

ATTACHMENT A

Application Package

GENERAL APPLICATION

APPLICATION DATE: _____

2600 Hollywood Blvd

Room 315

Hollywood, FL 33022

Tel: (954) 921-3471

Email: Development@
Hollywoodfl.org

SUBMISSION REQUIREMENTS:

- One set of signed & sealed plans
(i.e. Architect or Engineer)
- One electronic **combined** PDF submission (max. 25mb)
- Completed Application Checklist
- Application fee

NOTE:

- This application must be **completed in full** and submitted with all documents to be placed on a Board or Committee's agenda.
- The applicant is responsible for obtaining the appropriate checklist for each type of application.
- Applicant(s) or their authorized legal agent must be present at all Board or Committee meetings.

**CLICK HERE FOR
FORMS, CHECKLISTS, &
MEETING DATES**

APPLICATION TYPE (CHECK ALL THAT APPLIES):

- ☐ Technical Advisory Committee ☐ Art in Public Places Committee ☒ Variance
☐ Planning and Development Board ☐ Historic Preservation Board ☐ Special Exception
☐ City Commission ☐ Administrative Approval

PROPERTY INFORMATION

Location Address: 3880 Circle Dr

Lot(s): _____ Block(s): _____ Subdivision: _____

Folio Number(s): 514207023710

Zoning Classification: RM-9, RM-8 Land Use Classification: Medium (16) Residential

Existing Property Use: Senior Living Facility Sq Ft/Number of Units: _____

Is the request the result of a violation notice? ☐ Yes ☒ No If yes, attach a copy of violation.

Has this property been presented to the City before? If yes, check all that apply and provide File Number(s) and Resolution(s): N/A

DEVELOPMENT PROPOSAL

Explanation of Request: Please refer to the Letter of Intent

Phased Project: Yes ☐ No ☒ Number of Phases:

Project	Proposal
Units/rooms (# of units)	# UNITS: <u>104</u> #Rooms <u>N/A</u>
Proposed Non-Residential Uses	<u>N/A</u> S.F.)
Open Space (% and SQ.FT.)	Required %: <u>40</u> (Area: <u> </u> S.F.)
Parking (# of spaces)	PARK. SPACES: (# <u>47</u>)
Height (# of stories)	(# STORIES) <u>3</u> (<u>129</u> FT.)
Gross Floor Area (SQ. FT)	Lot(s) Gross Area (<u>139,516</u> FT.)

Name of Current Property Owner: BRE ROOK SH PRESIDENTIAL PLACE LLC

Address of Property Owner: 233 S. WACKER DRIVE SUITE 4700 CHICAGO, IL 60606

Telephone: C/O 305-673-2585 Email Address: C/O tslavens@lsnlaw.com

Applicant Elevation Properties, LLC and/or its assignee Consultant ☐ Representative ☒ Tenant ☐

Address: 507 N NEW YORK AVE STE 300 WINTER PARK, FL 321 Telephone: C/O 305-673-2585

Email Address: C/O tslavens@lsnlaw.com

Email Address #2: _____

Date of Purchase: 2025 Is there an option to purchase the Property? Yes ☒ No ☐

If Yes, Attach Copy of the Contract.

Noticing Agent (FTAC & Board submissions only) : Diana Rio

E-mail Address: diana@rdmiami.com

CERTIFICATION OF COMPLIANCE WITH APPLICABLE REGULATIONS

The applicant/owner(s) signature certifies that he/she has been made aware of the criteria, regulations and guidelines applicable to the request. This information can be obtained in Room 315 of City Hall or on our website at www.hollywoodfl.org. The owner(s) further certifies that when required by applicable law, including but not limited to the City's Zoning and Land Development Regulations, they will post the site with a sign provided by the Office of Planning and Development Services. The owner(s) will photograph the sign the day of posting and submit photographs to the Office of Planning and Development Services as required by applicable law. Failure to post the sign will result in violation of State and Municipal Notification Requirements and Laws.

(I)(We) certify that (I) (we) understand and will comply with the provisions and regulations of the City's Zoning and Land Development Regulations, Design Guidelines, Design Guidelines for Historic Properties and City's Comprehensive Plan as they apply to this project. (I)(We) further certify that the above statements and drawings made on any paper or plans submitted herewith are true to the best of (my)(our) knowledge. (I)(We) understand that the application and attachments become part of the official public records of the City and are not returnable.

Signature of Current Owner: Marshall Nevins Date: 6-10-25

PRINT NAME: Marshall Nevins Date: _____
 Not in his personal capacity but solely as Managing Director and Vice President of
 BRE ROOK SH PRESIDENTIAL PLACE LLC

Signature of Consultant/Representative: _____ Date: _____
 On behalf of Elevation Properties, LLC and/or its assigns

PRINT NAME: _____ Date: _____

Signature of Tenant: N/A Date: _____

PRINT NAME: _____ Date: _____

Current Owner Power of Attorney

I am the current owner of the described real property and that I am aware of the nature and effect the request for Variance Approval to my property, which is hereby made by me or I am hereby authorizing Tracy Slavens Esq. and Kendrick Meek Jr. to be my legal representative before the Planning and Development (Board and/or Committee) relative to all matters concerning this application.

Sworn to and subscribed before me
 this 10 day of June

Yvonne Colon Gonzalez
 Notary Public
 State of New York

YVONNE COLON GONZALEZ
 NOTARY PUBLIC, STATE OF NEW YORK
 Registration No. 01G06284160
 Qualified in Suffolk County
 Commission Expires June 17, 2029

Marshall Nevins
 Signature of Current Owner

Marshall Nevins
 Print Name Not in his personal capacity but solely as
 Managing Director and Vice President of
 BRE ROOK SH PRESIDENTIAL PLACE LL

My Commission Expires 6/17/29 (Check One) ☒ Personally known to me; OR ☐ Produced Identification _____



GENERAL APPLICATION

CERTIFICATION OF COMPLIANCE WITH APPLICABLE REGULATIONS

The applicant/owner(s) signature certifies that he/she has been made aware of the criteria, regulations and guidelines applicable to the request. This information can be obtained in Room 315 of City Hall or on our website at www.hollywoodfl.org. The owner(s) further certifies that when required by applicable law, including but not limited to the City's Zoning and Land Development Regulations, they will post the site with a sign provided by the Office of Planning and Development Services. The owner(s) will photograph the sign the day of posting and submit photographs to the Office of Planning and Development Services as required by applicable law. Failure to post the sign will result in violation of State and Municipal Notification Requirements and Laws.

(I)(We) certify that (I) (we) understand and will comply with the provisions and regulations of the City's Zoning and Land Development Regulations, Design Guidelines, Design Guidelines for Historic Properties and City's Comprehensive Plan as they apply to this project. (I)(We) further certify that the above statements and drawings made on any paper or plans submitted herewith are true to the best of (my)(our) knowledge. (I)(We) understand that the application and attachments become part of the official public records of the City and are not returnable.

Signature of Current Owner: _____ Date: _____

PRINT NAME: _____ Date: _____

On behalf of BRE ROOK SH PRESIDENTIAL PLACE LLC

Signature of Consultant/Representative: T. Chris King Date: 6/10/25
On behalf of Elevation Properties, LLC and/or its assigns

PRINT NAME: T. Chris King Date: 6/10/25

Signature of Tenant: N/A Date: _____

PRINT NAME: _____ Date: _____

Current Owner Power of Attorney

I am the current owner of the described real property and that I am aware of the nature and effect the request for **Variance Approval** to my property, which is hereby made by me or I am hereby authorizing Tracy Slavens Esq. and Kendrick Meek Jr. to be my legal representative before the Planning and Development (Board and/or Committee) relative to all matters concerning this application.

Sworn to and subscribed before me
this _____ day of _____

Signature of Current Owner

Notary Public
State of Florida

Print Name

My Commission Expires: _____ (Check One) ☐ Personally known to me; OR ☐ Produced Identification _____

October 23, 2025

VIA ELECTRONIC DELIVERY

Ms. Andria Wingett
Director, Development Services
City of Hollywood Development Services
2600 Hollywood Boulevard, 2nd Floor
Hollywood, Florida 33020

**Re: Elevation Properties, LLC - Amended Letter of Intent for Variance Application
3880 South Circle Drive (Property ID: 514207023710)**

Dear Ms. Wingett:

Please accept this Letter of Intent on behalf of Elevation Properties, LLC, and/or assigns (the “Applicant”), in support of this application for Variance approval (the “Application”), in connection that certain ± 3.2 acre ($\pm 139,431$ square foot) parcel of land located at 3880 South Circle Drive, Hollywood, Florida, and further identified by Property ID No. 514207023710 (the “Property”), as shown below in red.



The Applicant seeks approval of the following non-use variances from the City of Hollywood Code (the “Code”) in connection with the conversion of the Property’s use from an Assisted Living Facility to Age-Restricted Senior Housing (+55), as follows:

1. **“Off-Street Parking Variance”** – A variance from Section 7.2. of the Code to permit 69 parking spaces where 177 parking spaces are required.
2. **“Direct Access Parking Variance”** – A variance from Section 7.1.C. of the Code to permit head-in and back-out parking spaces with direct access to a public street.
3. **“Rainbow Drive Curb Cut Variance”** – A variance from Section 155.08.E. of the Code to permit a curb cut on South Rainbow Drive that exceeds the maximum width permitted by the Code.
4. **“Crescent Drive Curb Cut Variance”** – A variance from Section 155.08.E. of the Code to permit a curb cut on South Crescent Drive that exceeds the maximum width permitted by the Code.
5. **“Rainbow Drive Setback Variance”** – A variance from Section 4.22.I. of the Code to permit a curb cut along South Rainbow Drive that traverses the required 10-foot setback for at-grade parking lots.
6. **“Crescent Drive Setback Variance”** – A variance from Section 4.22.I. of the Code to permit a curb cut along South Crescent Drive that traverses the required 10-foot setback for at-grade parking lots.
7. **“Minimum Unit Size Variance”** – A variance from Section 4.2.B. of the Code to permit a minimum unit size of 385 square feet, where a minimum of 500 square feet is required.
8. **“Average Unit Size Variance”** – A variance from Section 4.2.B. of the Code to permit an average unit size of approximately 482 square feet, where an average of 750 square feet is required.

This Letter of Intent is hereby amended to include an increased parking count, updated density calculations, and additional variance requests.

1. PROJECT INFORMATION

The Property is currently developed with a 3-story, 104 unit structure that is being operated as an assisted living and memory care facility (“ALF”) with 118 beds. Due to economic factors, the Property is being sold. The Applicant is under contract to purchase the Property and proposes to convert the building’s existing 104 units from ALF to Age-Restricted Senior Housing (+55) as part of an adaptive reuse of the building (the “Project”). Specifically, the Applicant proposes to renovate the building and upgrade the units, including the addition of kitchen facilities in each unit. The project will maintain the current density of 104 units.

At least 12 of the units will be committed to rents not to exceed 120% of area median income (“AMI”). The 120% AMI is defined as “moderate-income” in Policy 2.16.3 of the Broward

County Land Use Plan. In connection with rent restricted set-aside units, the Applicant will be eligible for the Affordable Housing Density Bonus pursuant to Policy 2.16.3, which it will utilize to meet the maximum allowable density for the Property. In accordance with the Affordable Housing Density Bonus, the Project will avail itself of 72 bonus units, which is consistent with the Broward County Land Use Plan.

2. FUTURE LAND USE / COMPREHENSIVE PLAN

The City of Hollywood Planning Department has determined that the Project's density calculations be based on the Broward County Land Use Plan, rather than the City of Hollywood Comprehensive Plan and Future Land Use Map. According to the Broward County Land Use Plan Map, the Property is designated Low-Medium (10) Residential. This designation permits dwelling units and related uses within residential land use categories, with a maximum allowable density of ten (10) dwelling units per acre. For reference, the City's Comprehensive Plan Land Use designation for the Property is Medium (16) Residential, which provides for a maximum density of 16 units per acre.

As noted above, Policy 2.16.3 of the Broward County Land Use Plan provides six (6) bonus units of additional density for every unit rent capped at 120% of AMI. The Applicant proposes to cap twelve (12) units within the Project at 120% AMI rents for thirty (30) years, thereby qualifying for up to seventy-two (72) bonus units. The Project includes a total of one hundred and four (104) residential units, and, while the Low-Medium (10) Residential land use category permits thirty-two (32) units by right, the set-aside of twelve (12) affordable units will yield an additional seventy-two (72) bonus units through the Affordable Housing Density Bonus program. Through this program, the Project is consistent with the maximum allowable density. It should be noted that, in accordance with Policy 2.16.3(7) of the Land Use Plan, the City of Hollywood must approve the requested bonus density at a publicly noticed meeting, which we hereby request in connection with the Project.

3. ZONING

Notwithstanding the Comprehensive Plan Land Use designations, the Property falls within two zoning districts: Low-Medium Density Multiple Family (RM-9) and Medium-High Density Multiple Family (RM-18). RM-9 Districts allow a maximum density of nine (9) units per acre and permit single-family homes, duplexes, townhouses, and apartment buildings. RM-18 Districts allow a maximum density of eighteen (18) units per acre and permit single-family homes, duplexes, townhouses, and apartment buildings.

The Property is surrounded by multi-family uses to the east and south, single-family uses to the west and south, and office uses to the north. The adaptive reuse of the building from an ALF to senior housing will be significantly more compatible with the pattern of residential development in the area. Additionally, the City is in need of quality housing for its senior residents. The

Project will help to satisfy the demand for housing and appropriately compliment the uses in the neighborhood.

4. REQUESTED VARIANCES

The Applicant is requesting approval of eight variances in connection with the Project. Collectively, these variances will enable the adaptive reuse of the existing structure, ensure adequate and functional on-site parking, and legalize the Property's existing improvements.

a. Section 7.2. – Off-Street Parking Requirements Variance

The Applicant is requesting a variance from Section 7.2. of the Code, which requires 1.5 parking spaces per unit for residents and 1 designated guest parking space for every 5 units for developments exceeding 10 units. Based on this standard, the Project would require 156 resident parking spaces and 21 guest parking spaces—to serve approximately 104 units. The Project proposes a total of 69 parking spaces—comprising 47 existing spaces and 22 new spaces.

The proposed parking supply equates to a ratio of approximately 0.66 spaces per unit, which accurately reflects the anticipated parking demand for a senior housing community. Experience from comparable senior housing developments operated by the Applicant demonstrates that the majority of residents either do not own vehicles or own only one, and that parking demand is driven primarily by short-term visitor parking rather than long-term resident needs. Enclosed with this application is a parking analysis of the Applicant's comparable projects, showing that parking ratios below 1.0 space per unit are typical and sufficient to meet operational needs.

In addition, a parking and trip generation methodology prepared by Kimley-Horn and Associates, Inc. supports the conclusion that the adaptive reuse of the property for senior housing will result in a net reduction of PM Peak Hour trips and that the proposed parking ratio will adequately serve the development. A tree evaluation prepared by CPH for the proposed parking expansion area is also included and confirms that the limited expansion will preserve as many existing mature trees as possible.

This variance request satisfies the criteria for approval as set forth in Section 5.3.F. of the Code, as follows:

1. *That the requested Variance maintains the basic intent and purpose of the subject regulations, particularly as it affects the stability and appearance of the city;*

The purpose of the parking regulations is to ensure that adequate parking is provided without creating congestion or negatively impacting the surrounding neighborhood. The requested variance meets that intent by providing an appropriate number of spaces based on the real-world parking characteristics of senior housing residents. The design will utilize existing parking areas and modest new expansions that ensure compatibility with the surrounding residential neighborhood and preserve mature trees wherever possible.

2. *That the requested Variance is otherwise compatible with the surrounding land uses and would not be detrimental to the community;*

The Project is located in an area characterized by similar residential and institutional uses. The reduced parking demand associated with seniors ensures that the project will not burden nearby streets or neighboring properties with overflow parking. Furthermore, the project's emphasis on accessibility, walkability, and community-oriented amenities will enhance the neighborhood fabric and not introduce any detrimental impacts to the community.

3. *That the requested Variance is consistent with and in furtherance of the Goals, Objectives and Policies of the adopted Comprehensive Plan, as amended from time to time, the applicable Neighborhood Plan and all other similar plans adopted by the city; and*

The City's Comprehensive Plan encourages redevelopment and adaptive reuse of existing buildings, the creation of diverse housing options for seniors, and sustainable design practices that minimize environmental impacts. The proposed variance supports these policies by enabling the development of affordable, accessible senior housing while preserving green space and keeping impervious surface areas to a minimum.

4. *That the need for the requested Variance is not economically based or self-imposed.*

The variance requested reflects the operational requirements of senior housing and aligns with empirical data from comparable facilities, ensuring an efficient and context-appropriate design.

b. Section 7.1.C. – Direct Access Parking Variance

The Applicant respectfully requests a variance from Section 7.1.C. of the Code to permit the construction of parking spaces with direct access to a public street. The proposed Project will retain the 47 existing parking spaces located on the north side of the property and add 9 new spaces along Crescent Drive and 14 new spaces along Rainbow Drive.

After evaluating multiple design alternatives to increase on-site parking capacity, the Applicant, in coordination with City staff, determined that the proposed configuration of back-out and head-in parking along Crescent and Rainbow Drives offers the most practical and balanced solution. This design increases accessibility for residents and visitors while minimizing site disturbance and preserving mature trees and open space.

This variance request satisfies the criteria for approval as set forth in Section 5.3.F. of the Code, as follows:

1. *That the requested Variance maintains the basic intent and purpose of the subject regulations, particularly as it affects the stability and appearance of the city;*

The intent of Section 7.1.C. is to promote safe, functional, and aesthetically pleasing parking arrangements that support neighborhood stability. The proposed design achieves these objectives by providing clearly defined parking areas and safe pedestrian crossings. In addition, this variance maintains the visual quality and character of the surrounding neighborhood by preserving mature tree canopy and greenspace elsewhere on site.

2. *That the requested Variance is otherwise compatible with the surrounding land uses and would not be detrimental to the community;*

The proposed Project is residential in nature and compatible with the surrounding neighborhood. The addition of small, direct-access parking lots will not create congestion or adversely affect adjacent properties.

3. *That the requested Variance is consistent with and in furtherance of the Goals, Objectives and Policies of the adopted Comprehensive Plan, as amended from time to time, the applicable Neighborhood Plan and all other similar plans adopted by the city; and*

The variance is compatible with surrounding land uses, will not create adverse traffic impacts, and supports the Comprehensive Plan's goals of adaptive reuse, accessibility, and environmental preservation.

4. *That the need for the requested Variance is not economically based or self-imposed.*

The need for this variance is driven by site-specific conditions, as well as public safety considerations identified in consultation with City staff.

c. Section 155.08.E. – Rainbow Drive Curb Cut Variance

The Applicant respectfully requests a variance from Section 155.08.E. of the Code, which requires curb cuts to account for no more than 30 percent of the lot frontage with a maximum 30 foot width. The Project proposes a curb cut along South Rainbow Drive measuring 202.84 feet in width, exceeding the maximum permitted by the Code.

Due to the Property's existing grade conditions, and the functional need for safe vehicular access, strict compliance with Section 155.08.E. would create a practical difficulty. The proposed curb cut is designed to provide adequate turning radii while minimizing impacts to the sidewalk and streetscape. Granting this variance will not be detrimental to the surrounding neighborhood and represents the minimum relief necessary to ensure safe and efficient site access.

This variance request satisfies the criteria for variance approval under Section 155.08.H.2. of the Code, as follows:

1. *That special conditions and circumstances exist which are peculiar to the land involved and which are not applicable to other lands.*

Due to the site's layout, grade, and circulation constraints, a slightly wider curb cut along Rainbow Drive is necessary to accommodate safe turning movements for residents and guests of the proposed Project. The proposed design represents the minimum width necessary to ensure safety and functionality.

2. *That a literal interpretation of the conditions set forth in this section would deprive the applicant of rights commonly enjoyed by other properties;*

Strict application of Section 155.08.E. would deprive the Applicant of safe and functional access to their Property commonly available to other similarly situated multi-family uses.

3. *That the special conditions and circumstances do not result from actions of the applicant; and*

The hardship does not result from actions of the Applicant but from the lot's configuration and the City's request to maximize off-street parking without triggering additional regulatory thresholds.

4. *That the granting of the variances requested will not confer on the applicant any special privilege that is denied by this section to other lands. No pre-existing conditions on neighboring land which are contrary to this section shall be considered grounds for the issuance of a variance.*

Granting this variance will not confer any special privilege unavailable to other properties, but rather ensures a safe and practical design consistent with the intent of Section 155.08.E.

d. Section 155.08.E. – Crescent Drive Curb Cut Variance

The Applicant respectfully requests a variance from Section 155.08.E. of the Code, which requires curb cuts to account for no more than 30 percent of the lot frontage with a maximum 30 foot width. The Project proposes a curb cut along South Crescent Drive measuring 117.76 feet in width, exceeding the maximum allowable width permitted by the Code.

Due to the Property's existing grade conditions and the need to provide safe and functional vehicular access to the proposed 9-space parking lot along Crescent Drive, strict application of Section 155.08.E. would present a practical difficulty. While the regulation is intended to enhance pedestrian safety, preserve on-street parking, and maintain the residential character of the streetscape, the proposed curb cut has been carefully designed to ensure adequate turning radii and safe ingress and egress with minimal impact on the sidewalk and surrounding streetscape.

This variance request satisfies the criteria for variance approval under Section 155.08.H.2. of the Code, as follows:

1. *That special conditions and circumstances exist which are peculiar to the land involved and which are not applicable to other lands.*

Due to the site's layout, grade, and circulation constraints, a slightly wider curb cut along Crescent Drive is necessary to accommodate safe turning movements for residents and guests of the proposed Project. The proposed design represents the minimum width necessary to ensure safety and functionality.

2. *That a literal interpretation of the conditions set forth in this section would deprive the applicant of rights commonly enjoyed by other properties;*

Strict application of Section 155.08.E. would deprive the Applicant of safe and functional access to their Property commonly available to other similarly situated multi-family uses.

3. *That the special conditions and circumstances do not result from actions of the applicant; and*

The curb cut's design was developed in close coordination with City staff to balance functional access with traffic safety and streetscape aesthetics.

4. *That the granting of the variances requested will not confer on the applicant any special privilege that is denied by this section to other lands. No pre-existing conditions on neighboring land which are contrary to this section shall be considered grounds for the issuance of a variance.*

Approval of this variance request will not confer any special privilege, but rather allow the Property to be used in a safe and practical manner, consistent with the intent of the Code.

e. Section 4.22.I. – Rainbow Drive Setback Variance

The Applicant requests a variance from Section 4.22.I. of the Code to allow a curb cut along Rainbow Drive that traverses the 10-foot setback for at-grade parking. Strict compliance with the setback requirement would prevent safe and functional access to the proposed parking area along Rainbow Drive, creating a practical difficulty for the Applicant and future residents of the Project.

The proposed curb cut is designed to minimize impacts within the setback while maintaining adequate turning radii and safe ingress and egress. Granting this variance preserves the intent of Section 4.22.I. by allowing safe, efficient access to the parking area without compromising the visual character of the streetscape, and ensures the Project remains compatible with surrounding land uses.

This variance satisfies the criteria for approval as set forth in Section 5.3.F. of the Code, as

follows:

1. *That the requested Variance maintains the basic intent and purpose of the subject regulations, particularly as it affects the stability and appearance of the city;*

Section 4.22.I. requires a 10-foot setback for at-grade parking lots over 150 feet to preserve a consistent streetscape and provide space for safe pedestrian and vehicular transitions. The proposed curb cut is necessary to provide safe and functional access to the proposed parking lot along Rainbow Drive. The design minimizes impacts within the setback while allowing adequate turning radii for residents, visitors, and emergency vehicles.

2. *That the requested Variance is otherwise compatible with the surrounding land uses and would not be detrimental to the community;*

The requested variance ensures safe ingress and egress without compromising the neighborhood's character. The curb cut is limited in width and positioned to maintain streetscape continuity and compatibility with the surrounding community.

3. *That the requested Variance is consistent with and in furtherance of the Goals, Objectives and Policies of the adopted Comprehensive Plan, as amended from time to time, the applicable Neighborhood Plan and all other similar plans adopted by the city; and*

This variance is consistent with the Goals, Objectives, and Policies of the Comprehensive Plan and applicable neighborhood plans by supporting safe, accessible, and functional development while respecting the visual and operational intent of the setback requirement.

4. *That the need for the requested Variance is not economically based or self-imposed.*

The need for this variance arises from the need for additional parking for the Project. Along with site-specific conditions, including the elongated lot frontage and circulation requirements for the proposed parking lot along Rainbow Drive.

f. Section 4.22.I. – Crescent Drive Setback Variance

The Applicant requests a variance from Section 4.22.I. of the Code to allow a curb cut along South Crescent Drive that traverses the required 10-foot setback for at-grade parking. Strict compliance with the setback requirement would prevent safe and functional access to the parking area along Crescent Drive, creating a practical difficulty for the Applicant and future residents of the Project.

The proposed curb cut is designed to minimize impacts within the setback while maintaining adequate turning radii as well as safe ingress and egress. Granting this variance preserves the intent of Section 4.22.I. by allowing safe, efficient access without compromising the visual

character of the streetscape, and ensures the project remains compatible with surrounding land uses.

This variance request satisfies the criteria for approval as set forth in Section 5.3.F. of the Code, as follows:

1. *That the requested Variance maintains the basic intent and purpose of the subject regulations, particularly as it affects the stability and appearance of the city;*

Section 4.22.I. requires a 10-foot setback for at-grade parking lots over 150 feet to preserve a consistent streetscape and provide space for safe pedestrian and vehicular transitions. The proposed curb cut is necessary to provide safe and functional access to the proposed parking lot along Crescent Drive.

2. *That the requested Variance is otherwise compatible with the surrounding land uses and would not be detrimental to the community;*

The requested variance ensures safe ingress and egress without compromising the neighborhood's character. The curb cut is designed to maintain streetscape continuity and compatibility with surrounding residential and institutional uses.

3. *That the requested Variance is consistent with and in furtherance of the Goals, Objectives and Policies of the adopted Comprehensive Plan, as amended from time to time, the applicable Neighborhood Plan and all other similar plans adopted by the city; and*

This variance is consistent with the Goals, Objectives, and Policies of the Comprehensive Plan and applicable neighborhood plans by supporting safe, accessible, and functional development while respecting the visual and operational intent of the setback requirement.

4. *That the need for the requested Variance is not economically based or self-imposed.*

The need for this variance arises from site-specific conditions, including the need for additional parking and the preservation of mature trees on site.

g. Section 4.2.B. – Minimum Unit Size Variance

The Applicant respectfully requests a variance from Section 4.2.B. of the Code, which requires that apartments within the RM-9 and RM-18 zoning districts have a minimum unit size of 500 square feet. The proposed Project will adaptively reuse the existing building to create a senior housing community while maintaining the current unit count and configuration of studios, one-bedroom, and two-bedroom units. As such, the Project's units have a minimum size of approximately 385 square feet, consistent with the building's existing conditions.

This variance request satisfies the criteria for approval as set forth in Section 5.3.F. of the Code,

as follows:

1. *That the requested Variance maintains the basic intent and purpose of the subject regulations, particularly as it affects the stability and appearance of the city;*

The intent of the City's minimum unit size standards is to ensure that residential units provide safe, livable, and functional living spaces that contribute to community stability. The existing units meet modern standards of livability and will be fully renovated to include updated finishes, accessibility features, and energy-efficient systems.

2. *That the requested Variance is otherwise compatible with the surrounding land uses and would not be detrimental to the community;*

The Property is surrounded by a mix of residential and institutional uses, and the adaptive reuse will remain compatible with the established character of the area. The proposed senior housing will generate less traffic, noise, and parking demand than a standard multifamily use, resulting in fewer external impacts.

3. *That the requested Variance is consistent with and in furtherance of the Goals, Objectives and Policies of the adopted Comprehensive Plan, as amended from time to time, the applicable Neighborhood Plan and all other similar plans adopted by the city; and*

The City's Comprehensive Plan promotes adaptive reuse, sustainable redevelopment, and the diversification of housing opportunities to meet the needs of residents of all ages and income levels. The proposed variance directly supports these goals by allowing the efficient reuse of an existing structure to deliver attainable, age-appropriate housing for seniors.

4. *That the need for the requested Variance is not economically based or self-imposed.*

The need for this variance stems from the existing building's structural layout and floor plan. This request is motivated by Applicant's intent to provide an affordable senior housing option within the physical constraints of the existing building.

h. Section 4.2.B. – Average Unit Size Variance

The Applicant respectfully requests a variance from Section 4.2.B. of the Code, which requires that apartments within the RM-9 and RM-18 zoning districts have an average unit size of 750 square feet. This variance is necessary to facilitate the adaptive reuse of the structure, originally designed and constructed under previous code requirements. As such, the Project's units have an average unit size of approximately 482 square feet, consistent with the building's existing conditions. Granting this variance will enable the conversion of this Property into a safe, functional, and much-needed housing option for seniors.

Enclosed with this application is a memorandum prepared by CPH, confirming that the existing

walls within the building meet 1-hour fire rating requirements.

This variance request satisfies the criteria for approval as set forth in Section 5.3.F. of the Code, as follows:

1. *That the requested Variance maintains the basic intent and purpose of the subject regulations, particularly as it affects the stability and appearance of the city;*

The intent of the City's average unit size standards is to ensure that residential units provide safe, livable, and functional living spaces that contribute to community stability. The existing units meet modern standards of livability and will be fully renovated to include updated finishes, accessibility features, and energy-efficient systems.

2. *That the requested Variance is otherwise compatible with the surrounding land uses and would not be detrimental to the community;*

The Property is surrounded by a mix of residential and institutional uses, and the adaptive reuse will remain compatible with the established character of the area. The proposed Project will generate less traffic, noise, and parking demand than a standard multifamily use, resulting in fewer external impacts.

3. *That the requested Variance is consistent with and in furtherance of the Goals, Objectives and Policies of the adopted Comprehensive Plan, as amended from time to time, the applicable Neighborhood Plan and all other similar plans adopted by the city; and*

The City's Comprehensive Plan promotes adaptive reuse, sustainable redevelopment, and the diversification of housing opportunities to meet the needs of residents of all ages and income levels. The proposed variance directly supports these goals by allowing the efficient reuse of an existing structure to deliver attainable, age-appropriate housing for seniors.

4. *That the need for the requested Variance is not economically based or self-imposed.*

The need for this variance stems from the existing building's structural layout and floor plan. This request is motivated by Applicant's intent to provide an affordable senior housing option within the physical constraints of the existing building.

5. CONCLUSION

The Applicant aims to optimize the Property's use by retaining and renovating the existing building. The proposed Project will enhance existing amenities and provide more quality housing for Hollywood's senior residents. The approval of the Affordable Housing Density Bonus units and the requested variances meet all applicable criteria for approval and will in no way negatively impact public health, safety, or welfare.

Based on the above, we respectfully seek your favorable review and recommendation of approval for the Application. If you have any questions or require additional information, please feel free to contact me directly at tslavens@lsnlaw.com or (305) 673-2585.

Respectfully submitted,

LSN Law, P.A.

A handwritten signature in blue ink that reads "Tracy Slavens". The signature is fluid and cursive, with the first name "Tracy" and last name "Slavens" clearly legible.

Tracy R. Slavens, Esq.

*Revision Narrative – DRC Approval
Presidential Place
3880 S Circle Dr
City of Hollywood, Florida 33021*



10/17/2025

Project Description

Presidential Place

Parcel No. 514207023710

Address: 3880 S Circle Dr, City of Hollywood, Florida 33021

Proposed Expansion Of Parking Areas Adjacent To S. Crescent Drive And S. Rainbow Drive. Site Improvements Include 22 Proposed Parking Spaces And One (1) Ada Parking In The Existing Parking Area. Site Will Add An Additional Amount Of Impervious Area And Reduce The Existing Amount Of Pervious Area. The Site Falls Under The Jurisdiction Of City Of Hollywood, Florida.

Revision Narrative/Summary

Revisions have been made throughout the Site Dimension Plan (Sheet C03.0) to address design updates and coordination items. An ADA space was added on the plan ((1)) to meet accessibility requirements, and the total site parking proposed ((2)) has been updated accordingly. The existing site parking ((3)) has also been incorporated into the Site Data Table to ensure accuracy between existing and proposed conditions. The proposed crosswalk was changed to a continuous sidewalk ((4)) to enhance pedestrian connectivity within the site. Additionally, the curb cut measurements ((5)) were verified and are now dimensioned from the property lines in compliance with City standards. The proposed parking geometrics ((6)) have been added to the Site Data Table, and the widths of swales and sidewalks ((7)) have been dimensioned from the property line for clarity and compliance. The curbing detail ((8)) was revised from Type “F” to Type “D” to align with current design standards and field conditions. Furthermore, parking requirements ((9)) have been updated to reflect the revised parking counts, and site visibility triangles ((10)) were added at all access points to ensure proper sight distance. The pavement type callouts ((11)) have been identified on the plan, distinguishing between standard and pervious surfaces, and the FPL easement ((12)) has been labeled to ensure coordination with utility providers and existing easement limits.



October 7, 2025

Tracy R. Slavens, Esquire
LSN Law, P.A.
3800 Northeast 1 Avenue, Suite 200
Miami, Florida 33137

Via Email Only

Dear Ms. Slavens:

Re: Platting requirements for a parcel legally described as Lots 11-30, Block 69, "Hollywood Hills," according to the Plat thereof, as recorded in Plat Book 6, Page 22, of the Public Records of Broward County, Florida; together with a portion of the vacated 15 foot alley adjacent to said Lots; less a portion of Lot 30 for right-of-way purposes. This parcel is generally located on the south side of South Circle Drive, between South Rainbow Drive and South Crescent Drive, in the City of Hollywood.

This letter is in response to your correspondence regarding the Broward County Land Use Plan's platting requirements for the above referenced parcel.

You have indicated the current proposal is for an adaptive reuse and renovation of both the interior and exterior of the existing multi-use residential development and that construction of a new principal structure is not contemplated at this time.

Planning Council staff has determined that replatting **would not be required** by Policy 2.13.1 of the Broward County Land Use Plan (BCLUP) for the proposed development, since the proposed renovations would not constitute a "principal building," pursuant to the guidelines of the *Administrative Rules Document: BrowardNext*.

Further, Planning Council staff has determined that replatting **would not be required** by Policy 2.13.1 of the BCLUP for the proposed development. As per the criteria of Policy 2.13.1, replatting is required for the issuance of building permits when constructing a non-residential or unified residential development, unless all of the following conditions are met:

- a. The lot or parcel is smaller than 10 acres and is unrelated to any adjacent development;
- b. A majority of the lot or parcel has been specifically delineated in a recorded plat;
- c. All land within the lot or parcel which is necessary to comply with the County Trafficways Plan has been conveyed to the public by deed or easement; and
- d. The proposed development is in compliance with the applicable land development regulations.

Tracy R. Slavens
October 7, 2025
Page Two

The subject parcel is less than 10 acres (approximately 3.3 acres) and meets the specifically delineated requirement. This platting interpretation is subject to the municipality finding that the proposed development is unrelated to any adjacent development, as noted in "a." above. It is noted that lands dedicated for right-of-way purposes do not negatively impact whether or not a subject property meets the specifically delineated requirement.

Planning Council staff notes that when a specifically delineated parcel (i.e. Lots 11-30) is combined with land which has been included in a plat recorded before June 4, 1953, but not specifically delineated or with vacated rights-of-way (i.e. a portion of the vacated 15 foot alley adjacent to said Lots), Policy 2.13.1 of the BCLUP does not require replatting if the specifically delineated portion of the parcel constitutes the majority of the enlarged parcel; in this case, the specifically delineated portion constitutes a majority of the enlarged parcel.

Some jurisdictions may be more restrictive and require platting in more situations than the BCLUP. The City of Hollywood's platting requirements should be investigated.

The contents of this letter are not a judgment as to whether this development proposal complies with State or local vehicular access provisions, the Broward County Trafficways Plan, permitted uses and densities, local zoning, the land development regulations of the municipality or the development review requirements of the BCLUP, including concurrency requirements.

If you have any additional questions concerning the BCLUP's platting requirements, please contact Huda Ashwas at your convenience.

Respectfully,



Barbara Blake Boy
Executive Director

BBB:HHA

cc/email: George R. Keller, Jr., CPPT, City Manager
City of Hollywood

Andria Wingett, Director, Development Services
City of Hollywood





CITY OF
SANFORD
OFFICE OF THE MAYOR &
CITY COMMISSION

Dear Community Leader,

I am writing to express my strong support for Elevation Financial Group's application to convert an underutilized senior property into much-needed housing in your community.

As the City Commissioner for District 4 in Sanford, Florida, I have had the opportunity to see firsthand the positive impact Elevation made when they completed a similar project in my district. When Elevation purchased the property, it had fallen into disrepair and out of use. Elevation invested millions of private dollars into the building, bringing it back to life as a beautiful and financially sustainable apartment option for seniors without any need for public financial support. The results were truly outstanding.

Our city provided Elevation with the necessary variances for land use and parking reduction to make this possible, and I am so glad that we did, as our flexibility made this stunning outcome a reality for the senior residents of my district.

Not only has the property been beautifully renovated, but it now serves as a welcoming and safe home for senior residents who benefit from a sense of independence and community. The development has enhanced the surrounding neighborhood and filled a much-needed gap in affordable senior housing options, a growing need in Central Florida.

Given the successful experience in my district, I am confident that Elevation will bring the same level of professionalism, care, and quality to this new project in your community, as well. Their proven track record in Sanford speaks for itself, and I fully support their efforts to replicate this success elsewhere. Thank you for your consideration. I trust that Elevation's thoughtful approach and demonstrated success will earn your favorable review, just as it did ours.

Sincerely,

CLAUDIA THOMAS

Commissioner, District 4

[City of Sanford](#)

Office: 407-562-2849

Website: claudia4sanford.com

Facebook: facebook.com/CommissionerClaudiaThomas

Instagram: www.instagram.com/claudiaforsanford

City Social Media:

facebook.com/SanfordFLCityGovt

youtube.com/CityofSanfordfl

Instagram.com/CityofSanfordfl



Art Woodruff
Mayor

Sheena Britton
District 1

Kerry S. Wiggins, Sr.
District 2

Patrick Austin
District 3

Claudia Thomas
District 4

Norton N. Bonaparte, Jr.
City Manager



401 South Park Avenue • Winter Park, Florida 32789

407-599-3234 • 407-599-3436 fax
cityofwinterpark.org

Office of
the **Mayor**

October 9, 2025

Dear Community Leaders,

As Mayor of the City of Winter Park, it is both an honor and a pleasure to lend my support for the application presented to you by **Elevation Financial Group**, a valued member of our business community and a shining example of corporate citizenship grounded in service, leadership, discipline, and kindness.

Elevation Financial Group has consistently demonstrated an extraordinary commitment to expanding and preserving housing for our nation's seniors and families. Their thoughtful approach to revitalization supports the long-term stability of our neighborhoods and aligns strongly with the values of quality, integrity, and community stewardship.

It has been a particular pleasure to work directly with the leadership team at Elevation on developments within the City of Winter Park. Their professionalism, collaborative spirit, and deep respect for our community's character and needs have been evident in every interaction. Their projects are marked by a clear dedication to doing well by doing good.

Beyond their real estate work, Elevation is a remarkable philanthropic force through the **Elevation Scholars**, which helps high-achieving students from underserved backgrounds pursue their college dreams. This initiative reflects the company's broader mission: to invest in people, unlock potential, and uplift communities.

Given Elevation's proven record of responsible development, community engagement, and good corporate citizenship, I am proud to **express my strong support for their application** before you. I believe their proposal reflects not only sound planning principles but also a genuine commitment to contributing positively to the housing needs of your community and beyond.

On behalf of the city and the residents of Winter Park, I hope you will give thoughtful consideration and ultimately approve their application.

Sincerely,

Sheila DeCiccio, Mayor
City of Winter Park



Town of Brewster

2198 Main Street
Brewster, MA 02631
(508) 896-3701
www.brewster-ma.gov

Office of the:
Town Manager

October 9, 2025

Dear Community Leaders,

I am writing to you in support of Elevation Financial Group's efforts to expand and preserve housing in your community. In 2020, Elevation purchased a former assisted living and skilled nursing building in our town, Brewster, Massachusetts. Though it had operated for many years, this business was no longer financially sustainable and was forced to close for economic reasons, the same reasons that likely place the building in your community in a similar situation.

Elevation approached the Town of Brewster about converting the property into age restricted senior apartments. The property was not zoned for this use, but the community was supportive of re-use of this building for housing. I am pleased to say that after understanding the vision and plan for the property and getting to know Elevation and its leadership team, our Town approved the necessary variances to convert this building into senior apartments. Five years later, we are so glad that we did.

Today, Serenity Apartments at Brewster is a reliable and stable living option for seniors in our community, providing 132 units of much needed housing and a stable source of tax revenue for our town, all while saving a building that was vacant and in need of major updates, turning it into a thriving and well-maintained living community.

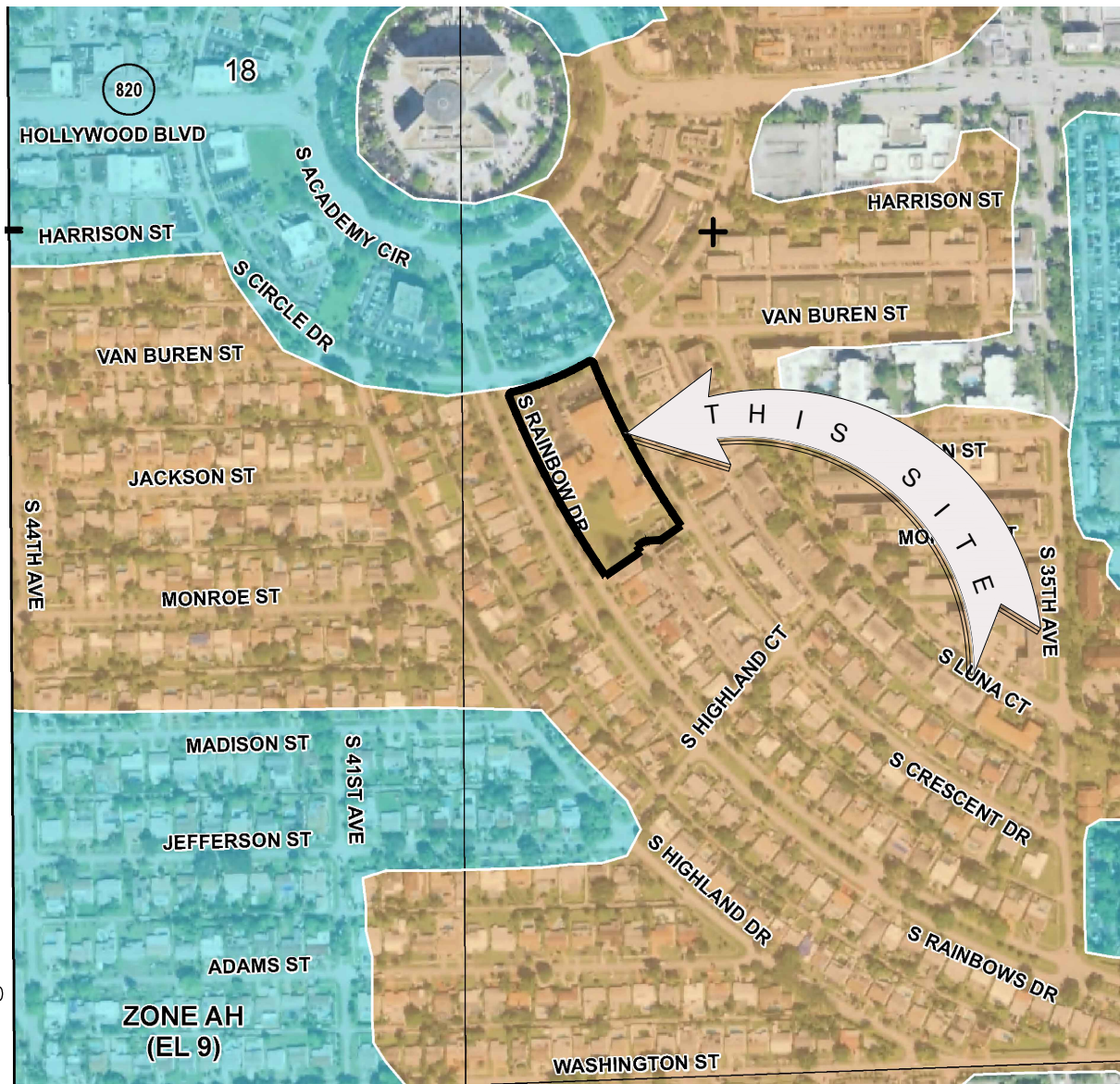
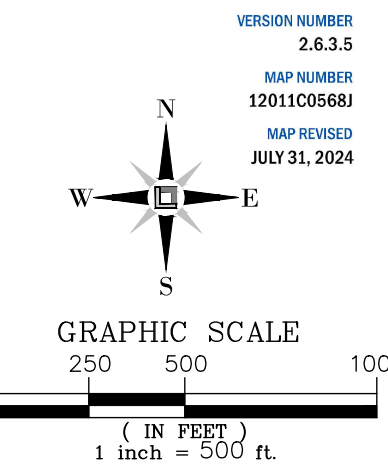
Recognizing this success, our town zoning board unanimously approved an expansion plan for this community earlier this year, permitting Elevation to building an additional 23 apartment homes for the property. After several years of stable operation, our town's zoning board members also recognized that the parking needs for seniors are much lower than traditional multifamily properties and unanimously approved permission for Elevation to construct these additional units without any additional parking requirements, saving precious green space in our town. The Town also worked with the local Rural Transit Authority to site a bus stop at this location, providing necessary transit for many of our seniors who do not have their own vehicle.

Overall, we have found Elevation to not only be a good corporate citizen for our town, but a committed partner in our efforts to expand and preserve affordable housing for the people who need it most. I hope you will seriously consider Elevation's proposal for your community. I can say from experience, you won't regret it.

Please feel free to reach out to me, should you have any questions at dkalinick@brewster-ma.gov.

Sincerely,

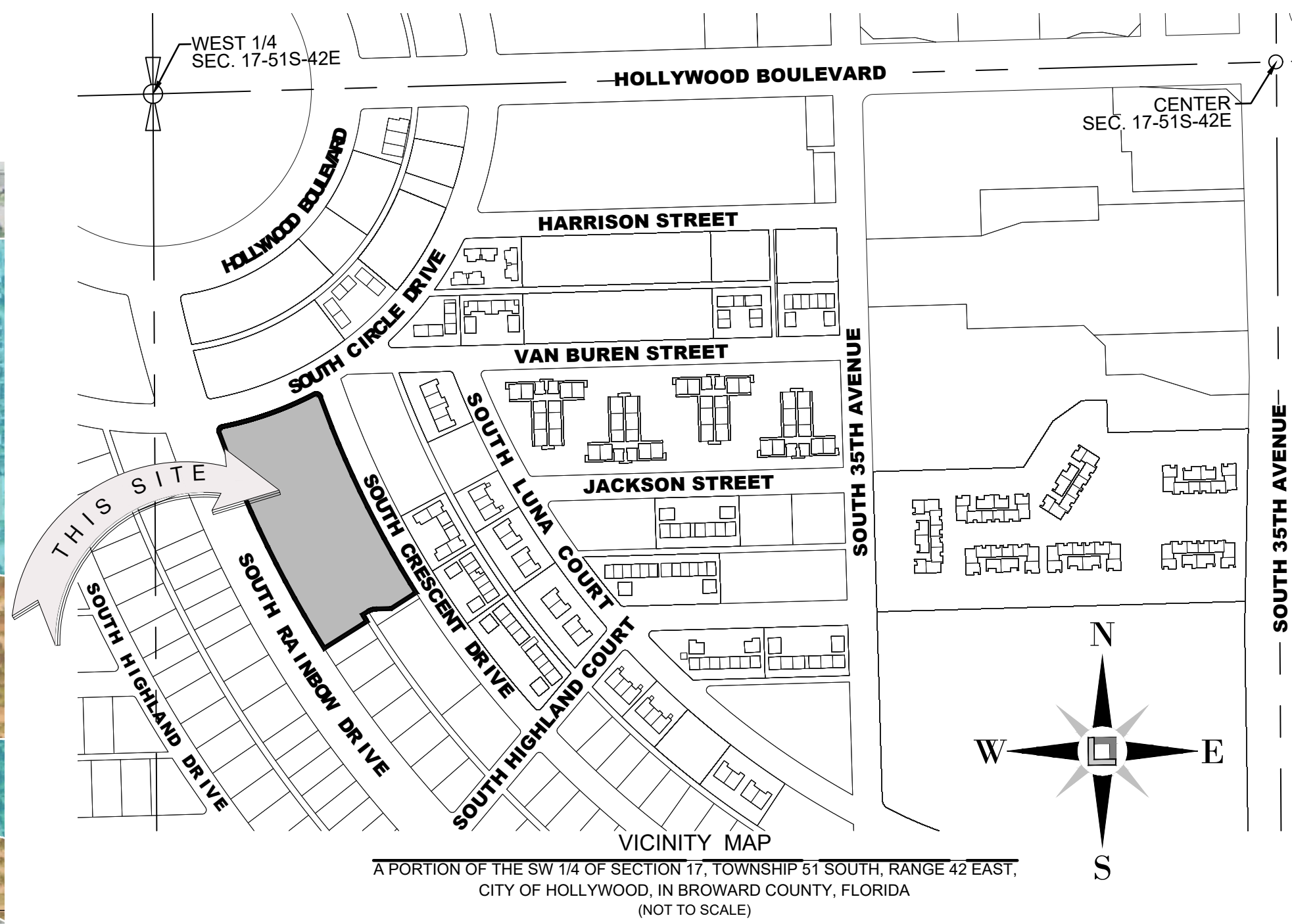
Donna J. Kalinick,
Assistant Town Manager



TREE TABLE						
Tree #	DBH (in.)	Common Name	Botanical Name	Height (ft.)	Spread (ft.)	Condition
1001	3/4/4	Alexander Palm	<i>Pythosperma elegans</i>	20		3
1002	3/3	Alexander Palm	<i>Pythosperma elegans</i>	20		3
1003	4/4	Alexander Palm	<i>Pythosperma elegans</i>	20		3
1004	19	Live Oak	<i>Quercus virginiana</i>	25	25	3
1005	24	Gumbo Limbo	<i>Bursera simaruba</i>	25	20	3
1006	18	Live Oak	<i>Quercus virginiana</i>	30	20	3
1007	27	Live Oak	<i>Quercus virginiana</i>	25	30	3
1008	4	Live Oak	<i>Quercus virginiana</i>	15	10	2
1009	6/6/5	Alexander Palm	<i>Pythosperma elegans</i>	8/8/8		3
1010	3	Live Oak	<i>Quercus virginiana</i>	15	12	2
1011	4/4/4	Alexander Palm	<i>Pythosperma elegans</i>	20/18/18		3
1012	6	Japanese Blueberry	<i>Elaeocarpus decipiens</i>	25	15	3
1013	4/4	Alexander Palm	<i>Pythosperma elegans</i>	18/18		3
1014	3/4	Alexander Palm	<i>Pythosperma elegans</i>	16/18		3
1015	3/3/5	Alexander Palm	<i>Pythosperma elegans</i>	20/20/20		3
1016	4/4	Alexander Palm	<i>Pythosperma elegans</i>	7/7		3
1017	5/5/5	Alexander Palm	<i>Pythosperma elegans</i>	16/15/18		3
1018	4/3/4	Alexander Palm	<i>Pythosperma elegans</i>	16/16/16		3
1019	7	Japanese Blueberry	<i>Elaeocarpus decipiens</i>	15	16	3
1020	4	Alexander Palm	<i>Pythosperma elegans</i>	6		3
1021	10	Japanese Blueberry	<i>Elaeocarpus decipiens</i>	20	20	3
1022	10/10	Japanese Blueberry	<i>Elaeocarpus decipiens</i>	25	20	3
1023	5/5/5	Alexander Palm	<i>Pythosperma elegans</i>	7/8/8		3
1024	9	Japanese Blueberry	<i>Elaeocarpus decipiens</i>	25	18	3
1025	8	Japanese Blueberry	<i>Elaeocarpus decipiens</i>	25	20	3
1026	4/5/5	Alexander Palm	<i>Pythosperma elegans</i>	7/8/8		3
1027	11	Gumbo Limbo	<i>Bursera simaruba</i>	20	20	3
1028	15	Gumbo Limbo	<i>Bursera simaruba</i>	20	20	3
1029	12	Gumbo Limbo	<i>Bursera simaruba</i>	20	20	3
1030	18	Gumbo Limbo	<i>Bursera simaruba</i>	20	20	3
1031	5/5	Alexander Palm	<i>Pythosperma elegans</i>	10/10		3
1032	14	Gumbo Limbo	<i>Bursera simaruba</i>	18	25	3
1033	12	Gumbo Limbo	<i>Bursera simaruba</i>	18	20	3
1034	3/3	Alexander Palm	<i>Pythosperma elegans</i>	25/25		3
1035	3/4/4	Alexander Palm	<i>Pythosperma elegans</i>	20/20/22		3
1036	3/4	Alexander Palm	<i>Pythosperma elegans</i>	20/20		3
1037	18	Live Oak	<i>Quercus virginiana</i>	30	28	3
1038	17	Live Oak	<i>Quercus virginiana</i>	25	20	3
1039	5	Geigertree	<i>Cordia sebestena</i>	12	10	3
1040	6	Geigertree	<i>Cordia sebestena</i>	15	10	3
1041	5	Geigertree	<i>Cordia sebestena</i>	15	10	3
1042	5	Geigertree	<i>Cordia sebestena</i>	15	10	3
1043	5	Geigertree	<i>Cordia sebestena</i>	20	10	3
1044	15	Mahogany	<i>Swietenia mahagoni</i>	30	25	3
1045	3	Live Oak	<i>Quercus virginiana</i>	15	6	3
1046	2	Live Oak	<i>Quercus virginiana</i>	15	6	3
1047	2	Live Oak	<i>Quercus virginiana</i>	15	6	3
1048	7	Mahogany	<i>Swietenia mahagoni</i>			6
1049	2	Live Oak	<i>Quercus virginiana</i>	15	6	3
1050	2	Live Oak	<i>Quercus virginiana</i>	15	6	3
1051	2	Live Oak	<i>Quercus virginiana</i>	15	6	3
1052	11	Carrotwood	<i>Cupanopsis anacardioides</i>	25	15	3
1053	10	Live Oak	<i>Quercus virginiana</i>	35	12	3
1054	5	Alexander Palm	<i>Pythosperma elegans</i>	10		3
1055	6/6/6/6	Umbrella Tree	<i>Schefflera actinophyllum</i>	25	20	3
1056	15	Gumbo Limbo	<i>Bursera simaruba</i>	30	30	3
1057	25	Gumbo Limbo	<i>Bursera simaruba</i>	32	20	3
1058	10	Carrotwood	<i>Cupanopsis anacardioides</i>	20	12	3
1059	3	Gumbo Limbo	<i>Bursera simaruba</i>	12	2	4
1060	14	Gumbo Limbo	<i>Bursera simaruba</i>	25	25	3

ALTA/NSPS LAND TITLE SURVEY
PRESIDENTIAL PLACE SENIOR HOUSING

A PORTION OF THE SOUTHWEST 1/4 OF SECTION 17, TOWNSHIP 51 SOUTH, RANGE 42 EAST,
CITY OF HOLLYWOOD, IN BROWARD COUNTY, FLORIDA.



TREE TABLE (CONTINUED)						
Tree #	DBH (in.)	Common Name	Botanical Name	Height (ft.)	Spread (ft.)	Condition
1061	14	Gumbo Limbo	<i>Bursera simaruba</i>	25	25	3
1062	4	Carrotwood	<i>Cupanopsis anacardioides</i>	28	8	3
1063	6	Carrotwood	<i>Cupanopsis anacardioides</i>	25	8	3
1064	16	Live Oak	<i>Quercus virginiana</i>	30	30	2
1065	18	Live Oak	<i>Quercus virginiana</i>	30	30	2
1066	3	Live Oak	<i>Quercus virginiana</i>	20	12	2
1067	2	Live Oak	<i>Quercus virginiana</i>			6
1068	2	Live Oak	<i>Quercus virginiana</i>	15	6	2
1069	2	Live Oak	<i>Quercus virginiana</i>			6
1070	2	Live Oak	<i>Quercus virginiana</i>			6
1071	2	Live Oak	<i>Quercus virginiana</i>			6
1072	6/6/5	Alexander Palm	<i>Pythosperma elegans</i>	7/7/7		2
1073	6	Japanese Blueberry	<i>Eleocarpus decipiens</i>	20	10	3
1074	3/4	Alexander Palm	<i>Pythosperma elegans</i>	20/20		3
1075	4/4/4	Alexander Palm	<i>Pythosperma elegans</i>	20/20		3
1076	18	Live Oak	<i>Quercus virginiana</i>	35	30	2
1077	3/2/2/2/1/1/1	Area Palm	<i>Dypsis lutescens</i>	10		2
1078	4/4/4	Alexander Palm	<i>Pythosperma elegans</i>	25/25/25		2
1079	3/4/5	Alexander Palm	<i>Pythosperma elegans</i>	20/22/24		2
1080	4/4	Alexander Palm	<i>Pythosperma elegans</i>	20/20		2
1081	15	Live Oak	<i>Quercus virginiana</i>	30	20	2
1082	14	Live Oak	<i>Quercus virginiana</i>	30	20	2
1083	3/4	Alexander Palm	<i>Pythosperma elegans</i>	20/20		2
1084	3	Japanese Blueberry	<i>Eleocarpus decipiens</i>	25	8	2
1085	5/4/3	Alexander Palm	<i>Pythosperma elegans</i>	20/20/20		2
1086	3/4/4	Alexander Palm	<i>Pythosperma elegans</i>	20/20/20		2
1087	4/4/5	Alexander Palm	<i>Pythosperma elegans</i>	10/10/10		2
1088	22	Live Oak	<i>Quercus virginiana</i>	35	40	3
1089	10	Pigeon Plum	<i>Coccoloba diversifolia</i>	18	12	2
1090	17	Live Oak	<i>Quercus virginiana</i>	30	35	3
1091	16	Live Oak	<i>Quercus virginiana</i>	24	25	3
1092	14	Live Oak	<i>Quercus virginiana</i>	20	24	2
1093	20	Live Oak	<i>Quercus virginiana</i>	28	32	2
1094	18	Live Oak	<i>Quercus virginiana</i>	25	35	3
1095	14	Cabbage Palm	<i>Sabal palmetto</i>	10		3
1096	16	Live Oak	<i>Quercus virginiana</i>	22	30	2
1097	16	Live Oak	<i>Quercus virginiana</i>	25	25	2
1098	15	Live Oak	<i>Quercus virginiana</i>	30	25	2
1099	22	Live Oak	<i>Quercus virginiana</i>	30	32	2
1100	16	Live Oak	<i>Quercus virginiana</i>	25	21	2
1101	12	Spindle Palm	<i>Hyophorbe verschaffeltii</i>	7		2
1102	18	Live Oak	<i>Quercus virginiana</i>	30	27	2
1103	15	Live Oak	<i>Quercus virginiana</i>	30	27	3
1104	17	Live Oak	<i>Quercus virginiana</i>	25	27	3
1105	15	Live Oak	<i>Quercus virginiana</i>	30	30	3
1106	15	Live Oak	<i>Quercus virginiana</i>	35	22	3
1107	20	Live Oak	<i>Quercus virginiana</i>	35	31	3
1108	13	Live Oak	<i>Quercus virginiana</i>	30	26	3
1109	17	Live Oak	<i>Quercus virginiana</i>	30	27	3
1110	24	Live Oak	<i>Quercus virginiana</i>	28	38	3
1111	13	Spindle Palm	<i>Hyophorbe verschaffeltii</i>	9		2
1112	14	Spindle Palm	<i>Hyophorbe verschaffeltii</i>	9		2
1113	4/4	Alexander Palm	<i>Pythosperma elegans</i>	25/25		2
1114	7	Live Oak	<i>Quercus virginiana</i>	25	25	3
1115	6	Live Oak	<i>Quercus virginiana</i>	30	20	4
1116	7	Live Oak	<i>Quercus virginiana</i>	25	20	3
1117	6/6/6	Christmas Palm	<i>Adonidia merrellii</i>	10/10/10		2
1118	6/6/6	Christmas Palm	<i>Adonidia merrellii</i>	10/10/10		2

Tree condition ratings are based on an evaluation of health, structure and form as described in the Guide to Plant Appraisal, 10th Edition 1-Excellent 2-Good 3-Fair 4-Poor 5-Very Poor 6-Dead

SURVEYOR'S NOTES:

1. THE INTENT OF THIS SURVEY MAP AND NOTES ARE TO DEPICT A TOPOGRAPHIC AND BOUNDARY SURVEY.
2. WELL IDENTIFIED FEATURES AS DEPICTED ON THE SURVEY MAP, WERE MEASURED TO AN ESTIMATED HORIZONTAL POSITION ACCURACY OF 1/10 OF A FOOT, A VERTICAL POSITION ACCURACY OF 1/100 OF A FOOT ON HARD SURFACES, AND 1/10 OF A FOOT ON SOFT SURFACES.
3. THE CLIENT IS HEREBY ADVISED THAT THERE MAY BE LEGAL RESTRICTIONS ON THE SUBJECT PROPERTY THAT ARE NOT SHOWN ON THE SURVEY MAP.
4. THE SURVEYOR MAKES NO REPRESENTATION AS TO OWNERSHIP OR POSSESSION OF THE SUBJECT PROPERTY BY ANY ENTITY OR INDIVIDUAL.
5. NO EXCAVATION OR DETERMINATION WAS MADE AS TO HOW THE SUBJECT PROPERTY IS SERVED BY UTILITIES OR THE LOCATION OF BUILDING FOUNDATIONS.
6. UNLESS OTHERWISE NOTED, RECORD AND MEASURED DATA ARE IN SUBSTANTIAL AGREEMENT, WHERE FIELD AND PLAT DIMENSIONS DIFFER, (M) DENOTES FIELD-MEASURED AND (D) DENOTES DEED DISTANCES. ALL DISTANCES AND MEASUREMENTS ARE IN ACCORDANCE WITH THE UNITED STATES STANDARD FOOT.
7. SYMBOLS SHOWN IN THIS SURVEY DRAWING ARE NOT TO SCALE AND SHALL NOT BE USED TO SIZE SUCH ELEMENTS.
8. THIS SURVEY IS NOT VALID WITHOUT THE DIGITAL SIGNATURE AND SEAL AND/OR THE SIGNATURE AND ORIGINAL RAISED SEAL OF A FLORIDA LICENSED SURVEYOR AND MAPPER.
9. SET IRON PIPE OR REBAR WITH CAP # 7335 (18" IN LENGTH BY 0.2 SQUARE INCHES, MINIMUM) AT ALL CORNERS UNLESS OTHERWISE NOTED.
10. BEARINGS ARE BASED ON AN ASSUMED VALUE OF S57°49'34"W ALONG THE SOUTH LINE OF OF 11, BLOCK 69, OF HOLLYWOOD HILLS, ACCORDING TO THE PLAT THEREOF AS RECORDED IN PLAT BOOK 6, PAGE 22, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.
11. ALL ELEVATIONS SHOWN HEREON ARE BASED ON THE BENCHMARKS BY WAY OF A CLOSED LEVEL LOOP USING FGCS THIRD ORDER GEODETIC LEVELING PROCEDURES BETWEEN THE FOLLOWING BENCHMARKS:

BENCHMARK: BM192 BCED
ELEVATION: +8.48' (NGVD '29)
LOCATION: NE QUADRANT OF HOLLYWOOD BLVD & N 52 AVE
DESCRIPTION: BRASS NGS DISK BELOW SURFACE AT A SIGN MARKER

BENCHMARK: FDOT BLC03
ELEVATION: + 8.15' (NGVD '29)
LOCATION: AT THE NORTH RADIUS RETURN OF 3810 HOLLYWOOD BLVD, ON THE SOUTH R/W LINE OF HOLLYWOOD BLVD
DESCRIPTION: BRASS FDOT DISK IN CONCRETE

12. DRAINAGE AS-BUILT LINE COMES FROM SHEET C1 OF PAVING, GRADING AND DRAINAGE PLAN AND AS-BUILT UNDER PROJECT NUMBER 95-1351, DATED MARCH 1997, LAST REVISED ON JULY 11, 2000, PREPARED BY BERRY & CALVIN INC.
13. THE FOLLOWING ARE THE EXISTING NUMBER OF PARKING SPACES: 45 REGULAR & 2 HANDICAP.

PROPERTY INFORMATION:

1. CONTAINING 139,516 SQUARE FEET OR 3.20 ACRES, MORE OR LESS.
2. TAX PARCEL NUMBER: 514207-02-3710
3. PARCEL ADDRESS: 3880 SOUTH CIRCLE DRIVE, HOLLYWOOD, FLORIDA, 33021
4. THIS PROJECT AREA APPEARS TO BE LOCATED IN FLOOD ZONE "X", AS PER FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) COMMUNITY 125113, PANEL NUMBER 568 OF 751, MAP NO. 12011C0568J, REVISED JULY 31, 2024.

DATA SOURCES USED FOR SURVEY:

1. THAT SPECIAL WARRANTY DEED RECORDED AS INSTRUMENT #114256030 IN THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.
2. PLAT OF HOLLYWOOD HILLS, ACCORDING TO THE PLAT THEREOF AS RECORDED IN PLAT BOOK 6, PAGE 22, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.
3. FLORIDA'S DEPARTMENT OF TRANSPORTATION SPECIFIC PURPOSE SURVEY OF FINANCIAL PROJECT NO. 228160-1-32-01.
4. TREE DATA SHOWN HEREON WAS PROVIDED TO THE SURVEYOR BY DAVID LANDERS, SENIOR ENVIRONMENTAL SCIENTIST, WHO CAN BE REACHED AT SLANDERS@CPHCORP.COM. TREE CONDITION RATINGS ARE BASED ON AN EVALUATION OF HEALTH, STRUCTURE AND FORM AS DESCRIBED IN THE GUIDE TO PLANT APPRAISAL, 10TH EDITION.
5. PORTIONS OF THAT COMMITMENT FOR TITLE INSURANCE REFERENCED BELOW:

OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY
FILE NUMBER NCT2401318
REV. 05-09-2025
COMMITMENT DATE: MAY 5, 2025 AT 8:00AM

B-II EXCEPTIONS:

8. EASEMENTS OR CLAIMS OF EASEMENTS NOT RECORDED IN THE PUBLIC RECORDS. **(NO DOCUMENTS PROVIDED TO THE SURVEYOR FOR EVALUATION)**
9. ANY LIEN, OR RIGHT TO A LIEN, FOR SERVICES, LABOR OR MATERIAL FURNISHED, IMPOSED BY LAW AND NOT RECORDED IN THE PUBLIC RECORDS. **(NO DOCUMENTS PROVIDED TO THE SURVEYOR FOR EVALUATION)**
10. ANY LIEN PROVIDED BY COUNTY ORDINANCE OR BY CHAPTER 159, F.S., IN FAVOR OF ANY CITY, TOWN, VILLAGE OR PORT AUTHORITY, FOR UNPAID SERVICE CHARGES FOR SERVICES BY ANY WATER SYSTEMS, SEWER SYSTEMS OR GAS SYSTEMS SERVING THE LAND DESCRIBED HEREIN; AND ANY LIEN FOR WASTE FEES IN FAVOR OF ANY COUNTY OR MUNICIPALITY. **(NOT A SURVEY MATTER)**
11. EASEMENT GRANTED TO FLORIDA POWER AND LIGHT COMPANY, FILED DECEMBER 20, 1999 IN OFFICIAL RECORDS BOOK 30113, AT PAGE 507, PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA. **(AFFECTS THE SUBJECT PARCEL, PLOTTED)**
12. EASEMENT GRANTED TO THE CITY OF HOLLYWOOD FILE FEBRUARY 27, 1996 IN OFFICIAL RECORDS BOOK 24536, AT PAGE 163, PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA. **(AFFECTS THE SUBJECT PARCEL, PLOTTED)**

LEGAL DESCRIPTION: (PER COMMITMENT OF TITLE REFERENCED ABOVE)

LOTS 11 THROUGH 30, INCLUSIVE, BLOCK 69, HOLLYWOOD HILLS, ACCORDING TO THE PLAT THEREOF, RECORDED IN PLAT BOOK 6, PAGE 22, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA, TOGETHER WITH THE VACATED ALLEY AS DESCRIBED IN ORDINANCE 2184 AND FILED IN OFFICIAL RECORDS BOOK 2594, AT PAGE 861; AND LESS AND EXCEPT THAT PORTION OF LOT 30 DESCRIBED IN OFFICIAL RECORDS BOOK 2594, AT PAGE 858, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.

SAID LANDS LYING AND BEING IN THE CITY OF HOLLYWOOD, BROWARD COUNTY, FLORIDA.

ALSO DESCRIBED AS:

LOTS 11 THROUGH 30, INCLUSIVE, BLOCK 69, HOLLYWOOD HILLS, ACCORDING TO THE PLAT THEREOF, RECORDED IN PLAT BOOK 6, PAGE 22, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA, TOGETHER WITH THE VACATED ALLEY AS DESCRIBED IN ORDINANCE 2184 AND FILED IN OFFICIAL RECORDS BOOK 2594, AT PAGE 861; AND LESS AND EXCEPT THAT PORTION OF LOT 30 DESCRIBED IN OFFICIAL RECORDS BOOK 2594, AT PAGE 858, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER SAID LOT 11, SAID POINT ALSO BEING THE NORTHEAST CORNER OF LOT 10, OF SAID BLOCK 69; THENCE SOUTH 45 DEGREES 04 MINUTES 27 SECONDS WEST RADIAL TO THE NEXT DESCRIBED CURVE, 120.00 FEET TO A POINT ON THE ARC OF A CURVE CONCAVE TO THE NORTHEAST SAID POINT ALSO BEING LOCATED ON THE EASTERLY RIGHT-OF-WAY LINE OF SOUTH RAINBOW DRIVE; THENCE NORTHWESTERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 2635.00 FEET THROUGH A CENTRAL ANGLE OF 12 DEGREES 26 MINUTES 37 SECONDS AN ARC DISTANCE OF 572.27 FEET TO A POINT OF COMPOUND CURVATURE WITH A CURVE CONCAVE TO THE EAST; THENCE NORTHERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 10.00 FEET THROUGH A CENTRAL ANGLE OF 98 DEGREES 21 MINUTES 40 SECONDS AN ARC DISTANCE OF 17.17 FEET TO A POINT OF REVERSE CURVATURE WITH A CURVE CONCAVE TO THE NORTHWEST; THENCE NORTHEASTERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 817.00 FEET THROUGH A CENTRAL ANGLE OF 14 DEGREES 20 MINUTES 12 SECONDS AN ARC DISTANCE OF 232.95 FEET TO A POINT OF REVERSE CURVATURE WITH A CURVE CONCAVE TO THE SOUTH; THENCE EASTERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 10.00 FEET THROUGH A CENTRAL ANGLE OF 97 DEGREES 57 MINUTES 24 SECONDS AN ARC DISTANCE OF 17.10 FEET TO A POINT OF REVERSE CURVATURE WITH A CURVE CONCAVE TO THE NORTHEAST; THENCE SOUTHEASTERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 2380.00 FEET THROUGH A CENTRAL ANGLE OF 12 DEGREES 31 MINUTES 24 SECONDS AN ARC DISTANCE OF 520.21 FEET TO A POINT OF TANGENCY; THENCE SOUTH 35 DEGREES 35 MINUTES 24 SECONDS WEST RADIAL TO THE NEXT DESCRIBED CURVE, 120.00 FEET TO A POINT ON THE ARC OF A CURVE CONCAVE TO THE WEST; THENCE SOUTHWESTERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 2635.00 FEET THROUGH A CENTRAL ANGLE OF 12 DEGREES 26 MINUTES 37 SECONDS AN ARC DISTANCE OF 572.27 FEET TO A POINT OF COMPOUND CURVATURE WITH A CURVE CONCAVE TO THE WEST; THENCE SOUTHWESTERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 10.00 FEET THROUGH A CENTRAL ANGLE OF 98 DEGREES 21 MINUTES 40 SECONDS AN ARC DISTANCE OF 17.17 FEET TO A POINT OF REVERSE CURVATURE WITH A CURVE CONCAVE TO THE SOUTHWEST; THENCE SOUTHWESTERLY ON SAID NORTHWESTERLY LINE AND ON THE ARC OF SAID CURVE HAVING A RADIUS OF 40.00 FEET THROUGH A CENTRAL ANGLE OF 31 DEGREES 26 MINUTES 06 SECONDS AN ARC DISTANCE OF 21.95 FEET TO A POINT OF TANGENCY; THENCE SOUTH 45 DEGREES 02 MINUTES 35 SECONDS WEST ON SAID NORTHWESTERLY LINE, 15.00 FEET TO A POINT ON THE ARC OF A NON-TANGENT CURVE CONCAVE TO THE NORTHEAST WHOSE RADIUS POINT BEARS A BEARING OF NORTH 45 DEGREES 30 MINUTES 15 SECONDS EAST, SAID POINT BEING LOCATED ON THE EASTERLY LINE OF SAID LOT 11; THENCE SOUTHEASTERLY ON SAID EASTERLY LINE OF LOT 11 AND ON THE ARC OF SAID CURVE HAVING A RADIUS OF 2515.00 FEET THROUGH A CENTRAL ANGLE OF 00 DEGREES 25 MINUTES 28 SECONDS AN ARC DISTANCE OF 18.88 FEET TO THE POINT OF BEGINNING, SAID LANDS LYING IN THE CITY OF HOLLYWOOD, BROWARD COUNTY, FLORIDA.

SURVEYOR'S CERTIFICATE:

I HEREBY CERTIFY THAT THIS ALTA SURVEY WAS PERFORMED UNDER MY DIRECTION AND IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF AND FURTHER, THAT SAID ALTA SURVEY MEETS THE INTENT OF THE APPLICABLE PROVISIONS OF THE STANDARDS OF PRACTICE FOR LAND SURVEYING IN THE STATE OF FLORIDA, PURSUANT TO RULE 5J-17 OF THE FLORIDA ADMINISTRATIVE CODE AND ITS IMPLEMENTING LAW, CHAPTER 472.027 OF THE FLORIDA STATUTES. **LAST DATE OF FIELD SURVEY: SEPTEMBER 26, 2025.**

FOR LONGITUDE SURVEYORS LLC., A FLORIDA LIMITED LIABILITY COMPANY

GREG BOUIE
PROFESSIONAL SURVEYOR AND MAPPER LS7169
STATE OF FLORIDA

[illegible]

ALTA/NSPS LAND TITLE SURVEY

PRESIDENTIAL PLACE SENIOR HOUSING

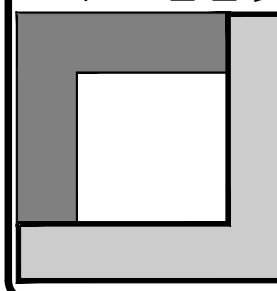
SITE LOCATION:
3880 SOUTH CIRCLE DRIVE
HOLLYWOOD, FLORIDA 33021

CPH CORP.
1992 SW 1ST STREET
MIAMI, FL 33135

LONGITUDE SURVEYORS

77700 N. KENDALL DRIVE SUITE 705
MIAMI, FL 33156
PH: (305) 463-0912

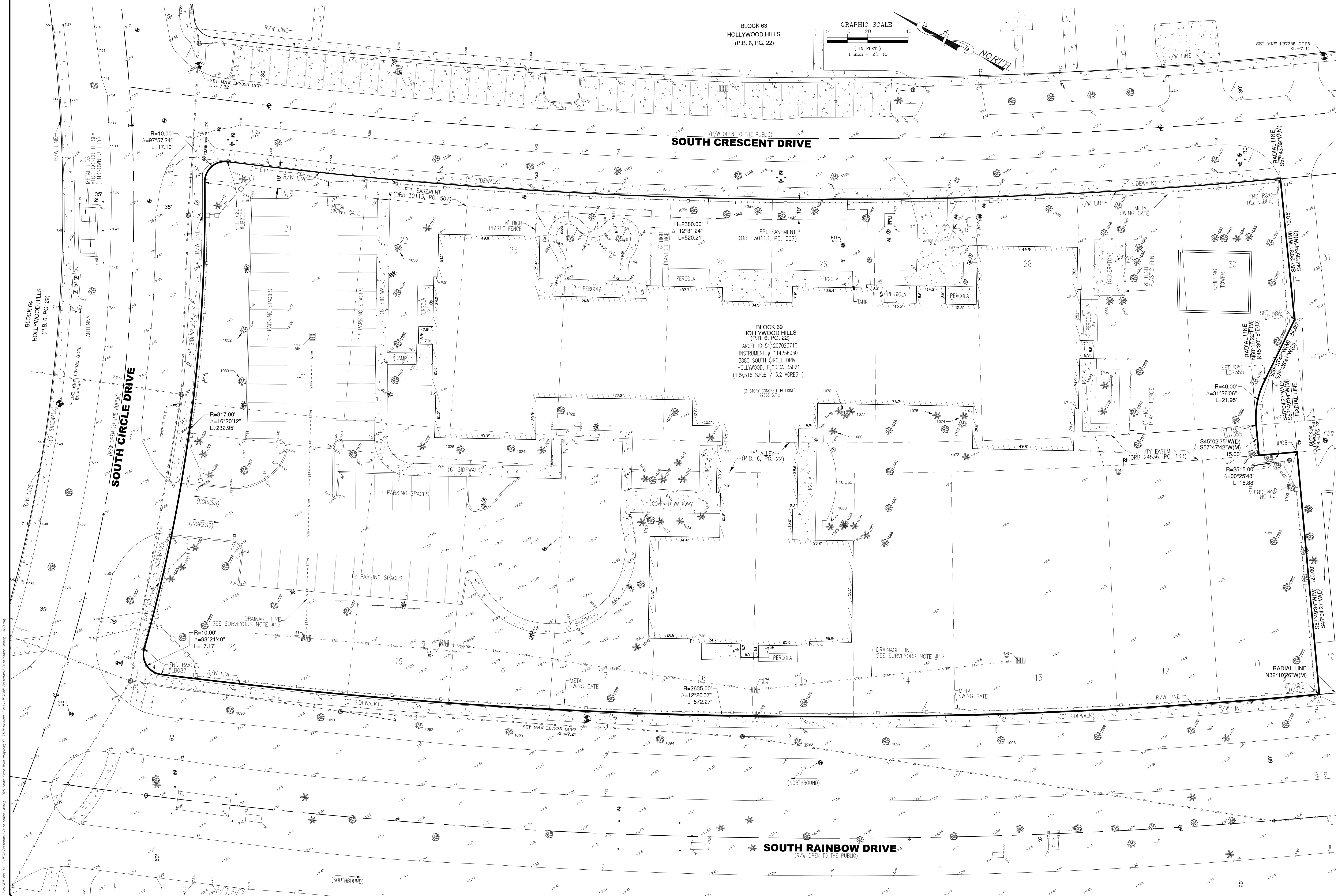
PH: (305) 463-0912
FLORIDA CERTIFICATE OF AUTHORIZATION LB 7335
WWW.LONGITUDESURVEYORS.COM



DRAWN: GB
CHECKED: ---
SCALE: AS NOTED
DATE: 9/25/25
PROJ. #: 25104.0.01
FIELD BOOK: -----

SHEET:
1
OF 2 SHEETS

PRESIDENTIAL PALACE SENIOR HOUSING

[illegible]

ALTA/NSPS LAND TITLE SURVEY
PRESIDENTIAL PALACE SENIOR HOUSING

LOCATION: 3880 SOUTH CIRCLE DRIVE
HOLLYWOOD, FLORIDA 33021

CPH CORP.
1992 SW 1ST STREET
MIAMI, FL 33134-0001

LONGITUDE SURVEYORS

77700 N. KENDALL DRIVE SUITE 705
MIAMI, FL 33156
PH: (305) 463-0912
FLORIDA CERTIFICATE OF AUTHORIZATION LB 7335
WWW.LONGITUDESURVEYORS.COM

DRAWN: GB
CHECKED: ---
SCALE: AS NOTED
DATE: 9/25/25
PROJ. #: 25104.0.01
FIELD BOOK: -----

SHEET:
2

SHEET INDEX						
SHEET NUMBER	SHEET TITLE	10-16-2025 - SUBMITTAL				
C01.0	COVER	●				
C03.0	SITE DIMENSION PLAN	●				
C03.1	PAVEMENT MARKING AND SIGNAGE PLAN	●				
C03.2	HARDSCAPE DETAILS I	●				
C03.3	HARDSCAPE DETAILS II	●				
C04.0	UTILITY PLAN	●				
C05.0	PAVING, GRADING & DRAINAGE PLAN	●				

LEGAL DESCRIPTION: (PER COMMITMENT OF TITLE)

LOTS 11 THROUGH 30, INCLUSIVE, BLOCK 69, HOLLYWOOD HILLS, ACCORDING TO THE PLAT THEREOF, RECORDED IN PLAT BOOK 6, PAGE 22, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA, TOGETHER WITH THE VACATED ALLEY AS DESCRIBED IN ORDINANCE 2184 AND FILED IN OFFICIAL RECORDS BOOK 2594, AT PAGE 861; AND LESS AND EXCEPT THAT PORTION OF LOT 30 DESCRIBED IN OFFICIAL RECORDS BOOK 2594, AT PAGE 858, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.

SAID LANDS LYING AND BEING IN THE CITY OF HOLLYWOOD, BROWARD COUNTY, FLORIDA.

ALSO DESCRIBED AS:

LOTS 11 THROUGH 30, INCLUSIVE, BLOCK 69, HOLLYWOOD HILLS, ACCORDING TO THE PLAT THEREOF, RECORDED IN PLAT BOOK 6, PAGE 22, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA, TOGETHER WITH THE VACATED ALLEY AS DESCRIBED IN ORDINANCE 2184 AND FILED IN OFFICIAL RECORDS BOOK 2594, AT PAGE 861; AND LESS AND EXCEPT THAT PORTION OF LOT 30 DESCRIBED IN OFFICIAL RECORDS BOOK 2594, AT PAGE 858, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER OF SAID LOT 11, SAID POINT ALSO BEING THE NORTHEAST CORNER OF LOT 10, OF SAID BLOCK 69; THENCE SOUTH 45 DEGREES 04 MINUTES 27 SECONDS WEST RADIAL TO THE NEXT DESCRIBED CURVE, 120.00 FEET TO A POINT ON THE ARC OF A CURVE CONCAVE TO THE NORTHEAST SAID POINT ALSO BEING LOCATED ON THE EASTERLY RIGHT-OF-WAY LINE OF SOUTH RAINBOW DRIVE; THENCE NORTHWESTERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 2635.00 FEET THROUGH A CENTRAL ANGLE OF 12 DEGREES 26 MINUTES 37 SECONDS AN ARC DISTANCE OF 572.27 FEET TO A POINT OF COMPOUND CURVATURE WITH A CURVE CONCAVE TO THE EAST; THENCE NORTHERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 10.00 FEET THROUGH A CENTRAL ANGLE OF 98 DEGREES 21 MINUTES 40 SECONDS AN ARC DISTANCE OF 17.17 FEET TO A POINT OF REVERSE CURVATURE WITH A CURVE CONCAVE TO THE NORTHWEST; THENCE NORTHEASTERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 817.00 FEET THROUGH A CENTRAL ANGLE OF 16 DEGREES 20 MINUTES 12 SECONDS AN ARC DISTANCE OF 232.95 FEET TO A POINT OF REVERSE CURVATURE WITH A CURVE CONCAVE TO THE SOUTH; THENCE EASTERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 10.00 FEET THROUGH A CENTRAL ANGLE OF 97 DEGREES 57 MINUTES 24 SECONDS AN ARC DISTANCE OF 17.10 FEET TO A POINT OF REVERSE CURVATURE WITH A CURVE CONCAVE TO THE NORTHEAST; THENCE SOUTHEASTERLY ON THE ARC OF SAID CURVE HAVING A RADIUS OF 2380.00 FEET THROUGH A CENTRAL ANGLE OF 12 DEGREES 31 MINUTES 24 SECONDS AN ARC DISTANCE OF 520.21 FEET TO A POINT OF NON-TANGENCY; THENCE SOUTH 44 DEGREES 35 MINUTES 24 SECONDS WEST ON THE NORTHWESTERLY LINE OF THAT PORTION OF SAID LOT 30 DESCRIBED IN OFFICIAL RECORDS BOOK 2594, AT PAGE 858, A DISTANCE OF 70.05 FEET; THENCE SOUTH 76 DEGREES 28 MINUTES 41 SECONDS WEST ON SAID NORTHWESTERLY LINE 34.00 FEET TO THE BEGINNING OF A TANGENT CURVE CONCAVE TO THE SOUTHEAST; THENCE SOUTHWESTERLY ON SAID NORTHWESTERLY LINE AND ON THE ARC OF SAID CURVE HAVING A RADIUS OF 40.00 FEET THROUGH A CENTRAL ANGLE OF 31 DEGREES 26 MINUTES 06 SECONDS AN ARC DISTANCE OF 21.95 FEET TO A POINT OF TANGENCY; THENCE SOUTH 45 DEGREES 02 MINUTES 35 SECONDS WEST ON SAID NORTHWESTERLY LINE, 15.00 FEET TO A POINT ON THE ARC OF A NON-TANGENT CURVE CONCAVE TO THE NORTHEAST WHOSE RADIUS POINT BEARS NORTH 45 DEGREES 30 MINUTES 15 SECONDS EAST, SAID POINT BEING LOCATED ON THE EASTERLY LINE OF SAID LOT 11; THENCE SOUTHEASTERLY ON SAID EASTERLY LINE OF LOT 11 AND ON THE ARC OF SAID CURVE HAVING A RADIUS OF 2515.00 FEET THROUGH A CENTRAL ANGLE OF 00 DEGREES 25 MINUTES 28 SECONDS AN ARC DISTANCE OF 18.88 FEET TO THE POINT OF BEGINNING. SAID LANDS LYING IN THE CITY OF HOLLYWOOD, BROWARD COUNTY, FLORIDA.

OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY
FILE NUMBER NCT2401318
REV. 05-09-2025
COMMITMENT DATE: MAY 5, 2025 AT 8:00AM

PROJECT DESCRIPTION:

PROPOSED EXPANSION OF PARKING AREAS ADJACENT TO S. CRESCENT DRIVE AND S. RAINBOW DRIVE. SITE IMPROVEMENTS INCLUDE 22 PROPOSED PARKING SPACES AND ONE (1) ADA PARKING IN THE EXISTING PARKING AREA. SITE WILL ADD AN ADDITIONAL AMOUNT OF IMPERVIOUS AREA AND REDUCE THE EXISTING AMOUNT OF PERVIOUS AREA. THE SITE FALLS UNDER THE JURISDICTION OF CITY OF HOLLYWOOD, FLORIDA.

CIVIL SET PRESIDENTIAL PLACE PARKING EXPANSION

3880 SOUTH CIRCLE DRIVE
HOLLYWOOD, FLORIDA 33021

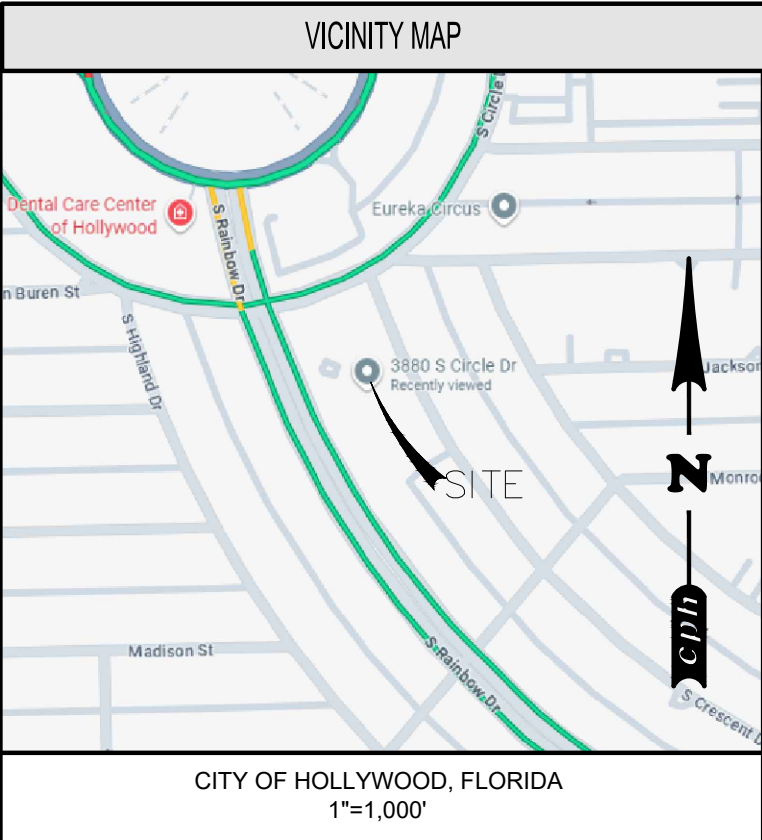
SOUTHWEST 1/4 OF SECTION 17, TOWNSHIP 51 SOUTH, RANGE 42 EAST

PREPARED BY:



www.cphcorp.com
A Full Service
A & E Firm

Plans Prepared By:
CPH CONSULTING, LLC
1992 SW 1ST STREET
MIAMI, FL 33135
PH: (305) 274-4805
Licenses:
Eng. C.O.A. No. 3215
Survey L.B. No. 7143
Arch. Lic. No. AA26000926
Lndscp. Lic. No. LC0000298



SITE INFORMATION	
SITE DATA	
<u>JURISDICTION:</u>	CITY OF HOLLYWOOD
<u>ZONING:</u>	RM-9 & RM-18
<u>FUTURE LAND USE:</u>	RM-18 - RESIDENTIAL (MRES)
<u>OVERLAY DISTRICT:</u>	MAXIMUM ALLOWED DENSITY, MEDIUM (16)
<u>ADDRESS:</u>	3880 S. CIRCLE DR, HOLLYWOOD, FL 33021
<u>PARCEL ID:</u>	514207023710
<u>STATEMENT OF INTENT:</u>	
<u>ADJACENT ZONING:</u>	
NORTH:	ROW
EAST:	RM-18
SOUTH:	RM-18
WEST:	ROW
<u>REQUIRED BUILDING SETBACKS:</u>	<u>PROVIDED BUILDING SETBACKS:</u>
(NW) FRONT: 20'	(NW) FRONT: 117.50'
(E) SIDE: 15'	(E) SIDE: 24.00'
(W) SIDE: 15'	(W) SIDE: 29.00'
(SE) REAR: 20'	(SE) REAR: 81.00'
<u>REQUIRED LANDSCAPE STRIP/SETBACKS:</u>	<u>PROVIDED LANDSCAPE STRIP/SETBACKS:</u>
(NW) FRONT: 20'	(NW) FRONT: VARIES
(E) SIDE: 5'	(E) SIDE: VARIES
(W) SIDE: 5'	(W) SIDE: VARIES
(SE) REAR: 0'	(SE) REAR: VARIES
<u>MAX. BUILDING HEIGHT:</u> 65'-0"	
<u>REQUIRED PARKING RATIO:</u>	
PROPOSED USE: MULTI-FAMILY APARTMENT UNITS	
NUMBER OF UNITS: 104	
REQUIRED PARKING RATIO: 1.5 SPACES PER UNIT	
GUEST PARKING : 1 PER 5 UNITS FOR DEVELOPMENTS WITH OVER 10 UNITS	
177 REQUIRED PARKING SPACES (21 SPACES ALLOCATED FOR GUESTS)	
<u>EXISTING PARKING (SURVEY PREPARED BY LONGITUDE SURVEYORS):</u>	
ACCESSIBLE: 2	
REGULAR: 45	
<u>PROPOSED PARKING :</u>	
EXISTING ACCESSIBLE SPACES: 2	
ACCESSIBLE: 1 (DIMS) = 16.5' X 8.5'	
EXISTING REGULAR SPACES: 44 (DIMS) = 16.5' X 8.5' (1 REG. SPACE REMOVED)	
REGULAR: 22 (DIMS) = 20.7' X 8.5'	
TOTAL = 69	
<u>PROPOSED REGULAR PARKING GEOMETRICS (SEE DETAIL C-16):</u>	
A - PARKING ANGLE = 60-DEG.	
B - STALL WIDTH = 8.5'	
C - AISLE WIDTH = 18.5'	
D - STALL LENGTH = 20.7'	
E - CURB DEPTH = 9.8'	
F - OVERALL LENGTH = 59.9'	
G - BACK TO BACK STALL LENGTH = 18.6'	
H - OVERALL BACK TO BACK LENGTH = 55.7'	
DRIVE AISLE WIDTH: 18.5' MIN. (ONE WAY)	
<u>SITE AREA CALCULATIONS:</u>	
SITE: 3.20 AC. (139,516 SF) (100%)	
PERVIOUS AREA: 1.79 AC. (77,771 SF) (45%)	
IMPERVIOUS AREA: 1.42 AC. (61,745 SF) (55%)	
BUILDING AREA: 0.73 AC. (31,835 SF) (23%)	
MINIMUM OPEN SPACE REQUIRED: 40%	
<u>FLOOD HAZARD: FLOOD ZONE "X"</u>	
F.I.R.M. MAP NO. 12011C0568J, DATED 07/31/24.	

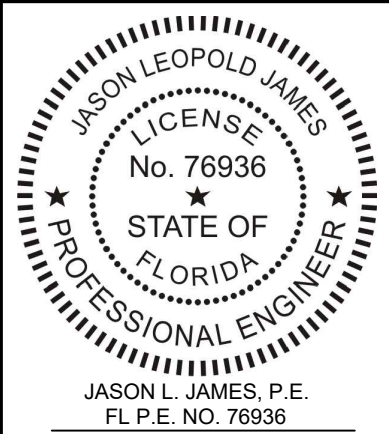
CODE VARIANCES		
CODE SECTION	CODE REQUIREMENT	PROPOSED CONDITION
7.2	FOR AN APARTMENT UNIT, 1.5 PARKING SPACES PER UNIT IS REQUIRED. IN ADDITION, FOR A DEVELOPMENT WITH MORE THAN 10 UNITS, THEN 1 SPACE PER 5 UNITS (MARKED AS GUEST PARKING) IS ALSO REQUIRED.	IN ADDITION TO THE EXISTING 45 REGULAR PARKING SPACES AND THE 2 EXISTING ACCESSIBLE SPACES, 22 REGULAR SPACES AND 1 ACCESSIBLE SPACE ARE PROPOSED FOR THE SITE TO HAVE A TOTAL OF 69 PARKING SPACES.
4.2(B)	FOR AN RM-9 & RM-18 DISTRICT, THE MINIMUM UNIT SIZE FOR AN APARTMENT BUILDING IS 500 SQ.FT.	REDUCTION OF MINIMUM UNIT SIZE TO 385 SQ.FT.
4.2(B)	FOR AN RM-9 & RM-18 DISTRICT, THE MINIMUM AVERAGE UNIT SIZE FOR AN APARTMENT BUILDING IS 750 SQ.FT.	REDUCTION OF MINIMUM AVERAGE UNIT SIZE TO 482 SQ.FT.
155.08(E)	CURB CUTS FOR NON-RESIDENTIAL PROPERTIES SHALL NOT EXCEED 30% OF THE PROPERTY FRONTAGE WITH A MAXIMUM 30 FOOT WIDTH EACH AND A MINIMUM SEPARATION OF 50 LINEAR FEET.	CURB CUT ON S. RAINBOW DRIVE EXCEEDS THE ALLOWABLE AND MEASURES AT 202.84 FT.
155.08(E)	CURB CUTS FOR NON-RESIDENTIAL PROPERTIES SHALL NOT EXCEED 30% OF THE PROPERTY FRONTAGE WITH A MAXIMUM 30 FOOT WIDTH EACH AND A MINIMUM SEPARATION OF 50 LINEAR FEET.	CURB CUT ON S. CRESCENT DRIVE EXCEEDS THE ALLOWABLE AND MEASURES AT 117.76 FT.
7.1(C)	HEAD-IN/BACK-OUT PARKING. PARKING SPACES SHALL NOT HAVE DIRECT ACCESS TO OR FROM A STREET OR ALLEY	HEAD-IN AND BACK-OUT PARKING PROPOSED ALONG S. RAINBOW DRIVE AND S. CRESCENT DRIVE.
4.22(I)	SIDE/STREET: FOR AT-GRADE PARKING LOTS, REQUIRED LANDSCAPE BUFFER IS 10 FT.	NO LANDSCAPE BUFFER PROPOSED ON S. RAINBOW DRIVE DUE TO PROPOSED IMPROVEMENTS IN CITY RIGHT-OF-WAY.
4.22(I)	SIDE/STREET: FOR AT-GRADE PARKING LOTS, REQUIRED LANDSCAPE BUFFER IS 10 FT.	NO LANDSCAPE BUFFER PROPOSED ON S. CRESCENT DRIVE DUE TO PROPOSED IMPROVEMENTS IN CITY RIGHT-OF-WAY.

PROJECT CONTACTS

VERTICAL DATUM: NGVD 29



Plans Prepared By:
CPH, LLC
A Full Service A & E Firm



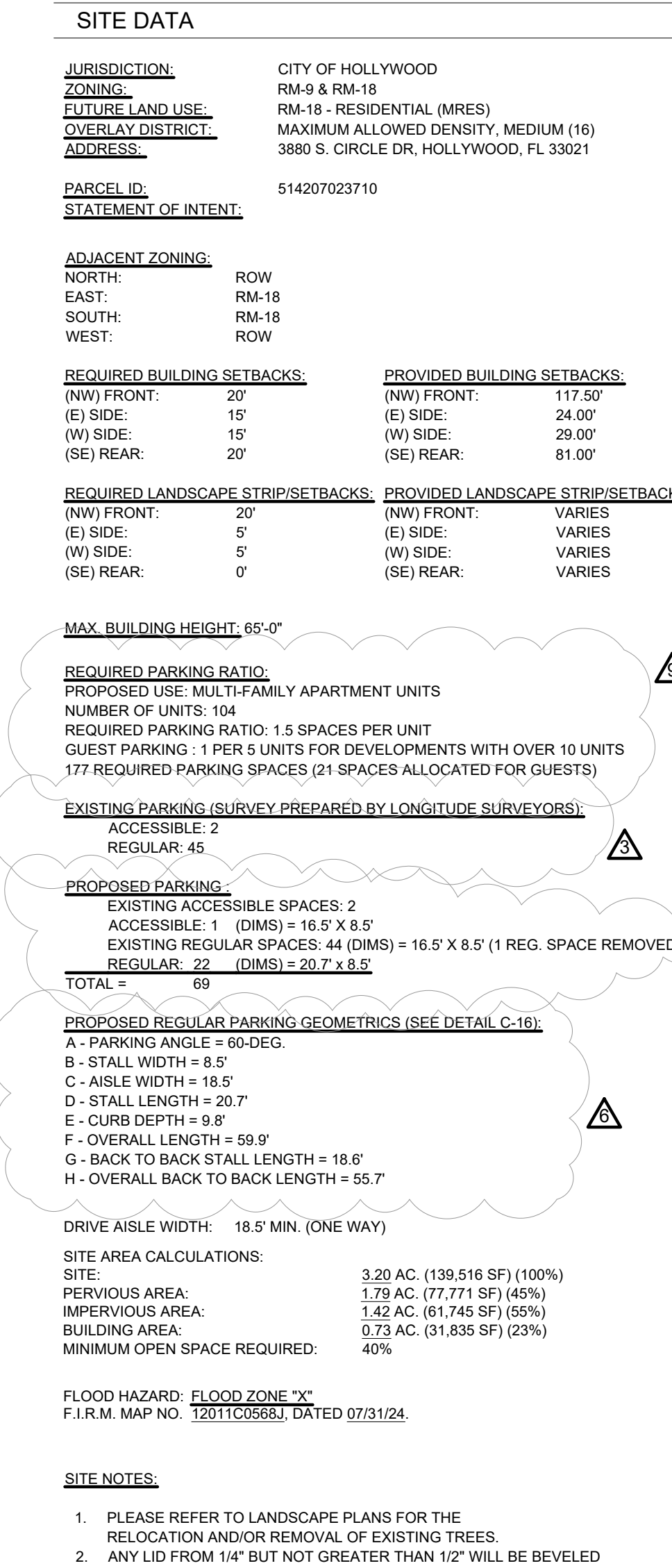
No.	Date	Revision
1	10-16-2025	PLAN REVISIONS

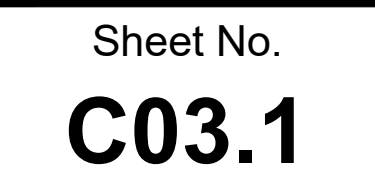
Designed: R.O.
Drawn: J.C.
Checked: R.O.
Job No.: 2500922
Date: 10/16/2025 © 2025

COVER
PRESIDENTIAL PLACE
3880 SOUTH CIRCLE DRIVE
CITY OF HOLLYWOOD, FLORIDA 33021
BROWARD COUNTY, FLORIDA

THIS SHEET NOT VALID FOR CONSTRUCTION WITHOUT COMPLETE SET OF PLANS. SEE GENERAL NOTES FOR MASTER LEGEND.

Sheet No.
C01.0

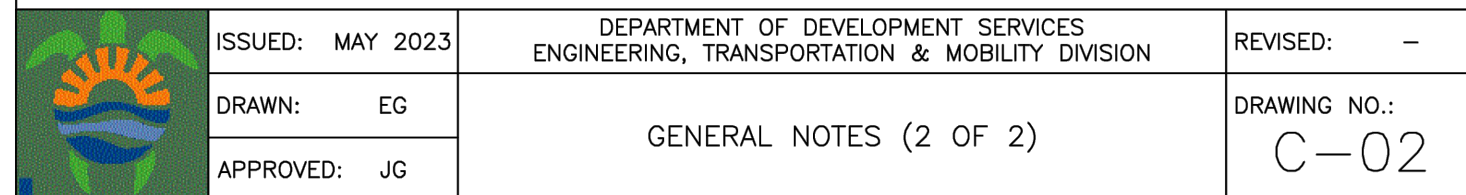




1. THE INFORMATION PROVIDED IN THESE DRAWINGS IS SOLELY TO ASSIST THE CONTRACTOR IN ASSESSING THE NATURE AND EXTENT OF CONDITIONS WHICH WILL BE ENCOUNTERED DURING THE COURSE OF THE WORK. THE CONTRACTORS ARE DIRECTED, PRIOR TO BIDDING, TO CONDUCT WHATEVER INVESTIGATIONS THEY DEEM NECESSARY TO ARRIVE AT THEIR OWN CONCLUSION REGARDING THE ACTUAL CONDITIONS THAT WILL BE ENCOUNTERED, AND UPON WHICH BIDS WILL BE BASED.

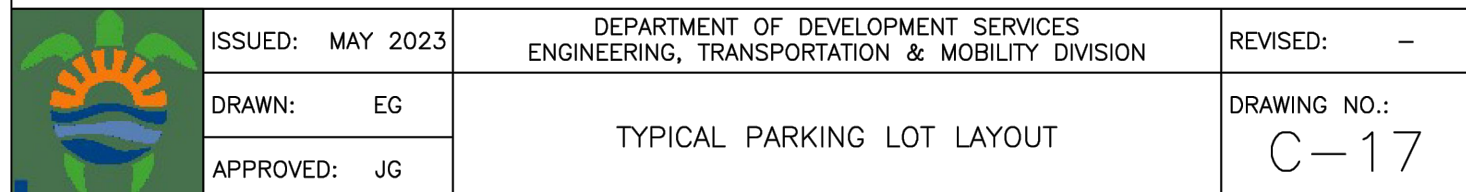
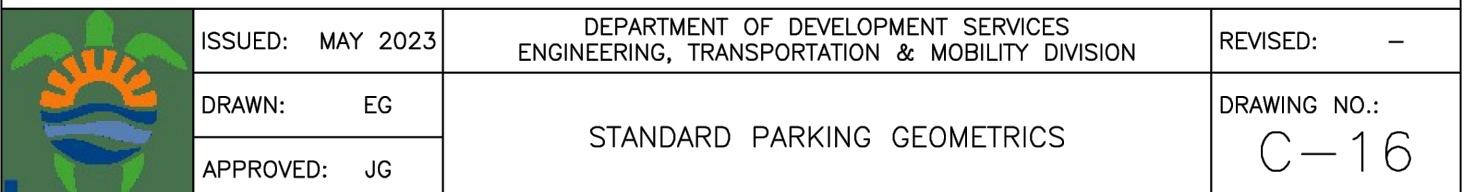
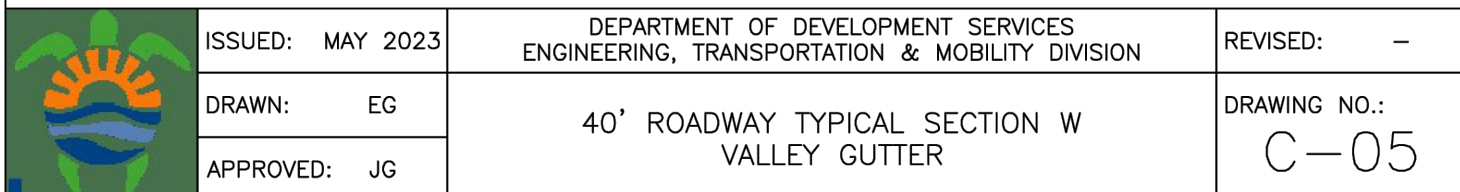
- | | | | |
|---|------------------|---|----------------------|
|  | ISSUED: MAY 2023 | DEPARTMENT OF DEVELOPMENT SERVICES
ENGINEERING, TRANSPORTATION & MOBILITY DIVISION | REVISED: - |
| | DRAWN: EG | GENERAL NOTES (1 OF 2) | DRAWING NO.:
C-01 |
| | APPROVED: JG | | |

- | | | | |
|---|------------------|---|-------------------|
|  | ISSUED: MAY 2023 | DEPARTMENT OF DEVELOPMENT SERVICES
ENGINEERING, TRANSPORTATION & MOBILITY DIVISION | REVISED: - |
| | DRAWN: EG | GENERAL NOTES (2 OF 2) | DRAWING NO.: C-02 |
| | APPROVED: JG | | |



1. ENGINEERING DIVISION SHALL NOT ISSUE R.O.W. PERMITS UNTIL THE UTILITY COMPANY SCHEDULE AND ATTEND A PRE-CONSTRUCTION MEETING.
2. A REPRESENTATIVE FROM THE UTILITY COMPANY MUST ATTEND THE PRE-CONSTRUCTION MEETING, ALONG WITH THE CONTRACTOR AND/OR SUB-CONTRACTOR ACTUALLY PERFORMING THE WORK. CONTACT INFORMATION FOR THE UTILITY COMPANY REPRESENTATIVE AND THE CONTRACTOR PERFORMING THE WORK MUST BE ON EACH PROJECT DURING THE PRE-CONSTRUCTION MEETING.
3. PERMITS WILL BE ISSUED FOR A DEFINED PERIOD OF TIME, AND ARE ONLY VALID DURING THE SPECIFIED TIME. THE PERMIT DURATION WILL BE DETERMINED DURING PLAN REVIEW AND WILL BE BASED ON THE SCOPE OF WORK. THE PERMIT WILL ONLY BE VALID DURING THE TIME PERIOD STATED ON THE APPROVED PLANS.
4. WHEN CROSSING OF THE CITY'S CRITICAL OPERATIONAL INFRASTRUCTURE, WHICH IS GENERALLY ANY UTILITY PIPE SIZES 12" AND GREATER, A COMPANY MUST SUBMIT A SIGNED LETTER TO THE CITY AUTHORIZED PERSON FROM THE CONTRACTOR, ACKNOWLEDGING THAT THE PROJECT PURPOSES TO CROSS SUCH CRITICAL INFRASTRUCTURE. THIS LETTER SHOULD BE SUBMITTED DURING INITIAL PERMIT SUBMITAL TO PREVENT DELAYS IN ISSUANCE OF R.O.W. PERMIT. THE LETTER SHALL BE ACCOMPANIED BY A MAP SHOWING THE LOCATION OF ALL SUCH CRITICAL INFRASTRUCTURE BEING CROSSED AND SUCH MAP SHALL BE ON THE COVER PAGE OF THE PERMIT PLAN SET.
5. PERMITS MUST BE ONSITE DURING CONSTRUCTION. ANY CITY EMPLOYEE WITH PROPER IDENTIFICATION MUST BE ON SITE AND A CONTRACTOR FOR THEIR PERMITTED SET OF PLANS, AND STOP WORK FOR ANY CONTRACTOR WHO DO NOT PRESENT A VALID PERMITTED SET OF PLANS, A CONTRACTOR WORKING OUTSIDE OF THE PERMIT ISSUANCE WINDOW IS CONSIDERED TO BE WORKING WITHOUT A PERMIT, AND WILL BE SUBJECT TO ISSUANCE OF A VIOLATION FOR DOING WORK WITHOUT A PERMIT. VIOLATIONS FOR WORK WITHOUT A PERMIT CAN RESULT IN SUSPENSION OF A CONTRACTOR'S ABILITY TO OBTAIN PERMITS TO WORK WITHIN THE CITY.
6. WHEN A PROPOSED UTILITY CROSSES AN EXISTING CITY GRAVITY SANITARY SEWER OR DRAINAGE PIPE, THE UTILITY COMPANY MUST TELEVISE THE PIPE, AND PROVIDE VIDEO AT THE POINT OF CROSSING. A CLEAR SKETCH WITH DIMENSIONS MUST BE PROVIDED IDENTIFYING THE CROSSING POINT. THIS HAS TO BE DONE PRIOR TO SCHEDULING FINAL INSPECTION.
7. WHENEVER A PROPOSED UTILITY CROSSES EXISTING CITY UTILITIES, THE DEPTH OF THE EXISTING UTILITY UTILITIES MUST BE REVEALED VIA VIDEO DIGS, OR AN ACCEPTED METHOD THAT CAN ACCURATELY DETERMINE THE TRUE DEPTH OF THE EXISTING UTILITY. BY A METHOD OTHER THAN VIDEO DIG IS UTILIZED TO IDENTIFY THE DEPTH OF EXISTING UTILITIES, THE PERMITTEE MUST NOTIFY THE CITY OF THE METHOD, AND PROVIDE INFORMATION ATTESTING TO THE ACCURACY OF THE METHOD, MINIMUM VERTICAL AND HORIZONTAL SEPARATIONS NOTED ON THE APPROVED PLANS MUST BE PROVIDED.
8. PRIOR TO FINAL INSPECTION, AN AS-BUILT PLAN AND BORE LOG MUST BE SUBMITTED TO THE CITY FOR REVIEW. AS PART OF THE REVIEW, THE CITY WILL VERIFY THE DEPTH OF THE EXISTING UTILITY AT THE POINT WHERE THE NEWLY INSTALLED UTILITY CROSSES IT, AND THE DEPTH AT WHICH THE NEW UTILITY WAS INSTALLED. THIS IS TO VERIFY THAT PROPER SEPARATION DISTANCES HAVE BEEN PROVIDED.

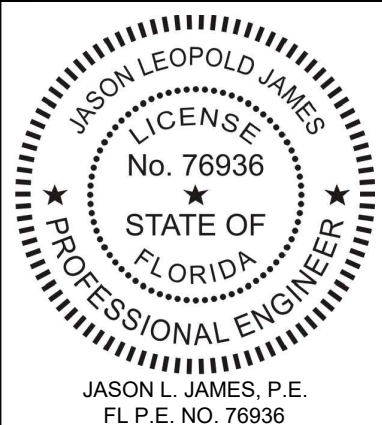
	ISSUED: MAY 2023	DEPARTMENT OF DEVELOPMENT SERVICES ENGINEERING, TRANSPORTATION & MOBILITY DIVISION	REVISED: —
	DRAWN: EG	ROW PERMITS STANDARD PROCEDURE	DRAWING NO.: C-03
	APPROVED: JG		



***Building Better
Communities Together***

1992 SW 1st. Street
Miami, FL 33135
Ph: 305.274.4805

Plans Prepared By:
CPH, LLC
A Full Service A & E Firm

[illegible]

Designed: R.O	
Drawn: J.C.	
Checked: R.O.	
Job No.: 2500922	
Date: 10/16/2025	© 2025

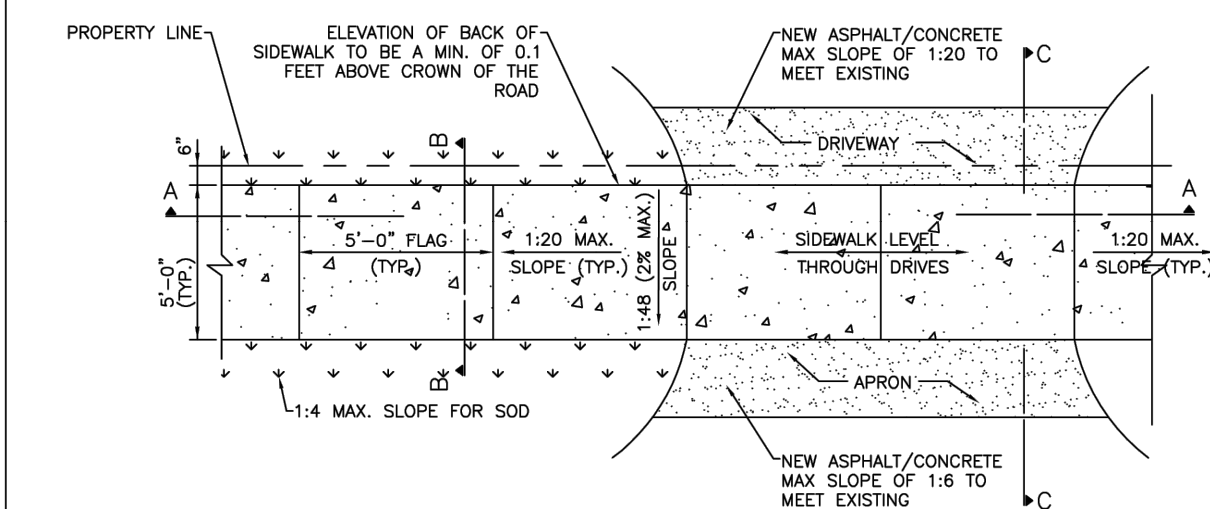
HARDSCAPE DETAILS I

PRESIDENTIAL PLACE
3880 SOUTH CIRCLE DRIVE
CITY OF HOLLYWOOD, FLORIDA 33021
BROWARD COUNTY, FLORIDA

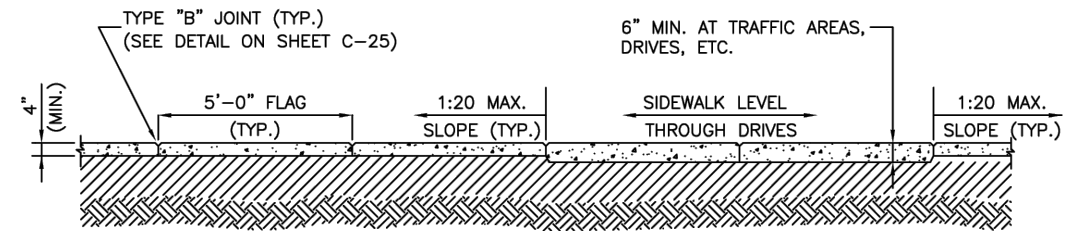
THIS SHEET NOT VALID FOR
CONSTRUCTION WITHOUT
COMPLETE SET OF PLANS.
SEE GENERAL NOTES FOR
MASTER LEGEND.

Sheet No.

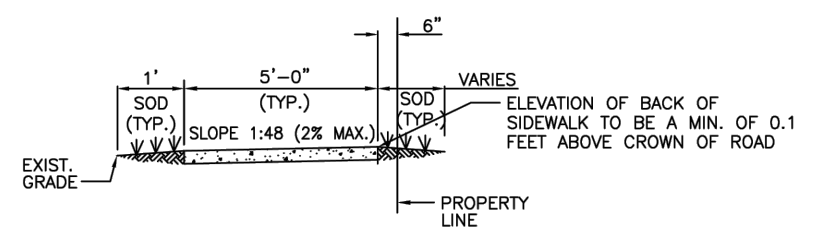
C03.2



PLAN



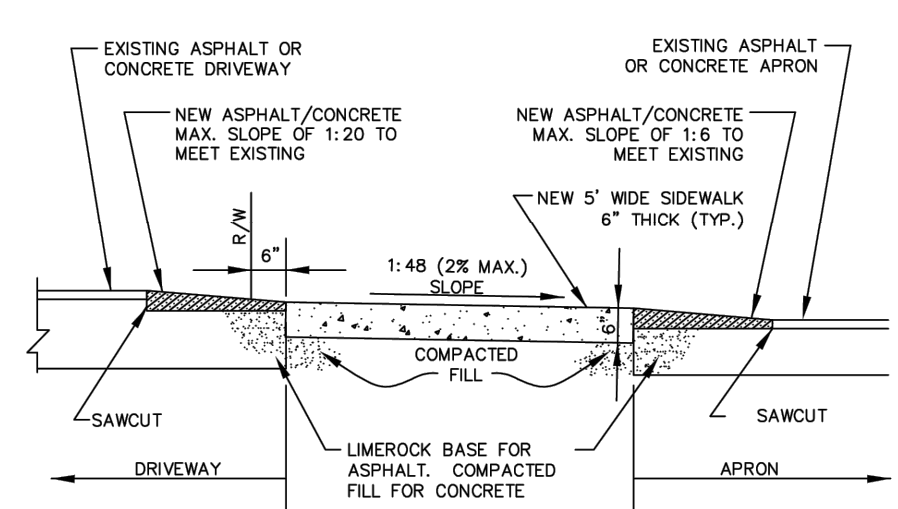
SECTION A-A



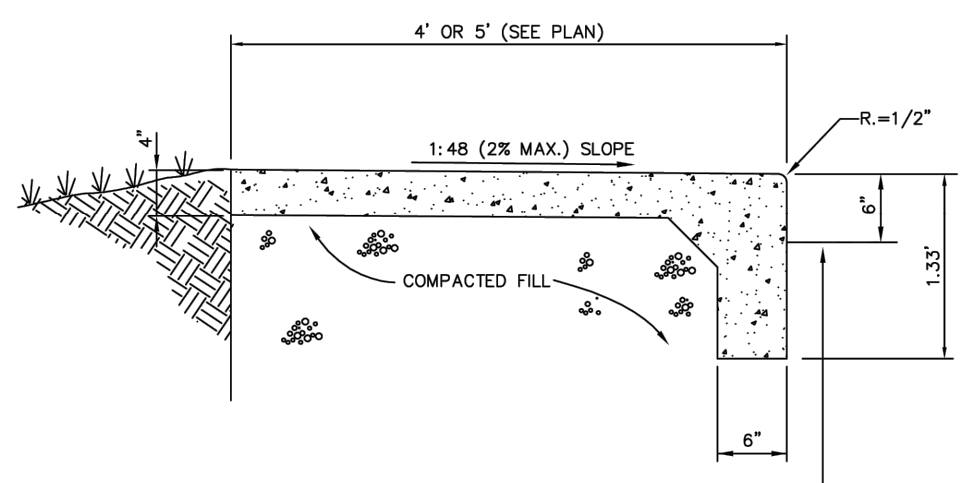
SECTION B-B

- NOTE:
1. ALL SIDEWALK CONSTRUCTION SHALL BE IN STRICT ACCORDANCE WITH THE ADA STANDARDS.
 2. LIGHT BROOM FINISH PERPENDICULAR TO THE DIRECTION OF THE SIDEWALK.
 3. ALL SIDEWALKS CROSS SLOPES SHALL BE 1:48 (2% MAX.); AND, RUNNING SLOPES 1:20 MAX.

	ISSUED: MAY 2023	DEPARTMENT OF DEVELOPMENT SERVICES ENGINEERING, TRANSPORTATION & MOBILITY DIVISION	REVISED: -
	DRAWN: EG		DRAWING NO.: C-23
	APPROVED: JG	SIDEWALK CONSTRUCTION DETAILS (1 OF 3)	

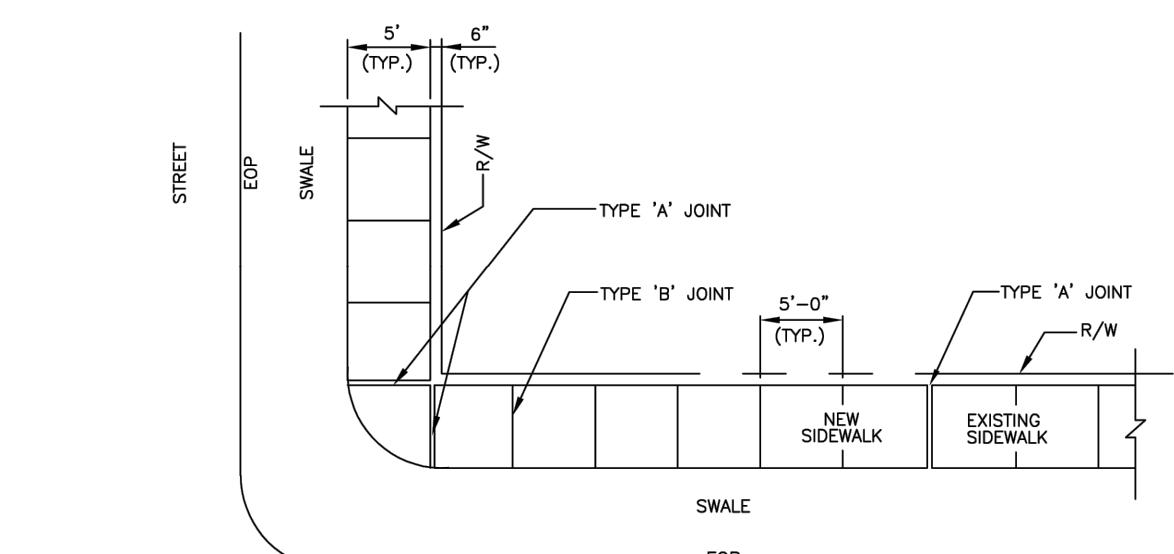


SECTION C-C

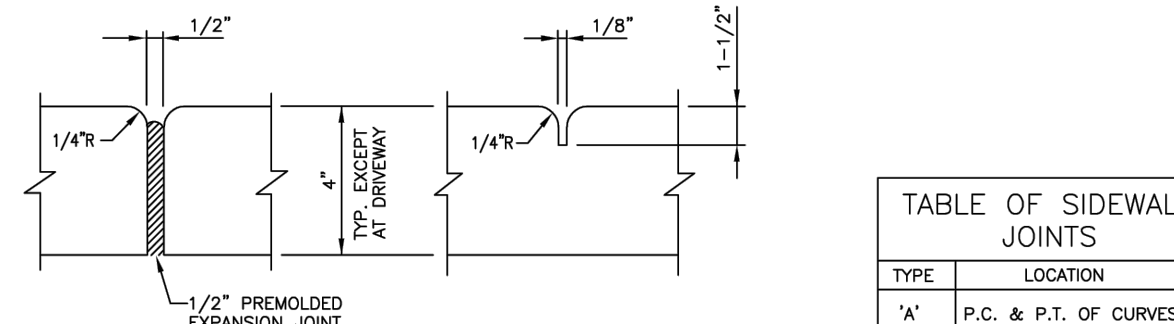


SIDEWALK WITH CURB

	ISSUED: MAY 2023	DEPARTMENT OF DEVELOPMENT SERVICES ENGINEERING, TRANSPORTATION & MOBILITY DIVISION	REVISED: -
	DRAWN: EG		DRAWING NO.: C-24
	APPROVED: JG	SIDEWALK CONSTRUCTION DETAILS (2 OF 3)	



PLAN



TYPE 'A'

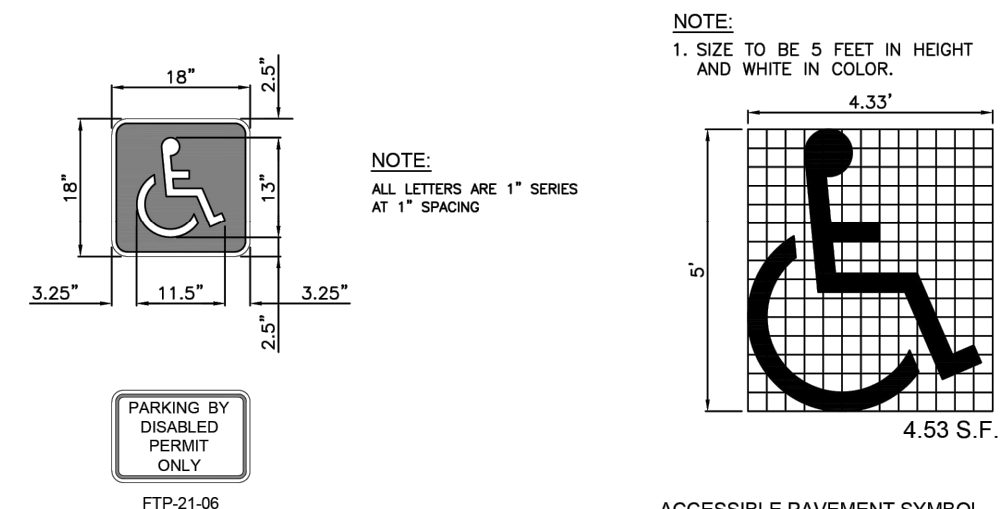
TYPE 'B'

SIDEWALK JOINTS

TYPE	LOCATION
'A'	P.C. & P.T. OF CURVES; JUNCTION OF EXISTING AND NEW SIDEWALKS; WHERE SIDEWALK ABUTS CONCRETE CURBS, DRIVEWAYS AND SIMILAR STRUCTURES.
'B'	5' CENTER TO CENTER ON SIDEWALKS.

- NOTES:
1. CONCRETE SHALL BE CLASS 1 WITH MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI AT 28 DAYS.
 2. USE OF FIBER REINFORCED CEMENT IS PROHIBITED.
 3. SIDEWALK LONGITUDINAL AND CROSS SLOPES SHALL MEET ADA STANDARDS.
 4. SIDEWALK CURB RAMP SHALL BE PROVIDED AT ALL DESIGNATED PEDESTRIAN CROSSING AT INTERSECTIONS PER FOOT STANDARD PLANS INDEX NO. 522-002.
 5. THE VERTICAL DEVIATION OF THE CONVEYOR OF A GIVEN UTILITY BOX/STRUCTURE SHALL NOT BE MORE THAN A 3/4" DIFFERENCE IN HEIGHT/ELEVATION OF THE FINISHED SIDEWALK SURFACE.
 6. 4" THICK MINIMUM (TYP.); 6" THICK AT DRIVEWAYS, EXTENDED TWO FEET ON BOTH SIDES BEYOND THE DRIVE.

	ISSUED: MAY 2023	DEPARTMENT OF DEVELOPMENT SERVICES ENGINEERING, TRANSPORTATION & MOBILITY DIVISION	REVISED: -
	DRAWN: EG		DRAWING NO.: C-25
	APPROVED: JG	SIDEWALK CONSTRUCTION DETAILS (3 OF 3)	

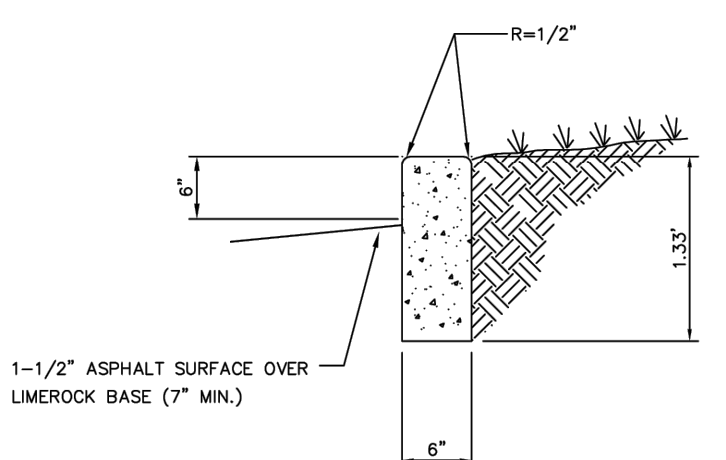


ACCESSIBLE PAVEMENT SYMBOL

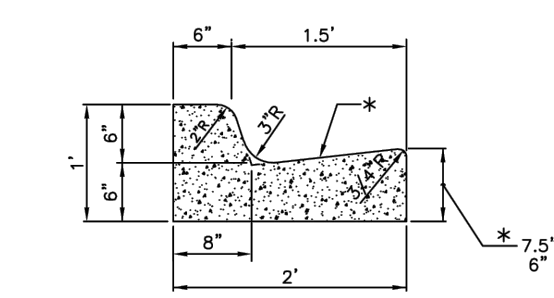
- SIGNAGE NOTES:
1. FTP-21-06 & FTP-22-06 SIGN SHALL BE IN ACCORDANCE WITH FOOT STANDARD PLANS INDEX 700-102.
 2. SIGN POST SHALL BE IN ACCORDANCE WITH BROWARD COUNTY TRAFFIC ENGINEERING DIVISION "GROUND SIGN ASSEMBLY DETAILS", LATEST REVISION.

- NOTES:
1. PROVIDE PAVEMENT SYMBOL IN ACCESSIBLE PARKING SPACES. THE SYMBOL SHALL BE WHITE IN COLOR PER ADA STANDARDS.
 2. BLUE MARKINGS SHALL BE SHADED.
 3. SLOPES NO GREATER THAN 1:48 SHALL BE PERMITTED IN ALL DIRECTIONS FOR BOTH THE ACCESSIBLE PARKING SPACE AND ACCESSIBLE PARKING ACCESS AISLE.
 4. ALL RAMP, SIDEWALK, CURB RAMP, AND ACCESSIBLE ROUTES SHALL BE ADA COMPLIANT.
 5. WIDTH MEASUREMENTS OF PARKING SPACES AND ACCESS AISLES SHALL BE MADE FROM THE CENTERLINE OF THE WHITE PAVEMENT MARKINGS. HOWEVER, WHEN PARKING SPACES OR ACCESS AISLES ARE ADJACENT TO A CURB OR EDGE OF PAVEMENT AND NOT ADJACENT TO ANOTHER PARKING SPACE OR ACCESS AISLE, THEN THE WIDTH MEASUREMENTS MAY INCLUDE THE FULL WIDTH OF THE LAST PAVEMENT MARKING.
 6. ALL ACCESSIBLE ELEMENTS SHALL BE COMPLIANT WITH THE DEPARTMENT OF JUSTICE 2010 ADA STANDARDS FOR ACCESSIBLE DESIGN AND THE FLORIDA BUILDING CODE LATEST EDITION FOR "ACCESSIBILITY".
 7. ALL PARKING PAVEMENT MARKINGS SHALL BE 4" REFLECTORIZED PAINT MEETING FOOT/BIKE STANDARDS.
 8. ACCESSIBLE PARKING SIGNS SHALL BE 60" MINIMUM ABOVE FINISH FLOOR OR GROUND SURFACE MEASURED FROM BOTTOM OF THE SIGN.

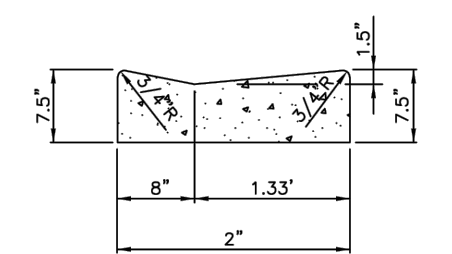
	ISSUED: MAY 2023	DEPARTMENT OF DEVELOPMENT SERVICES ENGINEERING, TRANSPORTATION & MOBILITY DIVISION	REVISED: -
	DRAWN: EG		DRAWING NO.: C-21B
	APPROVED: JG	ACCESSIBLE PARKING SPACE DETAILS (2 OF 2)	



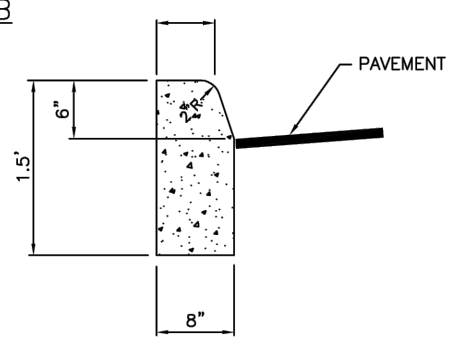
STANDARD CURB



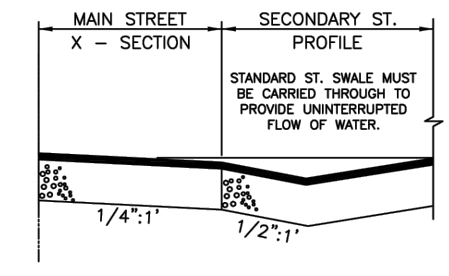
6" CURB & GUTTER (FDOT TYPE "F")



DRIVEWAY CURB & GUTTER



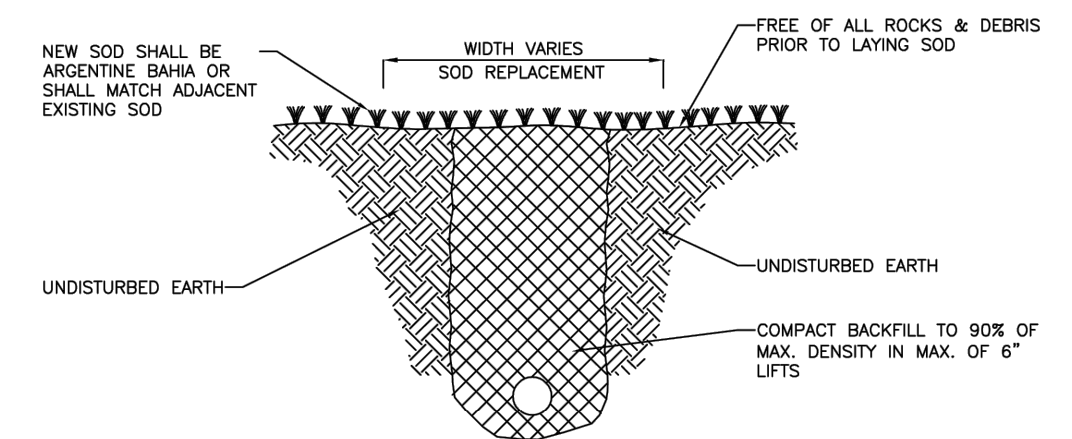
TYPE 'D' CURB



VALLEY GUTTER

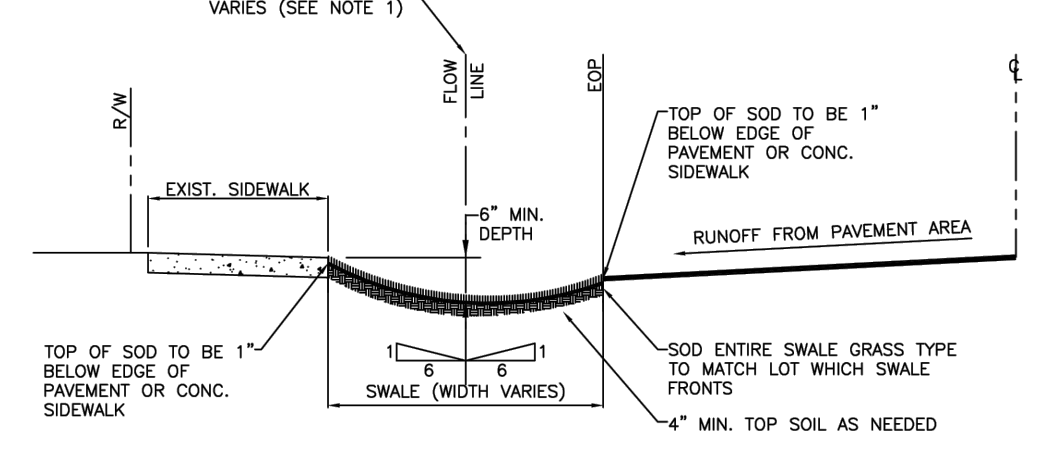
- * WHEN USED ON HIGH SIDE OF ROADWAYS, THE CROSS SLOPE OF THE GUTTER SHALL MATCH THE CROSS SLOPE OF THE ADJACENT PAVEMENT. THE THICKNESS OF THE LP SHALL BE 6", UNLESS OTHERWISE SHOWN ON PLANS.

	ISSUED: MAY 2023	DEPARTMENT OF DEVELOPMENT SERVICES ENGINEERING, TRANSPORTATION & MOBILITY DIVISION	REVISED: -
	DRAWN: EG		DRAWING NO.: C-26
	APPROVED: JG	CURB AND GUTTER DETAILS	



TURF RESTORATION DETAIL

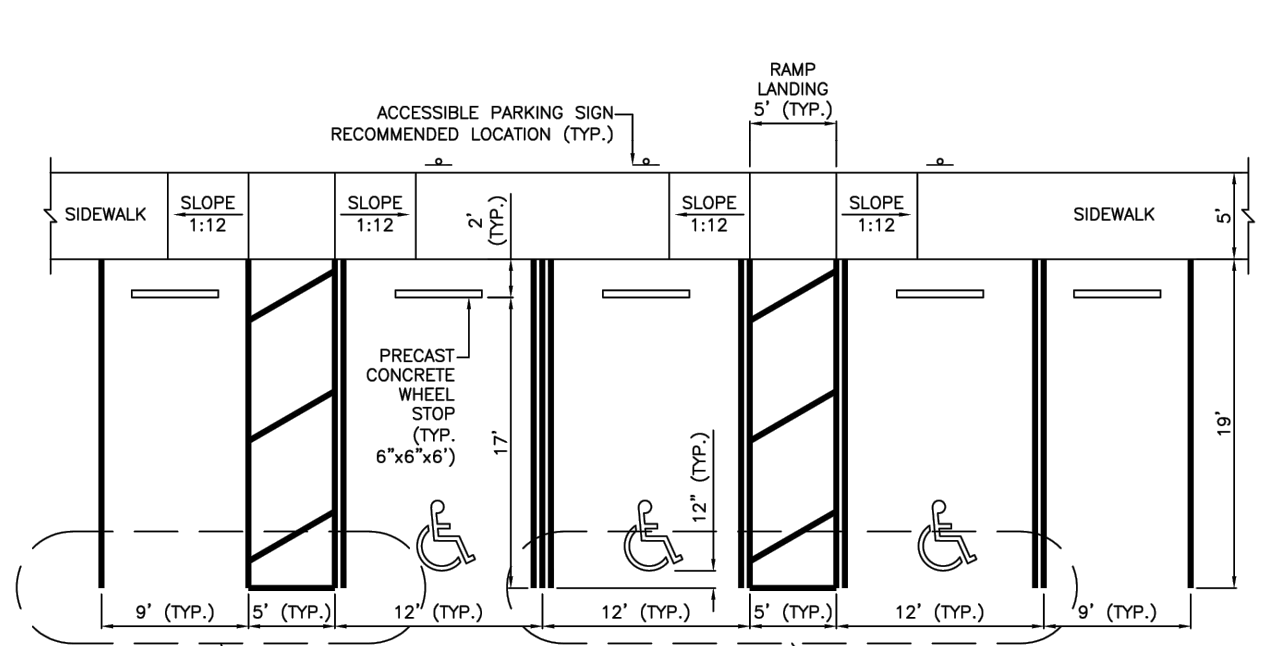
	ISSUED: MAY 2023	DEPARTMENT OF DEVELOPMENT SERVICES ENGINEERING, TRANSPORTATION & MOBILITY DIVISION	REVISED: -
	DRAWN: EG		DRAWING NO.: C-32
	APPROVED: JG	TURF RESTORATION DETAIL	



- NOTES:
1. HORIZONTAL BOTTOM (FLOW LINE) OF SWALE LOCATION IS TO BE PLACED AT THE POINT WHERE 6:1 SLOPE FROM THE EDGE OF S/W AND 6:1 SLOPE FROM EDGE OF PAVEMENT MEET.
 2. TOP OF SOD ELEVATION AS SHOWN ABOVE.

	ISSUED: MAY 2023	DEPARTMENT OF DEVELOPMENT SERVICES ENGINEERING, TRANSPORTATION & MOBILITY DIVISION	REVISED: -
	DRAWN: EG		DRAWING NO.: C-33
	APPROVED: JG	TYPICAL SWALE GRADING	

ACCESSIBLE PARKING SPACE COMPLYING WITH FLORIDA AND ADA REQUIREMENTS (*)



- (*) IN CASE OF CONFLICT PLANS TO COMPLY WITH ADA 406 & 502, AND FDOT 711-001 (SHEET 11)

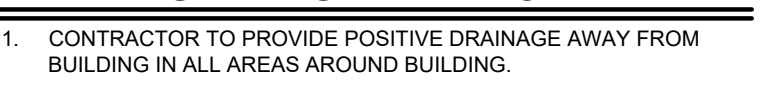
	ISSUED: MAY 2023	DEPARTMENT OF DEVELOPMENT SERVICES ENGINEERING, TRANSPORTATION & MOBILITY DIVISION	REVISED: -
	DRAWN: EG		DRAWING NO.: C-21A
	APPROVED: JG	ACCESSIBLE PARKING SPACE DETAILS (1 OF 2)	

No.	Date	Revision
1	10-16-2025	PLAN REVISIONS

Designed: R.O
Drawn: J.C.
Checked: R.O.
Job No.: 2500922
Date: 10/16/2025
© 2025

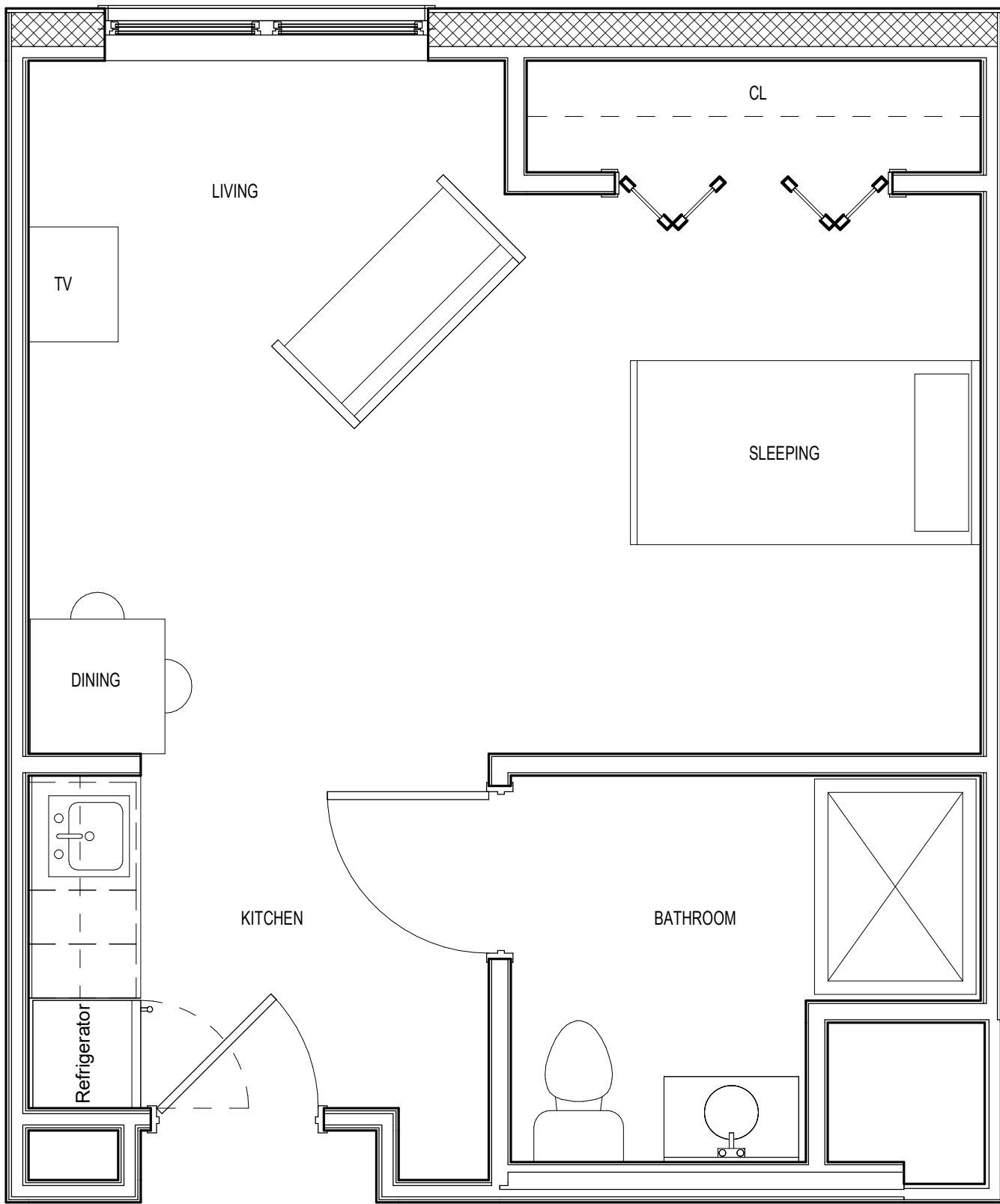
HARDSCAPE DETAILS II
PRESIDENTIAL PLACE
3880 SOUTH CIRCLE DRIVE
CITY OF HOLLYWOOD, FLORIDA 33021
BROWARD COUNTY, FLORIDA

THIS SHEET NOT VALID FOR
CONSTRUCTION WITHOUT
COMPLETE SET OF PLANS.
SEE GENERAL NOTES FOR
MASTER LEGEND.

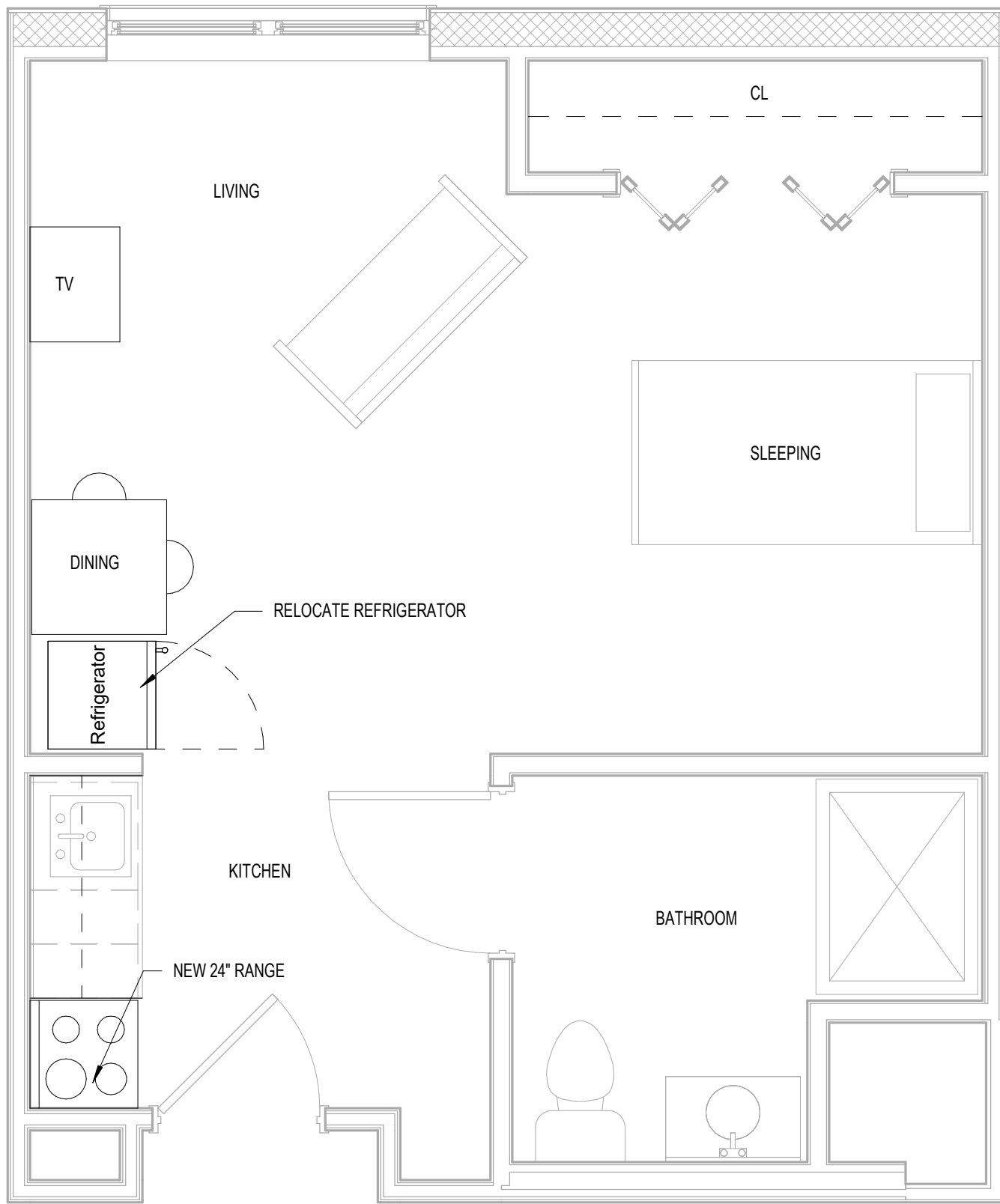




CURRENT KITCHEN ELEVATION



1 ARBORWOOD - EXISTING UNIT PLAN
3/8" = 1'-0"



2 ARBORWOOD - PROPOSED UNIT PLAN
3/8" = 1'-0"



All reproduction & intellectual property rights reserved © 2023

KINGS RESERVE - HOLLYWOOD

3880 S CIRCLE DRIVE, HOLLYWOOD, FL 33021

CONCEPTUAL DESIGN PACKAGE

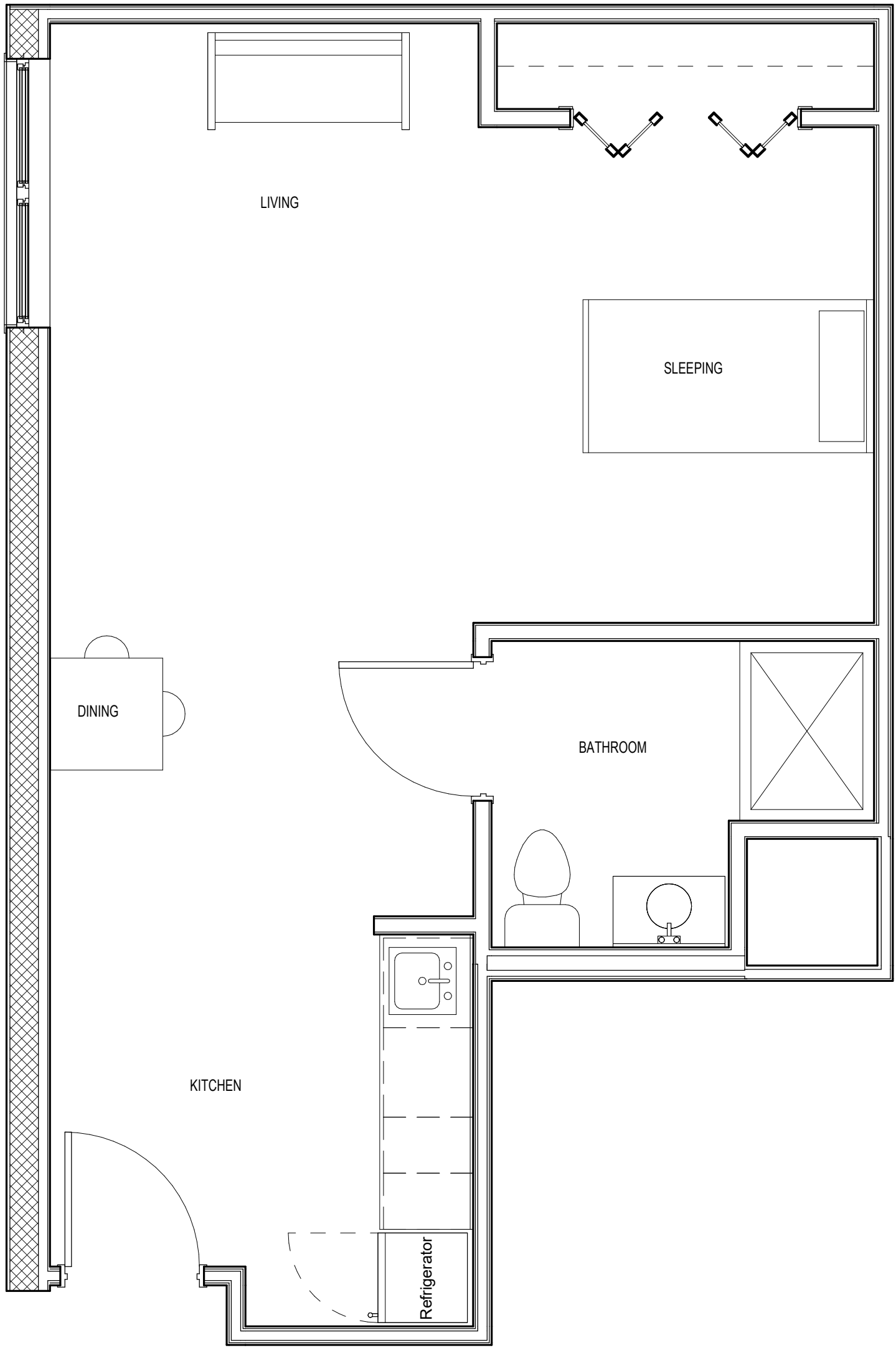
ARBORWOOD UNIT PLANS

Sheet No.
SD1.0
06/04/25

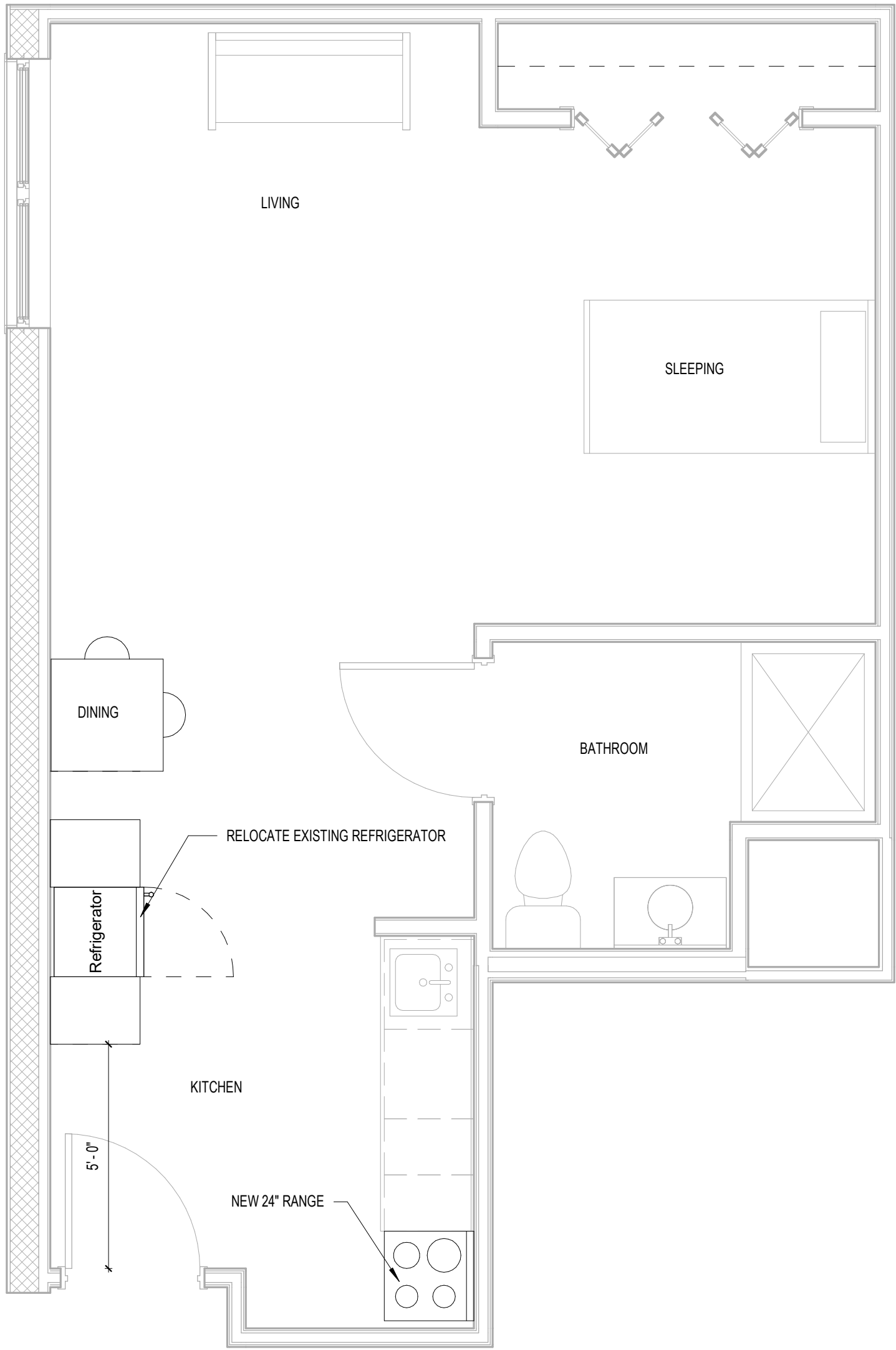




EXISTING KITCHEN ELEVATION



① DOGWOOD - EXISTING UNIT PLAN
3/8" = 1'-0"



② DOGWOOD - PROPOSED UNIT PLAN
3/8" = 1'-0"



All reproduction & intellectual property rights reserved © 2023

KINGS RESERVE - HOLLYWOOD

3880 S CIRCLE DRIVE, HOLLYWOOD, FL 33021

CONCEPTUAL DESIGN PACKAGE

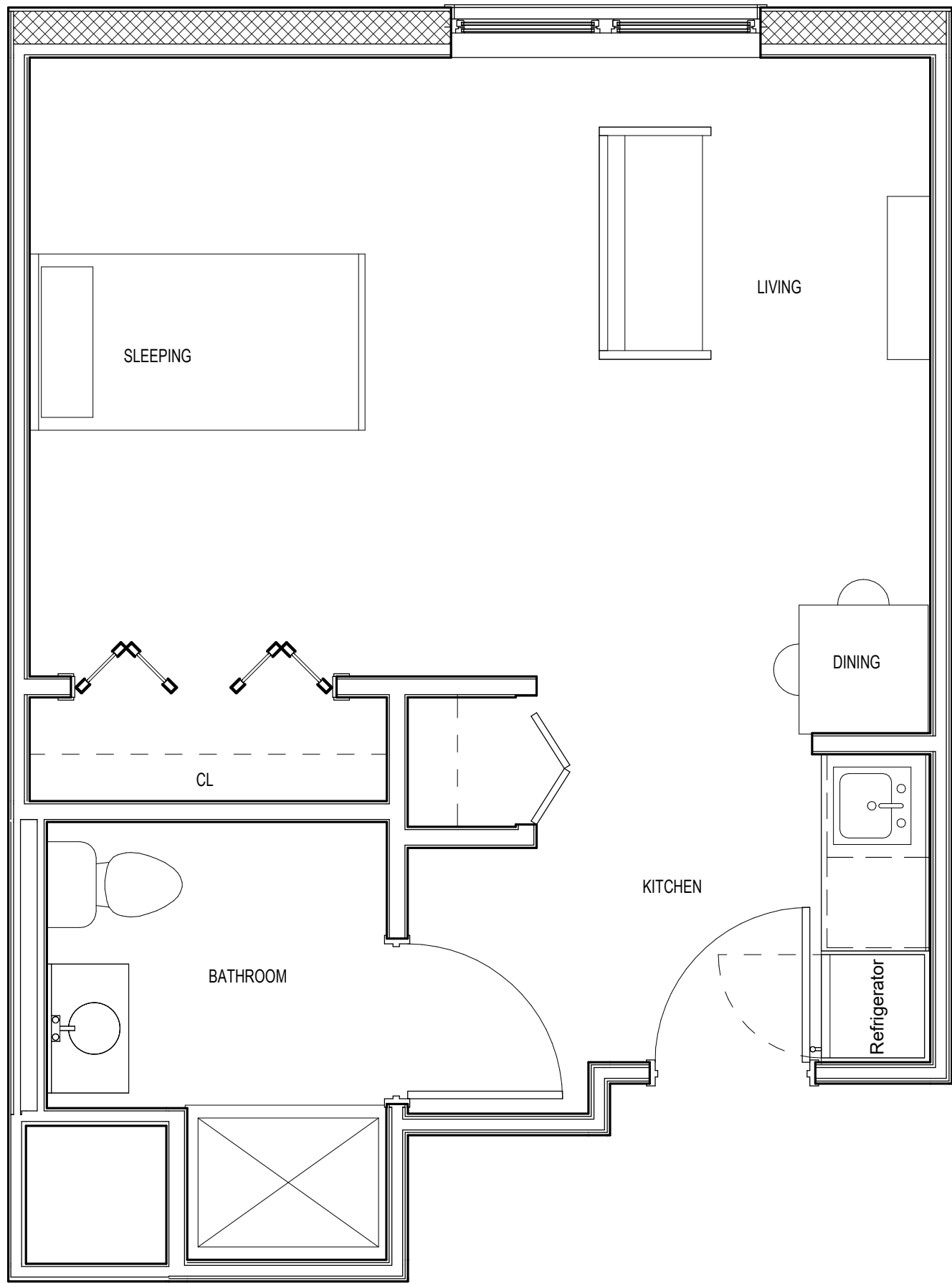
DOGWOOD UNIT PLANS

Sheet No.
SD1.1
06/02/25

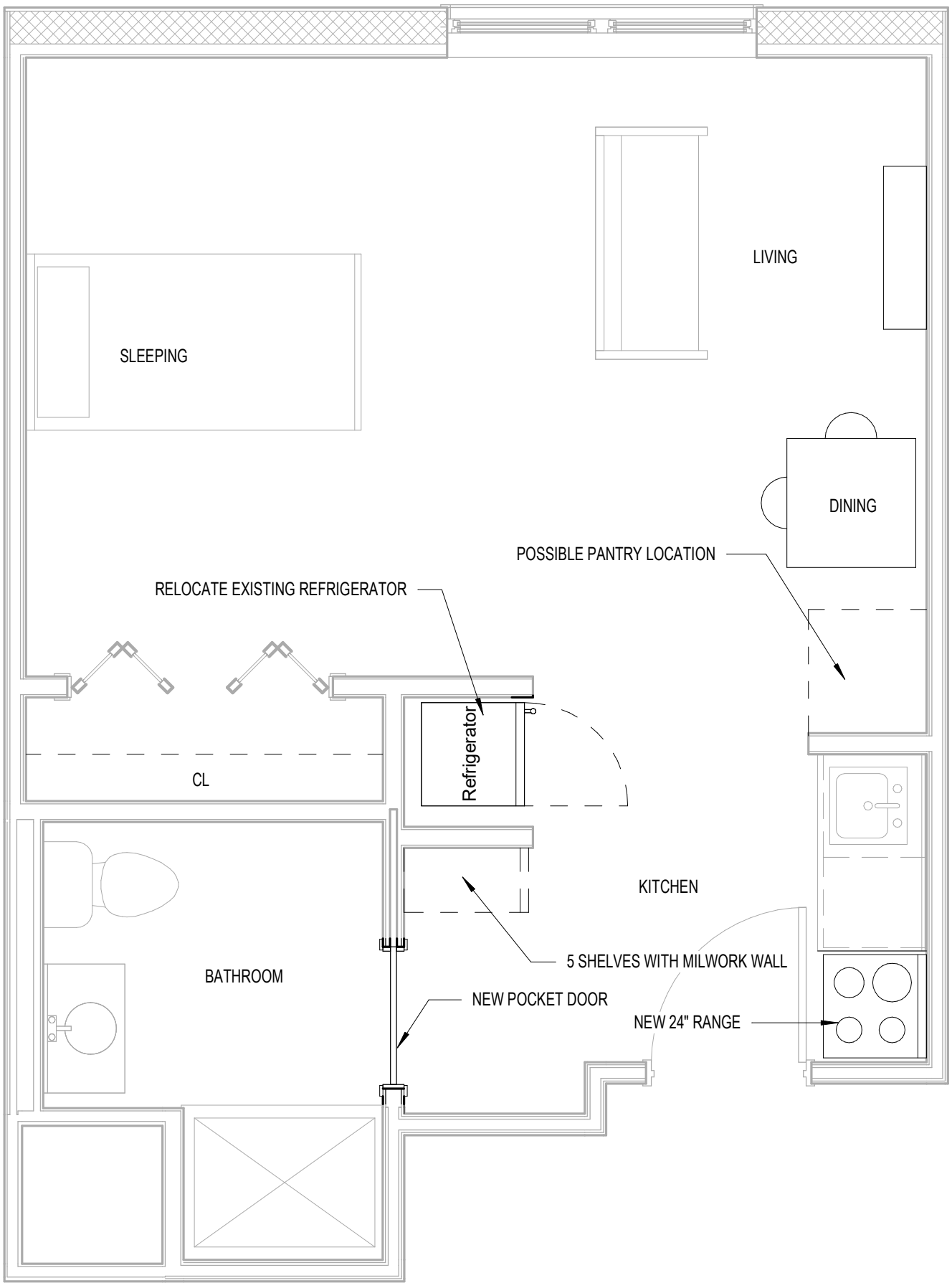




EXISTING KITCHEN ELEVATION



① BEECHWOOD - EXISTING UNIT PLAN
3/8" = 1'-0"



② BEECHWOOD - PROPOSED UNIT PLAN
3/8" = 1'-0"



All reproduction & intellectual property rights reserved © 2023

KINGS RESERVE - HOLLYWOOD

3880 S CIRCLE DRIVE, HOLLYWOOD, FL 33021

CONCEPTUAL DESIGN PACKAGE

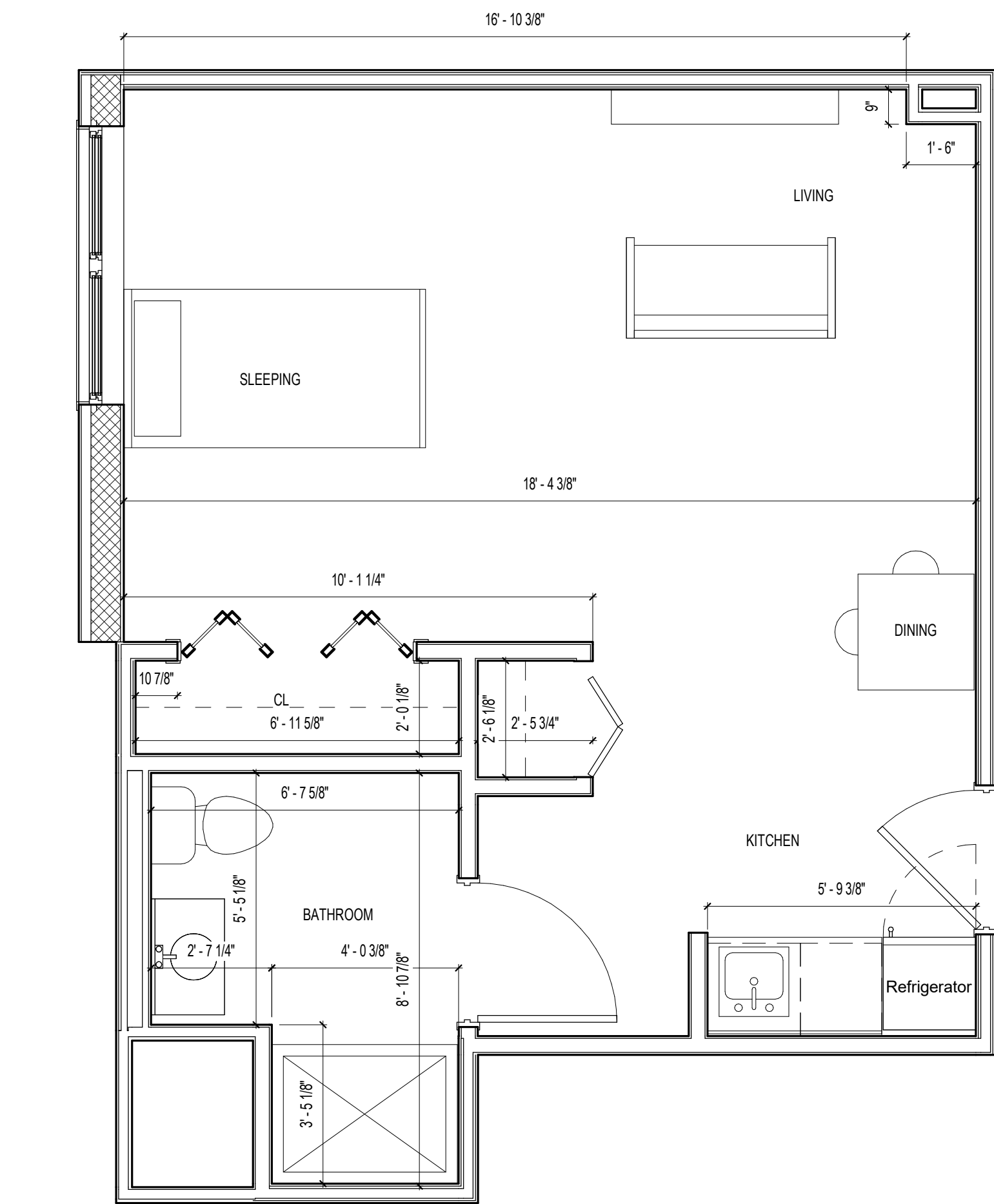
BEECHWOOD UNIT PLANS

Sheet No.
SD1.2
06/02/25

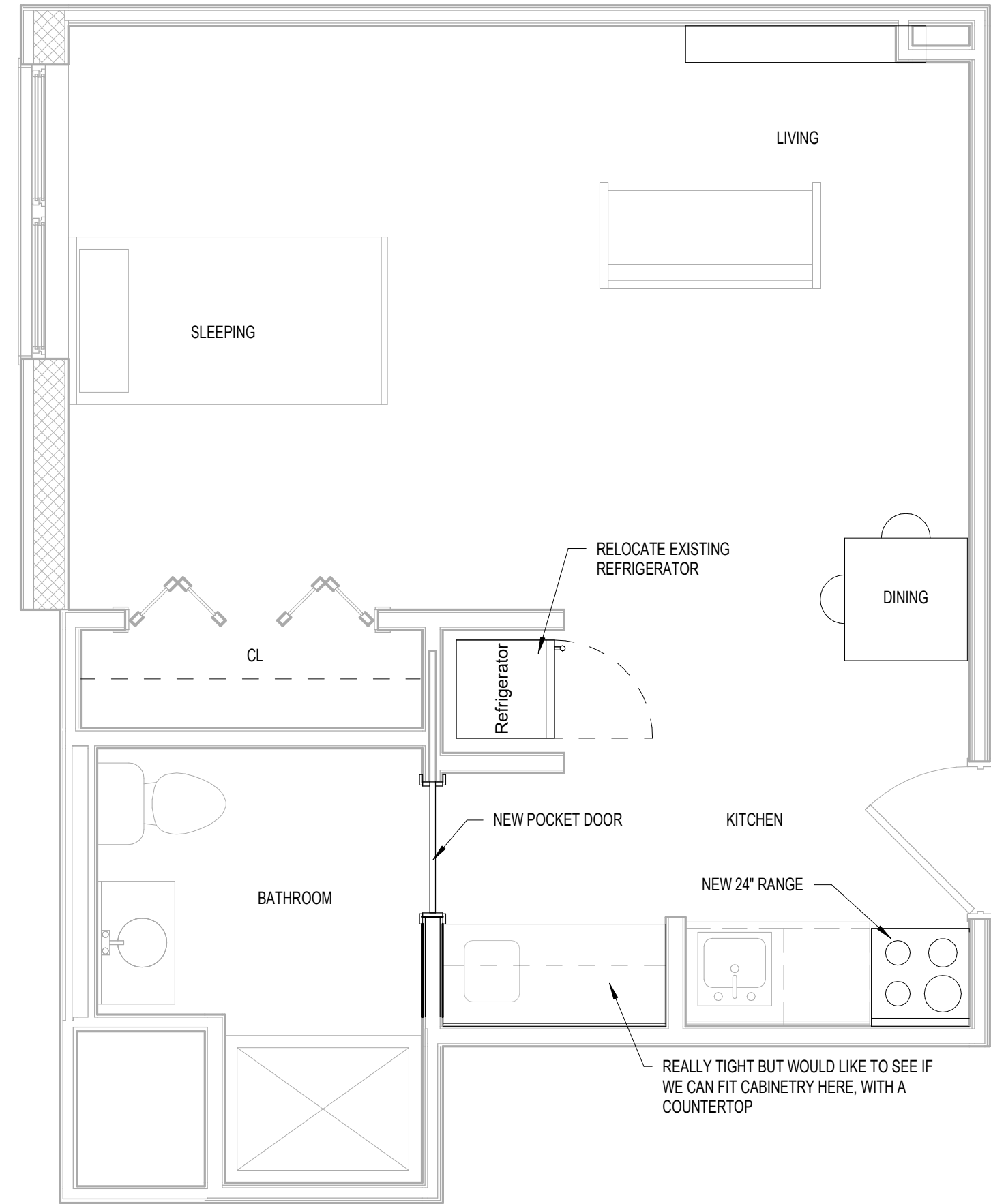




EXISTING KITCHEN ELEVATION



① CYPRESS - EXISTING UNIT PLAN
3/8" = 1'-0"



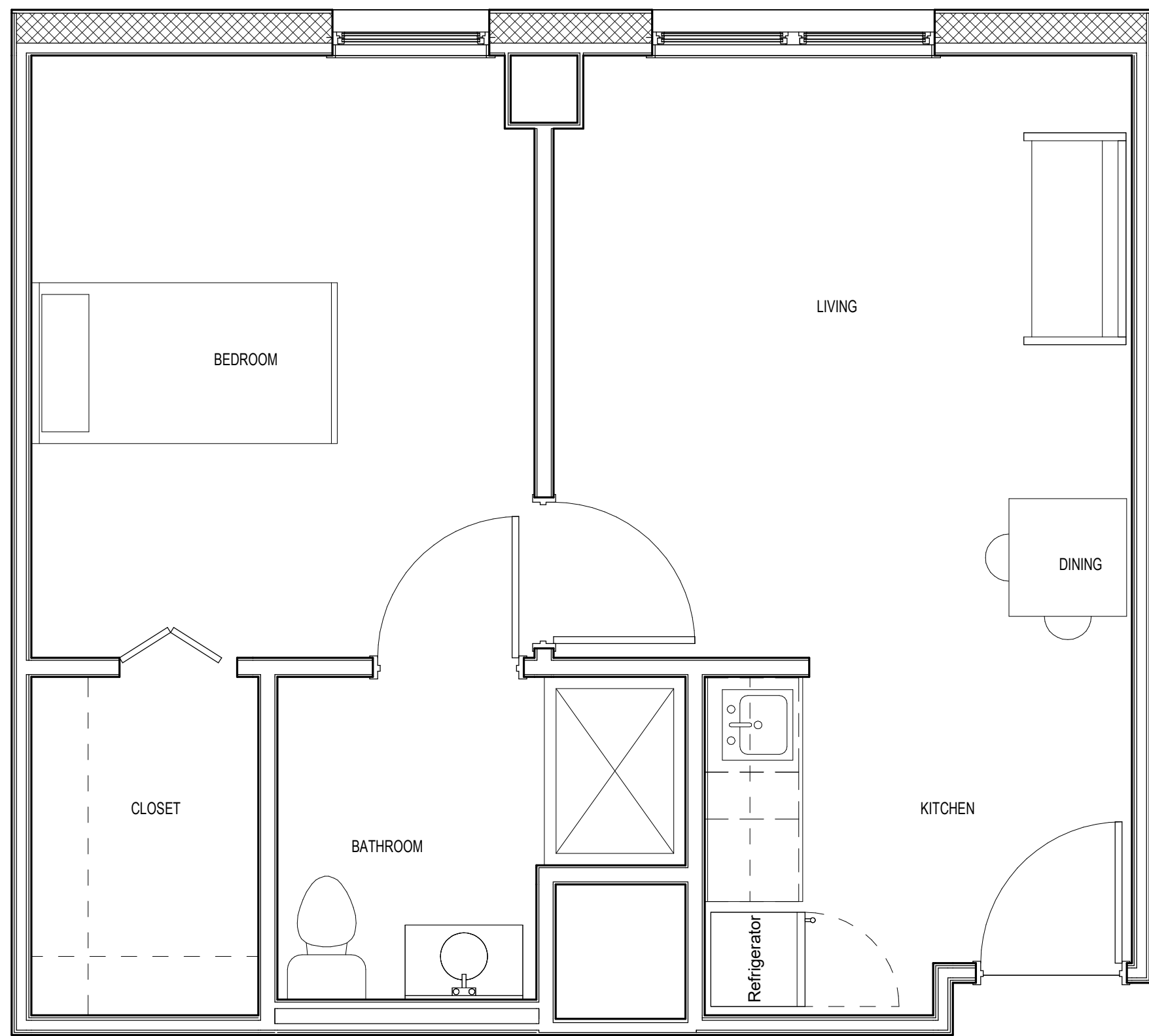
② CYPRESS - PROPOSED UNIT PLAN
3/8" = 1'-0"



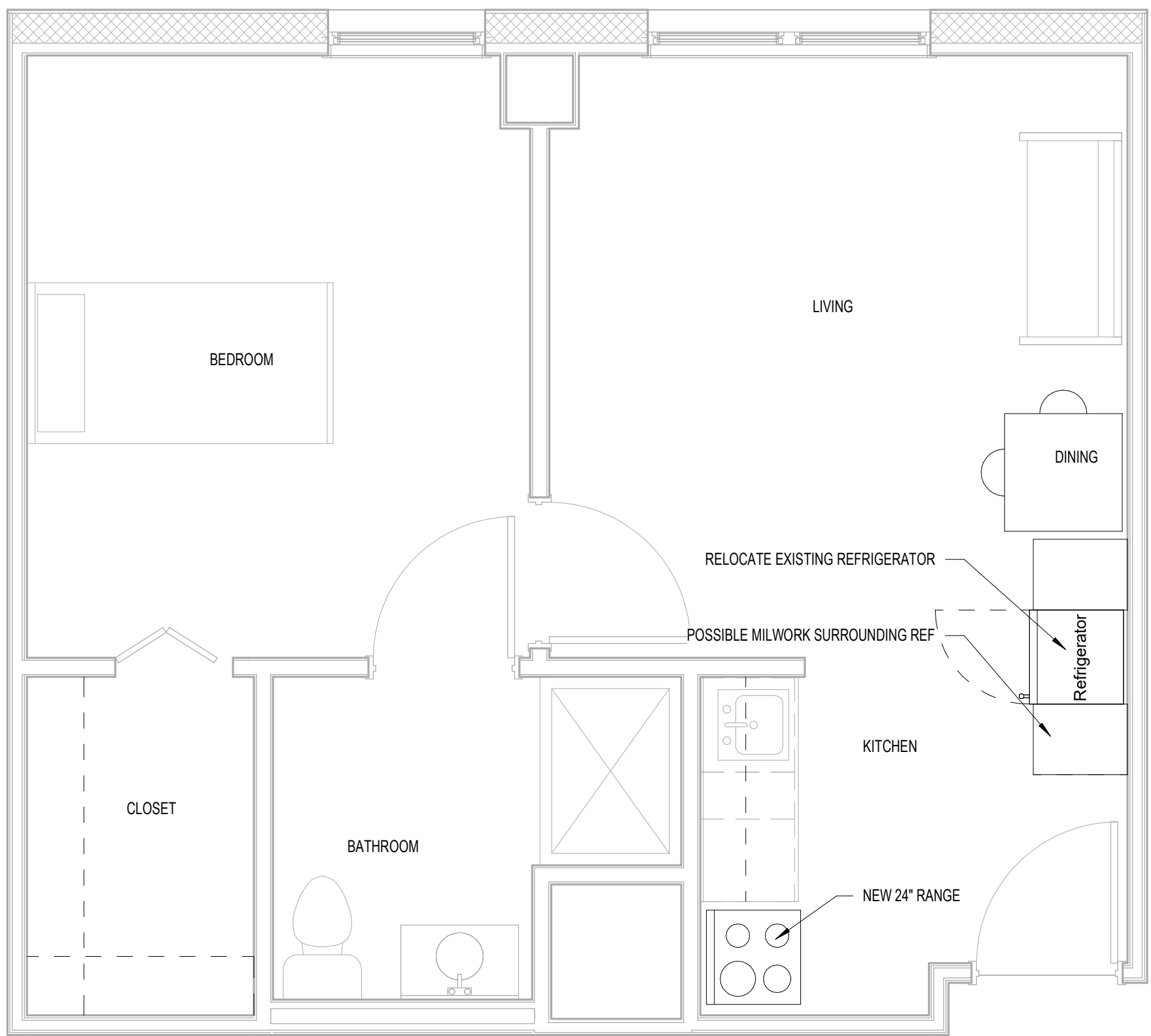
All reproduction & intellectual property rights reserved © 2023



EXISTING KITCHEN ELEVATION



1 EVERGREEN - EXISTING UNIT PLAN
3/8" = 1'-0"



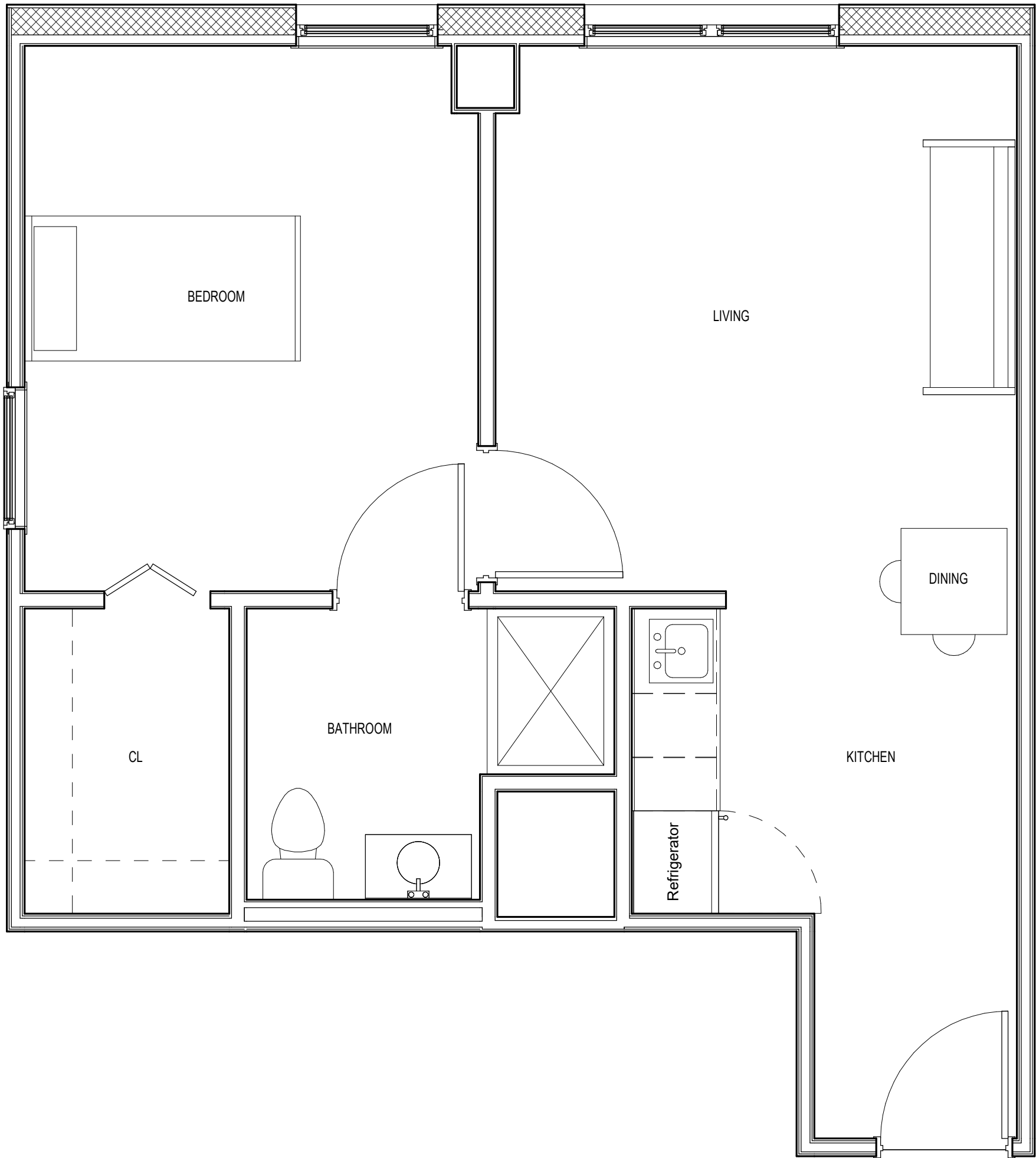
2 EVERGREEN - EXISTING UNIT PLAN
3/8" = 1'-0"



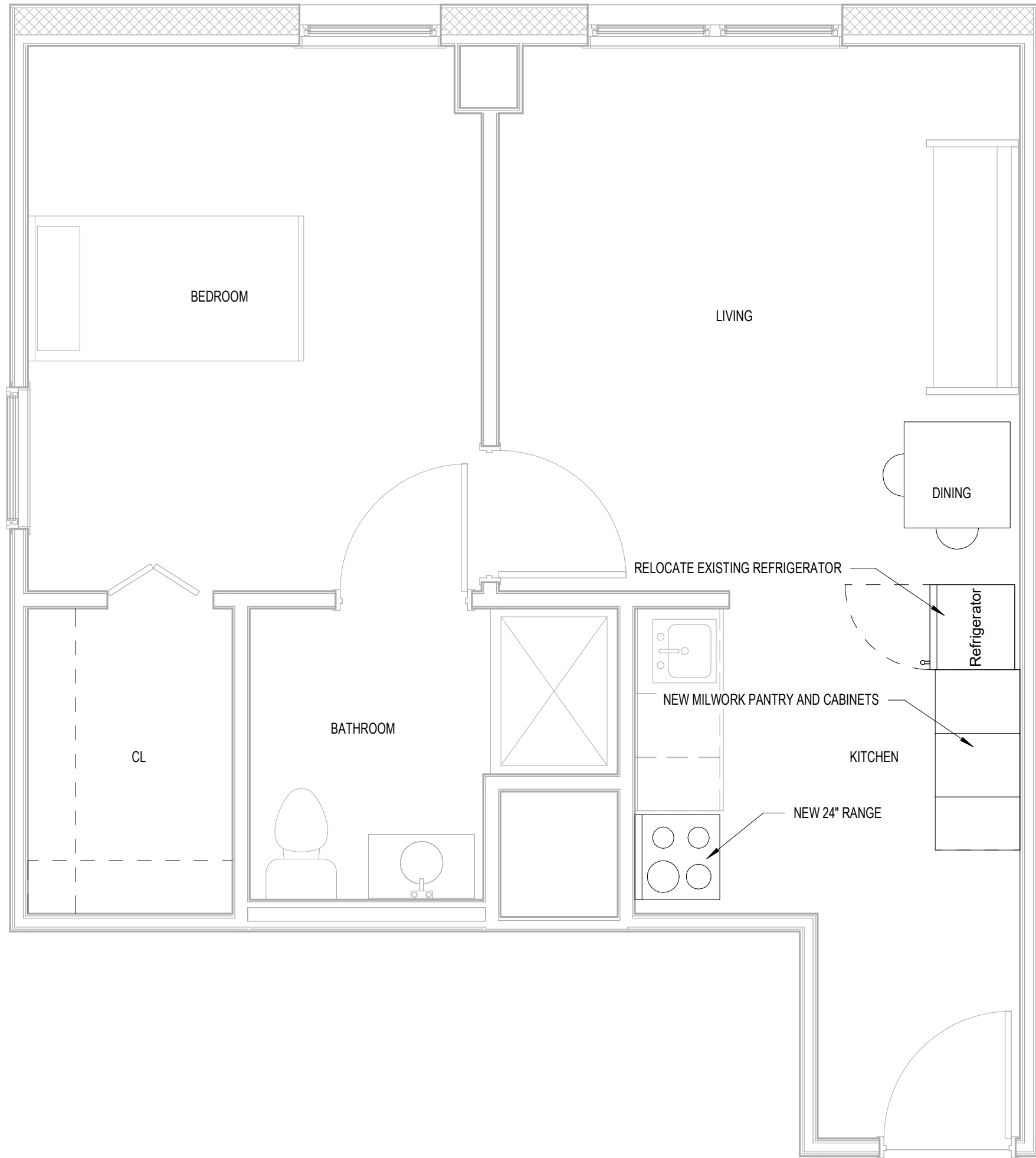
All reproduction & intellectual property rights reserved © 2023



EXISTING KITCHEN ELEVATION



① FORESTBROOK - EXISTING UNIT PLAN
3/8" = 1'-0"



② FORESTBROOK - PROPOSED UNIT PLAN
3/8" = 1'-0"



All reproduction & intellectual property rights reserved © 2023

KINGS RESERVE - HOLLYWOOD

3880 S CIRCLE DRIVE, HOLLYWOOD, FL 33021

CONCEPTUAL DESIGN PACKAGE

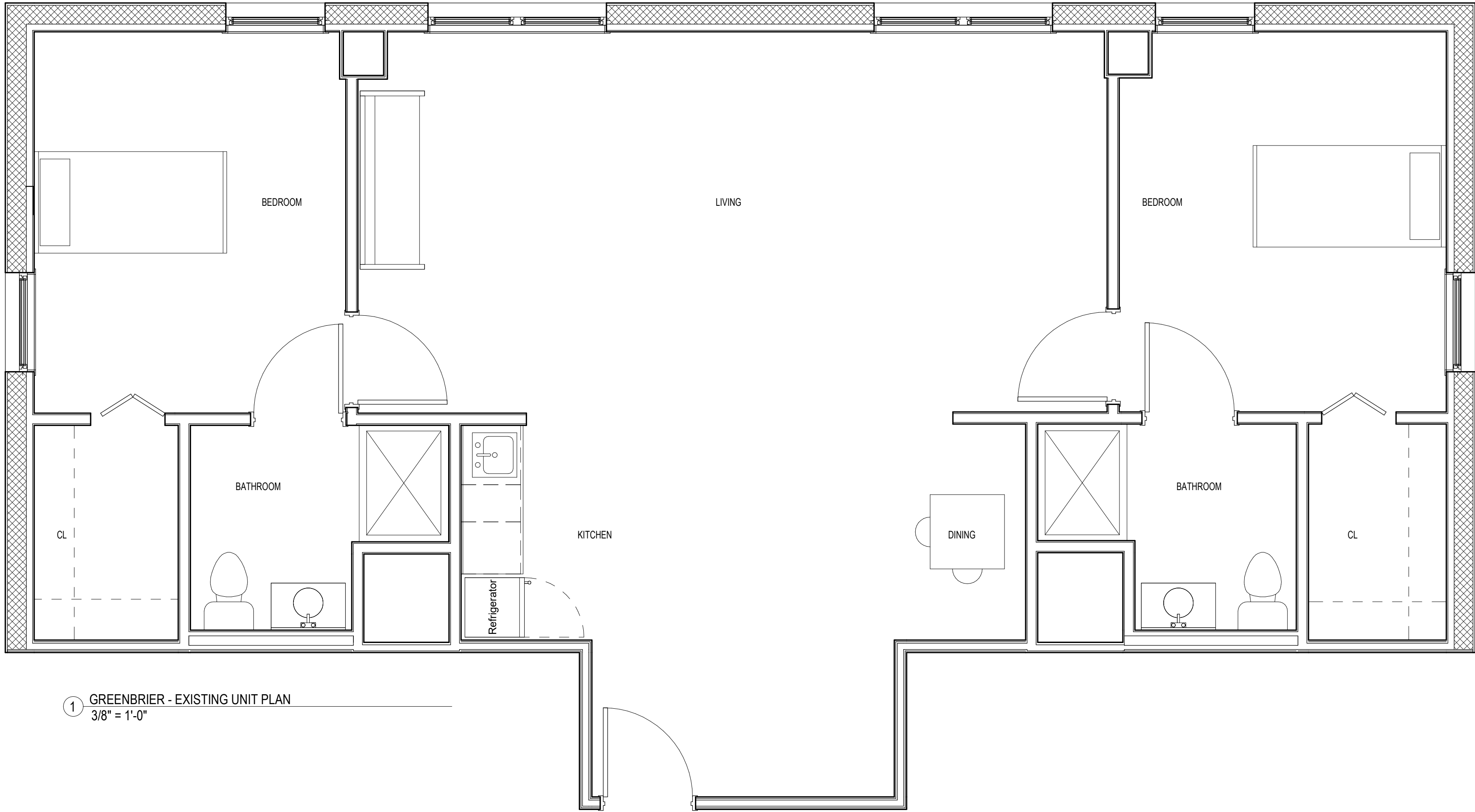
FORESTBROOK UNIT PLANS

Sheet No.
SD1.5
06/02/25

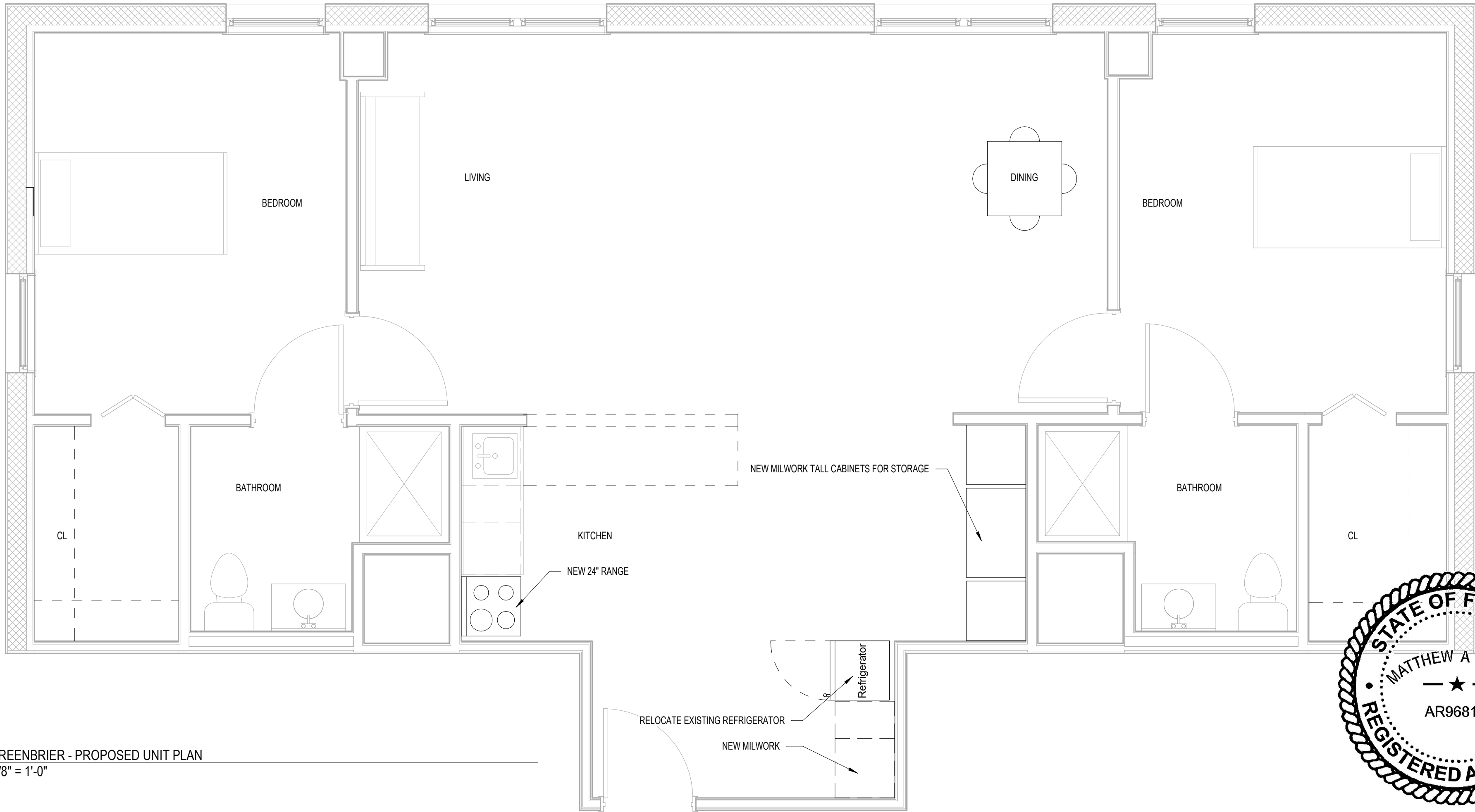




EXISTING KITCHEN ELEVATION



1 GREENBRIER - EXISTING UNIT PLAN
3/8" = 1'-0"



2 GREENBRIER - PROPOSED UNIT PLAN
3/8" = 1'-0"



All reproduction & intellectual property rights reserved © 2023

MEMORANDUM

To: Clarissa Ip, P.E.
City of Hollywood

From: Adrian K. Dabkowski, P.E., PTOE
Ariel B. Centurion, P.E.

AK
ABC

Date: October 16, 2025

**Subject: 3880 South Circle Drive
Traffic Assessment
Hollywood, Florida**

The purpose of this memorandum is to summarize the traffic assessment for the repurposing of the property located at 3880 South Circle Drive in Hollywood, Florida. The property proposed for repurposing is currently occupied by a 118-bed assisted living facility. The proposed repurposing will replace the assisted living facility with a 104-unit senior multifamily housing facility along with additional parking spaces in existing unpaved areas. Note that no demolition or modifications are proposed to the existing structure, existing interior layout, or existing loading operations. The planned renovations include upgrading the unit finishes and expanding their kitchens. In addition to maintaining the existing driveway along South Circle Drive, the repurposing will provide access to additional parking along South Crescent Drive and South Rainbow Drive adjacent to the site via one (1) ingress-only driveway and one (1) egress-only driveway along each roadway. The anticipated buildout year of the repurposing is 2026.

A location map and site plan are included in Attachment A. This traffic assessment has been completed consistent with the conditionally approved methodology. The conditionally approved methodology detailing the traffic assessment requirements is included in Attachment B. The following sections summarize the project trip generation calculations, data collection, trip distribution and assignment, and parking analysis.

TRIP GENERATION

Trip generation calculations for the existing development and proposed repurposing were performed using the Institute of Transportation Engineers' (ITE) *Trip Generation Manual*, 12th Edition. The trip generation for the existing development was determined using ITE Land Use Code (LUC) 254 (Assisted Living). The trip generation for the proposed repurposing was determined using ITE LUC 252 (Senior Adult Housing).

A multimodal (public transit, bicycle, and pedestrian) factor based on US Census *Means of Transportation to Work* data was reviewed for the census tract in the vicinity of the redevelopment. The US Census data indicated that there is a 3.6% percent (3.6%) multimodal factor within the vicinity of the redevelopment. It is expected that a portion of residents and guests will choose to walk, bike, or use public transit to and from the proposed redevelopment. US Census data is provided in Attachment D.

Sidewalks are currently provided along the north, east, and west sides of the site. Additionally, one (1) Broward County Transit (BCT) route currently operates in close proximity to the site during the A.M. and P.M. peak hours. Detailed transit route information is included in Attachment C.

- **BCT Route 7** operates along Hollywood Boulevard in the vicinity of the project site with the nearest stop located east of S Rainbow Drive. This route operates with approximately 30-minute headways in the eastbound and westbound directions during the A.M. and P.M. peak hours.

As shown in Table 1, the project is expected to result in a reduction of two (2) net new weekday A.M. peak hour vehicular trips and a reduction of two (2) net new weekday P.M. peak hour vehicular trips. Detailed trip generation information is included in Attachment D.

Table 1: Proposed Net New Trip Generation				
A.M. Peak Hour (P.M. Peak Hour)				
Land Use (ITE Code)	Scale	Entering Trips	Exiting Trips	Net New External Trips
<i>Existing Development</i>				
Assisted Living (254)	118 beds	12 (11)	9 (16)	21 (27)
<i>Proposed Redevelopment</i>				
Senior Adult Housing (252)	104 dwelling units	7 (14)	12 (11)	19 (25)
Net New Project Trips		-5 (3)	3 (-5)	-2 (-2)

DATA COLLECTION

A.M. peak period (7:00 A.M. to 9:00 A.M.) and P.M. peak period (4:00 P.M. to 6:00 P.M.) peak period turning movement counts were collected on October 8, 2025 (Wednesday) at the following intersections:

- South Crescent Drive and South Circle Drive
- South Rainbow Drive and South Circle Drive

All volumes were collected in 15-minute intervals and the peak hour was determined for each intersection. Turning movement counts also included pedestrian and bicycle data. The appropriate Florida Department of Transportation (FDOT) peak season conversion factor (PSCF) of 1.01 was applied to the traffic data. The turning movement counts and FDOT peak season factor category reports are included in Attachment E. Attachment F presents the existing turning movement volumes at the study intersections during the A.M. and P.M. peak hours.

TRIP DISTRIBUTION

The likely distribution of project traffic was forecasted for the trips expected to be generated by the proposed redevelopment. The trip distribution was based on traffic patterns observed in the collected peak period turning movement counts and the split of parking spaces accessible via each project driveway. Attachment F presents the project trip distribution and assignment for the weekday A.M. and P.M. peak hours.

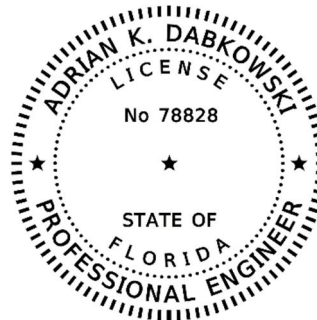
PARKING EVALUATION

A parking analysis was prepared to determine if the proposed parking supply can adequately serve the residents of the proposed repurposing. The site proposes to provide a total of 69 parking spaces or a parking ratio of 0.66 parking spaces per senior multifamily housing unit.

Parking supply, development data, and provided parking ratios (number of parking spaces per residential unit) were gathered for five (5) surrogate sites in south Florida on July 30-31, 2025 to compare the provided parking ratios of those existing facilities to the proposed repurposing. The five (5) senior multifamily housing facilities evaluated include:

- Gardens at Driftwood: 7350 Davie Road Extension, Hollywood, Florida 33024
- Saratoga Crossings: 709 W Dania Beach Boulevard, Dania Beach, Florida 33004
- Steve Protulis Tower: 2495 NW 54th Street, Miami, Florida 33142
- Archbishop Hurley Hall: 632 NW 1st Street, Hallandale Beach, Florida 33009
- Palm Springs Villas: 445 W 51st Place, Hialeah, Florida 33012

The average parking ratio for the surrogate sites is 0.48 parking spaces per unit, with a range of 0.37 to 0.65 parking spaces per unit. Detailed parking data is provided in Attachment G. The proposed repurposing will provide 0.66 spaces per unit. This ratio is greater than all parking ratios among the surrogate sites.



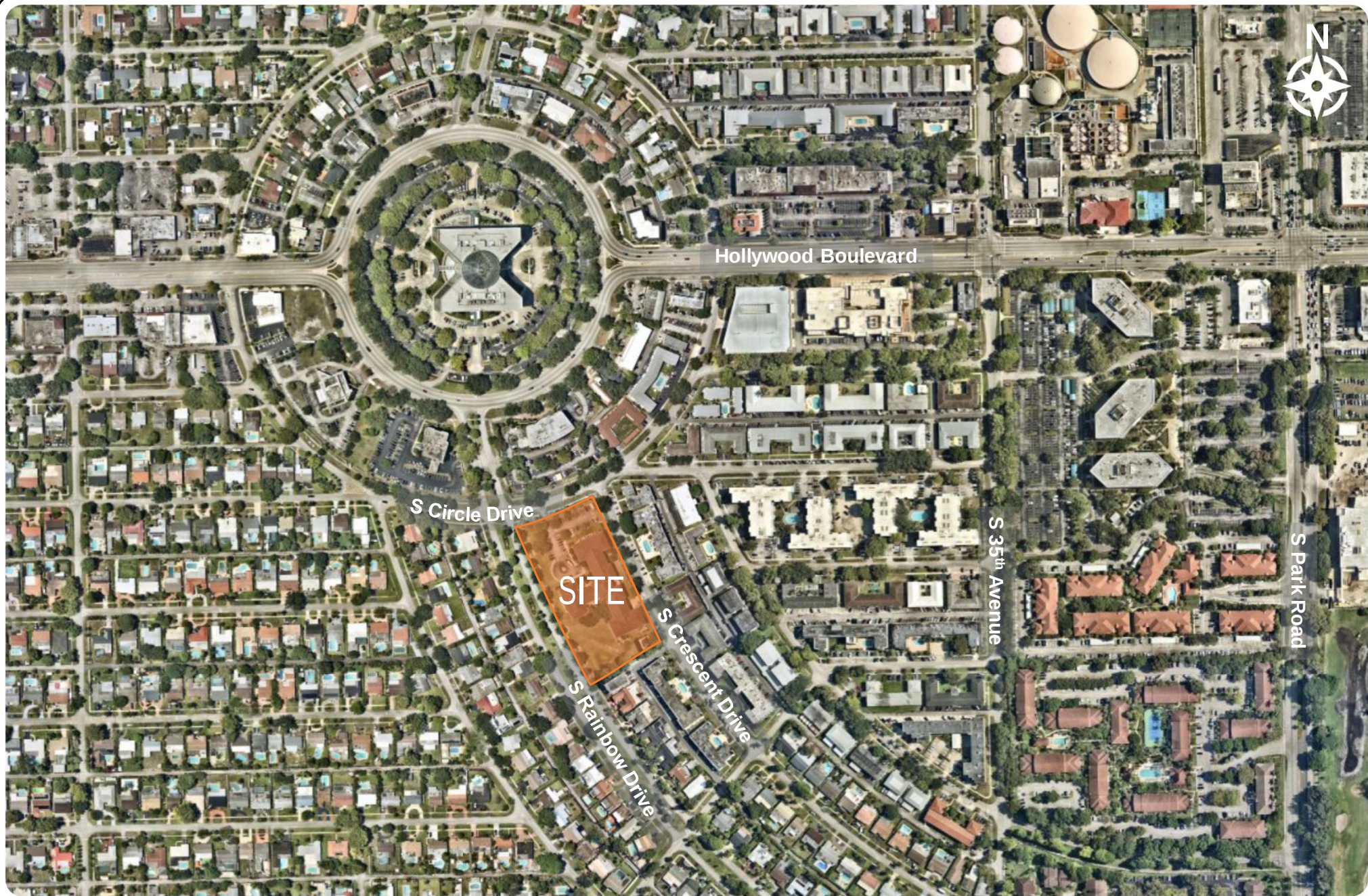
This item has been digitally signed and sealed by Adrian K. Dabkowski, P.E., PTOE, on the date adjacent to the seal.

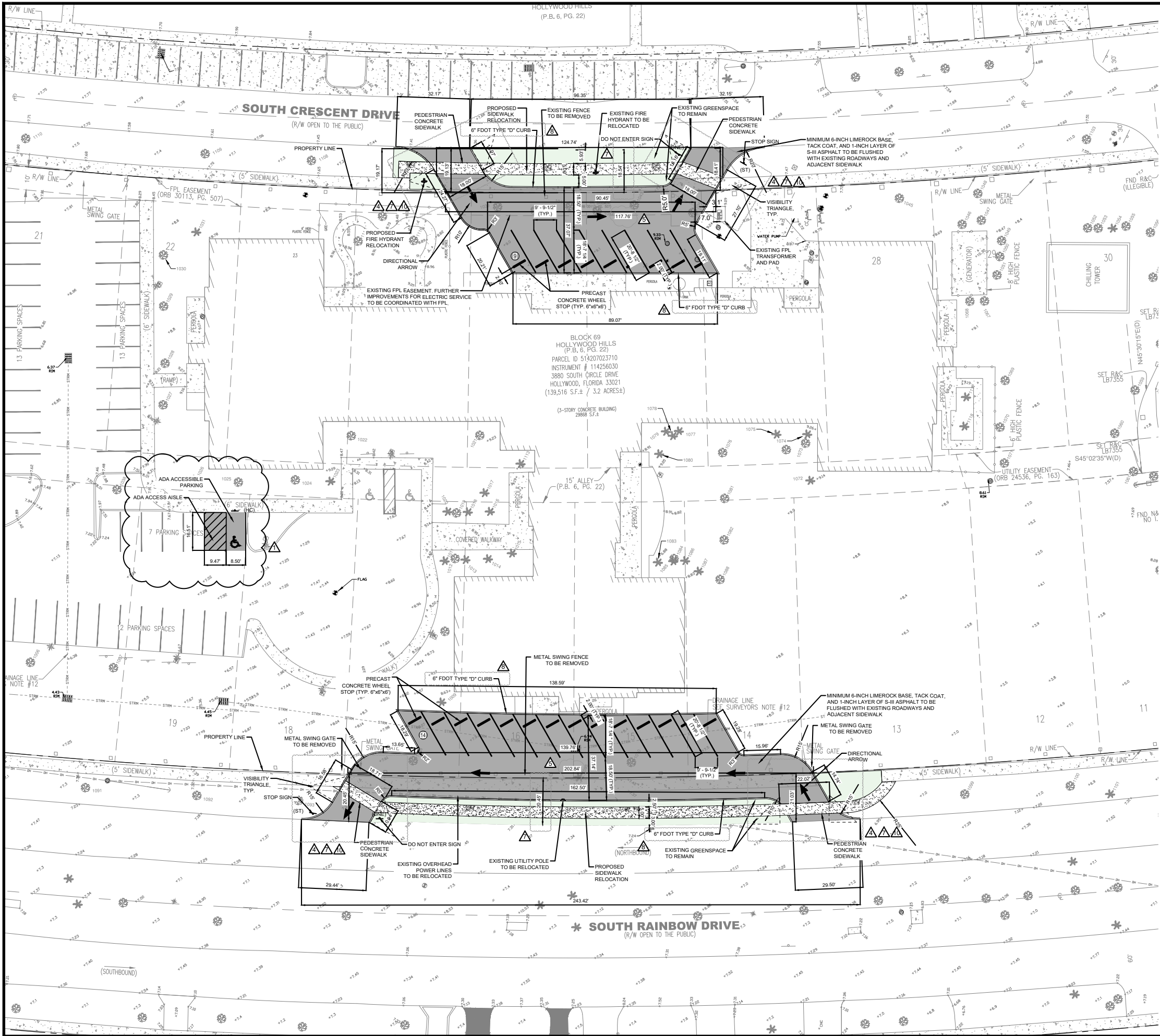
Signature must be verified on any electronic copies.

Adrian K. Dabkowski, P.E., PTOE
Florida Registration Number 78828
Kimley-Horn and Associates, Inc.
8201 Peters Road, Suite 2200
Plantation, Florida 33324

Attachment A

Location Map and Site Plan





SITE DATA

JURISDICTION: CITY OF HOLLYWOOD
ZONING: RM-9 & RM-18
FUTURE LAND USE: RM-18 - RESIDENTIAL (MRES)
OVERLAY DISTRICT: MAXIMUM ALLOWED DENSITY, MEDIUM (16)
ADDRESS: 3880 S. CIRCLE DR. HOLLYWOOD, FL 33021

PARCEL ID: 514207023710
STATEMENT OF INTENT:

ADJACENT ZONING:
NORTH: ROW
EAST: RM-18
SOUTH: RM-18
WEST: ROW

REQUIRED BUILDING SETBACKS:
(NW) FRONT: 20'
(E) SIDE: 15'
(W) SIDE: 15'
(SE) REAR: 20'

PROVIDED BUILDING SETBACKS:
(NW) FRONT: 117.50'
(E) SIDE: 24.00'
(W) SIDE: 29.00'
(SE) REAR: 81.00'

REQUIRED LANDSCAPE STRIP/SETBACKS:
(NW) FRONT: 20'
(E) SIDE: 5'
(W) SIDE: 5'
(SE) REAR: 0'

PROVIDED LANDSCAPE STRIP/SETBACKS:
(NW) FRONT: VARIES
(E) SIDE: VARIES
(W) SIDE: VARIES
(SE) REAR: VARIES

MAX. BUILDING HEIGHT: 65'-0"

REQUIRED PARKING RATIO:

PROPOSED USE: MULTI-FAMILY APARTMENT UNITS
NUMBER OF UNITS: 104
REQUIRED PARKING RATIO: 1.5 SPACES PER UNIT
GUEST PARKING: 1 PER 5 UNITS FOR DEVELOPMENTS WITH OVER 10 UNITS
TOTAL REQUIRED PARKING SPACES (14 SPACES ALLOCATED FOR GUESTS): 172

EXISTING PARKING SPACES (ASAPES BY LONGITUDE SURVEYOR):

ACCESSIBLE: 2

REGULAR: 45

PROPOSED PARKING:

EXISTING ACCESSIBLE SPACES: 2

ACCESSIBLE: 1 (DIMS) = 16.5' X 8.5'

EXISTING REGULAR SPACES: 44 (DIMS) = 16.5' X 8.5' (1 REG. SPACE REMOVED)

REGULAR: 22 (DIMS) = 20.7' X 8.5'

TOTAL: 66

PROPOSED REGULAR PARKING GEOMETRICS (SEE DETAIL C-16):

A - PARKING ANGLE = 60-DEG.

B - STALL WIDTH = 8.5'

C - AISLE WIDTH = 18.5'

D - STALL LENGTH = 20.7'

E - CURB DEPTH = 9.8'

F - OVERALL LENGTH = 59.9'

G - BACK TO BACK STALL LENGTH = 18.6'

H - OVERALL BACK TO BACK LENGTH = 55.7'

DRIVE AISLE WIDTH: 18.5' MIN. (ONE WAY)

SITE AREA CALCULATIONS:

SITE: 3.20 AC. (139,516 SF) (100%)

PERVIOUS AREA: 1.78 AC. (77,771 SF) (45%)

IMPERVIOUS AREA: 1.42 AC. (61,745 SF) (55%)

BUILDING AREA: 0.73 AC. (31,835 SF) (23%)

MINIMUM OPEN SPACE REQUIRED: 40%

FLOOD HAZARD: FLOOD ZONE "X"

F.I.R.M. MAP NO. 12011C0588, DATED 07/31/24

SITE NOTES:

- PLEASE REFER TO LANDSCAPE PLANS FOR THE RELOCATION AND/OR REMOVAL OF EXISTING TREES.
- ANY LID FROM 1/4" BUT NOT GREATER THAN 1/2" WILL BE BEVELED TO MEET ADA REQUIREMENTS

SITE REVISIONS:

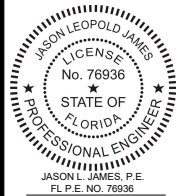
- ADA SPACE ADDED ON PLAN.
- TOTAL SITE PARKING PROPOSED
- EXISTING SITE PARKING ADDED TO SITE DATA
- PROPOSED CROSSWALK CHANGED TO SIDEWALK
- CURB CUT MEASURED AT PROPERTY LINES
- PROPOSED PARKING GEOMETRICS ADDED TO SITE DATA
- ADDED WIDTH OF SWALES AND SIDEWALK FROM PROPERTY LINE
- CURBING CHANGED FROM TYPE "F" TO TYPE "D"
- PARKING REQUIREMENTS UPDATED
- SITE VISIBILITY TRIANGLES ADDED



www.cphcorp.com
Building Better
Communities Together

1992 SW 1st Street
Miami, FL 33135
Ph: 305.274.4805

Plans Prepared By:
CPH, LLC
A Full Service A & E Firm



JASON L. JAMES, P.E.
FL P.E. NO. 76936

PLAN REVISIONS	Date	Revision
1	10-16-2025	No.

Designed: R.O.

Drawn: J.C.

Checked: R.O.

Job No.: 2500922

Date: 10/16/2025

2025

SITE DIMENSION PLAN (SDP)

PRESIDENTIAL PLACE
3880 SOUTH CIRCLE DRIVE
CITY OF HOLLYWOOD, FLORIDA 33021
BROWARD COUNTY, FLORIDA

THIS SHEET NOT VALID FOR
CONSTRUCTION WITHOUT
COMPLETE SET OF PLANS.
SEE GENERAL NOTES FOR
MASTER LEGEND.

Sheet No.

C03.0

Attachment B

Approved Methodology

MEMORANDUM

To: Clarissa Ip, P.E.
City Engineer
City of Hollywood

From: Adrian K. Dabkowski, P.E., PTOE AK
Ariel B. Centurion, P.E. ABC

Date: October 9, 2025

Subject: 3880 South Circle Drive
Traffic Assessment Methodology

The purpose of this memorandum is to summarize the traffic assessment methodology for the repurposing of the property located at 3880 South Circle Drive in Hollywood, Florida. The property proposed for repurposing is currently occupied by a 118-bed assisted living facility. The proposed repurposing will replace the assisted living facility with a 104-unit senior multifamily housing facility along with additional parking spaces in existing unpaved areas. Note that no demolition or modifications are proposed to the existing structure, existing interior layout, existing driveway access, or existing loading operations. The planned renovations include upgrading the unit finishes and expanding their kitchens. A location map and site plan alternatives for the site are included in Attachment A. Currently, there are two (2) site plan alternatives that will be coordinated with the City. The anticipated build-out year of the repurposing is 2026. The following sections summarize our proposed methodology.

TRIP GENERATION

Trip generation calculations for the existing development and proposed repurposing were performed using the Institute of Transportation Engineers' (ITE) *Trip Generation Manual*, 12th Edition. The trip generation for the existing development was determined using ITE Land Use Code (LUC) 254 (Assisted Living). The trip generation for the proposed repurposing was determined using ITE LUC 252 (Senior Adult Housing).

A multimodal (public transit, bicycle, and pedestrian) factor based on US Census *Means of Transportation to Work* data was reviewed for the census tract in the vicinity of the redevelopment. The US Census data indicated that there is a 3.6% percent (3.6%) multimodal factor within the vicinity of the redevelopment. It is expected that a portion of residents and guests will choose to walk, bike, or use public transit to and from the proposed redevelopment. US Census data is provided in Attachment B.

Sidewalks are currently provided along the north, east, and west sides of the site. Additionally, one (1) Broward County Transit (BCT) route currently operates in close proximity to the site during the A.M. and P.M. peak hours. Detailed transit route information is included in Attachment C.

- **BCT Route 7** operates along Hollywood Boulevard in the vicinity of the project site with the nearest stop located east of S Rainbow Drive. This route operates with approximately 30-minute headways in the eastbound and westbound directions during the A.M. and P.M. peak hours.

The project is expected to result in a reduction of two (2) net new vehicle trips during the A.M. and P.M. peak hours. Detailed trip generation calculations are included in Attachment B.

As the proposed repurposing is not expected to generate peak hour net new trips, analyses of external intersections are not proposed.

DATA COLLECTION

A.M. (7:00 A.M. to 9:00 A.M.) and P.M. (4:00 P.M. to 6:00 P.M.) peak period turning movement counts will be collected at the following intersections on a typical weekday (Tuesday, Wednesday, or Thursday).

1. South Circle Drive and South Crescent Drive
2. South Circle Drive and South Rainbow Drive

Turning movement counts will be collected in 15-minute intervals during the two (2) peak periods. Turning movement counts will also include pedestrians and bicyclists. All traffic counts will be adjusted to peak season conditions using the appropriate Florida Department of Transportation (FDOT) peak season category factors. All traffic data collected will be provided in the traffic assessment.

TRIP DISTRIBUTION

The likely distribution of project traffic will be forecast for the trips expected to be generated by the proposed redevelopment. The trip distribution will be based on traffic patterns observed in the collected peak period turning movement counts. Figures depicting the project trip distribution and assignment at the project driveways will be provided in the traffic assessment.

PARKING ANALYSIS

A parking analysis was prepared to determine if the proposed parking supply can adequately serve the residents of the proposed repurposing. The site proposes to provide a total of 70 parking spaces or a parking ratio of 0.67 parking spaces per senior multifamily housing unit.

Parking supply, development data, and provided parking ratios (number of parking spaces per residential unit) were gathered for five (5) surrogate sites in south Florida on July 30-31, 2025 to compare the provided parking ratios of those existing facilities to the proposed repurposing. The five (5) senior multifamily housing facilities evaluated include:

- Gardens at Driftwood: 7350 Davie Road Extension, Hollywood, Florida 33024
- Saratoga Crossings: 709 W Dania Beach Boulevard, Dania Beach, Florida 33004
- Steve Protulis Tower: 2495 NW 54th Street, Miami, Florida 33142
- Archbishop Hurley Hall: 632 NW 1st Street, Hallandale Beach, Florida 33009
- Palm Springs Villas: 445 W 51st Place, Hialeah, Florida 33012

The average parking ratio for the surrogate sites is 0.48 parking spaces per unit, with a range of 0.37 to 0.65 parking spaces per unit. Detailed parking data is provided in Attachment D. The proposed repurposing will provide 0.67 spaces per unit; which is greater than the highest parking ratio among comparable facilities, thereby providing the most parking relative to the peer sites. As such no additional parking analysis is proposed.

DOCUMENTATION

The results of the trip generation calculations and parking analysis will be summarized in separate technical memorandums. The trip generation memorandum will also include text and graphics necessary to summarize the assumptions and analysis. The parking analysis memorandum will include supporting documents including parking supply information, parking data collection, and parking space ratio calculations. This memorandum will also include text and graphics necessary to summarize the assumptions and analysis.

K:\FTL_TPTO\143897000 - 3880 S Circle Drive\Correspondence\Methodology\3880 S Circle Dr Traffic and Parking Methodology 20251007.docx

LISA S. BERNSTEIN, PE

— TRAFFIC ENGINEER —

Mr. Rick Mitinger, P.E.
City Transportation Engineer
City of Hollywood – Department of Development Services
2600 Hollywood Boulevard, Suite 308
Hollywood, Florida 33022-9045

October 13, 2025

Re: Agreement for Professional Services

Project Name: City of Hollywood Methodology 2nd Review – 3880 South Circle Drive
Project Location: 3880 South Circle Drive, Hollywood, FL 33021
Project No.: 25-0286

Dear Mr. Mitinger:

I have reviewed the revised methodology for the 3880 South Circle Drive project prepared October 9, 2025, by Kimley-Horn.

The methodology is APPROVED with the following comments:

1. The distribution will be reviewed when the traffic study is submitted.
2. The Parking Analysis is subject to review by Engineering and Planning for approval. Additional information may be requested. As previously stated, the parking studies and all data, including when the studies were done, will need to be provided for review.
3. The site plan will need to provide the following:
 - a. The pedestrian connectivity, from the public sidewalk to the building, and mass transit locations will need to be provided on the site plan.
 - b. The parking aisle on South Rainbow Drive will need to show that vehicles do not back over the pedestrian crossing. The 24-feet provided includes the pedestrian crossing, creating a conflict, and is much narrower than the South Crescent Drive parking area.
 - c. Pedestrian access from the proposed parking areas will need to be included. As a senior residential project, adequate access will need to be provided.
 - d. Signing and marking will need to be provided. South Rainbow Drive is one-way; the exit will need to be signed as a right turn only.
 - e. Please maintain the clear zone for the proposed sidewalk relocations.

4. Further comments may be generated upon submittal of the traffic study.

If you have any questions, please do not hesitate to contact me.

Sincerely,

A handwritten signature in blue ink, appearing to read 'LSB', is positioned above the printed name.

Lisa S. Bernstein, PE

Attachment C

Transit Service Data

For more details on our fares please
visit our web site at
Broward.org/BCT or call
customer service: 954-357-8400.

Reading A Timetable - It's Easy

1. The map shows the exact bus route.
2. Major route intersections are called time points.
Time points are shown with the symbol □.
3. The timetable lists major time points for bus route.
Listed under time points are scheduled departure times.
4. Reading from left to right, indicates the time for each bus trip.
5. The bus picks up and drops off riders at all BCT bus stop signs along the route where there is a Broward County bus stop sign.
6. Arrive at the bus stop five minutes early. Buses operate as close to published timetables as traffic conditions allow.

**Not paying your fare is a crime per
Florida Statute 812.015.
Violation constitutes a misdemeanor,
punishable by jail time and/or a fine.**

Information: 954-357-8400

Hearing-speech impaired:
Florida Relay Service- 711 or 1-800-955-8771
TTY- 954-357-8302

This publication can be made available in
alternative formats upon request.



This symbol is used on bus stop signs to
indicate accessible bus stops.



BOARD OF COUNTY COMMISSIONERS
An equal opportunity employer and provider of services.

1,000 copies of this public document were promulgated at a gross cost of \$275,
or \$0.275 per copy to inform the public about the Transit Division's
schedule and route information. Printed 10/24

Broward County Transit

ROUTE 7 ALL WEEK SCHEDULE

NW 210 Ave and Pines Boulevard to
Young Circle via Pines/Hollywood Boulevard

Effective 10/27/24



Safety Is Our Number One Priority



**BROWARD COUNTY
TRANSIT MOBILE APP**
BUY YOUR BUS PASSES
FROM YOUR PHONE!



Real Time Bus Information
MyRide.Broward.org



Broward.org/BCT
954-357-8400

MONDAY - FRIDAY

There are additional bus stops in between those listed.

EASTBOUND

To Young Circle

PINES BLVD & 196 AVE	PINES BLVD & DYKES RD	CENTURY VILLAGE PEMBROKE PINES	PEMBROKE LAKES MALL	PINES BLVD & DOUGLAS RD	BC SOUTH CAMPUS	HOLLYWOOD BLVD & US 441	YOUNG CIRCLE
1	3	4	5	6	7	8	9
			5:06a	4:56a		5:08a	5:25a
			5:36a	5:17a		5:29a	5:46a
			6:05a	5:47a		5:59a	6:16a
				6:16a		6:28a	6:45a
6:24a	6:32a		6:47a	6:59a	7:07a	7:19a	7:43a
6:55a	7:04a	7:13a	7:24a	7:36a	7:44a	7:55a	8:21a
			7:55a	8:07a	8:15a	8:26a	8:52a
7:50a	7:59a	8:08a	8:19a	8:31a	8:39a	8:50a	9:16a
			8:50a	9:02a	9:10a	9:20a	9:44a
8:46a	8:54a	9:03a	9:15a	9:27a	9:35a	9:45a	10:09a
			9:47a	9:59a	10:07a	10:17a	10:41a
9:42a	9:50a	9:59a	10:11a	10:23a	10:31a	10:41a	11:05a
			10:41a	10:53a	11:01a	11:11a	11:35a
10:34a	10:42a	10:51a	11:03a	11:15a	11:23a	11:33a	11:57a
			11:33a	11:45a	11:53a	12:03p	12:27p
11:25a	11:33a	11:42a	11:54a	12:06p	12:14p	12:24p	12:48p
			12:24p	12:38p	12:46p	12:56p	1:22p
12:15p	12:23p	12:32p	12:45p	12:59p	1:07p	1:17p	1:43p
			1:14p	1:28p	1:36p	1:46p	2:12p
1:07p	1:15p	1:24p	1:37p	1:51p	1:59p	2:09p	2:35p
			2:06p	2:20p	2:28p	2:38p	3:04p
2:02p	2:10p	2:19p	2:32p	2:46p	2:54p	3:04p	3:30p
			3:01p	3:18p	3:26p	3:37p	4:05p
2:57p	3:09p	3:22p	3:36p	3:53p	4:01p	4:12p	4:40p
			4:05p	4:22p	4:30p	4:41p	5:09p
3:55p	4:07p	4:20p	4:34p	4:51p	4:59p	5:10p	5:38p
			5:03p	5:20p	5:28p	5:39p	6:07p
4:50p	5:02p	5:15p	5:29p	5:46p	5:54p	6:05p	6:33p
			5:56p	6:13p	6:21p	6:32p	7:00p
5:46p	5:58p	6:11p	6:25p	6:42p	6:50p	7:01p	7:29p
			6:52p	7:06p	7:14p	7:23p	7:47p G
6:45p	6:54p	7:04p	7:17p	7:31p	7:39p	7:48p	8:12p
			7:47p	7:58p		8:12p	8:34p
7:45p	7:53p	8:02p	8:12p	8:23p		8:37p	8:59p
			8:42p	8:53p		9:06p	9:23p
8:37p	8:44p	8:53p	9:02p	9:13p		9:26p	9:43p G
			9:32p	9:43p		9:56p	10:13p
9:32p	9:39p		9:51p	10:01p		10:14p	10:32p
			10:21p	10:32p		10:45p	11:02p G
10:23p	10:30p		10:42p	10:52p		11:05p	11:23p G

WESTBOUND

To Pines Boulevard / NW 210 Ave

YOUNG CIRCLE	HOLLYWOOD BLVD & US 441	BC SOUTH CAMPUS	PINES BLVD & DOUGLAS RD	PEMBROKE LAKES MALL	CENTURY VILLAGE PEMBROKE PINES	PINES BLVD & DYKES RD	PINES BLVD & 209 AV	PINES BLVD & 196 AVE
9	8	7	6	5	4	3	2	1
5:02a	5:21a		5:32a	5:43a		5:54a	6:03a	6:08a
5:30a	5:49a		6:00a	6:11a				
6:00a	6:23a		6:36a	6:47a		7:00a	7:13a	7:19a
6:29a	6:52a		7:05a	7:16a				
6:58a	7:24a	7:32a	7:42a	7:55a	8:06a	8:17a	8:26a	8:32a
7:27a	7:53a	8:01a	8:11a	8:24a				
7:54a	8:20a	8:28a	8:38a	8:51a	9:02a	9:13a	9:22a	9:28a
8:20a	8:46a	8:54a	9:04a	9:17a				
8:46a	9:12a	9:20a	9:30a	9:43a	9:54a	10:05a	10:14a	10:20a
9:13a	9:39a	9:47a	9:57a	10:10a				
9:39a	10:05a	10:13a	10:23a	10:36a	10:47a	10:58a	11:07a	11:13a
10:06a	10:32a	10:40a	10:50a	11:03a				
10:31a	10:57a	11:05a	11:15a	11:28a	11:39a	11:50a	11:59a	12:05p
10:59a	11:25a	11:33a	11:43a	11:56a				
11:23a	11:49a	11:56a	12:05p	12:18p	12:28p	12:39p	12:48p	12:54p
11:48a	12:14p	12:21p	12:30p	12:43p				
12:14p	12:40p	12:47p	12:56p	1:09p	1:19p	1:30p	1:39p	1:45p
12:40p	1:06p	1:13p	1:22p	1:35p				
1:06p	1:34p	1:42p	1:52p	2:06p	2:18p	2:29p	2:39p	2:47p
1:35p	2:03p	2:11p	2:21p	2:35p				
2:04p	2:32p	2:40p	2:50p	3:04p	3:16p	3:27p	3:37p	3:45p
2:33p	3:01p	3:09p	3:19p	3:33p				
3:01p	3:32p	3:40p	3:50p	4:03p	4:15p	4:25p	4:34p	4:40p
3:29p	4:00p	4:08p	4:18p	4:31p				
3:57p	4:28p	4:36p	4:46p	4:59p	5:11p	5:21p	5:30p	5:36p
4:27p	4:58p	5:06p	5:16p	5:29p				
4:56p	5:27p	5:35p	5:45p	5:58p	6:10p	6:20p	6:29p	6:35p
5:27p	5:58p	6:06p	6:16p	6:29p				
5:56p	6:27p	6:35p	6:45p	6:58p	7:10p	7:20p	7:29p	7:35p
6:24p	6:50p	6:57p	7:06p	7:17p				
6:54p	7:20p	7:27p	7:36p	7:47p	7:57p	8:07p	8:15p	8:20p
7:21p	7:44p	7:50p	7:58p	8:07p				
7:52p	8:15p	8:21p	8:29p	8:38p	8:47p	8:57p	9:05p	9:10p
8:23p	8:44p		8:55p	9:05p				
8:54p	9:15p		9:26p	9:36p		9:47p	9:54p	9:59p
9:25p	9:46p		9:57p	10:07p				
9:55p	10:16p		10:27p	10:37p G				
10:25p	10:46p		10:57p	11:07p G				
10:51p	11:10p		11:20p	11:26p G				

NUMBERS IN BOXES REFER TO TIME POINTS ON MAP

Times with the letter "G" after them indicate bus returns to garage.

SATURDAY EASTBOUND To Young Circle

WESTBOUND To Pines Blvd/ NW 210 Ave

PINES BLVD & 196 AVE	PINES BLVD & DYKES RD	CENTURY VILLAGE PEMBROKE PINES	PEMBROKE LAKES MALL	PINES BLVD & DOUGLAS RD	BC SOUTH CAMPUS	HOLLYWOOD BLVD & US 441	YOUNG CIRCLE
1	3	4	5	6	7	8	9
			5:12a	5:23a		5:38a	
			5:20a	5:30a		5:41a	5:56a
			5:50a	6:00a		6:11a	6:29a
			6:20a	6:30a		6:41a	6:59a
6:30a	6:36a		6:47a	6:57a	7:02a	7:10a	7:27a
			7:20a	7:31a	7:38a	7:46a	8:04a
7:30a	7:38a	7:47a	7:57a	8:08a	8:15a	8:23a	8:41a
			8:20a	8:31a	8:38a	8:46a	9:04a
8:30a	8:38a	8:46a	8:56a	9:08a	9:15a	9:24a	9:44a
			9:23a	9:35a	9:42a	9:51a	10:11a
9:30a	9:38a	9:46a	9:56a	10:08a	10:15a	10:24a	10:44a
			10:21a	10:34a	10:42a	10:52a	11:15a
10:30a	10:38a	10:47a	11:00a	11:13a	11:21a	11:31a	11:54a
			11:27a	11:40a	11:48a	11:58a	12:21p
11:26a	11:34a	11:43a	11:56a	12:09p	12:17p	12:27p	12:50p
			12:22p	12:37p	12:45p	12:55p	1:19p
12:19p	12:27p	12:37p	12:52p	1:07p	1:15p	1:25p	1:49p
			1:22p	1:37p	1:45p	1:55p	2:19p
1:19p	1:27p	1:37p	1:52p	2:07p	2:15p	2:25p	2:49p
			2:21p	2:36p	2:44p	2:54p	3:18p
2:18p	2:26p	2:36p	2:51p	3:06p	3:14p	3:24p	3:48p
			3:18p	3:33p	3:41p	3:51p	4:15p
3:18p	3:25p	3:36p	3:51p	4:06p	4:14p	4:24p	4:48p
			4:18p	4:33p	4:40p	4:50p	5:13p
4:15p	4:23p	4:34p	4:47p	5:02p	5:09p	5:19p	5:42p
			5:16p	5:31p	5:38p	5:48p	6:11p
5:15p	5:23p	5:34p	5:47p	6:02p	6:09p	6:19p	6:42p
			6:22p	6:37p	6:44p	6:54p	7:17p
6:15p	6:23p	6:34p	6:47p	7:02p	7:09p	7:19p	7:42p
			7:25p	7:39p		7:52p	8:12p
7:15p	7:23p	7:32p	7:45p	7:59p		8:12p	8:32p
			8:30p	8:42p		8:52p	9:10p
8:16p	8:23p	8:31p	8:43p	8:55p		9:05p	9:23p
9:17p	9:24p		9:36p	9:48p		10:01p	10:22p
10:20p	10:27p		10:39p	10:51p		11:04p	11:25p G

YOUNG CIRCLE	HOLLYWOOD BLVD & US 441	BC SOUTH CAMPUS	PINES BLVD & DOUGLAS RD	PEMBROKE LAKES MALL	CENTURY VILLAGE PEMBROKE PINES	PINES BLVD & DYKES RD	PINES BLVD & 209 AV	PINES BLVD & 196 AVE
9	8	7	6	5	4	3	2	1
5:00a	5:17a		5:26a	5:36a		5:46a	5:53a	5:58a
5:25a	5:42a		5:51a	6:01a				
5:55a	6:12a		6:21a	6:31a		6:41a	6:48a	6:53a
6:25a	6:42a		6:51a	7:01a				
6:53a	7:14a	7:21a	7:29a	7:39a	7:49a	8:01a	8:10a	8:15a
7:21a	7:44a	7:51a	7:59a	8:10a				
7:51a	8:14a	8:21a	8:29a	8:40a	8:51a	9:02a	9:11a	9:16a
8:21a	8:44a	8:51a	8:59a	9:10a				
8:53a	9:16a	9:23a	9:31a	9:42a	9:53a	10:04a	10:13a	10:18a
9:18a	9:41a	9:48a	9:56a	10:07a				
9:43a	10:06a	10:13a	10:21a	10:32a	10:43a	10:54a	11:03a	11:08a
10:10a	10:36a	10:43a	10:52a	11:04a				
10:37a	11:03a	11:10a	11:19a	11:31a	11:42a	11:53a	12:02p	12:07p
11:07a	11:31a	11:38a	11:47a	11:59a				
11:36a	12:00p	12:07p	12:16p	12:28p	12:40p	12:52p	1:01p	1:06p
12:06p	12:30p	12:37p	12:46p	12:58p				
12:36p	1:00p	1:07p	1:16p	1:28p	1:40p	1:52p	2:01p	2:06p
1:03p	1:27p	1:34p	1:43p	1:55p				
1:29p	1:53p	2:00p	2:09p	2:21p	2:33p	2:45p	2:54p	2:59p
2:00p	2:26p	2:33p	2:42p	2:56p				
2:29p	2:55p	3:02p	3:11p	3:25p	3:37p	3:47p	3:56p	4:01p
3:01p	3:27p	3:34p	3:43p	3:57p				
3:33p	3:59p	4:06p	4:15p	4:29p	4:41p	4:51p	5:00p	5:05p
4:04p	4:30p	4:37p	4:46p	5:00p				
4:33p	4:59p	5:06p	5:15p	5:29p	5:41p	5:51p	6:00p	6:05p
5:05p	5:31p	5:38p	5:47p	6:01p	6:13p	6:23p	6:32p	6:37p
5:33p	5:59p	6:06p	6:15p	6:29p G				
5:59p	6:25p	6:32p	6:41p	6:55p	7:07p	7:17p	7:26p	7:31p
6:28p	6:50p	6:57p	7:05p	7:15p				
7:01p	7:23p	7:30p	7:38p	7:48p	7:58p	8:08p	8:16p	8:20p
7:27p	7:51p		8:03p	8:13p				
7:58p	8:22p		8:34p	8:44p	8:54p	9:04p	9:12p	9:17p G
8:29p	8:53p		9:05p	9:15p G				
9:00p	9:21p		9:31p	9:40p		9:52p	10:00p	10:04p
9:31p	9:55p		10:07p	10:17p G				
10:03p	10:27p		10:39p	10:49p G				
10:50p	11:14p		11:26p	11:36p G				

NUMBERS IN BOXES REFER TO TIME POINTS ON MAP
Times with the letter "G" after them indicate bus returns to garage.

SUNDAY EASTBOUND To Young Circle**WESTBOUND** To Pines Blvd/ NW 210 Ave

9:07a	9:16a	9:25a	9:02a	9:13a	9:20a	9:27a	9:46a	8:32a	8:55a	9:01a	9:09a	9:21a	9:30a	9:40a	9:48a	9:53a
			9:35a	9:46a	9:53a	10:00a	10:19a	9:06a	9:27a	9:33a	9:41a	9:52a				
			10:05a	10:16a	10:23a	10:30a	10:49a	9:34a	9:55a	10:01a	10:09a	10:20a	10:29a	10:39a	10:47a	10:52a
10:05a	10:14a	10:23a	10:33a	10:44a	10:51a	10:58a	11:17a	10:04a	10:25a	10:31a	10:39a	10:50a				
			11:04a	11:17a	11:24a	11:32a	11:51a	10:29a	10:50a	10:56a	11:04a	11:15a	11:24a	11:34a	11:42a	11:47a
11:05a	11:13a	11:22a	11:33a	11:46a	11:53a	12:01p	12:20p	11:01a	11:24a	11:30a	11:38a	11:51a				
			12:04p	12:17p	12:24p	12:32p	12:54p	11:29a	11:52a	11:58a	12:06p	12:19p	12:30p	12:40p	12:47p	12:52p
12:02p	12:10p	12:19p	12:30p	12:43p	12:50p	12:58p	1:20p	12:02p	12:25p	12:31p	12:39p	12:52p				
			1:04p	1:18p	1:25p	1:33p	1:54p	12:30p	12:53p	12:59p	1:07p	1:20p	1:31p	1:41p	1:48p	1:53p
1:02p	1:09p	1:18p	1:31p	1:45p	1:52p	2:00p	2:21p	1:04p	1:27p	1:33p	1:41p	1:54p				
			2:06p	2:20p	2:27p	2:35p	2:56p	1:30p	1:53p	1:59p	2:07p	2:20p	2:31p	2:41p	2:48p	2:53p
2:03p	2:10p	2:19p	2:32p	2:46p	2:53p	3:01p	3:22p	2:04p	2:27p	2:33p	2:41p	2:54p				
			3:05p	3:19p	3:26p	3:34p	3:55p	2:31p	2:54p	3:00p	3:08p	3:21p	3:32p	3:42p	3:49p	3:54p
3:03p	3:10p	3:20p	3:33p	3:47p	3:54p	4:02p	4:23p	3:06p	3:29p	3:35p	3:43p	3:56p				
			4:07p	4:21p	4:28p	4:36p	4:57p	3:33p	3:56p	4:03p	4:11p	4:24p	4:34p	4:43p	4:51p	4:57p
4:04p	4:11p	4:21p	4:34p	4:48p	4:55p	5:03p	5:24p	4:07p	4:30p	4:37p	4:45p	4:58p				
			5:08p	5:22p	5:30p	5:40p	6:01p	4:34p	4:57p	5:04p	5:12p	5:25p	5:35p	5:44p	5:52p	5:58p
5:07p	5:14p	5:24p	5:37p	5:51p	5:59p	6:09p	6:30p	5:07p	5:30p	5:37p	5:45p	5:58p				
			6:08p	6:22p	6:29p	6:40p	6:59p	5:36p	5:59p	6:06p	6:14p	6:27p	6:37p	6:46p	6:54p	7:00p
6:08p	6:15p	6:24p	6:36p	6:50p	6:57p	7:08p	7:27p	6:11p	6:34p	6:40p	6:48p	7:01p				
			7:11p	7:25p		7:38p	7:57p G	6:41p	7:04p	7:10p	7:18p	7:31p	7:40p	7:49p	7:57p	8:02p
7:10p	7:17p	7:26p	7:38p	7:52p		8:05p	8:24p G	7:11p	7:34p		7:44p	7:52p				
			8:05p	8:19p		8:32p	8:51p G	7:44p	8:04p		8:14p	8:22p G				
8:12p	8:19p	8:27p	8:39p	8:53p		9:06p	9:25p G									

NUMBERS IN BOXES REFER TO TIME POINTS ON MAP
Times with the letter "G" after them indicate bus returns to garage.

NW 210 Ave and Pines Blvd to Young Circle
via Pines/Hollywood Boulevard



Breeze services temporarily suspended.
Please contact BCT customer Service or
visit our website for latest service updates.



TRANSIT WATCH

**WHEN IT COMES TO OUR SAFETY,
WE CAN ALWAYS
USE AN EXTRA PAIR OF EYES
AND EARS. BE ALERT.
CALL 954-357-LOOK (5665).
TELL US.**

CUSTOMER SERVICE CALL CENTER

954-357-8400

Monday 6AM through Saturday 8PM (24 hours)

Sunday 6AM - 8PM

Holidays 8:30AM - 4:45PM

Transit Operations Agents help with:

- Trip planning
- Identifying Bus Pass sales locations
- Routes, times and transfer information
- Special event information

Lost and Found: Monday, Tuesday, Thursday and Friday, 9AM - 4PM.

HOLIDAY BUS SERVICE

Sunday bus service is provided on the following holidays:

New Year's Day	Independence Day
Labor Day	Thanksgiving Day
Memorial Day	Christmas Day

FARES

Exact fare, dollar bill or coins required. Operators do not carry change.

Fares are: Regular, Premium Express, Senior/Youth/Disabled/Medicare.* Children (under 40 inches ride FREE)

FARE DEALS

All Day Bus Pass offers unlimited rides on all routes. On sale aboard all BCT buses.

NOTE: Other cost saving passes cannot be purchased on BCT buses, but are available at the Central Bus Terminal and at authorized distributors.

10 Ride Pass: 10 Rides any time, any day. Expires after the tenth ride is taken.

7 Day Pass: Unlimited rides for seven consecutive days. Starts on the first day card is used. Expires after the seventh day.

31 Day Adult Pass: Unlimited rides for 31 consecutive days. Starts on the first day card is used.

31 Day Reduced Pass: Youth*, Seniors*, Disabled*, Medicare*, College Student*. Unlimited rides for 31 consecutive days. Starts on the first day card is used.

**Premium Express 10 Ride Pass: 10 rides any time, any day. Expires after tenth ride is taken.

**Premium Express 31 Day Pass: Unlimited rides for 31 consecutive days. Starts on the first day card is used.

Bus Passes are not redeemable, refundable or transferrable. Damaged cards are invalid. Lost, stolen or damaged cards will not be replaced.

*NOTICE: Proof of age is required for Youth fare (18 years or younger) and for Senior fare (65 years or older). For College Student Bus Pass, a college photo ID card is required. For Disabled and Medicare fare, proof of disability (Medicare card) and photo I.D. is required. Eligible Senior fare patrons are encouraged to acquire their BCT Reduced Fare Photo ID cards.

** Premium Bus Pass can be purchased online at Broward.org/bct and at select Broward County library locations.

TRANSFER POLICY - EFFECTIVE 7/10/11

TRANSFERS BETWEEN REGULAR BCT BUS SERVICE AND BCT EXPRESS BUS SERVICE

Passengers using any BCT bus pass and transferring from a regular BCT route, to an Express bus route, must pay a \$1.00 upgrade fee. Passengers with a Premium bus pass do not have to pay the \$1.00 upgrade fee.

Passengers paying with cash, on a regular BCT bus route, will not be able to transfer to an Express bus route without paying the full premium fare when boarding the Express bus.

Passengers using an All-Day bus pass will be required to pay the \$1.00 upgrade fee when boarding Express buses.

PREMIUM BUS PASS CUSTOMERS

The BCT 31-Day Premium Bus Pass is acceptable on all BCT regular bus routes.

TRANSFERS FROM BCT TO OTHER SOUTH FLORIDA TRANSIT SYSTEMS

When boarding a BCT bus, passenger pays the appropriate BCT fare and may request a transfer from the bus operator if transferring to Miami-Dade Transit (MDT), Palm Tran or Tri-Rail.

TRANSFERS TO BCT FROM OTHER SOUTH FLORIDA TRANSIT SYSTEMS

When transferring from MDT, Palm Tran and Tri-Rail to BCT regular fixed-route bus service, passenger pays \$.50 with a transfer issued by MDT or Palm Tran and proof of fare payment such as Easy Card and receipt issued by Tri-Rail. Tri-Rail passengers boarding BCT at any locations other than at a Tri-Rail station will be required to pay the full fare.

TRANSFERS BETWEEN OTHER SOUTH FLORIDA TRANSIT SYSTEMS AND PREMIUM EXPRESS BUS SERVICE

Transfers to MDT or Tri-Rail from Premium Express Service, a transfer is issued and passenger must pay appropriate MDT or Tri-Rail fare.

Transfer from MDT or Tri-Rail to Premium Express Service, a \$.50 transfer fee is required with the appropriate transfer from MDT or Tri-Rail.

The Premium Express Service does not connect with Palm Tran.

The Easy Card issued by MDT and Tri-Rail is not accepted as payment on any BCT bus.

PROTECTIONS OF TITLE VI OF THE CIVIL RIGHTS ACT OF 1964 AS AMENDED

Any person(s) or group(s) who believes that they have been subjected to discrimination because of race, color, or national origin, under any transit program or activity provided by Broward County Transit (BCT), may call 954-357-8481 to file a Title VI discrimination complaint or write to Broward County Transit Division, Compliance Manager, 1 N. University Drive, Suite 3100A, Plantation, FL 33324

Attachment D

Trip Generation Calculations

Existing Development A.M. Peak Hour Trip Generation Calculations

TRIP GENERATION CHARACTERISTICS						DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			MULTIMODAL REDUCTION		VEHICLE TRIPS			INTERNAL CAPTURE		EXTERNAL VEHICLE TRIPS			PASS-BY CAPTURE		NEW EXTERNAL VEHICLE TRIPS			
	Land Use	ITE Edition	ITE LUC	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	Factor	MR Trips	In	Out	Total	Rate	IC Trips	In	Out	Total	Rate	PB Trips	In	Out	Total
1	Assisted Living	12	254	118	BED	T = 0.19(X)	59%	41%	13	9	22	3.6%	1	12	9	21	0.0%	0	12	9	21	0.0%	0	12	9	21
2																										
Total:									13	9	22	3.6%	1	12	9	21	0.0%	0	12	9	21	0.0%	0	12	9	21

Proposed Redevelopment A.M. Peak Hour Trip Generation Calculations

TRIP GENERATION CHARACTERISTICS							DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			MULTIMODAL REDUCTION		VEHICLE TRIPS			INTERNAL CAPTURE		EXTERNAL VEHICLE TRIPS			PASS-BY CAPTURE		NEW EXTERNAL VEHICLE TRIPS		
	Land Use	ITE Edition	ITE LUC	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	Factor	MR Trips	In	Out	Total	Rate	IC Trips	In	Out	Total	Rate	PB Trips	In	Out	Total
1	Senior Adult Housing	12	252	104	DU	T = 0.19(X) + 0.17	34%	66%	7	13	20	3.6%	1	7	12	19	0.0%	0	7	12	19	0.0%	0	7	12	19
2																										
Total:									7	13	20	3.6%	1	7	12	19	0.0%	0	7	12	19	0.0%	0	7	12	19
																						NET NEW TRIPS		-5	3	-2

Existing Development P.M. Peak Hour Trip Generation Calculations

TRIP GENERATION CHARACTERISTICS						DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			MULTIMODAL REDUCTION		VEHICLE TRIPS			INTERNAL CAPTURE		EXTERNAL VEHICLE TRIPS			PASS-BY CAPTURE		NEW EXTERNAL VEHICLE TRIPS			
	Land Use	ITE Edition	ITE LUC	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	Factor	MR Trips	In	Out	Total	Rate	IC Trips	In	Out	Total	Rate	PB Trips	In	Out	Total
1	Assisted Living	12	254	118	BED	T = 0.24(X)	38%	62%	11	17	28	3.6%	1	11	16	27	0.0%	0	11	16	27	0.0%	0	11	16	27
2																										
Total:									11	17	28	3.6%	1	11	16	27	0.0%	0	11	16	27	0.0%	0	11	16	27

Proposed Redevelopment P.M. Peak Hour Trip Generation Calculations

TRIP GENERATION CHARACTERISTICS							DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			MULTIMODAL REDUCTION		VEHICLE TRIPS			INTERNAL CAPTURE		EXTERNAL VEHICLE TRIPS			PASS-BY CAPTURE		NEW EXTERNAL VEHICLE TRIPS		
	Land Use	ITE Edition	ITE LUC	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	Factor	MR Trips	In	Out	Total	Rate	IC Trips	In	Out	Total	Rate	PB Trips	In	Out	Total
1	Senior Adult Housing	12	252	104	DU	T = 0.25(X) + 0.03	56%	44%	15	11	26	3.6%	1	14	11	25	0.0%	0	14	11	25	0.0%	0	14	11	25
2																										
Total:									15	11	26	3.6%	1	14	11	25	0.0%	0	14	11	25	0.0%	0	14	11	25
																						NET NEW TRIPS		3	-5	-2

Means of Transportation to Work

Note: This is a modified view of the original table produced by the U.S. Census Bureau. This download or printed version may have missing information from the original table.

Census Tract 917.02; Broward County; Florida

Label

$$(21+27+45)/(2,840-237) = 3.6\%$$

Estimate

Margin of Error

▼ Total:	2,840	±529
▼ Car, truck, or van:	2,501	±521
Drove alone	2,247	±441
▼ Carpooled:	254	±205
In 2-person carpool	254	±205
In 3-person carpool	0	±15
In 4-person carpool	0	±15
In 5- or 6-person carpool	0	±15
In 7-or-more-person carpool	0	±15
▼ Public transportation (excluding taxicab):	21	±25
Bus	13	±20
Subway or elevated rail	0	±15
Long-distance train or commuter rail	8	±15
Light rail, streetcar or trolley (carro público in Puerto Rico)	0	±15
Ferryboat	0	±15
Taxicab	9	±15
Motorcycle	0	±15
Bicycle	27	±44
Walked	45	±50
Other means	0	±15
Worked from home	237	±150

Attachment E

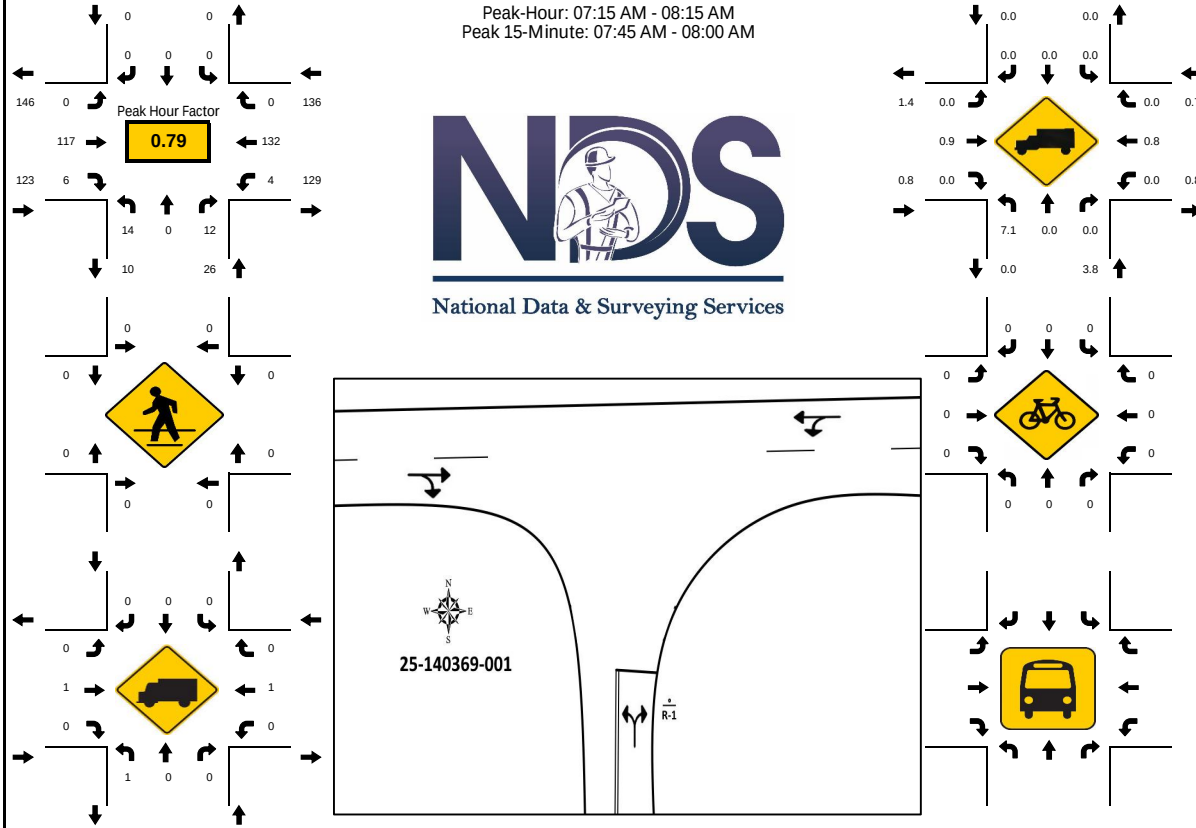
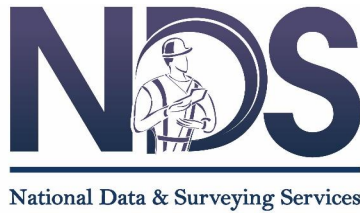
Traffic Data

Turning Movement Counts

LOCATION: S Crescent Dr & S Circle Dr
CITY/STATE: Hollywood, FL

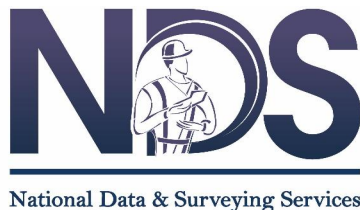
PROJECT ID: 25-140369-001
DATE: Wed, Oct 08, 2025

Peak-Hour: 07:15 AM - 08:15 AM
Peak 15-Minute: 07:45 AM - 08:00 AM

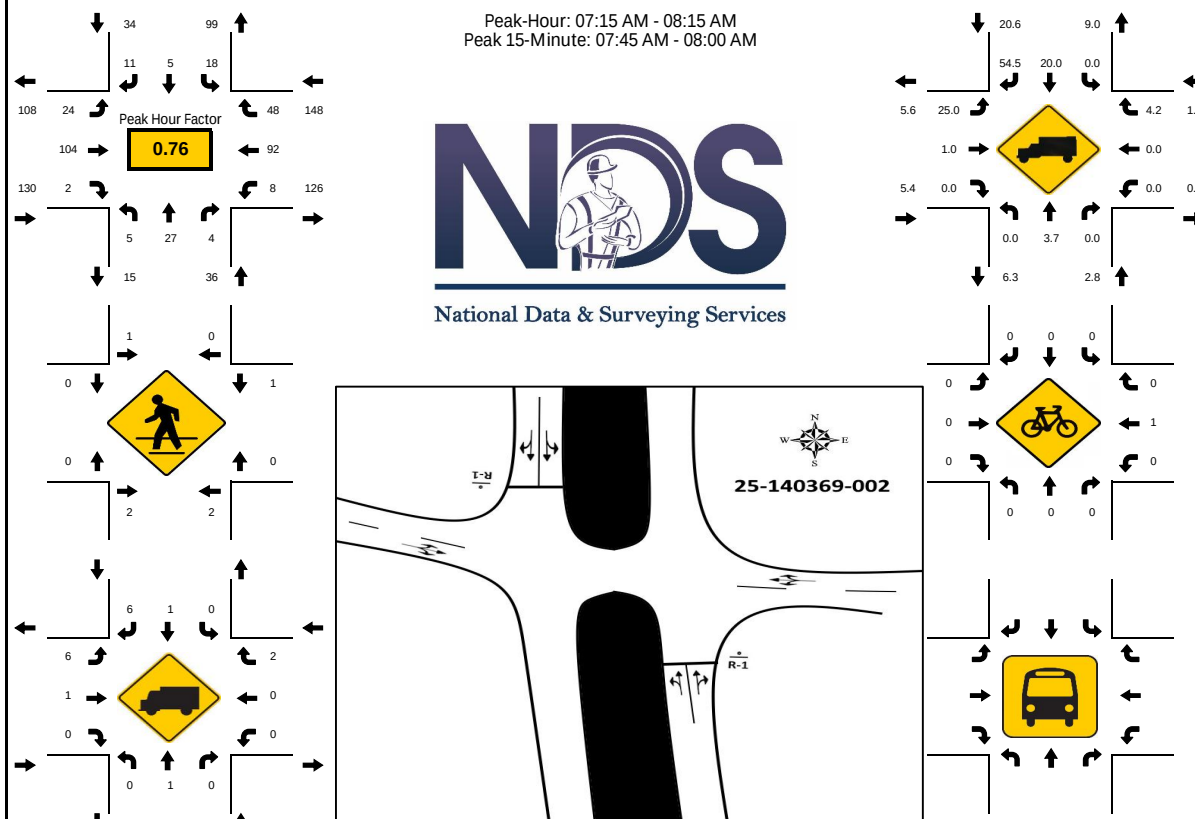


15-Min Count Period Beginning At	S Crescent Dr Northbound					S Crescent Dr Southbound					S Circle Dr Eastbound					S Circle Dr Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
7:00 AM	3	0	4	0		0	0	0	0		0	33	2	0		1	14	0	0		57	272
7:15 AM	1	0	1	0		0	0	0	0		0	22	0	0		0	26	0	0		50	285
7:30 AM	9	0	4	0		0	0	0	0		0	30	2	0		1	29	0	0		75	281
7:45 AM	3	0	3	0		0	0	0	0		0	44	1	0		2	37	0	0		90	274
8:00 AM	1	0	4	0		0	0	0	0		0	21	3	0		1	40	0	0		70	241
8:15 AM	1	0	3	0		0	0	0	0		0	18	1	0		0	23	0	0		46	171
8:30 AM	5	0	5	0		0	0	0	0		0	23	3	0		1	31	0	0		68	125
8:45 AM	1	0	2	0		0	0	0	0		0	31	1	0		2	20	0	0		57	57
Peak 15-Min Flowrates	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Total	
All Vehicles	36	0	16	0		0	0	0	0		0	176	12	0		8	160	0	0		408	
Heavy Trucks	4	0	0	0		0	0	0	0		0	4	0	0		0	4	0	0		12	
Pedestrians	0	0	0	0		0	0	0	0		0	0	0	0		0	0	0	0		0	
Bicycles	0	0	0	0		0	0	0	0		0	0	0	0		0	0	0	0		0	
Buses	0	0	0	0		0	0	0	0		0	0	0	0		0	0	0	0		0	
Stopped Buses	0	0	0	0		0	0	0	0		0	0	0	0		0	0	0	0		0	

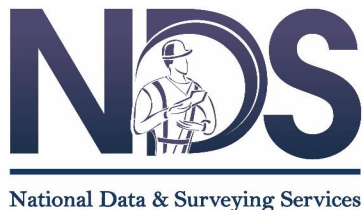
PROJECT ID: 25-140369-001
DATE: Wed, Oct 08, 2025

[illegible]

PROJECT ID: 25-140369-002
DATE: Wed, Oct 08, 2025

[illegible]

PROJECT ID: 25-140369-002
DATE: Wed, Oct 08, 2025

[illegible]

Peak Season Category Report

2024 PEAK SEASON FACTOR CATEGORY REPORT - REPORT TYPE: ALL
CATEGORY: 8601 CEN.-W OF US1 TO SR7

WEEK	DATES	SF	MOCF: 0.99 PSCF
1	01/01/2024 - 01/06/2024	0.99	1.00
2	01/07/2024 - 01/13/2024	1.01	1.02
3	01/14/2024 - 01/20/2024	1.04	1.05
4	01/21/2024 - 01/27/2024	1.03	1.04
5	01/28/2024 - 02/03/2024	1.02	1.03
6	02/04/2024 - 02/10/2024	1.01	1.02
7	02/11/2024 - 02/17/2024	1.00	1.01
8	02/18/2024 - 02/24/2024	1.00	1.01
9	02/25/2024 - 03/02/2024	1.00	1.01
10	03/03/2024 - 03/09/2024	0.99	1.00
11	03/10/2024 - 03/16/2024	0.99	1.00
12	03/17/2024 - 03/23/2024	0.99	1.00
13	03/24/2024 - 03/30/2024	0.99	1.00
14	03/31/2024 - 04/06/2024	0.99	1.00
15	04/07/2024 - 04/13/2024	0.99	1.00
16	04/14/2024 - 04/20/2024	0.99	1.00
17	04/21/2024 - 04/27/2024	0.99	1.00
18	04/28/2024 - 05/04/2024	1.00	1.01
19	05/05/2024 - 05/11/2024	1.00	1.01
20	05/12/2024 - 05/18/2024	1.00	1.01
21	05/19/2024 - 05/25/2024	1.01	1.02
22	05/26/2024 - 06/01/2024	1.02	1.03
23	06/02/2024 - 06/08/2024	1.03	1.04
24	06/09/2024 - 06/15/2024	1.04	1.05
25	06/16/2024 - 06/22/2024	1.04	1.05
26	06/23/2024 - 06/29/2024	1.04	1.05
27	06/30/2024 - 07/06/2024	1.04	1.05
28	07/07/2024 - 07/13/2024	1.04	1.05
29	07/14/2024 - 07/20/2024	1.04	1.05
30	07/21/2024 - 07/27/2024	1.02	1.03
31	07/28/2024 - 08/03/2024	1.01	1.02
32	08/04/2024 - 08/10/2024	1.00	1.01
33	08/11/2024 - 08/17/2024	0.99	1.00
34	08/18/2024 - 08/24/2024	0.99	1.00
35	08/25/2024 - 08/31/2024	0.99	1.00
36	09/01/2024 - 09/07/2024	1.00	1.01
37	09/08/2024 - 09/14/2024	1.00	1.01
38	09/15/2024 - 09/21/2024	1.00	1.01
*39	09/22/2024 - 09/28/2024	1.00	1.01
*40	09/29/2024 - 10/05/2024	1.00	1.01
*41	10/06/2024 - 10/12/2024	1.00	1.01
*42	10/13/2024 - 10/19/2024	1.00	1.01
*43	10/20/2024 - 10/26/2024	1.00	1.01
*44	10/27/2024 - 11/02/2024	0.99	1.00
*45	11/03/2024 - 11/09/2024	0.99	1.00
*46	11/10/2024 - 11/16/2024	0.99	1.00
*47	11/17/2024 - 11/23/2024	0.99	1.00
*48	11/24/2024 - 11/30/2024	0.99	1.00
*49	12/01/2024 - 12/07/2024	0.99	1.00
*50	12/08/2024 - 12/14/2024	0.99	1.00
*51	12/15/2024 - 12/21/2024	0.99	1.00
52	12/22/2024 - 12/28/2024	1.01	1.02
53	12/29/2024 - 12/31/2024	1.04	1.05

* PEAK SEASON

04-MAR-2025 16:32:53

830UPD

4_8601_PKSEASON.TXT

Attachment F

Volume Development Data

Volume Development Figures



 Study Roadway
 Study Intersection
XX A.M. Peak Hour Traffic
(XX) P.M. Peak Hour Traffic

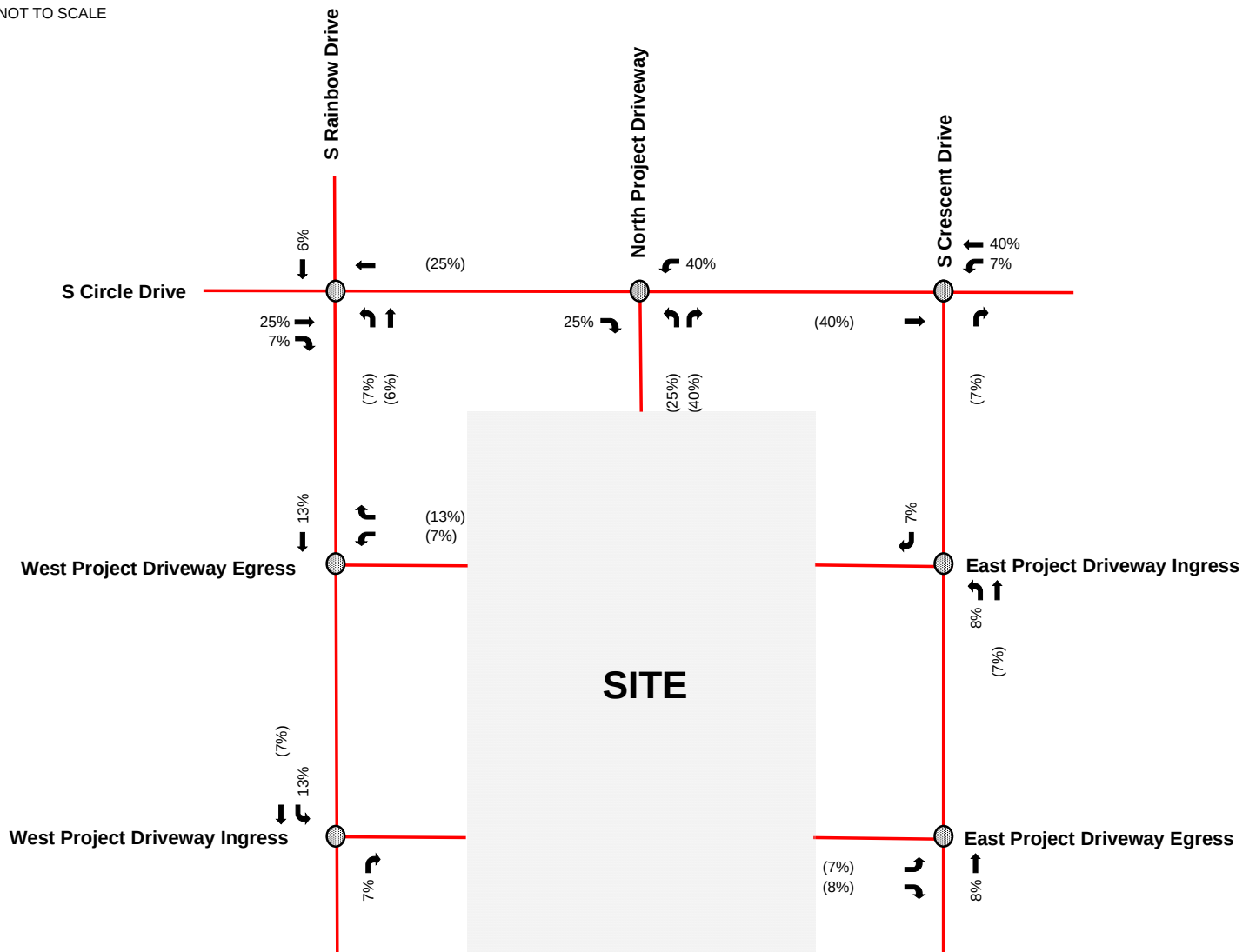




NOT TO SCALE

Legend

- Study Roadway
- Study Intersection
- XX% Entering Trip Distribution
- (XX%) Exiting Trip Distribution

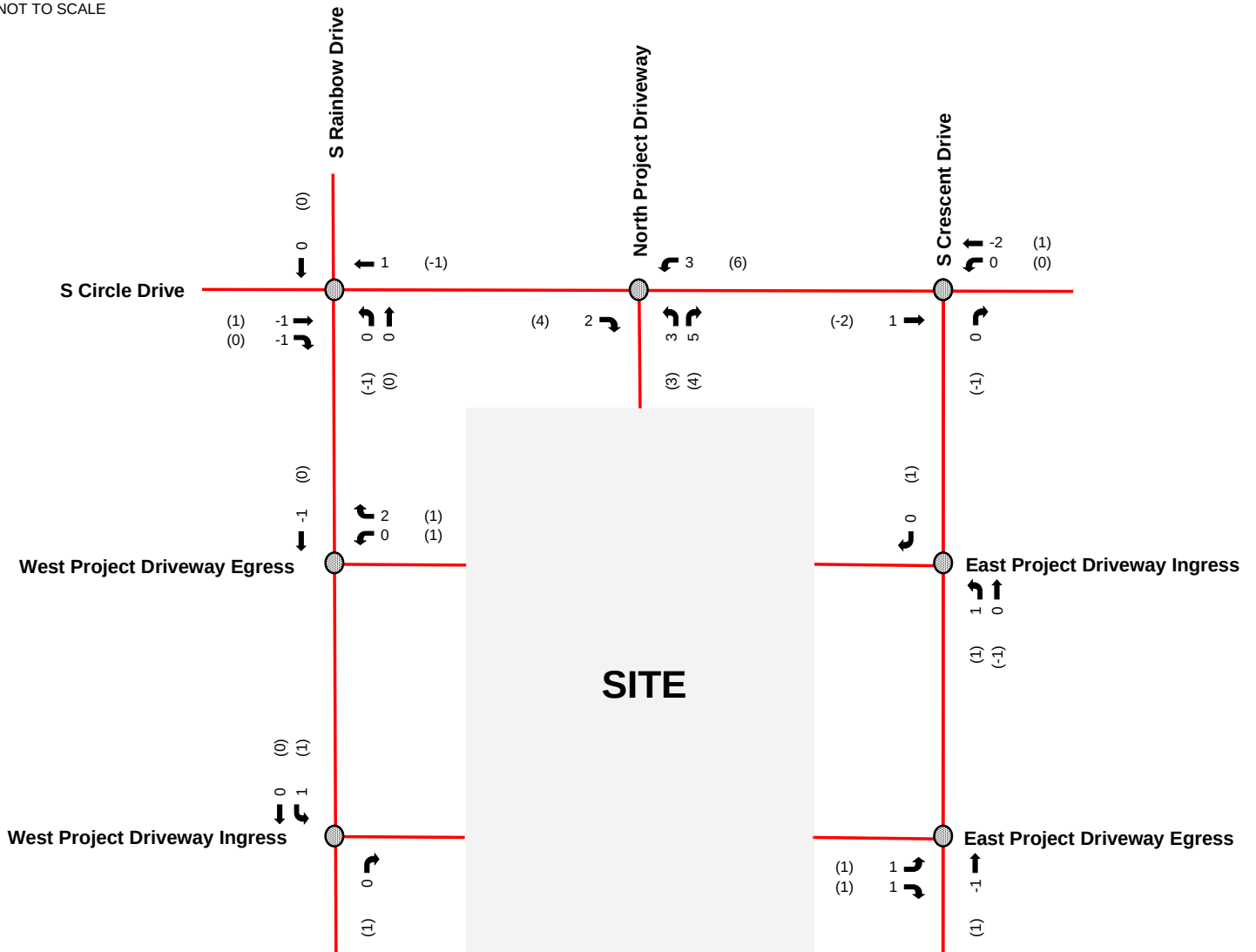




NOT TO SCALE

Legend

- Study Roadway
- Study Intersection
- XX A.M. Peak Hour Trip Assignment
- (XX) P.M. Peak Hour Trip Assignment



*The traffic volumes in and out of the project driveways are total project volumes while traffic volumes at external intersections are net new trips accounting for the existing development.

Volume Development Worksheets

TRAFFIC VOLUMES AT STUDY INTERSECTIONS

INTERSECTION: S Circle Drive and S Crescent Drive
 COUNT DATE: October 8, 2025
 AM PEAK HOUR FACTOR: 0.79
 PM PEAK HOUR FACTOR: 0.91

"AM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Raw Turning Movements		0	117	6		4	132	0		14	0	12		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

AM EXISTING CONDITIONS		0	118	6		4	133	0		14	0	12		0	0	0
------------------------	--	---	-----	---	--	---	-----	---	--	----	---	----	--	---	---	---

"PM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Raw Turning Movements		0	98	11		14	152	0		13	0	10		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

PM EXISTING CONDITIONS		0	99	11		14	154	0		13	0	10		0	0	0
------------------------	--	---	----	----	--	----	-----	---	--	----	---	----	--	---	---	---

"AM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering					7.0%	40.0%										
	Exiting			40.0%								7.0%					

"PM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering					7.0%	40.0%										
	Exiting			40.0%								7.0%					

"AM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
AM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New			1		0	-2					0					
AM TOTAL PROJECT TRAFFIC			0	1	0		0	-2	0		0	0	0		0	0	0

"PM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
PM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New			-2		0	1					-1					
PM TOTAL PROJECT TRAFFIC			0	-2	0		0	1	0		0	0	-1		0	0	0

TRAFFIC VOLUMES AT STUDY INTERSECTIONS

INTERSECTION: S Circle Drive and S Rainbow Drive
 COUNT DATE: October 8, 2025
 AM PEAK HOUR FACTOR: 0.76
 PM PEAK HOUR FACTOR: 0.91

"AM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Raw Turning Movements		24	104	2			92	48		5	27	4		18	5	11
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

AM EXISTING CONDITIONS		24	105	2		8	93	48		5	27	4		18	5	11
------------------------	--	----	-----	---	--	---	----	----	--	---	----	---	--	----	---	----

"PM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Raw Turning Movements		10	60	2		10	124	29		2	15	1		42	15	22
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

PM EXISTING CONDITIONS		10	61	2		10	125	29		2	15	1		42	15	22
------------------------	--	----	----	---	--	----	-----	----	--	---	----	---	--	----	----	----

"AM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering			25.0%	7.0%											6.0%	
	Exiting							25.0%			7.0%	6.0%					

"PM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering			25.0%	7.0%											6.0%	
	Exiting							25.0%			7.0%	6.0%					

"AM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
AM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New			-1	-1			1			0	0				0	
AM TOTAL PROJECT TRAFFIC			0	-1	-1		0	1	0		0	0	0		0	0	0

"PM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
PM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New			1	0			-1			-1	0				0	
PM TOTAL PROJECT TRAFFIC			0	1	0		0	-1	0		-1	0	0		0	0	0

TRAFFIC VOLUMES AT STUDY INTERSECTIONS

INTERSECTION: S Circle Drive and North Project Driveway
 COUNT DATE: October 8, 2025
 AM PEAK HOUR FACTOR: 0.92
 PM PEAK HOUR FACTOR: 0.92

"AM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Raw Turning Movements		0	0	0		0	0	0		0	0	0		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

AM EXISTING CONDITIONS	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
		0	0	0		0	0	0		0	0	0		0	0	0

"PM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Raw Turning Movements		0	0	0		0	0	0		0	0	0		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

PM EXISTING CONDITIONS	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
		0	0	0		0	0	0		0	0	0		0	0	0

"AM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering				25.0%		40.0%										
	Exiting										25.0%		40.0%				

"PM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering				25.0%		40.0%										
	Exiting										25.0%		40.0%				

"AM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
AM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New				2		3				3		5				
AM TOTAL PROJECT TRAFFIC			0	0	2		3	0	0		3	0	5		0	0	0

"PM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
PM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New				4		6				3		4				
PM TOTAL PROJECT TRAFFIC			0	0	4		6	0	0		3	0	4		0	0	0

TRAFFIC VOLUMES AT STUDY INTERSECTIONS

INTERSECTION: West Project Driveway Egress and S Rainbow Drive
 COUNT DATE: October 8, 2025
 AM PEAK HOUR FACTOR: 0.92
 PM PEAK HOUR FACTOR: 0.92

"AM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Raw Turning Movements		0	0	0		0	0	0		0	0	0		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

AM EXISTING CONDITIONS		0	0	0		0	0	0		0	0	0		0	0	0
------------------------	--	---	---	---	--	---	---	---	--	---	---	---	--	---	---	---

"PM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Raw Turning Movements		0	0	0		0	0	0		0	0	0		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

PM EXISTING CONDITIONS		0	0	0		0	0	0		0	0	0		0	0	0
------------------------	--	---	---	---	--	---	---	---	--	---	---	---	--	---	---	---

"AM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering															13.0%	
	Exiting						7.0%		13.0%								

"PM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering															13.0%	
	Exiting						7.0%		13.0%								

"AM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
AM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New						0		2							-1	
AM TOTAL PROJECT TRAFFIC			0	0	0		0	0	2		0	0	0		0	-1	0

"PM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
PM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New						1		1							0	
PM TOTAL PROJECT TRAFFIC			0	0	0		1	0	1		0	0	0		0	0	0

TRAFFIC VOLUMES AT STUDY INTERSECTIONS

INTERSECTION:	West Project Driveway Ingress and S Rainbow Drive
COUNT DATE:	October 8, 2025
AM PEAK HOUR FACTOR:	0.92
PM PEAK HOUR FACTOR:	0.92

"AM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Raw Turning Movements		0	0	0		0	0	0		0	0	0		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

AM EXISTING CONDITIONS		0	0	0		0	0	0		0	0	0		0	0	0
------------------------	--	---	---	---	--	---	---	---	--	---	---	---	--	---	---	---

"PM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Raw Turning Movements		0	0	0		0	0	0		0	0	0		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

PM EXISTING CONDITIONS		0	0	0		0	0	0		0	0	0		0	0	0
------------------------	--	---	---	---	--	---	---	---	--	---	---	---	--	---	---	---

"AM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering												7.0%		13.0%		
	Exiting															7.0%	

"PM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering												7.0%		13.0%		
	Exiting															7.0%	

"AM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
AM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New												0		1	0	
AM TOTAL PROJECT TRAFFIC			0	0	0		0	0	0		0	0	0		1	0	0

"PM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
PM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New												1		1	0	
PM TOTAL PROJECT TRAFFIC			0	0	0		0	0	0		0	0	1		1	0	0

TRAFFIC VOLUMES AT STUDY INTERSECTIONS

INTERSECTION: East Project Driveway Ingress and S Crescent Drive
 COUNT DATE: October 8, 2025
 AM PEAK HOUR FACTOR: 0.92
 PM PEAK HOUR FACTOR: 0.92

"AM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Raw Turning Movements		0	0	0		0	0	0		0	0	0		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

AM EXISTING CONDITIONS	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
		0	0	0		0	0	0		0	0	0		0	0	0

"PM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Raw Turning Movements		0	0	0		0	0	0		0	0	0		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

PM EXISTING CONDITIONS	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
		0	0	0		0	0	0		0	0	0		0	0	0

"AM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering										8.0%						7.0%
	Exiting											7.0%					

"PM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering										8.0%						7.0%
	Exiting											7.0%					

"AM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
AM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New										1	0					0
AM TOTAL PROJECT TRAFFIC			0	0	0		0	0	0		1	0	0		0	0	0

"PM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
PM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New										1	-1					1
PM TOTAL PROJECT TRAFFIC			0	0	0		0	0	0		1	-1	0		0	0	1

TRAFFIC VOLUMES AT STUDY INTERSECTIONS

INTERSECTION: East Project Driveway Egress and S Crescent Drive
 COUNT DATE: October 8, 2025
 AM PEAK HOUR FACTOR: 0.92
 PM PEAK HOUR FACTOR: 0.92

"AM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Raw Turning Movements		0	0	0		0	0	0		0	0	0		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

AM EXISTING CONDITIONS	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
		0	0	0		0	0	0		0	0	0		0	0	0

"PM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Raw Turning Movements		0	0	0		0	0	0		0	0	0		0	0	0
Peak Season Correction Factor	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01	1.01

PM EXISTING CONDITIONS	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
		0	0	0		0	0	0		0	0	0		0	0	0

"AM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering											8.0%					
	Exiting		7.0%		8.0%												

"PM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering																
	Exiting																
Valet Distribution	Entering																
	Exiting																
Net New Distribution	Entering											8.0%					
	Exiting		7.0%		8.0%												

"AM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
AM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New		1		1							-1					
AM TOTAL PROJECT TRAFFIC			1	0	1		0	0	0		0	-1	0		0	0	0

"PM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
PM TRAFFIC DIVERSIONS																	
Project Trips	Pass - By																
	Valet																
	Net New		1		1							1					
PM TOTAL PROJECT TRAFFIC			1	0	1		0	0	0		0	1	0		0	0	0

Attachment G

Parking Data

Property	Address	Miles from Subject	Age Restriction	Units	Parking Spaces (Costar)	Parking Ratio	Occupancy
Gardens at Driftwood	7350 Davie Road Ext, Hollywood, FL 33024	4	55+	120	44	0.37	100%
Saratoga Crossings	709 W Dania Beach Blvd. Dania, FL 33004	3.5	55+	128	64	0.50	N/A
Steve Protulis Tower East	2495 NW 54th St, Miami, FL 33142	13.8	55+	50	24	0.48	N/A
Archbishop Hurley Hall	632 NW 1st St, Hallandale Beach, FL 33009	2.1	62+	120	78	0.65	N/A
Palm Springs Villas	445 W 51st Pl, Hialeah, FL 33012	11.7	62+	50	20	0.40	98.1%
Average Parking Ratio for Similar Sites:						0.48	