of Hollywood's Landscape Manual. Bracing should remain in place for at least 6 months, to allow the tree or palm's roots system to become established. It is also recommended that wood bracing used for palms be painted either a dark green or black to reduce their visual impact.

MINIMUM LANDSCAPE REQUIREMENTS

At a minimum, the landscape requirements established in the City of Hollywood's Land Development Codes and Landscape Manual shall apply. These shall include rights-of-way buffers, perimeter property buffers, parking/vehicular areas, building foundations and equipment/utility screening requirements. Sizes and specifications, if in excess of City codes, shall supersede the City's code requirements.

LANDSCAPE LIGHTING

Low voltage or solar lighting can be used for pedestrian pathways as well as accent lighting for plant materials. Low voltage is not recommended (or in most cases permitted) for street lights or for illuminating public gathering places, however as a means of energy conservation, is greatly encouraged. Lighting used for the illumination of large trees and palms, should be durable, inconspicuous, and vandal resistant. If lighting is intended for the illumination of large trees and palms within a pedestrian walkway (tree grates), a "flush mounted" type fixture should be used to insure continued compliance with ADA guidelines. All lighting components must be UL (Underwriters Laboratories) approved and installed by a licensed electrician

IRRIGATION

As South Florida's population continues to grow, so does the demand for potable water and the ever increasing threat of drought. Because of this, water conservation has become extremely important to insure we have adequate supply for all the area's needs. New landscapes can have the greatest impact on our water supply as over 50% of all water used is for irrigation and other outdoor uses. By applying Xeriscape, Broward NatureScape and LEED principles, this percentage can be reduced significantly. The application of these conservation methods is greatly encouraged. At a minimum, 100% coverage must be achieved.

XERISCAPE

Following Xeriscape principles by planting drought tolerant plant materials, materials that require only minor maintenance, providing efficient irrigation and grouping materials with similar irrigation needs, will greatly aid in the conservation of our water supply, reduce long term costs, and be a lasting aesthetic improvement. The seven principles of Xeriscape may be found in Section 6 of the City of Hollywood's Landscape Manual. The Landscape Architect should be familiar with these principles and apply them wherever and whenever possible and applicable.

BROWARD NATURESCAPE

NatureScape encourages the use of native plants in landscaping. These plants are uniquely adapted to living in South Florida. Natives usually require little watering once established, and are naturally pest resistant, meaning limited use of expensive and toxic chemicals. "NatureScape is part of Broward County's overall water resource management plan – the Integrated Water Resource Plan (IWRP). This plan was developed to ensure there is enough quality water for everyone, today and in the future".

LEED FOR NEIGHBORHOOD DEVELOPMENT

LEED ND - Leadership in Energy and Environmental Design, Neighborhood Development – principles, as developed by the Congress for New Urbanism, Natural Resources Defense Council and the U.S. Green Building Council, shall be in effect for the Sheridan Stationside Village Project.

IRRIGATION MATERIALS

All piping that is to cross a paved/concrete/or brick paver area, is to be sleeved in Schedule #40 PVC. The use of drip irrigation, especially in contained areas, is encouraged. Low pressure valves, low angle trajectory irrigation heads, and "funny pipe" are also to be used as a means of conserving water and keeping installation and maintenance costs down. Proper, acceptable industry standards regarding workmanship and installation shall be adhered to.

RECLAIMED WATER

Sheridan Stationside Village will include a dual piping network that includes "purple pipe" to carry reclaimed water for irrigation purposes. The network will be connected to the City's reclaimed water system when it is determined by the Utilities Director that it is feasible and in the best interest of the City of Hollywood. Also, as a part of the desired LEED-ND certification, Sheridan Stationside Village will make every effort to implement a comprehensive stormwater management system that infiltrates, reuses, or evapotranspires runoff from 1" of rainfall from a significant percentage of the project's development footprint and other areas that have been graded as to be effectively impervious.

ENGINEERING DESIGN GUIDELINES

ENGINEERING DESIGN GUIDELINES

Engineering standards shall comply with the City of Hollywood's Land Development Regulations, unless specifically noted in these design guidelines.

ROADWAY

The minimum slope for roadways and roadway swales shall be 0.3% and the maximum shall be 7%.

Curbs shall be painted yellow and proper signs placed to indicate no parking zones in those areas where the Fire Department deems necessary for on-street laddering area. The three feet no parking pavement striping shall be eliminated in lieu of a painted curb and signage.

UTILITIES

Water, sewer and drainage utilities shall be located underground, within public right-of-ways or in easements, and they shall be shown on all as-built drawings.

Other public utilities (FP&L, Cable TV, BellSouth, gas, etc.) are expected to be randomly placed and will not necessarily be placed within public right-of-ways; however, they will have to be underground and easements based on as-built information will be required.

Minimum separation distances between water, sewer and drainage systems shall comply with F.A.C. Rule 62-555.314.

The following table summarizes those requirements.

DRAINAGE

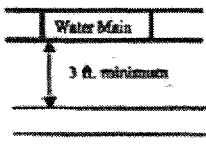
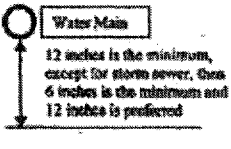
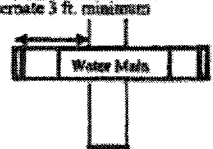
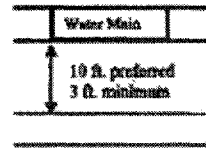
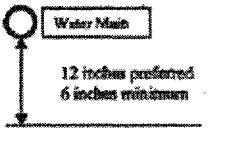
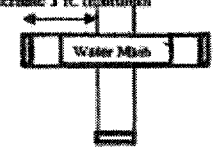
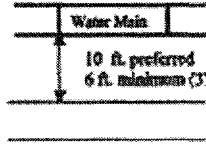
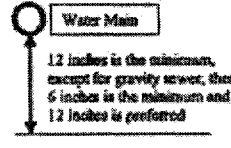
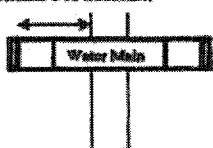
Underground storage cisterns may be allowed in lieu of or in combination with lakes or other surface-water runoff storage systems.

Dry retention/detention areas shall have a side slope of 3:1.

Lakes may be bordered by bulkhead for its entire perimeter. However, a boat launching system must be provided so as to facilitate maintenance from the water body itself. Lake maintenance easements may be eliminated in lieu of a perimeter walkway system adjacent to the bulkhead and the provision of a boat launching facility.

For safety reasons a six-foot wide earthen shelf, no greater than eighteen inches (18") below the high water table, shall be provided adjacent to and for the entire length of a bulk-headed water body.

LOCATION OF PUBLIC WATER SYSTEM MAINS IN ACCORDANCE WITH F.A.C. RULE 62-555.314

| Other Pipe | Horizontal Separation | Crossings (1) | Joint Spacing @ Crossings (Full Joint Centered) |
|---|---|--|---|
| Storm Sewer, Stormwater Force Main, Recycled Water (2) |  |  |  |
| Vacuum Sanitary Sewer |  |  |  |
| Gravity or Pressure Sanitary Sewer, Sanitary Sewer Force Main, Recycled Water (4) |  |  |  |
| On-Site Sewage Treatment & Disposal System | 10 ft. minimum | --- | --- |

(1) Water main should cross above other pipe. When water main must be below other pipe, the minimum separation is 12 inches.

(2) Recycled water regulated under Part III of Chapter 62-610, F.A.C.

(3) 3 ft. for gravity sanitary sewer where the bottom of the water main is laid at least 6 inches above the top of the gravity sanitary sewer.

(4) Recycled water not regulated under Part III of Chapter 62-610, F.A.C.

Disclaimer - This document is provided for your information only. Please refer to F.A.C. Rule 62-555.314 for additional construction requirements.

7 GENERAL REQUIREMENTS

GENERAL REQUIREMENTS

To the extent that the provisions of this PD Guidelines (Development Guidelines for Sheridan Stationside Village) constitute a modification to the provisions of Articles 7 and 9 of the City Code, City Commission approval of these PD Guidelines constitutes express modification of the applicable provisions of Articles 7 and 9 of the City Code, pursuant to Section 4.15 E. 6 and 7 of the City Code.

All construction standards shall be consistent with and supplement applicable standards in city codes, ordinances and regulations, and other agency standards.

8
APPENDIX

EXAMPLE PARKING REQUIREMENT CALCULATION

Uses being served by the parking structure:

Office = 150,000 sf

Retail = 100,000 sf

Hotel = 150 rooms

Residential = 100 units

Base Rate required parking:

Office – 150,000 * (2.5/1,000) = 375 spaces

Retail – 100,000 * (4/1,000) = 400 spaces

Hotel – 150 * (0.75) = 112.5 spaces = 113 spaces

Res. – 100 * (1.3) + 100/5 = 130 + 20 = 150 spaces

TOTAL = 1,038 spaces

Shared Parking Table:

In order to prepare the table, you must multiply the base rate required parking for each use by the percentage in each time period that is shown in Tables 2 & 3 of the "Minimum Parking Requirements" section of Chapter 1.

Because this parking structure serves other uses, the guest parking requirement of 20 spaces for the residential is waived (Only 130 residential spaces are included in the tables).

Based on the adjacent tables, the required parking for these uses would be the maximum number of spaces needed from any of these two tables.

Total required parking = 869 spaces

Parking required by base rate = 1,038 spaces

Total parking reduction = 169 spaces (16%)

Table 2 – Shared Parking Matrix (Weekday)

| Time | Res. | Retail | Office | Transit | Rstrnt. | Hotel | Total |
|-------------|-----------|------------|------------|----------|----------|-----------|------------|
| 6 AM | 130 | 0 | 11 | 0 | 0 | 113 | 254 |
| 7 AM | 113 | 32 | 75 | 0 | 0 | 96 | 316 |
| 8 AM | 103 | 72 | 236 | 0 | 0 | 73 | 484 |
| 9 AM | 95 | 168 | 349 | 0 | 0 | 62 | 674 |
| 10 AM | 88 | 272 | 375 | 0 | 0 | 51 | 786 |
| 11 AM | 77 | 348 | 375 | 0 | 0 | 40 | 839 |
| 12 PM | 78 | 388 | 338 | 0 | 0 | 34 | 837 |
| 1 PM | 77 | 400 | 338 | 0 | 0 | 34 | 848 |
| 2 PM | 78 | 388 | 364 | 0 | 0 | 40 | 869 |
| 3 PM | 79 | 380 | 349 | 0 | 0 | 40 | 848 |
| 4 PM | 86 | 348 | 289 | 0 | 0 | 51 | 773 |
| 5 PM | 100 | 316 | 176 | 0 | 0 | 68 | 660 |
| 6 PM | 111 | 328 | 86 | 0 | 0 | 79 | 604 |
| 7 PM | 122 | 356 | 26 | 0 | 0 | 85 | 589 |
| 8 PM | 125 | 348 | 26 | 0 | 0 | 102 | 601 |
| 9 PM | 127 | 244 | 11 | 0 | 0 | 107 | 490 |
| 10 PM | 129 | 128 | 11 | 0 | 0 | 113 | 381 |
| 11 PM | 130 | 52 | 0 | 0 | 0 | 113 | 295 |
| 12 AM | 130 | 0 | 0 | 0 | 0 | 113 | 243 |

Table 3 – Shared Parking Matrix (Saturday)

| Time | Res. | Retail | Office | Transit | Rstrnt. | Hotel | Total |
|-------------|-----------|------------|-----------|----------|----------|-----------|------------|
| 6 AM | 130 | 0 | 0 | 0 | 0 | 102 | 232 |
| 7 AM | 124 | 12 | 11 | 0 | 0 | 79 | 226 |
| 8 AM | 114 | 40 | 38 | 0 | 0 | 68 | 260 |
| 9 AM | 105 | 120 | 49 | 0 | 0 | 57 | 331 |
| 10 AM | 92 | 180 | 49 | 0 | 0 | 45 | 366 |
| 11 AM | 92 | 292 | 64 | 0 | 0 | 40 | 488 |
| 12 PM | 92 | 340 | 64 | 0 | 0 | 34 | 530 |
| 1 PM | 91 | 380 | 49 | 0 | 0 | 34 | 554 |
| 2 PM | 92 | 400 | 38 | 0 | 0 | 40 | 569 |
| 3 PM | 95 | 400 | 26 | 0 | 0 | 45 | 566 |
| 4 PM | 98 | 360 | 26 | 0 | 0 | 57 | 540 |
| 5 PM | 105 | 300 | 11 | 0 | 0 | 68 | 484 |
| 6 PM | 111 | 260 | 11 | 0 | 0 | 79 | 461 |
| 7 PM | 113 | 240 | 11 | 0 | 0 | 90 | 455 |
| 8 PM | 120 | 220 | 11 | 0 | 0 | 102 | 453 |
| 9 PM | 124 | 160 | 0 | 0 | 0 | 107 | 391 |
| 10 PM | 125 | 152 | 0 | 0 | 0 | 113 | 390 |
| 11 PM | 127 | 52 | 0 | 0 | 0 | 113 | 292 |
| 12 AM | 130 | 0 | 0 | 0 | 0 | 113 | 243 |

GLOSSARY

"Funny" Pipe:

This is a type of flexible irrigation pipe that is used in areas that are too small or otherwise restrictive to the use of conventional PVC irrigation pipes. This kind of pipe is usually available as 1/4 inch thick pipe made out of Polyurethane.

High Albedo Materials:

These are materials that are highly reflective of visible light. They help avoid trapping heat from sunlight by reflecting light energy back into the sky. Materials such as concrete or others that are painted white are considered High Albedo Materials.

Purple Pipe:

These pipes are usually the regular PVC pipes that are simply color-coded purple. The color purple on such pipes indicates that it carries reclaimed or recycled water.

Exhibit "C"

CONDITIONS OF APPROVAL

STATIONSIDE VILLAGE PLANNED DEVELOPMENT

The conditions of approval are as follows:

1. **Workforce Housing.** Twenty Percent (20%) of the one thousand fifty (1,050) dwelling units (211 units) within the Project will be "workforce-income" housing units, defined in Section 420.5095, F.S. (2007), as housing for persons having a total annual anticipated income for the household that is not more than one hundred forty percent (140%) of the area median income, adjusted for household size.

2. **Green Development.** The Developers intend to design and develop the Project utilizing principles of sustainable or "green" design and "smart growth" for the benefit of the community, the future building occupants, tenants and owners, and the global environment in general. Toward that end, the Developers have registered the project with the US Green Building Council's ("USGBC") Leadership in Energy and Environmental Design ("LEED") for Neighborhood Developments ("LEED-ND") Pilot Program ("Pilot Program"). The Pilot Program is intended to gauge the effectiveness of the LEED Neighborhood Development Rating System and does not require registered pilot projects to achieve LEED certification.

The Developers shall not be held individually, jointly, severably, or otherwise liable for failure of all or any part of the Project to achieve certification under any sustainable design or development program described herein. Furthermore, failure of all or a part of the Project to achieve certification under the standards set forth herein shall not delay, prohibit, or prejudice any other applications submitted in connection with the Project that have been or are required to be presented before any county or municipal governmental body, commission, or board.

3. **Reclaimed Water.** To the extent available and permitted by all regulatory authorities with jurisdiction, the Project shall incorporate the use of reclaimed water for non-potable uses such as toilet systems, irrigation, mechanical systems and custodial uses.

4. **Landscaping Plan.** The Project will employ a landscape and irrigation design consistent with the Xeriscape principles adopted by the "NatureScape Broward" program and the City of Hollywood Landscape Manual, and incorporate same into the Sheridan Stationside Development Regulations. The Developers will make reasonable efforts to obtain certification under the University of Florida's Florida Yards and Neighborhoods Program ("FY&N"), by incorporating as many items as possible from the checklist attached hereto as **Exhibit "1"**. At a minimum, the landscaping plan for the Project will use native flora and be designed to conserve water, reduce contaminated runoff, and provide a viable habitat for indigenous wildlife.

5. **Comprehensive Traffic Mitigation Plan.** Except as set forth on the attached **Exhibit "2"**, prior to issuance of the first certificate of occupancy for any development within the Sheridan Stationside TOD, Developers agree, subject to regulatory approval, to construct the road improvements more particularly described in **Exhibit "2"** attached hereto.

6. **"Okomo Coral House" and Park.** The Developers agree to preserve the "Okomo Coral House" at its existing location and a significant portion of the surrounding oak hammock, which shall be offered for dedication and designed in cooperation with the City of Hollywood for use as minimum 6-acre public city park ("Public Park"). The Public Park shall be improved and offered to the City no later than as required by the City's Concurrency requirements. Said Public Park to be platted prior to acceptance by the City.

Exhibit "1"



Yard Certification Checklist

Does your yard measure up?

Florida Yards and Neighborhoods (FY&N) honors model landscapes as certified Florida-Friendly Yards.

To be certified as Florida Yard, your landscape must:

- Collect at least 36 inches on this Yardstick Checklist
- Receive full points for practices marked with 2 asterisks**
- Receive partial credit for practices marked with 1 asterisk*
- Comply with all existing codes and laws

Water Efficiently

- Mow lawns high to encourage a deeper, more drought and pest tolerant root system. ** 2"
- Irrigate lawn and landscape only when they wilt. Apply < 3/4 inches water per application. 3"
- For a yard that uses an irrigation system (in-ground or hose-end sprinklers):*
- Calibrate irrigation/sprinkler system to apply < 3/4 inches of water. ** 3"
- Put a rain gauge in your yard to track irrigation amounts. ** 2"
- Install a rain shut-off device for in-ground irrigation systems. ** 2"
- Make sure irrigation system waters lawn areas separately from plant beds. 2"
- Use drip or micro-irrigation in plant and flower beds. 2"
- For a yard that does not use an irrigation system:*
- Design and maintain a landscape that exists predominantly on rainfall once plants are established. 6"

Mulch

- Maintain a 2-3" layer of organic mulch over tree roots, shrubs and plant beds, leaving a 2 inch space between the plant base and the mulch. * 2"
- Create self-mulching areas under trees where leaves can remain as they fall. 1"
- Use by-product mulches such as pine bark, melaleuca or recycled mulches. 1"
- Replenish mulch once or twice a year to maintain 2-3" depth. 1"

Recycle

- Whenever possible, recycle grass clippings by allowing them to remain on the lawn. ** 2"
- Use leaves and pine needles found in your yard as mulch. 2"
- Create and maintain a compost pile with yard clippings, leaves, kitchen scraps, etc. 3"

Wildlife

- Plant vines, shrubs, and trees that provide cover, nesting areas or food sources for birds, butterflies and other wildlife. 3"
- Provide a water source, such as a bird bath or a small pond for wildlife. 1"
- Provide wildlife shelters such as a bat house, bird house, brush pile, etc. 1"
- Identify five kinds of wildlife (insects, reptile, birds, etc.) that live in your yard. 2"

Yard Pests

- Treat only affected plants or lawn areas with pesticide applications. Avoid indiscriminate spraying. ** 3"
- Check your landscape every 1-2 weeks for signs of problems. 2"
- Learn to identify 5 beneficial insects that provide natural control of harmful pests. 2"
- Use environmentally friendly pesticides such as horticultural oils and insecticidal soaps. 2"
- Use non-chemical approaches to pest control, such as pruning off affected areas, hand removing insects, etc., whenever possible. 3"

Right Plant - Right Place

| | |
|---|----|
| Ensure that your landscape does not contain plants identified by legal code as invasive exotics, such as Brazilian Pepper, Melaleuca, Australian Pine, and Chinese Tallow. ** | 2" |
| Replace problem-prone plants with low maintenance native or non-native species. | 2" |
| Group plants according to their water and maintenance needs. | 2" |
| Determine how much grass you need for children, pets, and recreation. Replace the rest with low maintenance ground covers, shrubs, mulch, or other porous surfaces. | 3" |
| Use trees and shrubs to shade eastern and western walls of home and air conditioner compressor. | 1" |
| Use deciduous trees on southern exposures to allow the sun to passively heat your home in winter. | 1" |
| Reduce yard waste by choosing plants that will not require frequent pruning at maturity. | 1" |
| Preserve native plants when building on a new site. Maintain a protective "do not disturb" barrier under the drip line of trees. | 3" |

Fertilizing

| | |
|--|----|
| Fertilize as needed to maintain quality of lawns and landscape plants. * | 2" |
| Use natural organic or other slow release fertilizers. * | 2" |
| Use iron instead of nitrogen to make your lawn green during the summer. | 1" |

Stormwater Runoff

| | |
|--|----|
| Direct downspouts and gutters to drain onto the lawn, plant beds, or containment areas. * | 1" |
| Plant groundcovers or use mulch on thinly vegetated areas to decrease erosion. * | 2" |
| Use mulch, bricks, flagstones, gravel, or other porous surfaces on walkways, patios or drives. | 1" |
| Collect and use rainwater to irrigate plants. | 2" |
| Create swales or terracing to catch and filter stormwater. | 3" |
| Pick up after pets to reduce bacterial and nutrient pollution in stormdrain systems. | 1" |
| Clean up oil spills and leaks using cat litter on driveways. | 2" |
| Sweep grass clippings, fertilizer, and soil from driveway onto lawn. Remove trash from street gutters. | 2" |

On the Waterfront

| | |
|---|----|
| Remove invasive exotic aquatic plants by cutting, pulling, or raking. Remove dead plant material from water after using herbicides to reduce pollution. | 2" |
| Protect your mangroves. All pruning must be in compliance with existing laws. ** | 2" |
| Establish a border of low maintenance plants between your lawn and shoreline/seawall to absorb nutrients and to provide wildlife habitat. | 2" |
| Establish a 10-30foot "no fertilizer" zone along your shoreline. | 2" |
| Where feasible, plant native vegetation in the littoral zone in front of your seawall or along shoreline. | 4" |
| Decrease wave action and increase habitat by placing clean, native limestone rock in front of your seawall. | 3" |

TOTAL INCHES _____

If your yard measures up, call your local Cooperative Extension Service and ask for a Florida Yard Adviser.

Exhibit "2"

Schedule of Roadway Improvements

2010 2013 2018

| | | | | |
|----------------------------|--|---|---|---|
| Land Use Plan Amendment | <u>Sheridan Street at N. Park Road</u> <ul style="list-style-type: none"> • Installation of video detection software/hardware; and • Installation of northbound right-turn overlap phase with opposing U-Turn Yield to right-turn sign (R10-16) | ● | | |
| | <u>Sheridan Street at N. 29th Avenue</u> <ul style="list-style-type: none"> • Reconstruct the northbound approach to include a left-turn lane, a shared left-through lane, and dual right-turn lanes • Construct an additional westbound left turn lane • Reconstruct the eastbound approach laneage to provide for a left-turn lane, two through lanes designated for Interstate 95 northbound traffic, three through lanes, one through lane designated for Interstate 95 southbound traffic, and a right-turn lane into the site; and • Modify the existing traffic signal to accommodate the proposed improvements. | ● | | |
| | <u>Sheridan Street at Interstate 95</u> <ul style="list-style-type: none"> • Construct an additional left-turn lane on the southbound off-ramp • Construct an additional left-turn lane on the northbound off-ramp • Increase the merge distance on the southbound entrance ramp; and • Modify the existing traffic signals to accommodate the proposed improvements. | ● | | |
| | <u>Taft Street at SR 7/US 441</u> <ul style="list-style-type: none"> • Construct an westbound right turn lane • Installation of eastbound right-turn overlap phase with opposing U-Turn Yield to right-turn sign (R10-16) | ● | | |
| | <u>Taft Street at N. 26th Avenue</u> <ul style="list-style-type: none"> • Installation of traffic signal • Construct eastbound, westbound, northbound and southbound left turn lanes • Signal timing optimization | ● | | |
| | <u>Taft Street at US 1/Federal Highway</u> <ul style="list-style-type: none"> • Construct eastbound and westbound right turn lanes | ● | | |
| Local Operational Analysis | <u>Sheridan Street at SR 7/US 441</u> <ul style="list-style-type: none"> • Installation of video detection software/hardware • Installation of northbound, eastbound and westbound right-turn overlap phase with opposing U-Turn Yield to right-turn signs (R10-16) • Signal timing optimization | ● | | |
| | <u>Sheridan Street at N. 56th Avenue</u> <ul style="list-style-type: none"> • Installation of additional northbound and southbound left turn lanes • Signal timing optimization | | ● | |
| | <u>Sheridan Street at N. 46th Avenue</u> <ul style="list-style-type: none"> • Installation of additional northbound and southbound left turn lanes • Extend eastbound left turn lane to 275 feet • Extend westbound left turn lane to 330 feet • Signal timing optimization | | | ● |

| | | | | |
|-------------------------|---|---|--|---|
| | <u>Sheridan Street at N. 40th Avenue</u> <ul style="list-style-type: none"> • Re-stripe pavement markings on north and south legs • Signal timing optimization | | | ● |
| | <u>Sheridan Street at N. 29th Avenue</u> <ul style="list-style-type: none"> • Re-stripe the southbound approach to include a left-turn lane, a shared left-through lane, and dual right-turn lanes • Signal timing optimization | ● | | |
| | <u>Sheridan Street at N. 26th Avenue</u> <ul style="list-style-type: none"> • Installation of video detection software/hardware • Installation of southbound right-turn overlap phase with opposing U-Turn Yield to right-turn sign (R10-16) • Signal timing optimization | ● | | |
| | <u>Taft Street at SR 7/US 441</u> <ul style="list-style-type: none"> • Installation of video detection software/hardware • Signal timing optimization | ● | | |
| | <u>Taft Street at Okomo Drive</u> <ul style="list-style-type: none"> • Construct an 200 foot eastbound left-turn lane • Construct and exclusive westbound right-turn lane • Construct exclusive southbound left-turn and right-turn lanes | ● | | |
| | <u>Taft Street at N. 28th Avenue</u> <ul style="list-style-type: none"> • Re-stripe pavement markings at intersection | ● | | |
| Contingent Improvements | <u>Taft Street at N. 40th Avenue*</u> <ul style="list-style-type: none"> • Construct a northbound and southbound left-turn lane | | | ● |

* - Proposed Improvements at this location are scheduled for construction by Memorial Hospital by year 2012. In the event that Memorial Hospital improvement are not completed, construction of this improvement will be the responsibility of the applicant for phase year 2018.

CONSISTENCY WITH THE COMPREHENSIVE PLAN

The Sheridan Stationside Transit Oriented Development (TOD) is requesting a rezoning to Planned Development (PD). The goal of the Land Use Element is to "promote a distribution of land uses to enhance and improve the residential, business, resort and natural communities while allowing land owners to maximize the use of their property." The surrounding community has a mix of commercial, single family and multifamily developments.

Land Use Element Objective 6: Encourage multi-use areas and mixed use concentrations of density near existing or planned major employment centers and major transportation routes in order to promote energy conservation and mass transit, preserve air quality, reduce the cost of services, encourage affordable housing and promote economic development.

Land Use Element Policy 8.11: The City shall consider the individual and cumulative impacts of land use plan amendments on historic resources and on the existing and planned transportation facilities.

Transportation Element Policy 3.2.1 The City shall revise its Land Development Code and develop programs to promote the use of alternative transportation modes, including but not limited to, bicycle, pedestrian, and waterways.

Conservation Element Objective 10: Encourage an increase in the size of green space and existing natural areas.

Recreation and Open Space Element Objective 1: Satisfy the recreation and open space needs of residents and visitors of all ages by adequate and efficient provision of both facilities and programs through the coordination of both public and private development and the utilization of private and public resources.

The TOD land use is designed to facilitate multi-use and mixed-use development, including encouraging a reduction for the need for automobile travel. Implementation of a Planned Development zoning, in association with this new land use, will allow the applicant to create a unique village atmosphere on the approximate 40-acre site.

CONSISTENCY WITH THE CITY-WIDE MASTER PLAN

Sheridan Stationside Village is found in Sub-Area 6, which includes the neighborhoods of Hillcrest, Hollywood Hills and Park East. More particularly, Sheridan Village is included as part of the Park East neighborhood. Housing in this Sub-Area is considered a component that can serve as a foundation to be built upon, rather than one facing serious redevelopment issues. Some of the neighborhood issues for Park East include preserving the existing housing stock, improving the streetscape and traffic calming to prevent speeding and pass-through traffic.

Also included in this sub-area is a mix of, commercial, office, institutional and industrial uses. Of these uses, there are a number of sub-sectors that function relatively independently, including the "underutilized" Tri-Rail station in this project. This station is identified as having "long-term potential based on market demand to create a mass

transit village linked to the Tri-Rail station with underutilized large land area with available infrastructure for development.”

Guiding Principles:

- Attract and retain businesses that will increase economic opportunities for the City while enhancing the quality of life for residents.
- Provide pedestrian, vehicular and public transportation linkages locally and regionally.
- Promote the highest and best use of land in each sector of the City without compromising the goals of the surrounding community.
- Preserve stable single-family neighborhoods while providing alternative housing options that will meet the needs of all residents in the City of Hollywood.

The proposed PD zoning designation will allow for construction of a more harmonious development for the site and will assist in achieving the City’s vision of transit-oriented improvements. Sheridan Stationside Village will be a true mixed-use development integrating the existing Tri-Rail station as a hub along with other providers such as Broward County Transit. Located at the intersection of I-95 and Sheridan Street, the proposed development will create a diverse urban village fostering a live, work, play and ride atmosphere. A mix of housing at varying densities will transition from higher densities near the station in the “Mixed-Use Quarter” to lower densities towards the south in the “Residential Quarter”.

Policy CW.15: Place a priority on protecting, preserving and enhancing residential neighborhoods.

Objective 6: Encourage multi-use areas and mixed uses concentrations of density near existing or planned major employment centers and major transportation routes in order to promote energy conservation and mass transit, preserve air quality, reduce the cost of services, encourage affordable housing, and promote economic development.

Policy CW.72: Provide transportation solutions that connect residents and visitors with employment, shopping, entertainment, recreation, medical, training and education, and public services venues.

Policy 6.1: Upgrade commercial development along...Sheridan Street by revising zoning regulations to accommodate mixed-use developments, when appropriate.

Planned Developments such as Stationside Village will further economic development and promote vitality in Hollywood, particularly within the urban areas near major transportation corridors through the use and establishment of mixed-use categories. A mix of housing at various densities will transition from higher densities near the station to lower densities towards the site’s “Residential Quarter”. This residential component, when combined with the office and commercial uses, will create an atmosphere where residents can live and work.

Policy 6.12: Provide density bonuses to Inter-Modal Transportation Oriented Developments.

Policy 6.17: Identify appropriate locations within the City to encourage greater densities and intensities to promote mixed-use developments, such as the transit village concept.

Broward County Transit and Shuttles currently provide fixed-route service to the site. Future route and service expansion improvements for the area have been identified in the five-year Transit Development Plan. The South Florida Regional Transit Authority has nearly completed its Double Track Corridor Improvement Program, which will increase headways between trains and reduce travel times. In the 2030 Long Range Transportation Plan a joint use development project for corridor improvements is listed for the area.

Although the proposed development has a significant impact in transit demand, future headway and route improvements identified in the five-year update to the Transit Development Plan, and in the proposed BCT Transit Investment Plan should provide adequate capacity to meet the future service demands.

The existing multi-modal Tri-Rail Station area is envisioned by FDOT and Tri-Rail to become "a more intensely developed station site area with potential mixed-uses that will be directly connected to the rail system, the Interstate roadway system and the City of Hollywood." Ridership is expected to increase at both this station and the Hollywood Boulevard station once the additional north-south service line is complete.

Policy CW.69: Promote transit use by residents and visitors through innovative land use planning techniques, as well as, close coordination and possible partnering with transit service providers, including Broward County Transit.

Sheridan Stationside Village will combine livable a community with existing and expanded mass transit facilities. Neighborhoods include a range of housing options, jobs, commercial services, and recreational opportunities all within easy access of transit services. Communities such as this are designed to enable residents, employees, and shoppers to visit commercial establishments, go to work, and return home, all without the need of individual automobiles.

TOD's such as Sheridan Stationside Village are most effective when they are urbanized, even in a suburban setting. Pedestrian-scale designs, such as compact blocks, pleasant walkways and engaging public spaces, draws people to return repeatedly. Urban development supports transit oriented uses where suburban development does not.

Policy CW.134: Implement transportation strategies that reduce auto travel and improve traffic flow, thereby reducing auto emissions.

Policy CW.145: Establish land use patterns and transportation facilities that encourage the use of public transportation and reduced trip lengths in an effort to alleviate vehicle emissions.

Transit Oriented Developments are commonly located in higher-density, mixed-use, urban pedestrian districts with high quality transit service. External single-occupancy vehicle trips can be reduced by developments such as these as much or more by people walking within a mixed-use urban district as they can by using transit within and between transit centers. The incorporation of the existing Tri-Rail station will also help reduce the amount of single-occupancy vehicles throughout the rest of the city.

SUMMARY OF PARK EAST NEIGHBORHOOD PLAN

Objective: To evaluate and develop traffic patterns that are both safe and functional.

Objective: To preserve the residential character of the neighborhood.

Objective: To preserve the current residential density levels.

Sheridan Stationside Village will meet the objectives set forth by the Park East Neighborhood through the development of residential units along Sheridan Street. Corridor-based developments such as these will help preserve existing neighborhoods by reducing the possibility for higher-density intrusion in the communities.

SUMMARY OF FINDINGS:

Analysis of Criteria and Findings for Rezoning as stated in the City of Hollywood Zoning and Land Development Regulations, Article 5.4 (G)(4)(a)(b).

- A. That the petition for a change of zoning does not meet any one of the following criteria whereby the request would be considered contract or spot zoning:

CRITERIA 1: The proposed rezoning would give privileges not generally extended to property similarly situated in the area.

ANALYSIS: The Planned Development (PD) zoning district allows for the development of parcels with greater flexibility by removing some of the detailed restrictions of conventional zoning. As mentioned previously, this site is currently requesting adoption of a land use designation of Transit Oriented Development (TOD). The purpose of a TOD is to "encourage mixed use development in areas served by regional transit stations, such as Tri-Rail stations, major transit hubs, and neighborhood and regional transit stations..." The PD rezoning will also allow for consistency with the TOD land use. The applicant states "The Property is a valuable potential TOD site bordered by the Sheridan Station Tri-Rail Station and I-95. The area is also in need of redevelopment and the proposed urban in-fill development is consistent with the surrounding areas."

FINDING: Inconsistent

CRITERIA 2: The proposal will result in similarly situated property being treated differently.

ANALYSIS: The City-Wide Master Plan and the Comprehensive Plan encourage development for this parcel. Currently underway is a land use request to TOD for the site, which will allow mixed-use development. However, the current zoning districts do not allow for mixed-use developments. The proposed PD zoning district allows mixed use, which is consistent with the manner in which other properties in the area have been treated.

FINDING: Inconsistent

CRITERIA 3: The proposed rezoning request does not fall within the existing land use designation(s) for the subject property.

ANALYSIS: The proposed rezoning does fall within the existing land use for the subject property and the PD zoning district is consistent with the TOD land use. The existing Trailer Park District (TD) zoning allows for residential trailers and prohibits commercial/office uses for those other than shops for the care or treatment of trailer occupants. Additionally, the Industrial and Commercial zonings do not allow for residential development. The Planned Development zoning designation will provide the applicant with a zoning classification better-suited for the land use by allowing mixed-use developments.

As provided by the applicant, "The proposed rezoning will provide consistency between the pending TOD designation and the zoning applicable to the Property. The PD District encourages "mixed uses establishing combinations of residential with commercial and/or office uses and supporting facilities" and "provide[s] maximum opportunity for application of innovative site planning concepts to the creation of aesthetically pleasing environments for living on properties of adequate size, shape and location." The development has been designed to comply with these guidelines and will create a diverse transit village that promotes the urban concept of work, live, play and ride."

FINDING: Inconsistent

CRITERIA 4: The proposed change will result in an isolated district unrelated to adjacent or nearby districts.

ANALYSIS: The flexible nature of the PD zoning district permits the establishment of specialized regulations to ensure compatibility with the surrounding neighborhoods. With its proximity to a mix of uses, including residential, commercial and industrial, the incorporation of a mixed-use PD will not result in a development unrelated to nearby districts. "The site is surrounded by property zoned for industrial and commercial uses. However, the development does not result in an isolated district because it is designed as a true mixed-use development and will incorporate residential, hotel, office and retail uses with the existing Tri-Rail station and other forms of Broward County transit" states the applicant.

FINDING: Inconsistent

Spot Zoning Finding:

Based on findings generated from the change of zoning criteria analysis, the application for change of zoning from C-4, TD and IM-1 to PD for properties generally located north

of Taft Street, south of Sheridan Street, east of CSX railway and west of I-95 does not appear to be spot zoning pursuant to the criteria listed in Section 5.4(G)(4)(a) of the Zoning and Land Development Regulations.

B. That the petition for change of zoning is consistent with six or more of the following criteria:

CRITERIA 1: The proposed change is consistent with and in furtherance of the Goals, Objectives and Policies of the Comprehensive Plan.

ANALYSIS: As identified earlier in this report in the "Consistency with the Comprehensive Plan" section, redevelopment of this parcel to a mixed-use project is in accordance with the goals of the Comprehensive Plan. One example is Objective 6, which is to "Encourage multi-use areas and mixed use concentrations of density near existing or planned major employment centers and major transportation routes in order to promote energy conservation and mass transit, preserve air quality, reduce the cost of services, encourage affordable housing and promote economic development."

The Comprehensive Plan further encourages the use of alternative transportation modes from automobile use. Sheridan Stationside Village will directly meet this objective through its incorporation of the existing Tri-Rail Station as a main component of the site. By creating a village-like atmosphere with residential and commercial uses within walking distance from not only the station, but also each other, the need for an automobile to visit multiple uses can be limited.

The applicant states "The proposed rezoning is consistent with the primary goal found within the Comprehensive Plan to 'promote a distribution of land use that will enhance and improve the residential, business, resort and natural communities while allowing land owners to maximize use of their property.' The proposed rezoning is also consistent with the objective of the City and County in creating the TOD land use designation"

FINDING: Consistent

CRITERIA 2: The rezoning will result in uses permitted under the proposed district classification which would be in the general public interest and/or would not be merely in the interest of an individual or small group.

ANALYSIS: The PD zoning district is encouraged because they "Allow mixed uses establishing compatible combinations of residential with commercial and/or office uses and supporting facilities." The Zoning and Land Development Code also encourages PDs as they "Conserve the natural amenities of the land by encouraging the preservation and improvement of scenic and functional space."

Sheridan Stationside Village proposes a mixed use development which is in the general public interest as set forth in the City-Wide Master Plan and Comprehensive Plan. A mixed use development such as this is not permitted within the current zoning designations of C-4, IM-1 and TD, thus necessitating a zoning change. As a transit oriented development, Sheridan Stationside Village will also promote pedestrian travel and encourage mass transit usage to further serve and benefit the public.

"The proposed development will serve the public interest by provided a mixed-use development that permits residents to work, live, plan and ride, reducing the need for personal automobile use and encouraging and supporting transit usage. The proposed development will also promote pedestrian activity by including open public spaces, wide pedestrian paths, active street front uses, and a community park. The design includes streetscape features such as street trees, seating benches, decorative light fixtures, information kiosks, and a public plaza."

FINDING: Consistent

CRITERIA 3: The proposed change will result in a development that is consistent in scale (building height, mass, siting) with other buildings in the neighborhood.

ANALYSIS: The subject property is unique as it is situated within close proximity not only to residential neighborhoods but also industrial and commercial developments. More specifically, it is located between the Rinker concrete plant to the west, I-95 to the east, Sheridan Street to the north and Taft Street to the south. Beyond I-95 to the east are multiple family and single family homes. Land uses in these areas are comprised of industrial to the north and west, commercial/transportation to the south and transportation to the east.

This mix of uses has also resulted in a mix of structures, whether it is one-story single family homes or the concrete batch plant facilities approved for approximately 80'. Proposed building heights include 30' in the Park Quarter, 135' in the Residential Quarter and 120' in the Mixed-Use Quarter. As the Residential Quarter is internal, taller buildings will be pushed to the center of the site.

The applicant states the proposed development will "be consistent with surrounding neighborhoods by creating a transition from the active uses along Sheridan Street towards the residential and public park areas on the southern portion of the site. The development will be composed of three quarters: The Mixed Use quarter, The Residential Quarter and the Park Quarter." The applicant also provides "As described in the "Development Guidelines for Sheridan Stationside Village, the height of the

buildings are dependant on their location within the development. The taller buildings are centralized in the interior of the site, which leaves shorter, more compatible heights on the exterior areas."

FINDING: Consistent

CRITERIA 4: Conditions have substantially changed from the date the present zoning district classification was placed on the property which make the passage of the proposed change necessary.

ANALYSIS: "Based on the City's 2005 Evaluation and Appraisal Report ("EAR"), the City has identified the need for land use categories that address the increase of residents. The TOD land use category permits increased density where transportation needs may also be met....The needs identified by the City's EAR demonstrate that conditions have substantially changed from the date the zoning was placed on the property."

Redevelopment of this site from a trailer park and vast parking lot will allow the owners to create a sustainable site within the city. As Broward County and the City of Hollywood continue to grow, the use of mass transit as an alternative to vehicular traffic will be vital for residents and visitors. Incorporating the Tri-Rail Station with a Park and Ride garage and bus facilities as main components to the site will directly engage these uses.

FINDING: Consistent

CRITERIA 5: The proposed change will not adversely influence living conditions in the neighborhood.

ANALYSIS: The proposed change will remove the trailer park and large parking lots currently existing on the site. The new mixed use development allowed by the PD rezoning will encourage an environment in which residents will not only have the opportunity to live, work and play in the area, but also the same building.

"The City-Wide Master Plan had identified the Tri-Rail Station located on this Property as having a "long-term potential based on market demand to create a mass transit village linked to the Tri-Rail station with underutilized large land area with available infrastructure for development." Further, the City-Wide Master Plan provides that "the existing multi-modal Tri-Rail station area is envisioned by FDOT and Tri-Rail to become a more intensely developed station site area with potential mixed uses that will be directly connected to the rail system and the City of Hollywood." as acknowledged by the applicant in their Justification Statement.

FINDING: Consistent

CRITERIA 6: The proposed change is consistent with public safety concerns and will not create traffic concerns.

ANALYSIS: "The development provides an alternative to vehicular use and will provide residents mass transit options that increase public safety. Residents will have access to the Tri-Rail system as well as other forms of Broward County Transit. The Applicant has also submitted a Traffic Impact Analysis that evaluates the existing, future non-project, and future total impacts of the project on the capacity and roadway level of service."

As mentioned previously, the proposed change will eliminate an existing trailer park and large parking lots and increase public safety in the surrounding area. Furthermore, the inclusion of mass transit facilities will also help to encourage less use of vehicular traffic, thereby reducing traffic congestion. As the use of mass transit by residents and visitors will not be an immediate result, the applicant has proposed numerous trafficway improvements for the surrounding network.

FINDING: Consistent

CRITERIA 7: The proposed change will not adversely affect property values in the adjacent area.

ANALYSIS: The proposed change will help to increase property values in the area by redeveloping under utilized properties with aged structures to a new urban setting. "The Property is surrounded by the Rinker plant, the Tri-Rail Station and I-95. The unique location of the Property has created a valuable redevelopment opportunity that benefits both the City and County. The City-Wide Master Plan has noted that the Property is "underutilized" and in need of redevelopment."

The applicant also provides "The proposed development will be the first TOD within Broward County and will respond to the local need for both residential and commercial development. The proposed development will create a true mixed-use development that provides additional workforce housing, mass transit options and commercial uses in a primarily industrial area."

FINDING: Consistent

CRITERIA 8: It is impossible to find other adequate sites in the City for the proposed use on property presently zoned for such use.

ANALYSIS: Hollywood is primarily a built-out community. Locating larger tracts, especially 40 acres such as Sheridan Stationside Village, has proven to be difficult to accomplish. Additionally, there are only two Tri-Rail stations within the city, one of which is located within this site. "As stated previously, the Property is uniquely

situated on a site that is bound by the Tri-Rail and I-95....The proposed development will redevelop the site into a TOD and will create the first mixed-use development of its kind within the City and County."

"A TOD by definition must be located by a mass transit stop or station. The size of the Property also permits the construction of a TOD which will encourage the work, live, play and ride concept."

FINDING: Consistent

Zoning Change Findings

Based on findings generated from the change of zoning criteria analysis, the application for change of zoning from C-4, TD and IM-1 to PD for properties generally located north of Taft Street, south of Sheridan Street, east of CSX railway and west of I-95 (Sheridan Stationside) is consistent with more than six (6) required criteria listed in Section 5.4(G)(4)(b) of the Zoning and Land Development Regulations.

MASTER DEVELOPMENT PLAN

In addition to the requested change in zoning district, the Zoning and Land Development Regulations require submittal and approval of a corresponding Master Development Plan for all PD developments. As Sheridan Stationside Village will be a true Planned Development constructed over a number of years, the applicant has created Development Guidelines to lead future phases of the site. Guidelines such as these will help ensure future development will continue to be in-tune with previous phases. The proposed Master Development Plan and Development Guidelines can be found in Attachment "B" of this package.

The information below includes a brief synopsis to the Development Guidelines by Chapter.

Schematic Representation (p. 3)

The Master Development Plan depicts information such as number of units, lot coverage, setbacks and minimum parking amounts. Typically PD's combine, mix or average development regulations of surrounding neighborhoods to develop policies specific to their proposal. Portions of the Master Plan (i.e. densities and intensities) have been established through the Transit Oriented Development (TOD) land use amendment. As such, the Master Development Plan essentially establishes zoning regulations for each Planned Development.

The Design Guidelines will provide direction and ensure development will, among others,

- Utilize compact building forms and efficient circulation system to maximize the urban in-fill opportunity;
- Conserve part of the Live Oak canopy as a park and the community's recreational destination;

- Be environmentally responsible and ensure long-term neighborhood sustainability by using LEED established guidelines in conjunction with Chapter 52 of South Florida Building Code and other innovative concepts;

Implementing guidelines such as these at an early stage in the process will help better ensure the development is constructed in such a way to best suit not only the new site, but also the existing built environment.

As depicted in the Master Plan schematic, the 40+ acre site will be broken into sections known as "Quarters". These Quarters include:

- The Mixed-Use Quarter: Includes retail, office and hotel uses. The urban plaza (Zocalo) is included within this portion of the site. More than one vehicular roadway will be included. Minimum internal setbacks are 0' and maximum height is 120'.
- The Residential Quarter: Includes a wide range of residential uses and is the close proximity to the park to the south. Minimum internal setbacks are 0' and maximum height is 135'.
- The Park Quarter: Includes the coral rock house and 6-acre Park to be dedicated to the City. Will provide passive recreational activities and a place for community gatherings. Minimum internal setbacks are 25' and maximum height is 30'.

A Phasing Plan can be found on page 7 of the Development Guidelines. Phase I includes a majority of the Mixed-use Quarter and Park Quarter. Phase II includes the hotel (located within the Mixed-Use Quarter) and the Residential Quarter.

While the TOD land use identifies broad categories for the site (i.e. retail, office and residential), the Design Guidelines offer a tool to limit uses which may not be appropriate. The applicant has included a list of allowed uses (p. 5), including antique stores, book stores, drycleaners, schools, and tailors, among others. Additionally, the applicant reiterates from the land use amendment that 20% of all residential units will be designated as workforce housing.

A modification request from Article 7 and 9 of the Zoning and Land Development Code will be requested when the PD is presented to the City Commission with the first site plan. As the site will be developed as a transit oriented development, the reduction of automobile use is a key component. Reduced parking requirements (including shared parking) and modified parking design standards are included with this request.

Internal Circulation (p. 10)

As designed, the Sheridan Stationside Village will necessitate a variance of the required 25' landscape setback from all internal/external rights-of-ways. This request will be presented to the Development Review Board and City Commission when Design and Master Development Plan and Site Plans are considered. The PD zoning district does not have any required amounts for internal side yards, only street side setbacks. Proposed setbacks include:

- Enclosed buildings along internal streets: Varies. As identified under "dimensional Data", page 13
- Enclosed buildings bordering the railroad: 20' minimum

- Enclosed buildings bordering external streets: 5' minimum from public right-of-way and 15' minimum from back of curb, whichever is greater
- Enclosed buildings bordering internally: Minimum setbacks as identified within each Quarter as identified in Schematic Representation. (page 4)

As a transit oriented development, the internal street network must also be addressed. This portion of the Guidelines includes dimensional data for modifications to parking design standards, such as parking isle width and stall lengths. Also provided are street sections for visual representation for how each of the street types will work.

The necessity for features such as sidewalk amenity zones, crosswalks, raised table intersections, etc are outlined as necessary features in making this network successful. Additional guidelines for elements such as outdoor seating and bicycle circulation are also included. Providing uses such as these within adequate travel distances will be necessary in order to encourage residents to walk/bike to their destinations within the site.

Points of Connection (p. 22)

The main points of connections within the PD are 1) Intersection at Sheridan Street and N. 29th Avenue, 2) Existing I-95 southbound ramp access-way and the new extension to it, 3) Intersection at Taft Street and "Park Drive", 4) Entrance to the existing Tri-Rail "Sheridan Street Station". With the exception of connection #3, all of these points are within a 5 minute walking distance within Sheridan Stationside Village.

A 793-space parking garage will be included for the Florida Department of Transportation (FDOT) to use for Park and Ride for the Tri-Rail Station. Vehicular connection between Sheridan Street and the transit station is required to be direct as possible. Additionally, a bus station will be incorporated with immediate connection from the Tri-Rail station.

As a TOD, the use of mass transit by future residents is a key component to the development. With its proximity to Interstate 95, Sheridan Street and Taft Street, the need to address traffic improvements related to the development of the site at an early stage is of utmost importance. Throughout the land use amendment process the applicant has worked with City of Hollywood Department of Building and Engineering Services, Broward County and FDOT staff to conduct traffic studies. This included trip generation analysis and capacity analysis. These studies were reviewed and found consistent by all three agencies. Some of the traffic mitigation improvements proposed with the land use amendment include:

- Additional left-turn lanes at Sheridan Street and Interstate 95
- Modify existing traffic signals at Sheridan Street and Interstate 95
- Install traffic signal at Taft Street and 26th Avenue
- Reconstruct existing roadways to include turn lanes at various intersections throughout the local network

Community Facilities (p. 24)

A minimum of 6 acres dedicated to the City of Hollywood, the Park Quarter will include biking and walking trails and picnic areas. This area includes the Live Oak Hammock,

which currently houses the trailer park. The applicant will work to clean up the park area, including removal of invasive species and removal of concrete trailer pads.

Also included within this area is the historic coral rock house. This building will also be dedicated to the City. Plans for this building may include refurbishing it to provide a community center for the area.

The urban plaza, also known as the Zocalo, will be maintained at a minimum open space size of one acre uninterrupted by any vehicular street. This area will serve as the main gathering place for the site, and will include uses and activities such as outdoor dining, interactive water fountains and street fairs.

Landscape Design Guidelines (p. 26)

As with any site, the incorporation of landscape and proper irrigation are essential. The use of hardscape materials such as brick pavers, bollards, high albedo materials, planters and pots/urns are encouraged within the development. The materials can be used to provide special, vertical and visual reliefs from other materials.

The City's Landscape Manual will guide the landscaping within the development, while the Development Guidelines will supplement it. Although a more urban development, the use of landscape materials within Sheridan Stationside will be an important component. Canopy trees will be encouraged adhering to Xeriscape, NatureScape and LEED principles. Palms and hedges/shrubs will also be used throughout the site to provide additional foliage.

Engineering Design Guidelines, General Requirements and Appendix (p. 31)

These Chapters provide additional information regarding the development of the site, such as Engineering Details, general requirements, glossary and example shared parking analysis.

As stated previously, the Master Development Plan essentially creates development regulations specific to this PD. Additionally, the TOD land use endorses designs features such as incorporation of transit facilities, creative siting of buildings and incorporation of open space.

Additional components of the Master Plan (Attachment B) are the voluntary commitments from the applicant for items such as workforce housing, landscaping and roadway upgrades. ***Therefore, staff recommends approval of the Master Development Plan, with the acknowledgement and acceptance of the voluntary commitments from the applicant.***

| Symbol | Legend |
|--------|---------------------------------|
| 1 | Section |
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| 3 | 1/4 Sec. 40, 41, 42, 43 |
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| 204 | 1/4 Sec. 844, 845, 846, 847 |
| 205 | 1/4 Sec. 848, 849, 850, 851 |
| 206 | 1/4 Sec. 852, 853, 854, 855 |
| 207 | 1/4 Sec. 856, 857, 858, 859 |
| 208 | 1/4 Sec. 860, 861, 862, 863 |
| 209 | 1/4 Sec. 864, 865, 866, 867 |
| 210 | 1/4 Sec. 868, 869, 870, 871 |
| 211 | 1/4 Sec. 872, 873, 874, 875 |
| 212 | 1/4 Sec. 876, 877, 878, 879 |
| 213 | 1/4 Sec. 880, 881, 882, 883 |
| 214 | 1/4 Sec. 884, 885, 886, 887 |
| 215 | 1/4 Sec. 888, 889, 890, 891 |
| 216 | 1/4 Sec. 892, 893, 894, 895 |
| 217 | 1/4 Sec. 896, 897, 898, 899 |
| 218 | 1/4 Sec. 900, 901, 902, 903 |
| 219 | 1/4 Sec. 904, 905, 906, 907 |
| 220 | 1/4 Sec. 908, 909, 910, 911 |
| 221 | 1/4 Sec. 912, 913, 914, 915 |
| 222 | 1/4 Sec. 916, 917, 918, 919 |
| 223 | 1/4 Sec. 920, 921, 922, 923 |
| 224 | 1/4 Sec. 924, 925, 926, 927 |
| 225 | 1/4 Sec. 928, 929, 930, 931 |
| 226 | 1/4 Sec. 932, 933, 934, 935 |
| 227 | 1/4 Sec. 936, 937, 938, 939 |
| 228 | 1/4 Sec. 940, 941, 942, 943 |
| 229 | 1/4 Sec. 944, 945, 946, 947 |
| 230 | 1/4 Sec. 948, 949, 950, 951 |
| 231 | 1/4 Sec. 952, 953, 954, 955 |
| 232 | 1/4 Sec. 956, 957, 958, 959 |
| 233 | 1/4 Sec. 960, 961, 962, 963 |
| 234 | 1/4 Sec. 964, 965, 966, 967 |
| 235 | 1/4 Sec. 968, 969, 970, 971 |
| 236 | 1/4 Sec. 972, 973, 974, 975 |
| 237 | 1/4 Sec. 976, 977, 978, 979 |
| 238 | 1/4 Sec. 980, 981, 982, 983 |
| 239 | 1/4 Sec. 984, 985, 986, 987 |
| 240 | 1/4 Sec. 988, 989, 990, 991 |
| 241 | 1/4 Sec. 992, 993, 994, 995 |
| 242 | 1/4 Sec. 996, 997, 998, 999 |
| 243 | 1/4 Sec. 1000, 1001, 1002, 1003 |
| 244 | 1/4 Sec. 1004, 1005, 1006, 1007 |
| 245 | 1/4 Sec. 1008, 1009, 1010, 1011 |
| 246 | 1/4 Sec. 1012, 1013, 1014, 1015 |
| 247 | 1/4 Sec. 1016, 1017, 1018, 1019 |
| 248 | 1/4 Sec. 1020, 1021, 1022, 1023 |
| 249 | 1/4 Sec. 1024, 1025, 1026, 1027 |
| 250 | 1/4 Sec. 1028, 1029, 1030, 1031 |
| 251 | 1/4 Sec. 1032, 1033, 1034, 1035 |
| 252 | 1/4 Sec. 1036, 1037, 1038, 1039 |
| 253 | 1/4 Sec. 1040, 1041, 1042, 1043 |
| 254 | 1/4 Sec. 1044, 1045, 1046, 1047 |
| 255 | 1/4 Sec. 1048, 1049, 1050, 1051 |
| 256 | 1/4 Sec. 1052, 1053, 1054, 1055 |
| 257 | 1/4 Sec. 1056, 1057, 1058, 1059 |
| 258 | 1/4 Sec. 1060, 1061, 1062, 1063 |
| 259 | 1/4 Sec. 1064, 1065, 1066, 1067 |
| 260 | 1/4 Sec. 1068, 1069, 1070, 1071 |
| 261 | 1/4 Sec. 1072, 1073, 1074, 1075 |
| 262 | 1/4 Sec. 1076, 1077, 1078, 1079 |
| 263 | 1/4 Sec. 1080, 1081, 1082, 1083 |
| 264 | 1/4 Sec. 1084, 1085, 1086, 1087 |
| 265 | 1/4 Sec. 1088, 1089, 1090, 1091 |
| 266 | 1/4 Sec. 1092, 1093, 1094, 1095 |
| 267 | 1/4 Sec. 1096, 1097, 1098, 1099 |
| 268 | 1/4 Sec. 1100, 1101, 1102, 1103 |
| 269 | 1/4 Sec. 1104, 1105, 1106, 1107 |
| 270 | 1/4 Sec. 1108, 1109, 1110, 1111 |
| 271 | 1/4 Sec. 1112, 1113, 1114, 1115 |
| 272 | 1/4 Sec. 1116, 1117, 1118, 1119 |
| 273 | 1/4 Sec. 1120, 1121, 1122, 1123 |
| 274 | 1/4 Sec. 1124, 1125, 1126, 1127 |
| 275 | 1/4 Sec. 1128, 1129, 1130, 1131 |
| 276 | 1/4 Sec. 1132, 1133, 1134, 1135 |
| 277 | 1/4 Sec. 1136, 1137, 1138, 1139 |
| 278 | 1/4 Sec. 1140, 1141, 1142, 1143 |
| 279 | 1/4 Sec. 1144, 1145, 1146, 1147 |
| 280 | 1/4 Sec. 1148, 1149, 1150, 1151 |
| 281 | 1/4 Sec. 1152, 1153, 1154, 1155 |
| 282 | 1/4 Sec. 1156, 1157, 1158, 1159 |
| 283 | 1/4 Sec. 1160, 1161, 1162, 1163 |
| 284 | 1/4 Sec. 1164, 1165, 1166, 1167 |
| 285 | 1/4 Sec. 1168, 1169, 1170, 1171 |
| 286 | 1/4 Sec. 1172, 1173, 1174, 1175 |
| 287 | 1/4 Sec. 1176, 1177, 1178, 1179 |
| 288 | |

ORDINANCE NO O-2008-35

(05-ZJ-72e/05-F-72f)

AN ORDINANCE OF THE CITY OF HOLLYWOOD, FLORIDA, CHANGING THE ZONING DESIGNATION OF THE PROPERTIES GENERALLY LOCATED NORTH OF TAFT STREET, SOUTH OF SHERIDAN STREET, EAST OF CSX RAILWAY AND WEST OF I-95 (A PORTION OF SHERIDAN STATIONSIDE VILLAGE, SOUTH OF THE CITY OF HOLLYWOOD WATER TOWER) FROM TRAILER DISTRICT (TD) TO PLANNED DEVELOPMENT DISTRICT (PD), AMENDING THE APPROVED PLANNED DEVELOPMENT MASTER DEVELOPMENT PLAN KNOWN AS "SHERIDAN STATIONSIDE VILLAGE MASTER PLAN" (ORIGINALLY APPROVED BY ORDINANCE NO O-2007-35) TO INCORPORATE SAID PROPERTIES INTO THE PLANNED DEVELOPMENT AND MASTER PLAN, APPROVING THE APPLICATION OF THE 5% FLEXIBILITY RULE (LOW/MEDIUM RESIDENTIAL TO COMMERCIAL/GENERAL BUSINESS) WITHIN FLEXIBILITY ZONE 87 TO PERMIT COMMERCIAL/GENERAL BUSINESS LAND USE ON THE SUBJECT PROPERTIES PURSUANT TO THE CITY'S COMPREHENSIVE PLAN AND POLICY 01 01 04 OF THE BROWARD COUNTY LAND USE PLAN, AND AMENDING THE CITY'S ZONING MAP TO REFLECT THE CHANGE IN ZONING DESIGNATION

WHEREAS, the Zoning and Land Development Regulations provide that an application for a change of zoning may be filed, and

WHEREAS, an application (05-ZJ-72e) was filed with the Office of Planning of the City of Hollywood requesting a change of zoning from Trailer District (TD) to Planned Development District (PD) for property generally located north of Taft Street, south of Sheridan Street, east of CSX railway and west of I-95 (a portion of Sheridan Stationside Village, south of the City of Hollywood water tower) with approximately 1.3 acres as more particularly described in Exhibit "A" (subject properties) attached hereto and incorporated herein by reference and requesting that the Sheridan Stationside Village Master Plan be amended to include the subject properties into said Plan, and

WHEREAS, the purpose of this request for change in zoning designation is to include the subject properties in the Sheridan Stationside Village Planned Development and Master Plan which was approved by the City Commission pursuant to Ordinance O-2007-35 whereby the total land acreage will increase to approximately 40 gross acres and the subject properties will consist of an office building, and

WHEREAS, the existing properties have a current City zoning designation of TD and a Future Land Use Designation of Low/Medium Residential (6-10 units per acre), and

WHEREAS, the subject properties are adjacent to Government Use on the north, PD on the south and west, and I-95 to the East; and

WHEREAS, pursuant to Section 4 15 G 3 of the Zoning and Land Development Regulations the proposed amendment to the Sheridan Stationside Village Master Plan to include the subject properties constitutes a substantial alteration to the character of the development and requires review and approval by the City Commission, and

WHEREAS, the Director of the Office of Planning and City staff, following analysis of the proposed amendment to the Sheridan Stationside Village Master Development Plan and its associated documents, have determined that the proposed amendment to the Plan is consistent with the Zoning and Land Development Regulations, is consistent with the City of Hollywood Comprehensive Plan, and have, therefore, recommended that the Master Development Plan be approved, and

WHEREAS, the Director of the Office of Planning, following analysis of the application and its associated documents, has determined that the proposed change of zoning district is consistent with the Zoning and Land Development Regulations, is consistent with the City of Hollywood Comprehensive Plan and the City-Wide Master Plan, and has, therefore, recommended that it be approved, and

WHEREAS, the subject properties cannot be used for Commercial/General Business Use without the application of the 5% Flexibility Rule, and

WHEREAS, the City's Comprehensive Plan (Future Land Use Element – Permitted Uses) and Policy 01 01 04 of the Broward County Land Use Plan require the application of the 5% Flexibility Rule (Residential to Commercial) to permit a change in land use on a residentially designated parcel within Flexibility Zone 87 which will effectively change the underlying land use to General Business, and

WHEREAS, the application of the 5% flexibility rule to the subject properties will allow for the construction of an office building, and

WHEREAS, the allocation of 1 3 net acres required to permit the residential to commercial change of land use designation for the subject property described in Exhibit "A" will leave a balance of approximately 24 7 acres in Flexibility Zone 87, and

WHEREAS, the Director of the Office of Planning, following analysis of the application of the 5% flexibility rule, recommended it be approved, and

WHEREAS, on September 22, 2008, the Planning and Zoning Board met and reviewed the above noted request for a change in zoning designation to PD (Planned Development District), and approval of the proposed amendment to the Sheridan Stationside Village Master Development Plan and have forwarded a recommendation of approval to the City Commission, and

WHEREAS, on September 22, 2008, the Planning and Zoning Board, acting as the Local Planning Agency, reviewed the above noted request for the application of the 5% flexibility rule and have forwarded a recommendation of approval to the City Commission, and

WHEREAS, the City Commission of the City of Hollywood has conducted duly advertised hearings on the proposed rezoning ordinance and application of the 5% flexibility rule through the above referenced petitions and has considered all comments received concerning the proposed amendment as required by state law and local ordinances and resolutions, and

WHEREAS, the City Commission finds that the rezoning request to PD (Planned Development District), along with approval of the Amended Master Development Plan and the application of the 5% Flexibility Rule, are consistent with the Zoning and Land Development Regulations, are consistent with the City of Hollywood's Comprehensive Plan, are consistent with the City-Wide Master Plan, are consistent with Policy 01 01 04 of the Broward County Land Use Plan, and are in the best interest of the citizens of the City of Hollywood;

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COMMISSION OF THE CITY OF HOLLYWOOD, FLORIDA.

Section 1 That the applicant has presented competent substantial evidence that the requested rezoning to PD is consistent with the Zoning and Land Development Regulations, is consistent with the City of Hollywood's Comprehensive Plan, and there is no legitimate public purpose in maintaining the existing zoning

Section 2. That the subject properties, as more particularly described in Exhibit "A", are hereby rezoned from the zoning designation of TD (Trailer District) to PD (Planned Development District)

Section 3. That Ordinance No O-2007-35 which approved the Sheridan Stationside Village Master Development Plan ("Plan") shall be further amended to include the properties described in Exhibit "A", and the Plan is hereby amended as more specifically described in Exhibit "B" attached hereto and incorporated herein by reference

Section 4 That those provisions of Ordinance No O-2007-35 which are not amended by this Ordinance shall remain in full force and effect.

Section 5. That the 5% Flexibility Rule pursuant to the City of Hollywood's Comprehensive Plan and Policy 01 01 04 of the Broward County Land Use Plan is hereby applied to allocate approximately 1 3 acres of flexibility acreage to the subject properties described in Exhibit "A", attached hereto and incorporated herein by reference

Section 6. That the allocation of 1 3 acres of commercial flex within Flexibility Zone 87 to the subject properties as more specifically describe in Exhibit "A" is hereby approved

Section 7 That the Official Zoning Map of the City of Hollywood is hereby amended to incorporate the above described change in zoning designation for the subject parcel

Section 8 That all sections or parts of sections of the Zoning and Land Development Regulations, Code of Ordinances, and all ordinances or parts thereof and all resolutions or parts thereof in conflict herewith are hereby repealed to the extent of such conflict.

Section 9 That if any word, phrase, clause, subsection or section of this ordinance is for any reason held unconstitutional or invalid, the invalidity thereof shall not affect the validity of any remaining portions of this ordinance

Section 10 That this Ordinance shall be in full force and effect immediately upon its passage and adoption.

Section 11 This Ordinance shall remain effective as long as the Sheridan Stationside Village Planned Development approved by O-2007-35 and the amended Master Plan remains in effect.

Advertised Dec 5, 2008

PASSED on first reading this 5 day of Nov, 2008.

PASSED AND ADOPTED on second reading this 17 day of Dec, 2008

RENDERED this 5 day of Jan, ~~2008~~ 2009.


PETER BOBER, MAYOR

ATTEST


PATRICIA A. CERNY, MMC
CITY CLERK

APPROVED AS TO FORM & LEGALITY
for the use and reliance of the
City of Hollywood, Florida, only

 
JEFFREY P. SHEFFEL
CITY ATTORNEY

12/19/08pac

EXHIBIT A

LAND DESCRIPTION SHERIDAN STATION

A PORTION OF PARCEL 101 AS DESCRIBED IN DEED RECORDED IN OFFICIAL RECORDS BOOK 17566, PAGE 532 OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA

That part of the NW ¼ of the NW ¼ of Section 9, Township 51 South, Range 42 East; more particularly described as follows

Commence at the Northwest corner of said Section 9; thence N 88°55'14"E along the North line of said Section 9, a distance of 883 90 feet; thence S 01°04'46"E, perpendicular to the last described course, a distance of 51.25 feet to the intersection of the Southerly Existing Right of Way line of Sheridan Street with the Westerly Existing Limited Access Right of Way line of Interstate 95 (I-95); thence S 00°55'49"E along said Westerly Right of Way line, a distance of 198 75 feet to the POINT OF BEGINNING, thence S 88°55'14"W a distance of 214 85 feet to a point on the Easterly Existing Right of Way line of North 29th Avenue, thence S 00°58'26"E along said Easterly Right of Way line a distance of 359 40 feet; thence N 89°01'34"E, a distance of 138 62 feet to a point on said Westerly Existing Limited Access Right of Way line of Interstate 95 (I-95); thence N 08°41'36"E along said Westerly Existing Limited Access Right of Way line of Interstate 95 (I-95), a distance of 174 80 feet; thence N 13°03'46"E along said Westerly Existing Limited Access Right of Way line, a distance of 193.26 feet to the POINT OF BEGINNING

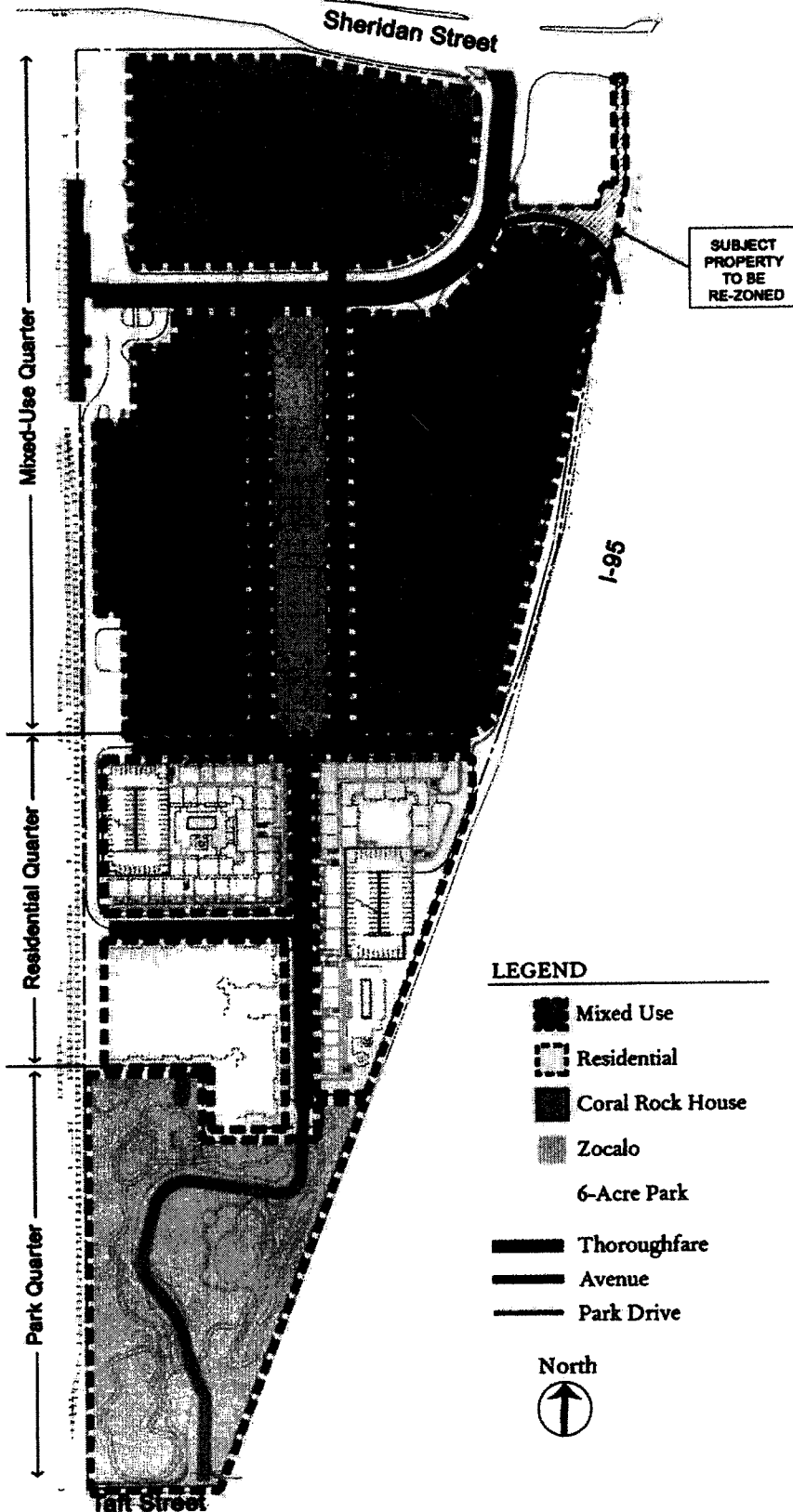
LESS THE FOLLOWING DESCRIBED PARCEL DESCRIBED IN QUITCLAIM DEED RECORDED IN OFFICIAL RECORDS BOOK 20754 PAGE 994 OF THE PUBLIC RECORDS OF BROWARD COUNTY FLORIDA

That part of the Northwest quarter (NW ¼) of the Northwest quarter (NW ¼) of Section 9, Township 51 South, Range 42 East, lying in Broward County, Florida being more particularly described as follows

Commence at the Northwest corner said Section 9, thence North 88°55'14" East along the North line of said Section 9, a distance of 841.80 feet; thence South 01°04'46" East a distance of 250 00 feet to the POINT OF BEGINNING, thence South 13°03'46" West, a distance of 41 52 feet, thence South 88°55'14" West, a distance of 147 37 feet, thence North 01°00'43" West, a distance of 40 26 feet; thence North 88°55'14" East, a distance of 157 47 feet to the POINT OF BEGINNING

Said lands lying in the City of Hollywood, Broward County, Florida and containing a total net area of 56 087 square feet (1 288 acres), more or less

EXHIBIT B

[illegible]

The maximum amount of office use may increase to the FOD land use plan amendment approved level of 299.00 ±, provided that the revised development program is evaluated with respect to any requirement for ORS review.

| MAXIMUM BUILDING HEIGHTS | |
|---|----------|
| maximum height in feet and inches above existing ground level | |
| Midwest Tower Quarter | 110 feet |
| Residential Quarter | 125 feet |
| Park Quarter | 30 feet |

A building's height is measured from a point of average elevation along the street(s) it faces to the top of the flat roof deck of the top habitable floor. Decorative spires and ornaments, decorative architectural features, antenna rooms, roof stacks, and rooftop mechanical equipment rooms are not included in determining roof height. In no case shall any of these elements exceed 450 feet.

| | |
|---------------------------|--|
| Hotel | 0.75 special room |
| Hotel/restaurant** | 3.5 special 1,000 s.f. |
| Office | 2.5 special 1,000 s.f. |
| Receptionist ¹ | 1.5 special unit + 1 special 5' unit guest |

* The actual restaurant per party rate can be adjusted when total restaurant floor area for the project is less than 125 sq. total retail floor area. If restaurant floor area exceeds 125 sq. total retail floor area, the additional restaurant floor area shall provide parking at the rate of 10 spaces per 1,000 s.f.

** Guest parking is not required with use of shared parking.

| Time | Available | Required | Shared | Charging | Cost | Time | 100% |
|-------|-----------|----------|--------|----------|------|-------|------|
| 9 AM | 100% | 0% | 0% | 0% | 0% | 9 AM | 100% |
| 7 AM | 67% | 0% | 0% | 0% | 0% | 7 AM | 67% |
| 6 AM | 70% | 100% | 0% | 200% | 200% | 6 AM | 70% |
| 5 AM | 73% | 42% | 32% | 93% | 93% | 5 AM | 73% |
| 4 AM | 60% | 40% | 30% | 100% | 100% | 4 AM | 60% |
| 3 AM | 30% | 37% | 10% | 100% | 100% | 3 AM | 30% |
| 12 PM | 60% | 97% | 30% | 90% | 90% | 12 PM | 60% |
| 1 PM | 70% | 100% | 30% | 100% | 100% | 1 PM | 70% |
| 2 PM | 60% | 93% | 30% | 97% | 97% | 2 PM | 60% |
| 3 PM | 61% | 95% | 30% | 95% | 95% | 3 PM | 61% |
| 4 PM | 65% | 97% | 72% | 77% | 48% | 4 PM | 65% |
| 5 PM | 77% | 70% | 47% | 47% | 47% | 5 PM | 77% |
| 6 PM | 72% | 23% | 23% | 23% | 23% | 6 PM | 72% |
| 7 PM | 90% | 66% | 70% | 70% | 70% | 7 PM | 90% |
| 8 PM | 90% | 87% | 70% | 70% | 70% | 8 PM | 90% |
| 9 PM | 80% | 61% | 30% | 30% | 90% | 9 PM | 80% |
| 10 PM | 70% | 30% | 30% | 30% | 30% | 10 PM | 70% |
| 11 PM | 100% | 13% | 0% | 0% | 100% | 11 PM | 100% |
| 12 AM | 100% | 0% | 0% | 0% | 100% | 12 AM | 100% |

Indicates the garage total parking demand of any type on the day of the study. The number in the column of the day of the study would be the actual parking demand on that day.

Table 2 • Shortest Producer Month (continued)

| Year | Shortest Producer Month | Climate | Days | Notes |
|--------|-------------------------|---------|------|-------|
| 9 Mar | 100% | 0% | 0% | 0% |
| 7 Apr | 95% | 3% | 3% | 3% |
| 9 May | 80% | 10% | 10% | 10% |
| 9 Jun | 65% | 24% | 13% | 13% |
| 10 Jun | 75% | 49% | 13% | 13% |
| 11 Jun | 72% | 73% | 17% | 17% |
| 8 Jul | 73% | 65% | 17% | 17% |
| 7 Aug | 60% | 60% | 18% | 18% |
| 7 Sep | 71% | 100% | 16% | 16% |
| 5 Oct | 73% | 100% | 7% | 7% |
| 5 Nov | 79% | 30% | 7% | 7% |
| 5 Dec | 67% | 75% | 3% | 3% |
| 5 Jan | 65% | 65% | 3% | 3% |
| 7 Feb | 62% | 50% | 3% | 3% |
| 9 Feb | 59% | 43% | 0% | 0% |
| 10 Feb | 56% | 34% | 0% | 0% |
| 12 Feb | 53% | 33% | 0% | 0% |
| 14 Feb | 50% | 30% | 0% | 0% |

Including the gross total growing season of 200 days as the crop is planted on 10 Feb and harvested on 10 May. The 200 days include the frost-free growing season of 185 days.

4 7 08

MASTER PLAN

Note: Grayscale plain background is for schematic representation only, and it is subject to change.

SHERIDAN STATIONSIDE VILLAGE
Hollywood, Florida

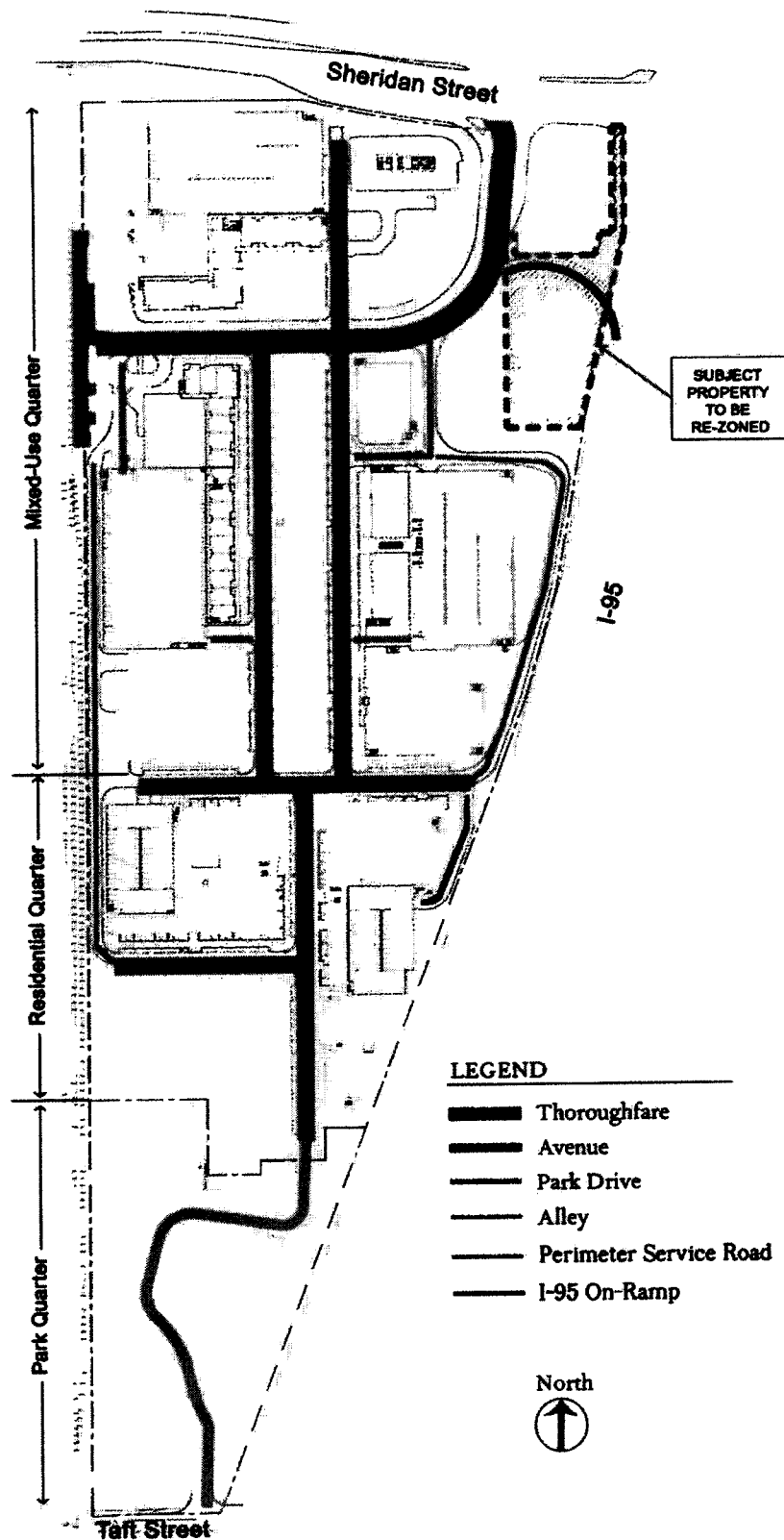
ROGER FRY & ASSOCIATES ARCHITECTS, P.A. 2791 Bird Avenue Coconut Grove, Florida 33133 phone 305 446-7787

ram



PINNACLE
HOUSING GROUP

**ROGER FRY
& ASSOCIATES
ARCHITECTS, P.A.**



4 7 08

STREET NETWORK

Note: Ghosted plan background is for schematic representation only, and is subject to change.

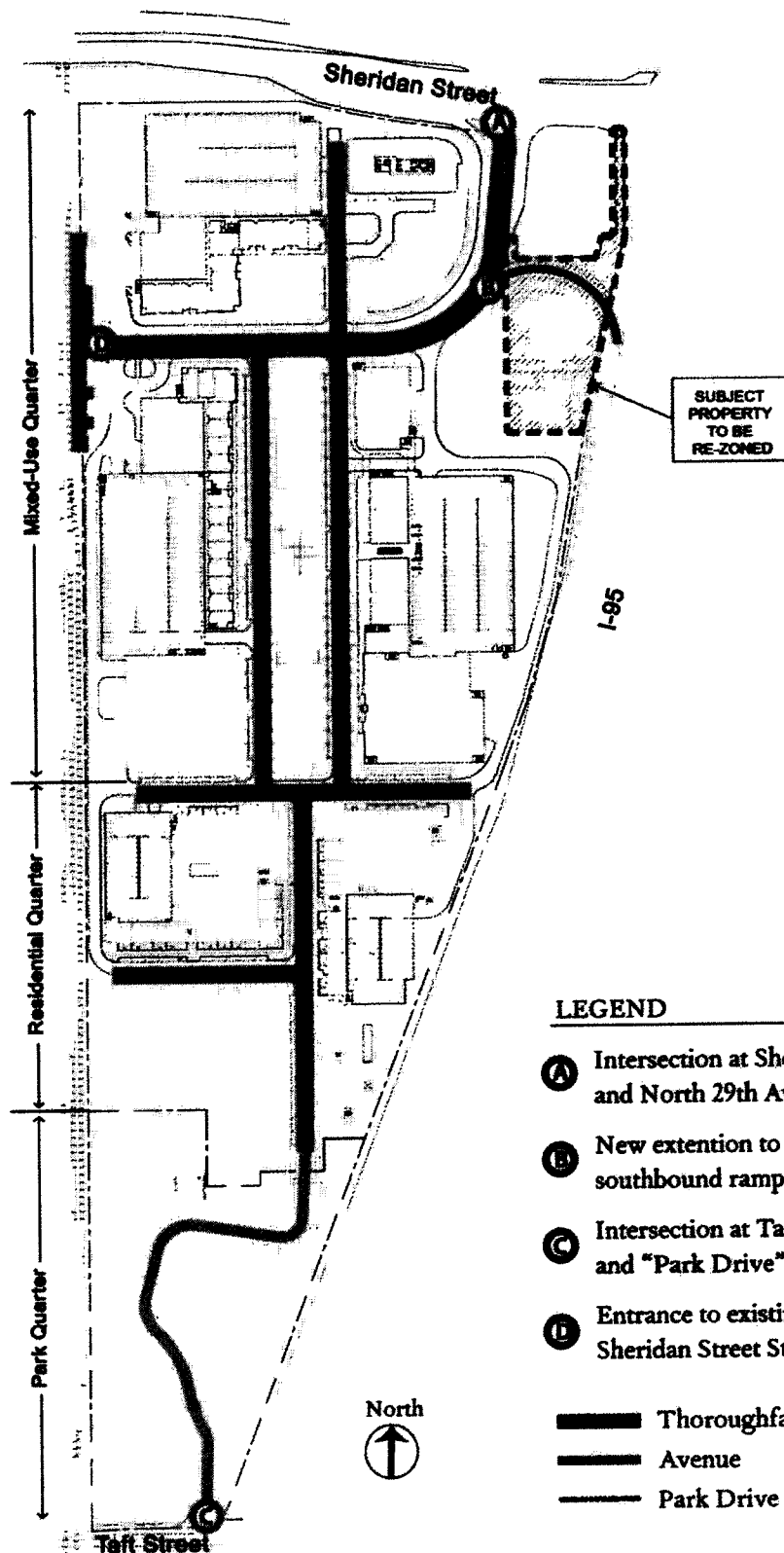
SHERIDAN STATIONSIDE VILLAGE
Hollywood, Florida

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PINNACLE
HOUSING GROUP

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& ASSOCIATES
ARCHITECTS, P.A.**



LEGEND

- (A) Intersection at Sheridan Street and North 29th Avenue
 - (B) New extension to existing I-95 southbound ramp access-way
 - (C) Intersection at Taft Street and "Park Drive"
 - (D) Entrance to existing Tri-Rail's Sheridan Street Station
- Thoroughfare
 - Avenue
 - Park Drive

4 7 08

POINTS OF CONNECTION

Note: Ghosted plan background is for schematic representation only, and it is subject to change.

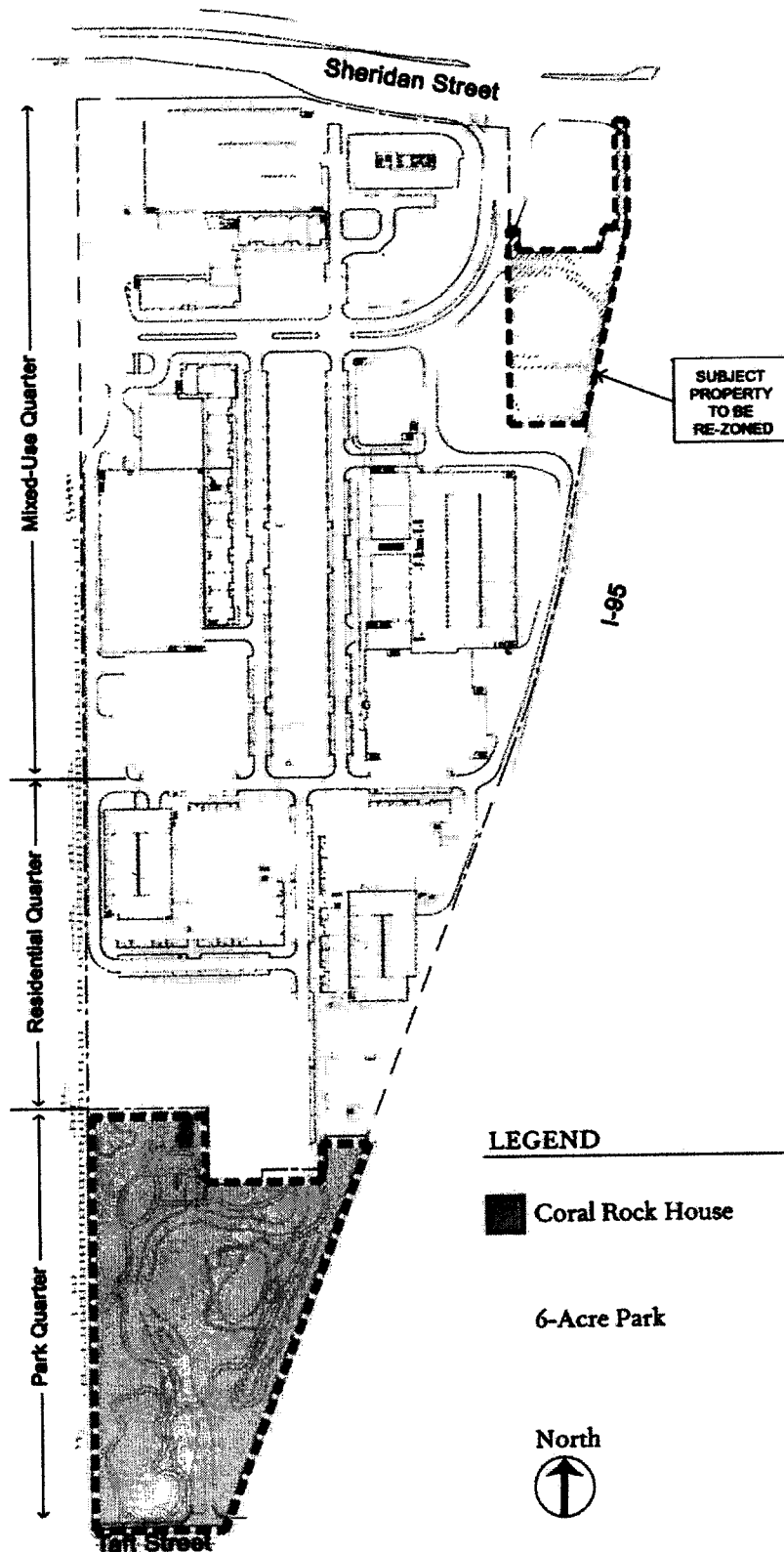
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LEGEND

■ Coral Rock House

6-Acre Park

North



4 7 08

COMMUNITY FACILITIES

Note: Ghosted plan background is for schematic representation only, and it is subject to change.

SHERIDAN STATIONSIDE VILLAGE
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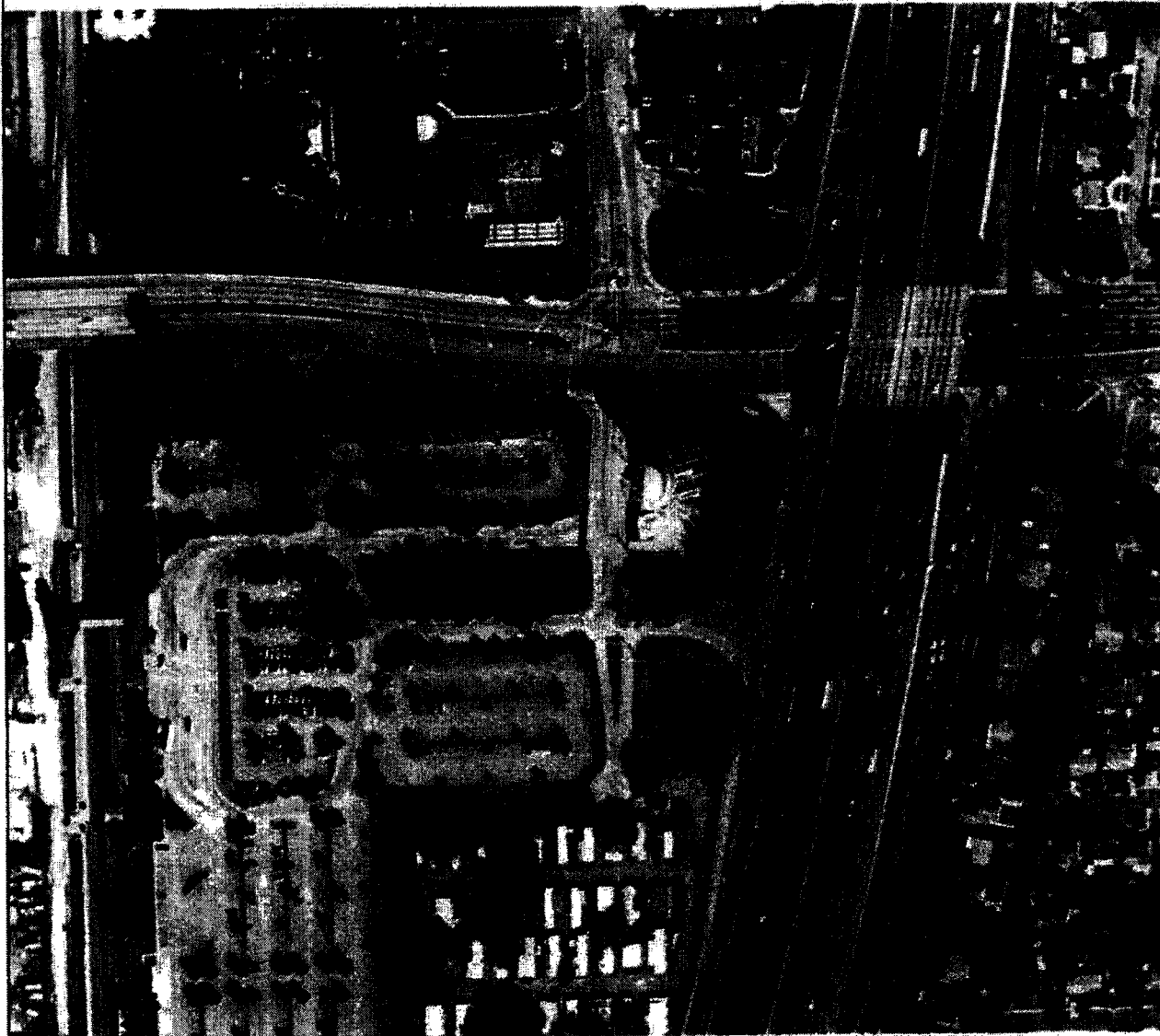
Improvements at Point of Connection A & B.

IMPROVEMENTS

- 1.) REDUCE ISLAND TO REALIGN SOUTHBOUND I-95 RAMP RIGHT-TURN LANES TO MERGE WITH LEFT MOST WESTBOUND LANES ON SHERIDAN ST.
- 2.) ADD A THIRD LEFT-TURN LANE ON I-95 SOUTH BOUND OFF RAMP.
- 3.) ADD RIGHT-TURN LANE AND CHANGE EXISTING TO A THIRD LEFT-TURN ON I-95 NORTH BOUND OFF RAMP.
- 4.) INCREASE LENGTH OF SOUTH BOUND ON RAMP AND ADD PARALLEL MERGE.
- 5.) ADD AND RESTRIPE TWO RIGHT-TURN LANES, A THROUGH LANE, AND TWO LEFT TURN LANES.
- 6.) REALIGN EASTBOUND LANES THROUGH TRAFFIC SIGNAL AT SHERIDAN AND N. 28TH INTERSECTION.
- 8.) ADD ADDITIONAL DEDICATED I-95 NORTHBOUND LEFT-TURN LANE.
- 9.) ADD ADDITIONAL EASTBOUND THRU LANE.
- 10.) ADD DEDICATED I-95 SOUTHBOUND RIGHT-TURN LANE.
- 11.) ADD FREE FLOWING EASTBOUND RIGHT-TURN LANE ONTO N. 28TH AVE.
- 12.) ADD AND REALIGN EASTBOUND LEFT-TURN LANE ONTO N. 28TH AVE.
- 13.) ADD AND RESTRIPE TWO RIGHT-TURN LANES, A THROUGH LANE, AND TWO LEFT TURN LANES.
- 14.) ADD A SECOND WESTBOUND LEFT-TURN LANE.
- 15.) INCREASE LENGTH OF SOUTH BOUND ON RAMP AND ADD PARALLEL MERGE.

LEGEND

-  NEW CONSTRUCTION
-  REALIGNED I-95 NORTHBOUND TURN LANES
-  REALIGNED THROUGH LANES
-  CHANGE IN LANE DESIGNATION



Improvements along Taft Street.

- 1.) CONSTRUCT A WESTBOUND RIGHT-TURN LANE AT STATE ROAD 7
- 2.) INSTALL AN EASTBOUND RIGHT-TURN OVERLAP PHASE.
- 3.) CONSTRUCT EASTBOUND AND WESTBOUND RIGHT-TURN LANES AT U.S. HIGHWAY 1
- 4.) CONSTRUCT EASTBOUND AND WESTBOUND LEFT-TURN LANES AT 40TH AVENUE.
- 5.) INSTALL A TRAFFIC SIGNAL AT 26TH AVENUE

4 7 08

TRAFFIC IMPROVEMENTS

Note: Ghosted plan background is for schematic representation only, and it is subject to change.

SHERIDAN STATIONSIDE VILLAGE
Hollywood, Florida

ROGER FRY & ASSOCIATES ARCHITECTS, P.A. 2791 Bird Avenue Coconut Grove, Florida 33133 phone 305-446-7787

ram

PINNACLE
HOUSING GROUP

ROGER FRY
& ASSOCIATES
ARCHITECTS, P.A.

Amendments to the Development Guidelines for Sheridan Stationside Village

The Development Guidelines for Sheridan Stationside Village, originally approved by Ordinance PO-2007-31 adopted on December 19, 2007, referred to herein as the “Guidelines,” are hereby amended as follows.

1 The Schematic Master Site Plan included in the Guidelines at page 4 is deleted and replaced by the figure attached hereto as Exhibit “1 ”

2. The text entitled “Schematic Overview” is amended as follows.

SCHEMATIC OVERVIEW

In order to maximize site potentials and make for a better neighborhood transition, the Sheridan Stationside Village district is divided into the following three quarters.

- The Mixed-Use Quarter,
- The Residential Quarter, and
- The Park Quarter

Additionally, in order to provide for a pedestrian-friendly street-life while accommodating the needs of the various uses, five street types are classified, and in order of their traffic volume/prominence, they are:

- Thoroughfare,
- Avenue,
- Park Drive,
- ~~Drive~~ Perimeter Service Road, and
- Alley

As a focal point of the district, the center of the site is an urban plaza called the Zocalo. It will provide a public gathering space incorporating the civic and cultural uses that are critical to providing the Village a unique identity

Complementing the Zocalo is the 6-acre park at the south end of the site. It draws its character from the dense canopy of mature Live Oaks.

3 The table entitled “Permitted Street Types” found at page 5 of the Guidelines is amended as follows.

PERMITTED STREET TYPES FOR THE QUARTERS

Mixed-Use Quarter:

- *Thoroughfare*”
- *“Avenue”*

- *Drive* *Perimeter Service Road*
- *'Alley'*

Residential Quarter:

- *"Avenue"*
- *"Drive"* *Perimeter Service Road*
- *"Alley"*

Park Quarter:

- *"Park Drive"*
- *"Drive"*

4 The Phasing Diagram included in the Guidelines at page 7 is deleted and replaced by the figure attached hereto as Exhibit "2."

5 Table 1 - "Base Parking Rate," located on page 8 of the Guidelines is amended as follows:

TABLE 1- BASE PARKING RATE

Use Rate

| | |
|-------------------------------------|--|
| <i>Hotel -</i> | <i>0.75 spaces/ room</i> |
| <i>Retail - <u>/restaurant*</u></i> | <i><u>3.5</u> 4 spaces/ 1,000 s.f</i> |
| <i>Office -</i> | <i>2.5 spaces/ 1 000 s.f</i> |
| <i>Restaurant</i> | <i>10 spaces/ 1,000 s.f.</i> |
| <i>Residential **</i> | <i>1.3 spaces/unit + 1 space/ 5 units guest</i> |

** The retail/restaurant base parking rate can be utilized when total restaurant floor area for the project is less than 15% of the total retail floor area. If restaurant floor area exceeds 15% of total retail floor area, the additional restaurant floor area shall provide parking at the rate of 10 spaces per 1,000 s.f.*

*** Guest parking is not required with use of shared parking.*

6. The text entitled "Shared Parking Matrix" beginning on page 8 of the Guidelines is amended as follows:

SHARED PARKING MATRIX

The shared parking matrices may be used if certain uses which utilize parking at different times throughout the day are in proximity to each other. As an example, restaurant uses see peak parking demand in the evening hours, whereas office uses have a low demand in the day hours. The same parking area can accommodate office uses during the day, and restaurant uses in the evening. The time of day variation rate was based on methodologies outlined in the Urban Land Institute's publication Shared Parking. In order for the shared parking rate to be used, the parking areas must be demonstrated to accommodate parking for at least two of the following land uses.

- Hotel
- Retail
- Restaurant
- Office
- Residential
- Tri-Rail/Transit

If at least two of these uses do not utilize the parking area, the shared parking rate cannot be applied. The parking requirement, determined by either of the rates outlined above as applicable, is multiplied by the percentages in Tables 2 and 3 for each use. The maximum parking demand must be calculated for the weekday time period and for the Saturday time period separately. This maximum demand determines the maximum parking requirement for the combined uses.

~~Tri-Rail/Transit parking can be included in the shared parking calculations according to Table 2 and 3 if approved by the appropriate governing agencies.~~

The shared parking matrices are illustrated in Tables 2 and 3

7 The Tables entitled "Table 2- Shared Parking Matrix (Weekday)" and "Table 3 - Shared Parking Matrix (Saturday)" at page 9 of the Guidelines are deleted and replaced by the Tables attached hereto as Exhibit "3-A" and "3-B "

8 The following text entitled "Tri-Rail/Transit Parking" on page 9 of the Guidelines is deleted in its entirety

~~Tri-Rail/Transit Parking~~

~~These parking spaces can be included in the shared parking calculations only with an explicit written permission of the appropriate governing agencies.~~

- 9 The figure labeled “Street Network Schematic Representation” on page 11 of the Guidelines is deleted and replaced by the figure attached as Exhibit “4 ”
- 10 The table entitled “Dimensional Data” found at page 13 of the Guidelines is deleted and replaced by the table attached as Exhibit “5 ”
- 11 The graphic entitled “Typical Thoroughfare Section” found at page 14 of the Guidelines is deleted and replaced by the graphic attached as Exhibit “6.”
12. The graphic entitled “Typical Avenue Section” found at page 15 of the Guidelines is deleted and replaced by the graphic attached as Exhibit “7,” including sections entitled “Typical Avenue & Street Section at Residential Quarter” and “Typical Avenue Section at Mixed Use Quarter ”
- 13 The graphic entitled “Typical Drive Section” found at page 17 of the Guidelines is deleted and replaced by the graphic attached as Exhibit “8,” entitled “Perimeter Service Road.”
- 14 The text entitled “The Zocalo & Events Space,” found at page 25 of the Guidelines, is hereby amended as follows:

THE ZOCALO & EVENTS SPACE

Only the “Thoroughfare” and “Avenue”, ~~and “Drive”~~ can border on the Zocalo, and the “Alley” and “Perimeter Service Road” are is prohibited.

Allowed uses and activities of the Zocalo include:

- Retail sales & services
- Restaurant & Dining uses
- Temporary street vendors
- Water fountains
- Street fairs

- Community gatherings

The Mixed-Use Quarter must provide for an on-site storage location to store a limited amount of equipment and furnishings that may be utilized to hold an event.

15 The text entitled "Trees," at page 28 of the Guidelines, located within the "Landscape Materials" section, is amended as follows.

TREES

The use of native canopy trees are greatly encouraged as a means of providing shade and adhering to Xeriscape, NatureScape, and LEED principles. Canopy Trees are encouraged in and around parking areas, to provide shade for parked cars, and along pedestrian walk ways and outdoor seating areas. Any existing on-site trees or palms intended to remain in place, shall be protected as per the City of Hollywood's Landscape Manual. Any on-site existing trees or palms scheduled to be relocated on site shall also adhere to all applicable local and state guidelines for pruning, excavating, storing and transplanting these materials.

Tree sizes should be in proportion to adjacent building heights and shall not interfere with any potential overhead utilities or required lighting. However, if trees are to be planted along the major vehicular and pedestrian streets through out the project, they shall be a minimum of ~~16-18'~~ 14'-16' overall height, 8' minimum spread, with a clear trunk of at least 6-7' for pedestrian clearance. These trees shall be planted a minimum of 4' from the edge of curb to avoid vehicular damage from overhanging limbs.

16. The text entitled "Sod" at page 29 of the Guidelines, within the "Landscape Materials" section, is amended as follows:

SOD

St. Augustine sod (*Stenotaphrum* spp.) shall be used on all areas of the site not covered by concrete, asphalt, brick pavers, building, or any other "hard surface", or shrub, hedge and groundcover beds. The sod line around planting beds, where applicable, should be a minimum of 4" from the edge of the shrubs, hedges and groundcovers, and a minimum of 18" from the trunks of trees and palms. Retention areas may be "seeded" if applicable and if these areas are no in direct public view Bahia sod (*Paspalum notatum*) is acceptable as a substitute for St. Augustine sod in park and retention areas.

17 The text entitled “Planting Soils” and “Palm Sand” at page 29 of the Guidelines, within the “Landscape Construction Materials” section, is amended as follows.

PLANTING SOILS - Planting soil for backfill shall be ~~an evenly blended mixture of 50% “muck,” 25% coarse sand, 15% sphagnum peat moss and 10% sterilized cow manure a 50/50% mix, nematode free, planting soil for annual beds to be comprised of 50% Canadian peat moss, 25% salt free coarse sand & 25% aerolite.~~ Add 2 pounds of fertilizer, specified to each cubic yard of soil and thoroughly mix. Mix shall be suitable for plant growth and free from had clods, stiff clay, hardpan, gravel, brush, large roots, nematodes, weeds, refuse or other deleterious material, and of reasonably uniform quality

PALM SAND - Planting soil mixture to be placed as backfill around the root balls of all Palms shall consist of a mixture as specified above. ~~Bottom ¼ of planting pit shall be backfilled with clean, coarse-grained builder’s sand.~~

18 The text entitled “General Requirements” at page 34 of the Guidelines is amended as follows.

GENERAL REQUIREMENTS

To the extend that the provisions of the PD Guidelines (Development Guidelines for Sheridan Stationside Village) constitute a modification tot the provisions of Articles 2, 5, 7 and 9 of the City Code, City Commission approval of theses PD Guidelines constitutes express modification of the applicable provisions of Articles 2, 5, 7 and 9 of the City Code, pursuant to Section 4 15 E. 6 and 7 of the City Code.

All construction standards shall be consistent with and supplement applicable standards in city codes, ordinances and regulations, and other agency standards.

18 The text entitled “Example Parking Requirement Calculation” at page 36 of the Guidelines is amended as follows

EXAMPLE PARKING REQUIREMENT CALCULATION

Uses being served by the parking structure:

Office = 150,000 sf

Retail = 100,000 sf

Hotel = 150 rooms

Residential = 100 units

Base Rate required parking:

Office - 150,000 s.f @ (2.5/1,000) = 375 spaces

Retail - 100,000 s.f @ (~~4~~ 3.5/1,000) = ~~400~~ 350 spaces

Hotel - 150 rooms @ (0.75) = 112.5 spaces = 113 spaces

Res. - 100 units @ (1.3) + 100/5 = 130 + 20 = 150 spaces

TOTAL. ~~1,038~~ 988 spaces

Shared Parking Table:

In order to prepare the table, you must multiply the base rate required parking for each use by the percentage in each time period that is shown in Tables 2 & 3 of the "Minimum Parking Requirements" section of Chapter 1

Because this parking structure serves other uses, the guest parking requirement of 20 spaces for the residential is waived (Only 130 residential spaces are included in the tables).

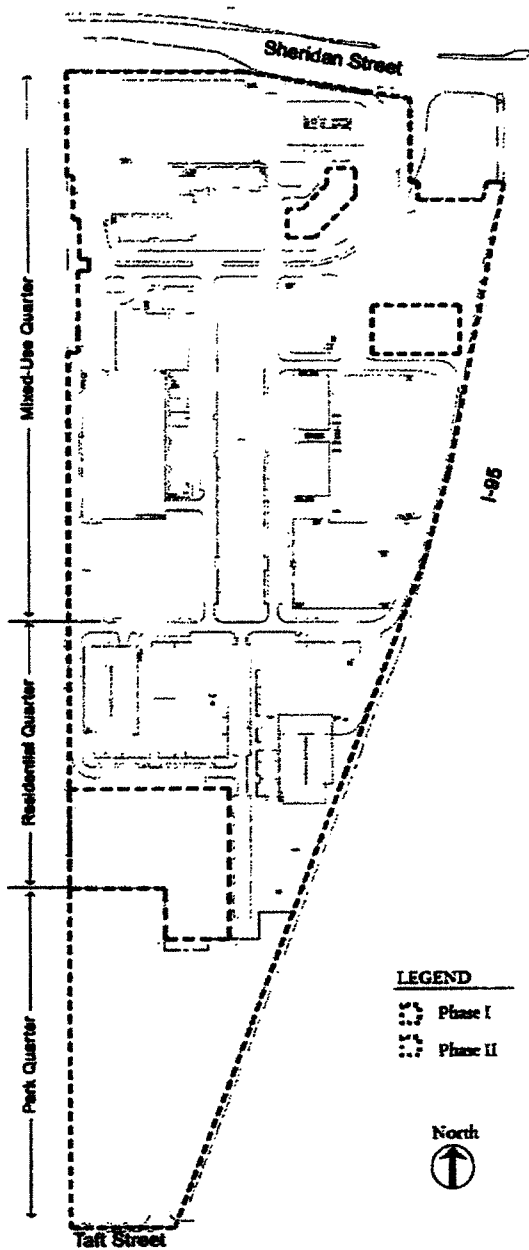
Based on the adjacent tables, the required parking for these uses would be the maximum number of spaces needed from any of these two tables.

Total required parking = ~~869~~ 821 spaces

Parking required by base rate = ~~1,038~~ 988 spaces

Total parking reduction = ~~169~~ 167 spaces (~~16~~ 17%)

19 The Tables entitled "Table 2 - Shared Parking Matrix (Weekday)" and "Table 3 - Shared Parking Matrix (Saturday)" are deleted and replaced by the Tables attached as Exhibit "9-A" and "9-B," attached hereto



Phasing Diagram

PHASING

Sheridan Stationside Village is to be developed in two major phases:

- Phase I
- Phase II

These phases can be further subdivided into smaller phases, but prior to the issuance of building permits for more than 500 residential units, a minimum of 100,000 square feet of commercial retail and/or office square footage must have been completed or be under construction.

The building program of Phase I is to be developed in the Mixed-Use Quarter. Furthermore, phasing might change based on market conditions.

The 6-acre park area will be established in Phase I.

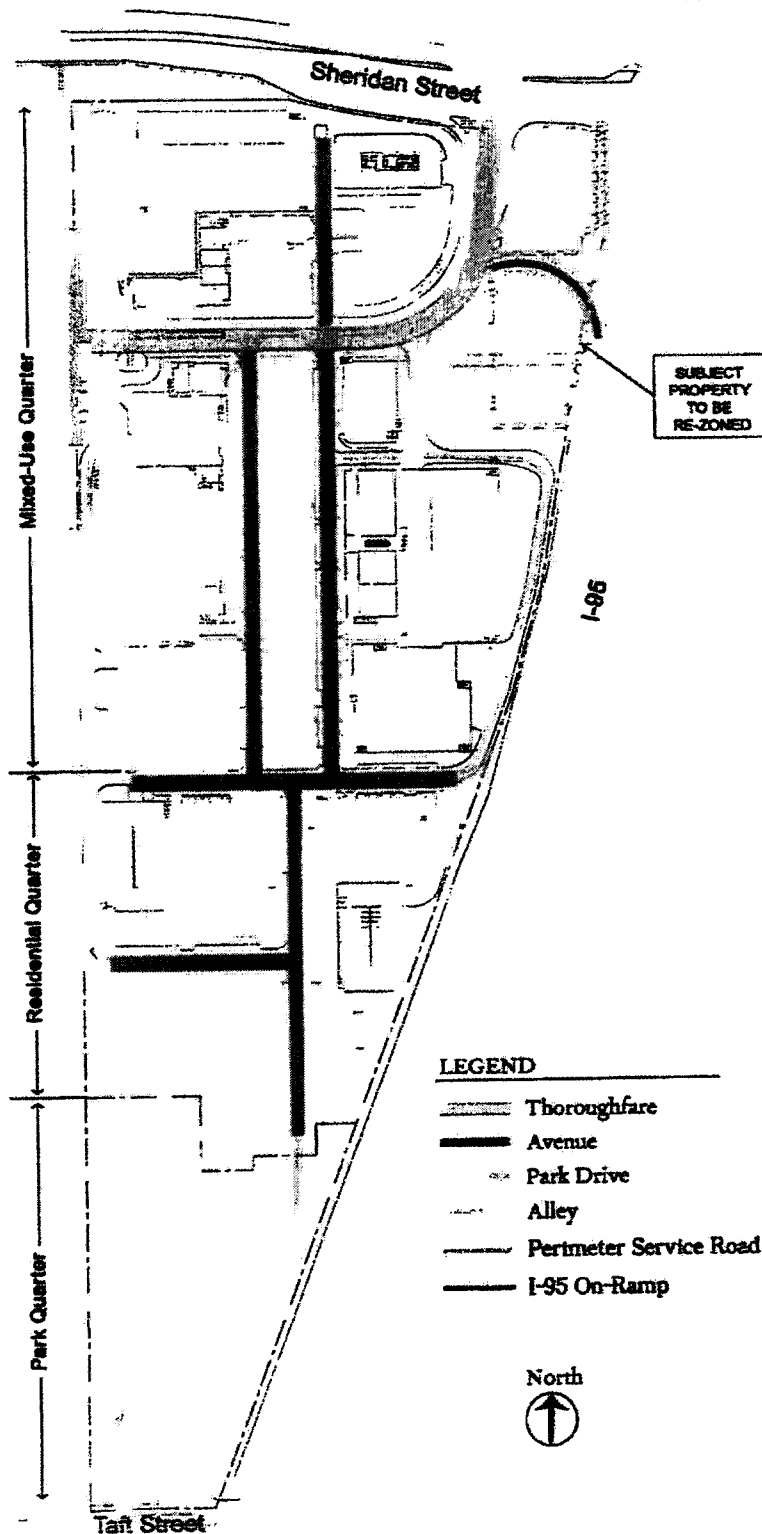
| Table 2 - Shared Parking Matrix (Weekday) | | | | | | | |
|---|------|--------|--------|---------|--------|-------|-------|
| Time | Res. | Retail | Office | Transit | Restnt | Hotel | Total |
| 6:00 AM | 130 | 0 | 11 | 0 | 0 | 113 | 254 |
| 7:00 AM | 113 | 28 | 75 | 0 | 0 | 96 | 312 |
| 8:00 AM | 103 | 63 | 236 | 0 | 0 | 73 | 475 |
| 9:00 AM | 95 | 147 | 349 | 0 | 0 | 62 | 653 |
| 10:00 AM | 88 | 238 | 375 | 0 | 0 | 51 | 752 |
| 11:00 AM | 77 | 305 | 375 | 0 | 0 | 40 | 796 |
| 12:00 PM | 78 | 340 | 338 | 0 | 0 | 34 | 789 |
| 1:00 PM | 77 | 350 | 338 | 0 | 0 | 34 | 798 |
| 2:00 PM | 78 | 340 | 354 | 0 | 0 | 40 | 821 |
| 3:00 PM | 78 | 333 | 349 | 0 | 0 | 40 | 800 |
| 4:00 PM | 88 | 305 | 289 | 0 | 0 | 51 | 730 |
| 5:00 PM | 100 | 277 | 176 | 0 | 0 | 68 | 621 |
| 6:00 PM | 111 | 287 | 86 | 0 | 0 | 79 | 563 |
| 7:00 PM | 122 | 312 | 26 | 0 | 0 | 85 | 545 |
| 8:00 PM | 125 | 305 | 26 | 0 | 0 | 102 | 557 |
| 9:00 PM | 127 | 214 | 11 | 0 | 0 | 107 | 460 |
| 10:00 PM | 129 | 112 | 11 | 0 | 0 | 113 | 365 |
| 11:00 PM | 130 | 46 | 0 | 0 | 0 | 113 | 289 |
| 12:00 AM | 130 | 0 | 0 | 0 | 0 | 113 | 243 |

Exhibit "3-A"

| Table 3 - Shared Parking Matrix (Saturday) | | | | | | | |
|--|------|--------|--------|---------|---------|-------|-------|
| Time | Res. | Retail | Office | Transit | Restmnt | Hotel | Total |
| 6:00 AM | 130 | 0 | 0 | 0 | 0 | 102 | 232 |
| 7:00 AM | 124 | 11 | 11 | 0 | 0 | 79 | 224 |
| 8:00 AM | 114 | 35 | 38 | 0 | 0 | 68 | 255 |
| 9:00 AM | 105 | 105 | 49 | 0 | 0 | 57 | 316 |
| 10:00 AM | 92 | 158 | 49 | 0 | 0 | 45 | 344 |
| 11:00 AM | 92 | 256 | 64 | 0 | 0 | 40 | 451 |
| 12:00 PM | 92 | 298 | 64 | 0 | 0 | 34 | 487 |
| 1:00 PM | 91 | 333 | 49 | 0 | 0 | 34 | 506 |
| 2:00 PM | 92 | 350 | 38 | 0 | 0 | 40 | 519 |
| 3:00 PM | 95 | 350 | 26 | 0 | 0 | 45 | 516 |
| 4:00 PM | 98 | 315 | 26 | 0 | 0 | 57 | 495 |
| 5:00 PM | 105 | 263 | 11 | 0 | 0 | 68 | 447 |
| 6:00 PM | 111 | 228 | 11 | 0 | 0 | 79 | 428 |
| 7:00 PM | 113 | 210 | 11 | 0 | 0 | 90 | 425 |
| 8:00 PM | 120 | 193 | 11 | 0 | 0 | 102 | 425 |
| 9:00 PM | 124 | 140 | 0 | 0 | 0 | 107 | 371 |
| 10:00 PM | 125 | 133 | 0 | 0 | 0 | 113 | 371 |
| 11:00 PM | 127 | 46 | 0 | 0 | 0 | 113 | 286 |
| 12:00 AM | 130 | 0 | 0 | 0 | 0 | 113 | 243 |

Exhibit "3-B"

7-67-02



4 7 08

STREET NETWORK

Note: Glassed plan background is for schematic representation only, and it is subject to change.

SHERIDAN STATIONSIDE VILLAGE
Hollywood, Florida

ROGER FRY & ASSOCIATES ARCHITECTS, P.A. 2791 Blvd Avenue Coconut Grove, Florida 33133 phone 305-446-7787

ram

PINNACLE
HOUSING GROUP

Exhibit "4"

ROGER FRY
& ASSOCIATES
ARCHITECTS, P.A.

DIMENSIONAL DATA

| | Thoroughfare | Avenue and Street | Park Drive | Perimeter Service Road | Alley |
|--|---------------------------|--|--------------------------------|---------------------------|---------------------------|
| Street Dimensional Requirements: | | | | | |
| Minimum Lane width | 11'-15" if 1-lane roadway | 11'-15" if 1-lane roadway | 11'-15" if 1-lane roadway | 11'-20" if 1-lane roadway | 11'-15" if 1-lane roadway |
| Minimum Dimensions of a parallel parking stall | Not Permitted | 8.5' x 24' | 8.5' x 24' | 8.5' x 24' | 8.5' x 24' |
| Minimum angled parking unit stall size (including 90 deg.) | Not Permitted | 9' x 18'- 90 deg. Not Permitted | 9' x 18' | 9' x 18' | 9' x 18' |
| Minimum Dimensions of parallel truck loading berth | Not Permitted | 10' W x 25' L x 14' H | Not Permitted | 10' W x 25' L x 14' H | 10' W x 25' L x 14' H |
| Minimum Width of tree/ amenity zone adjoining parallel parking | N/A | 5' from back of curb | 5' from back of curb* | 0' from back of curb | 0' from back of curb |
| Minimum Width of tree/ amenity zone adjoining a drive lane | 8' from back of curb | 8' from back of curb | 5' from back of curb* | 0' from back of curb | 0' from back of curb |
| Maximum spacing between trees | 30' | 30' | N/A | None | None |
| Minimum size of pervious tree planting area | 8' | 5' | N/A | N/A | N/A |
| Minimum pedestrian walking zone width | 7' | 8' 6" if at a ground floor residential use | 8' required on any 1 side only | N/A | N/A |
| Minimum headroom clearance at sidewalk | 12' | 12' | N/A | N/A | N/A |
| Front Set-Back Requirements: | | | | | |
| Minimum building setback from back of curb | 15' | 13' | N/A | 0' | 0' |
| Maximum building setback from back of curb | 30' -- | 25' | N/A | No Limit | No Limit |
| Maximum overhead projections into required minimum setback | 6' | 8' | N/A | 0' | 0' |
| Use/Use Requirements: | | | | | |
| Residential uses at ground floor permitted | No | Yes | N/A | Yes | Yes |
| Structured Parking Garages at ground floor permitted | No | No | No | Yes | Yes |
| Maximum length of facade without intervening fenestration | 25' | 25' | N/A | No Limit | No Limit |
| Fences permitted | Yes | Only for outdoor seating @ mixed use quarter 3'-0" max @ residential quarter | Yes | Yes | Yes |
| Maximum height of top rail of fence | 6' -- | 3' | 6' | 10' | 10' |
| Arcades & colonnades (non-residential use) permitted | Yes | Yes | N/A | Yes | Yes |
| Minimum interior clear width of arcade (if provided) | 8' | 8' | N/A | 6' | 8' |
| Minimum interior clear height of arcade (if provided) | 12' | 12' | N/A | 9' | 9' |
| Parking & Loading Requirements: | | | | | |
| Parallel parking permitted | No | Yes | Yes | Yes | Yes |
| Maximum number of continuous parallel parking spaces | N/A | 10 | 3 | No Limit | No Limit |
| Angled parking permitted | No | Only if a 2-lane roadway | yes | Only if a 2-lane roadway | Only if a 2-lane roadway |
| Maximum number of continuous angled parking spaces | N/A | 10 | 6 | No Limit | No Limit |
| Stopping permitted | No | Yes | Yes | Yes | Yes |
| Loading permitted | No | Yes | No | Yes | Yes |
| Enclosed service area access permitted | No | Yes | N/A | Yes | Yes |

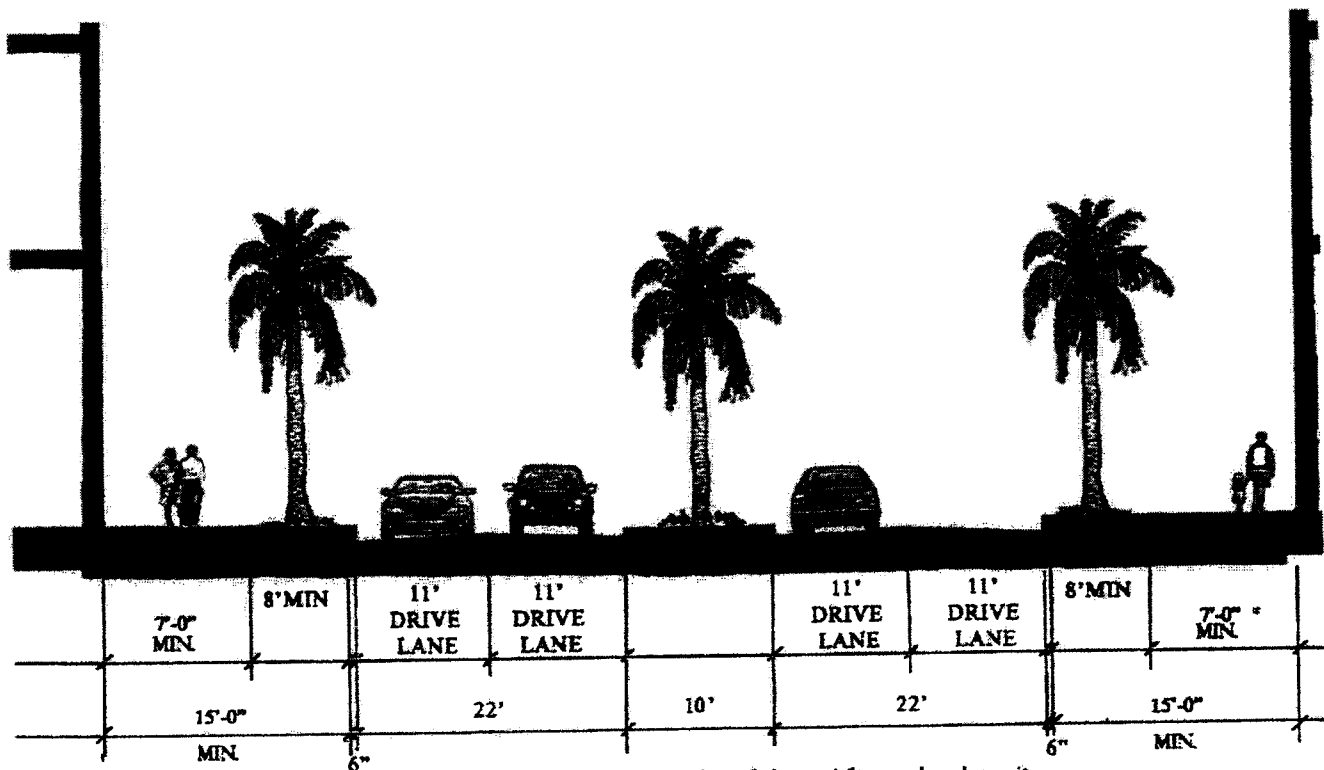
* May be reduced for tree preservation

-- Setbacks @ thoroughfare may be increased where thoroughfare abuts public plazas

--- Fences @ thoroughfare limited to roadway east of zocalo. No fences permitted @ thoroughfare west of zocalo.

THOROUGHFARE

"Thoroughfare" represent the primary vehicular routes into and out of the site. Transit buses and tractor trailers may travel on "Thoroughfare" to reach the Transit Station or service areas.

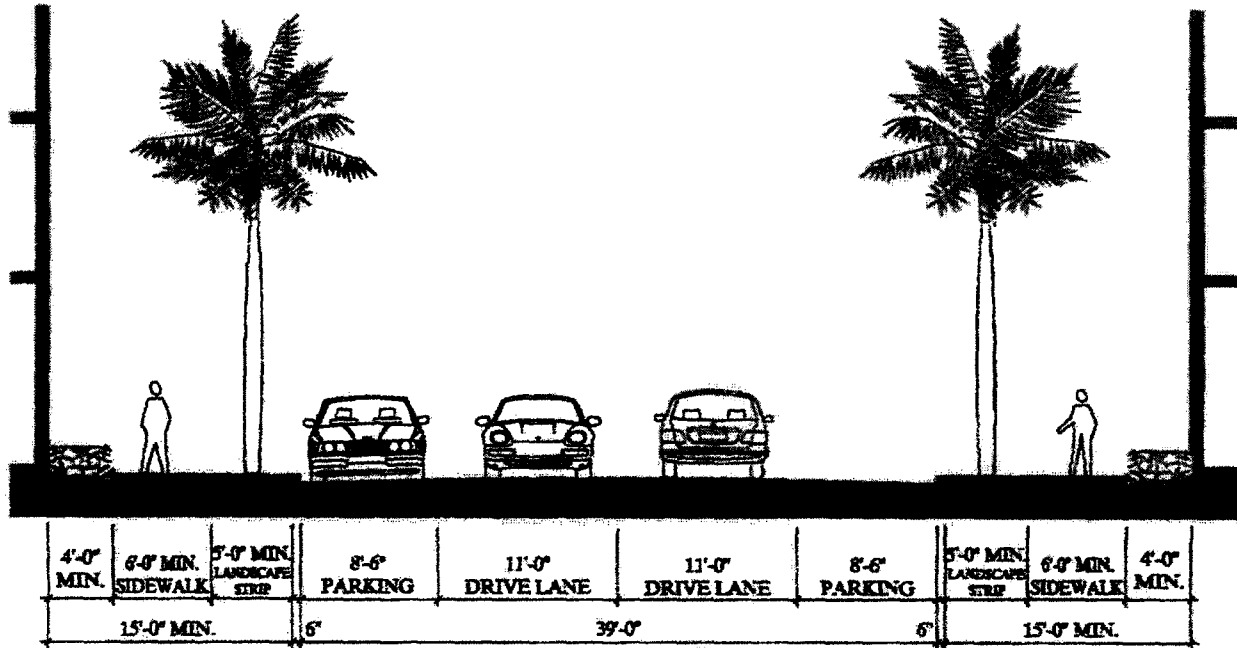


* Sidewalk required @ both sides of thoroughfare only where it abuts buildings or public plazas.

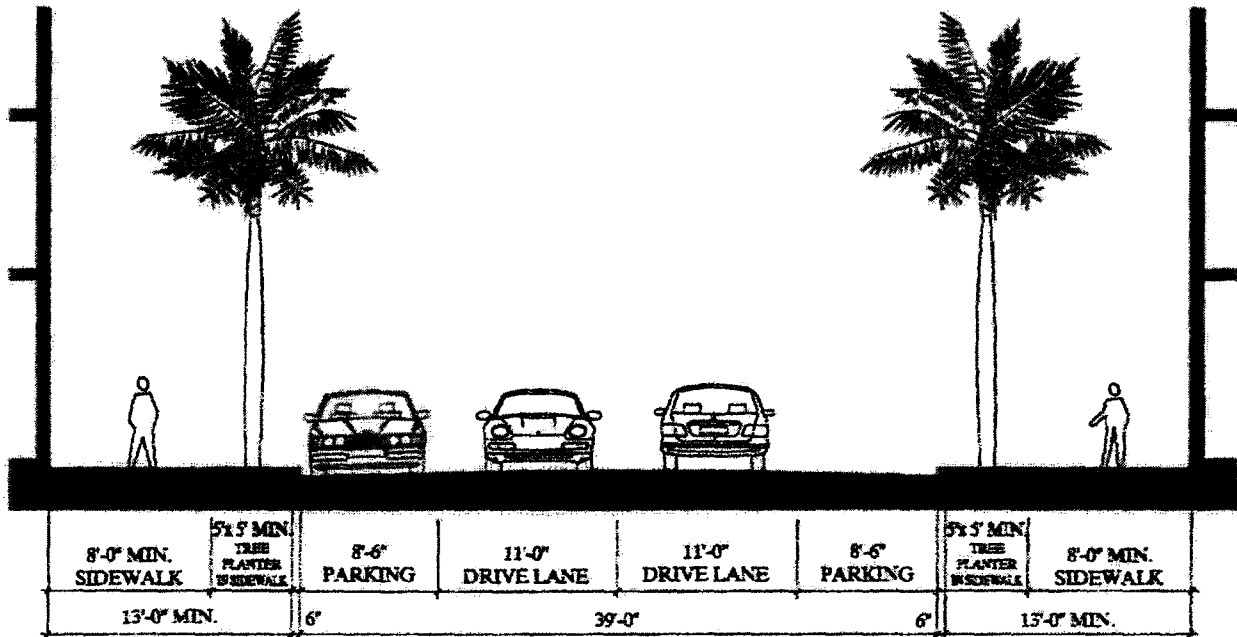
Typical Thoroughfare Section

Avenue

"Avenue" represent the primary road-form that connects the three Quarters internally. The "Avenue" is intended to provide a high level of interaction between vehicles and pedestrians to the storefronts and the Zocalo.



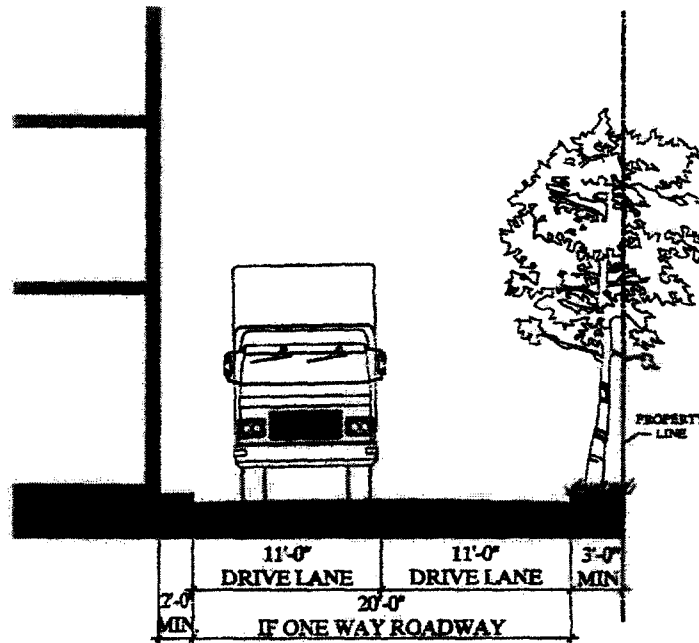
Typical Avenue & Street Section @ Residential Quarter



Typical Avenue & Street Section @ Mixed Use Quarter

PERIMETER SERVICE ROAD

"Perimeter service road" is a roadway for use by large delivery trucks. These roadways are to be located to provide access to commercial loading areas away from pedestrian & public areas.



Perimeter Service Road

Table 2 - Shared Parking Matrix (Weekday)

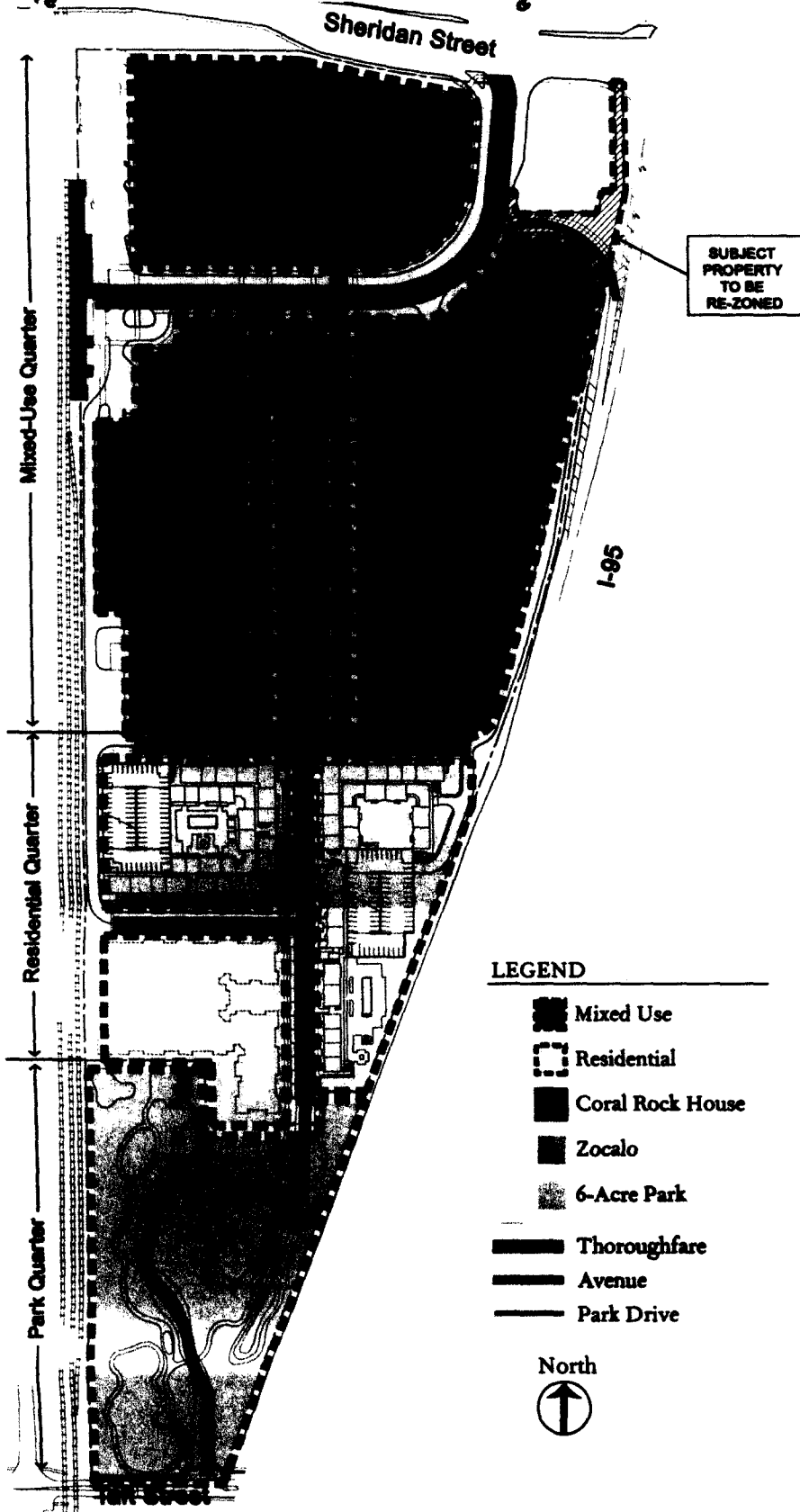
| Time | Residential | Retail/Restaurant | Office | Transit | Hotel |
|-------|-------------|-------------------|--------|---------|-------|
| 6 AM | 100% | 0% | 3% | 3% | 100% |
| 7 AM | 87% | 8% | 20% | 20% | 85% |
| 8 AM | 79% | 18% | 63% | 63% | 65% |
| 9 AM | 73% | 42% | 93% | 93% | 55% |
| 10 AM | 68% | 68% | 100% | 100% | 45% |
| 11 AM | 59% | 87% | 100% | 100% | 35% |
| 12 PM | 60% | 97% | 90% | 90% | 30% |
| 1 PM | 59% | 100% | 90% | 90% | 30% |
| 2 PM | 60% | 97% | 97% | 97% | 35% |
| 3 PM | 61% | 95% | 93% | 93% | 35% |
| 4 PM | 66% | 87% | 77% | 77% | 45% |
| 5 PM | 77% | 79% | 47% | 47% | 60% |
| 6 PM | 85% | 82% | 23% | 23% | 70% |
| 7 PM | 94% | 89% | 7% | 7% | 75% |
| 8 PM | 96% | 87% | 7% | 7% | 90% |
| 9 PM | 98% | 61% | 3% | 3% | 95% |
| 10 PM | 99% | 32% | 3% | 3% | 100% |
| 11 PM | 100% | 13% | 0% | 0% | 100% |
| 12 AM | 100% | 0% | 0% | 0% | 100% |

Multiplying the gross total parking demand of any use by the corresponding distribution percentage at a given time of the day will yield the actual parking demand at that time.

Table 3 - Shared Parking Matrix (Saturday)

| Time | Residential | Retail/Restaurant | Office | Transit | Hotel |
|-------|-------------|-------------------|--------|---------|-------|
| 6 AM | 100% | 0% | 0% | 0% | 90% |
| 7 AM | 95% | 3% | 3% | 3% | 70% |
| 8 AM | 88% | 10% | 10% | 10% | 60% |
| 9 AM | 81% | 30% | 13% | 13% | 50% |
| 10 AM | 71% | 45% | 13% | 13% | 40% |
| 11 AM | 71% | 73% | 17% | 17% | 35% |
| 12 PM | 71% | 85% | 17% | 17% | 30% |
| 1 PM | 70% | 95% | 13% | 13% | 30% |
| 2 PM | 71% | 100% | 10% | 10% | 35% |
| 3 PM | 73% | 100% | 7% | 7% | 40% |
| 4 PM | 75% | 90% | 7% | 7% | 50% |
| 5 PM | 81% | 75% | 3% | 3% | 60% |
| 6 PM | 85% | 65% | 3% | 3% | 70% |
| 7 PM | 87% | 60% | 3% | 3% | 80% |
| 8 PM | 92% | 55% | 3% | 3% | 90% |
| 9 PM | 95% | 40% | 0% | 0% | 95% |
| 10 PM | 96% | 38% | 0% | 0% | 100% |
| 11 PM | 98% | 13% | 0% | 0% | 100% |
| 12 AM | 100% | 0% | 0% | 0% | 100% |

Multiplying the gross total parking demand of any use by the corresponding distribution percentage at a given time of the day will yield the actual parking demand at that time.



| MAXIMUM DENSITY | PROGRAM |
|---------------------------|--------------|
| Hotel | 300,000 s.f. |
| Office | 240,000 s.f. |
| Hotel | 150 hotel |
| Residential | 1,000 units |
| Existing coral rock house | 1,700 s.f. |

| DENSITY & INTENSITY TABLE | |
|-------------------------------|-------------------------|
| Max. Area: | |
| Developable Area | 1,400,000 s.f. |
| Existing Area | 240,000 s.f. |
| Total | 1,700,000 s.f. |
| 48.4 acres | |
| Residential Density: | |
| 1,000 units/48.4 acres = | 20 units/acre |
| Hotel Area Density: | |
| 150 hotel/48.4 acres = | 4 hotel/acre |
| Commercial Area: | |
| Hotel | 300,000 s.f. |
| Office | 240,000 s.f. |
| Total | 540,000 s.f. |
| Commercial Intensity: | |
| 540,000 s.f./1,700,000 s.f. = | 31 commercial intensity |

The maximum amount of office use may increase to the 100 hotel use after commercial approval level of 200.00 s.f. provided that the overall development program is consistent with respect to any replacement for 200 units.

| MAXIMUM BUILDING HEIGHTS | |
|--------------------------|----------|
| Mixed-Use Quarter | 120 feet |
| Residential Quarter | 100 feet |
| Park Quarter | 30 feet |

A building height is measured from a point of average elevation along the average 10 feet to the top of the first roof deck at the top of the building floor. Construction of roof elements, decorative architectural features, elevator rooms, roof stairs, and rooftop mechanical equipment rooms are not included in determining roof height. In no case shall any of these elements exceed 100 feet.

| TABLE 1: BASE PARKING RATE | |
|----------------------------|---|
| Hotel | 0.75 spaces/room |
| Office | 3.5 spaces/1,000 s.f. |
| Apartment | 2.5 spaces/1,000 s.f. |
| Residential** | 1.5 spaces/unit + 1 space/3 units/guest |

* The restaurant base parking rate can be reduced when hotel restaurant floor area for the project is less than 10% of total hotel floor area. If restaurant floor area exceeds 10% of total hotel floor area, the restaurant floor area shall provide parking at the rate of 10 spaces per 1,000 s.f.

** Determining is not required with use of shared parking.

| Program | Residential | Hotel/Restaurant | Office | Hotel |
|---------|-------------|------------------|--------|-------|
| 5 AM | 100% | 0% | 2% | 100% |
| 6 AM | 97% | 0% | 30% | 97% |
| 7 AM | 79% | 10% | 63% | 60% |
| 8 AM | 73% | 42% | 93% | 50% |
| 9 AM | 68% | 68% | 100% | 40% |
| 10 AM | 68% | 67% | 100% | 30% |
| 11 AM | 63% | 97% | 90% | 20% |
| 12 PM | 63% | 97% | 90% | 20% |
| 1 PM | 63% | 97% | 90% | 20% |
| 2 PM | 63% | 97% | 90% | 20% |
| 3 PM | 63% | 97% | 90% | 20% |
| 4 PM | 63% | 97% | 90% | 20% |
| 5 PM | 63% | 97% | 90% | 20% |
| 6 PM | 63% | 97% | 90% | 20% |
| 7 PM | 63% | 97% | 90% | 20% |
| 8 PM | 63% | 97% | 90% | 20% |
| 9 PM | 63% | 97% | 90% | 20% |
| 10 PM | 63% | 97% | 90% | 20% |
| 11 PM | 63% | 97% | 90% | 20% |
| 12 AM | 63% | 97% | 90% | 20% |

| Program | Residential | Hotel/Restaurant | Office | Hotel |
|---------|-------------|------------------|--------|-------|
| 5 AM | 100% | 0% | 2% | 100% |
| 6 AM | 97% | 0% | 30% | 97% |
| 7 AM | 79% | 10% | 63% | 60% |
| 8 AM | 73% | 42% | 93% | 50% |
| 9 AM | 68% | 68% | 100% | 40% |
| 10 AM | 68% | 67% | 100% | 30% |
| 11 AM | 63% | 97% | 90% | 20% |
| 12 PM | 63% | 97% | 90% | 20% |
| 1 PM | 63% | 97% | 90% | 20% |
| 2 PM | 63% | 97% | 90% | 20% |
| 3 PM | 63% | 97% | 90% | 20% |
| 4 PM | 63% | 97% | 90% | 20% |
| 5 PM | 63% | 97% | 90% | 20% |
| 6 PM | 63% | 97% | 90% | 20% |
| 7 PM | 63% | 97% | 90% | 20% |
| 8 PM | 63% | 97% | 90% | 20% |
| 9 PM | 63% | 97% | 90% | 20% |
| 10 PM | 63% | 97% | 90% | 20% |
| 11 PM | 63% | 97% | 90% | 20% |
| 12 AM | 63% | 97% | 90% | 20% |

4 7-08

MASTER PLAN

Note: Ghosted plan background is for schematic representation only, and it is subject to change.

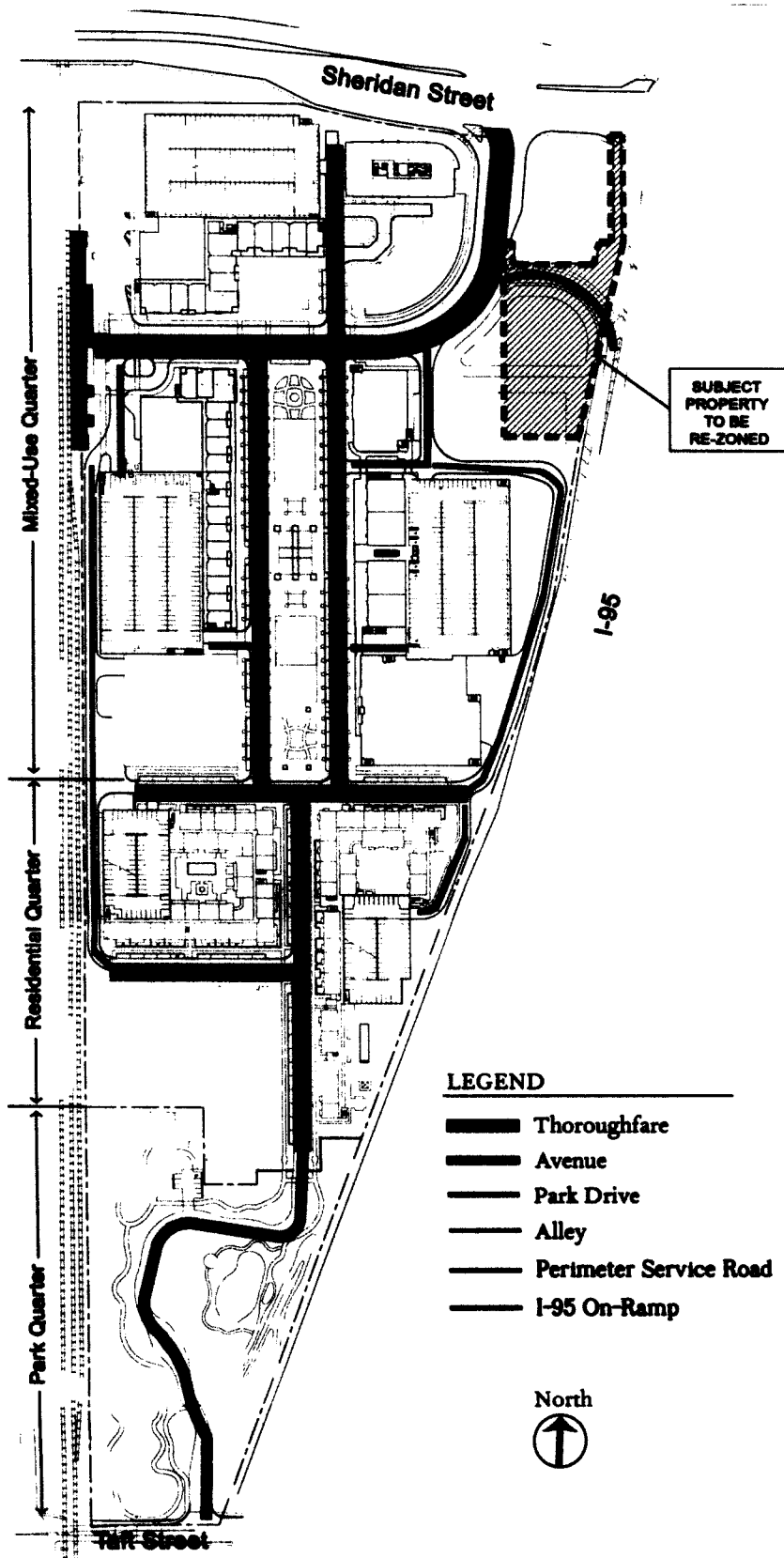
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Hollywood, Florida

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ram

PINNACLE
HOUSING GROUP

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4 7 08

STREET NETWORK

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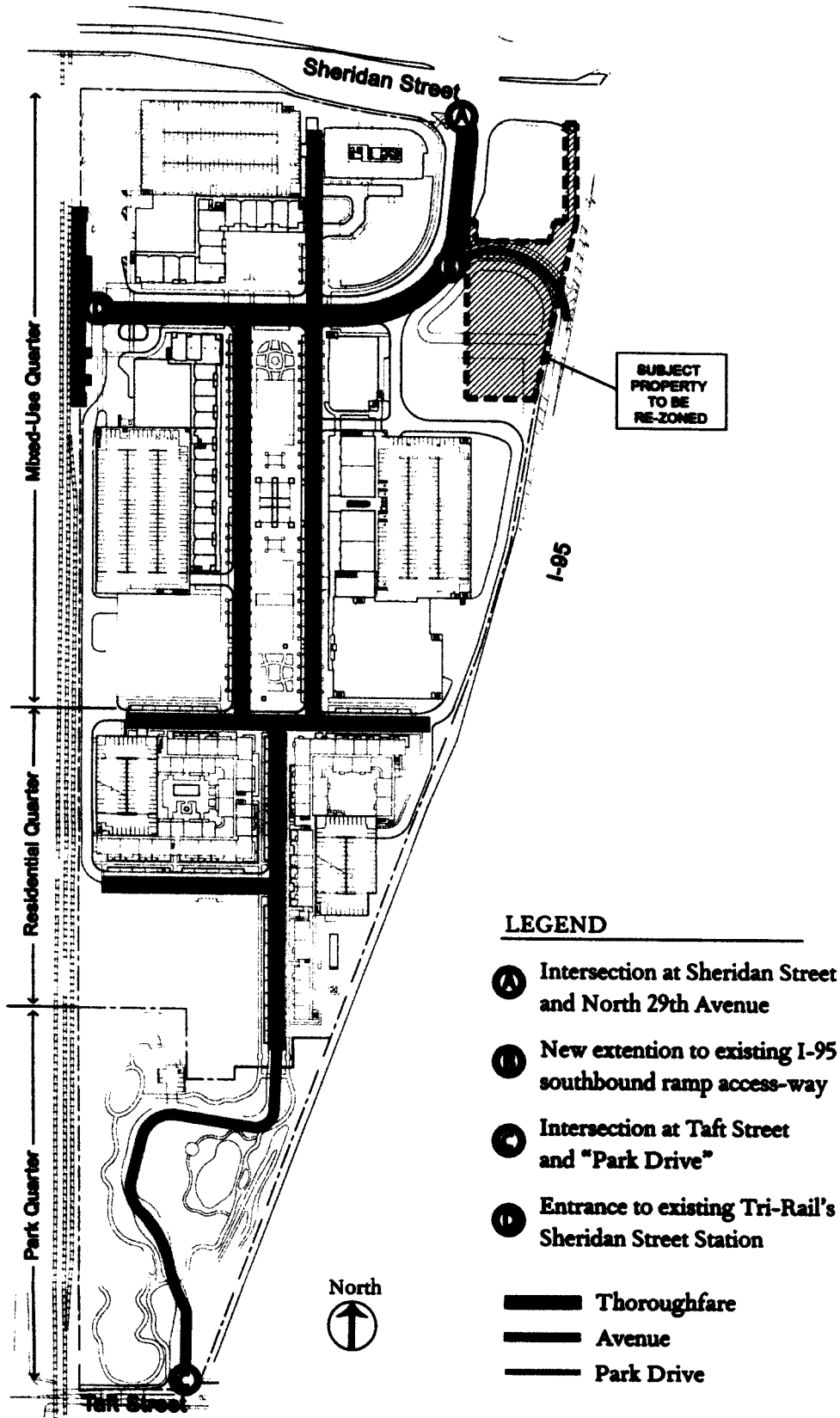
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POINTS OF CONNECTION

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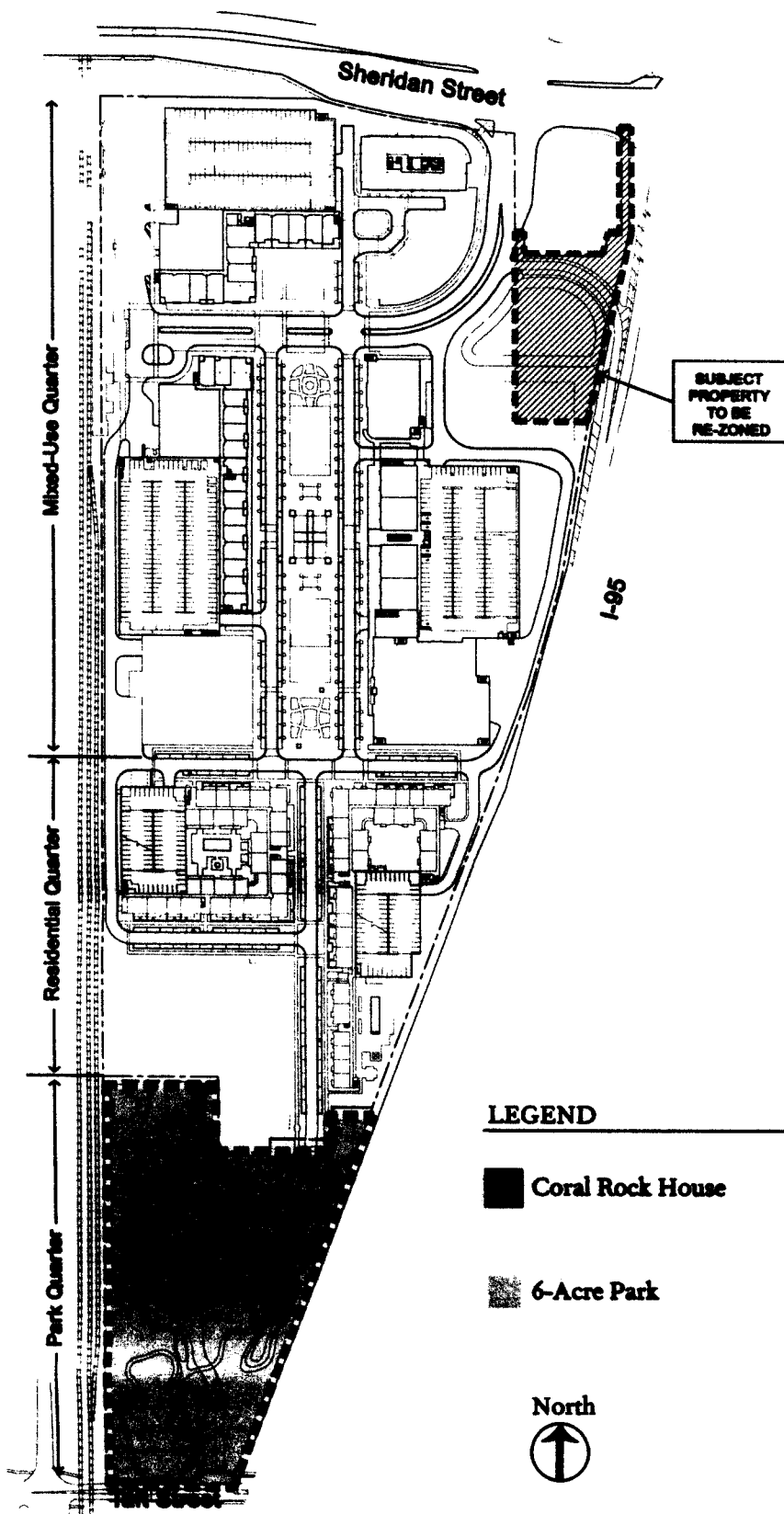
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LEGEND

■ Coral Rock House

▨ 6-Acre Park

North



4 7 - 08

COMMUNITY FACILITIES

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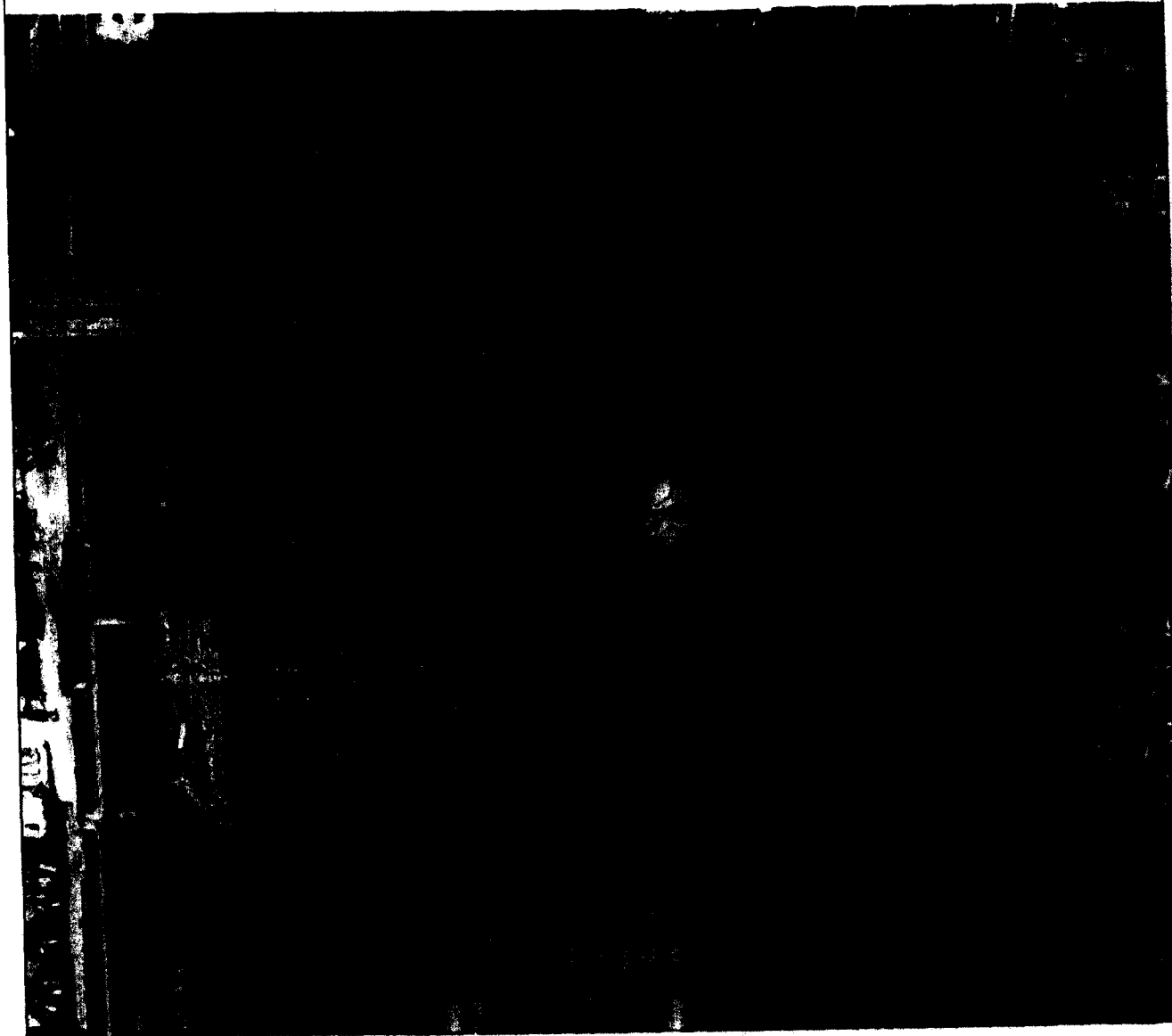
Improvements at Point of Connection A & B.

IMPROVEMENTS

- 1.) REDUCE ISLAND TO REALIGN SOUTHBOUND I-95 RAMP RIGHT-TURN LANES TO MERGE WITH LEFT MOST WESTBOUND LANES ON SHERIDAN ST.
- 2.) ADD A THIRD LEFT-TURN LANE ON I-95 SOUTH BOUND OFF RAMP
- 3.) ADD RIGHT-TURN LANE AND CHANGE EXISTING TO A THIRD LEFT-TURN ON I-95 NORTH BOUND OFF RAMP.
- 4.) ADD AND RESTRIPE TWO RIGHT-TURN LANES, A THROUGH LANE, AND TWO LEFT TURN LANES.
- 5.) REALIGN EASTBOUND LANES THROUGH TRAFFIC SIGNAL AT SHERIDAN AND N. 28TH INTERSECTION.
- 6.) ADD ADDITIONAL DEDICATED I-95 NORTHBOUND LEFT-TURN LANE.
- 7.) ADD ADDITIONAL EASTBOUND THRU LANE.
- 8.) ADD DEDICATED I-95 SOUTHBOUND RIGHT-TURN LANE.
- 9.) ADD FREE FLOWING EASTBOUND RIGHT-TURN LANE ONTO N. 28TH AVE.
- 10.) ADD AND REALIGN EASTBOUND LEFT-TURN LANE ONTO N. 28TH AVE.
- 11.) ADD AND RESTRIPE TWO RIGHT-TURN LANES, A THROUGH LANE, AND TWO LEFT TURN LANES.
- 12.) ADD A SECOND WESTBOUND LEFT-TURN LANE.
- 13.) INCREASE LENGTH OF SOUTH BOUND ON RAMP AND ADD PARALLEL MERGE.

LEGEND

- NEW CONSTRUCTION
- REALIGNED I-95 NORTHBOUND TURN LANES
- REALIGNED THROUGH LANES
- CHANGE IN LANE DESIGNATION



Improvements along Taft Street.

- 1) CONSTRUCT A WESTBOUND RIGHT-TURN LANE AT STATE ROAD 7
- 2) INSTALL AN EASTBOUND RIGHT-TURN OVERLAP PHASE.
- 3) CONSTRUCT EASTBOUND AND WESTBOUND RIGHT-TURN LANES AT U.S. HIGHWAY 1.
- 4) CONSTRUCT EASTBOUND AND WESTBOUND LEFT-TURN LANES AT 48TH AVENUE.
- 5) INSTALL A TRAFFIC SIGNAL AT 26TH AVENUE.

4 7 08

TRAFFIC IMPROVEMENTS

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ATTACHMENT III

Traffic Study



SHERIDAN STATIONSIDE VILLAGE

Traffic Study to Accompany Amendment to PD Master Plan

Prepared for
City of Hollywood

By
Calvin, Giordano & Associates



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EXECUTIVE SUMMARY

Calvin, Giordano and Associates (CGA) was retained by Okomo Associates LLC to complete a Traffic Impact Analysis for the development of a 500 residential units as part of the Sheridan Stationside Village project in the City of Hollywood, Florida. A previous study was completed for this project in 2007 which assessed potential impacts of the proposed project based on the following proposed land uses:

- 299,000 square feet of Office
- 300,000 square feet of Retail
- 150 Hotel Rooms
- 1,050 Residential Units

The original project had a final build-out year of 2015 and has since been revised to be completed in two (2) phases with a final build-out year of 2018:

1. Phase1 (2015)
 - a. 300 Low-Rise Apartments
 - b. 200 Mid-Rise Apartments
2. Phase 2 (2018)
 - a. 299,000 square feet of Office
 - b. 300,000 square feet of Retail
 - c. 150 room Hotel
 - d. 550 Residential Units

A capacity analysis of existing traffic conditions for the selected intersections was performed for the AM and PM peak hours. Existing traffic counts were conducted in July of 2013 and include traffic associated with new developments constructed in the area since the previous (2007) traffic study. Under existing conditions, the analysis revealed that all the intersections within the study area operate at an acceptable Level of Service (LOS E or better).

Phase 1 of the project is anticipated to generate 223 and 274 new trips during the AM and PM peak hours respectively. A capacity analysis was completed to determine if the roadway network (including planned improvements) would be able to accommodate the additional traffic generated by the proposed development. The analysis was performed

for existing conditions and future conditions (Year 2015 without project traffic and Year 2015 with project traffic).

The analysis revealed that under existing conditions and future (Year 2015) conditions, all intersections within the study area will operate at an acceptable Level of Service (LOS E or better) with or without project traffic.

The second phase of the project is anticipated to generate a total of 775 trips during the AM peak period and 1194 trips during the PM peak period. A capacity analysis was completed to determine if the roadway network (including scheduled improvements) would be able to accommodate the additional traffic generated by the proposed development.

Under both scenarios (without Phase 2 project traffic and with Phase 2 project traffic), the analyses revealed that all the intersections within the study area operate at an acceptable Level of Service (LOS E or better) with the exception of Sheridan Street and SR-7/US-441 which operates at LOS F during the PM peak period.

Based on the results of this study, the following improvements are recommended to provide safe and adequate access into the project site and improve traffic operations throughout the study area:

Phase 1

1. Construct a dedicated westbound right-turn lane at the intersection of Taft Street and Okomo Drive
2. Construct a dedicated eastbound left-turn lane at the intersection of Taft Street and Okomo Drive

Phase 2

3. Revise signal timing for the intersections of Sheridan Street and N. 29th Avenue, and Sheridan Street and SR-7/US-441. The suggested signal timing improvements exceed the purview of minor signal optimization and will require input from Broward County Traffic Engineering Division. Considering the fact that the intersections operate as part of a coordinated system, Broward County Traffic Engineering Division should be consulted to determine the appropriate timing changes

In addition, several improvements were agreed upon by Okomo Associates to be completed at different permitting stages of this project. The complete list of recommendations to be completed is listed in Section 5.0 of this report.